

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012469	1	9
DISTRICTS 1, 5 & 10 RAISED PAVEMENT MARKERS (2024) (S)						

ARKANSAS DEPARTMENT OF TRANSPORTATION
CONSTRUCTION PLANS FOR STATE HIGHWAY

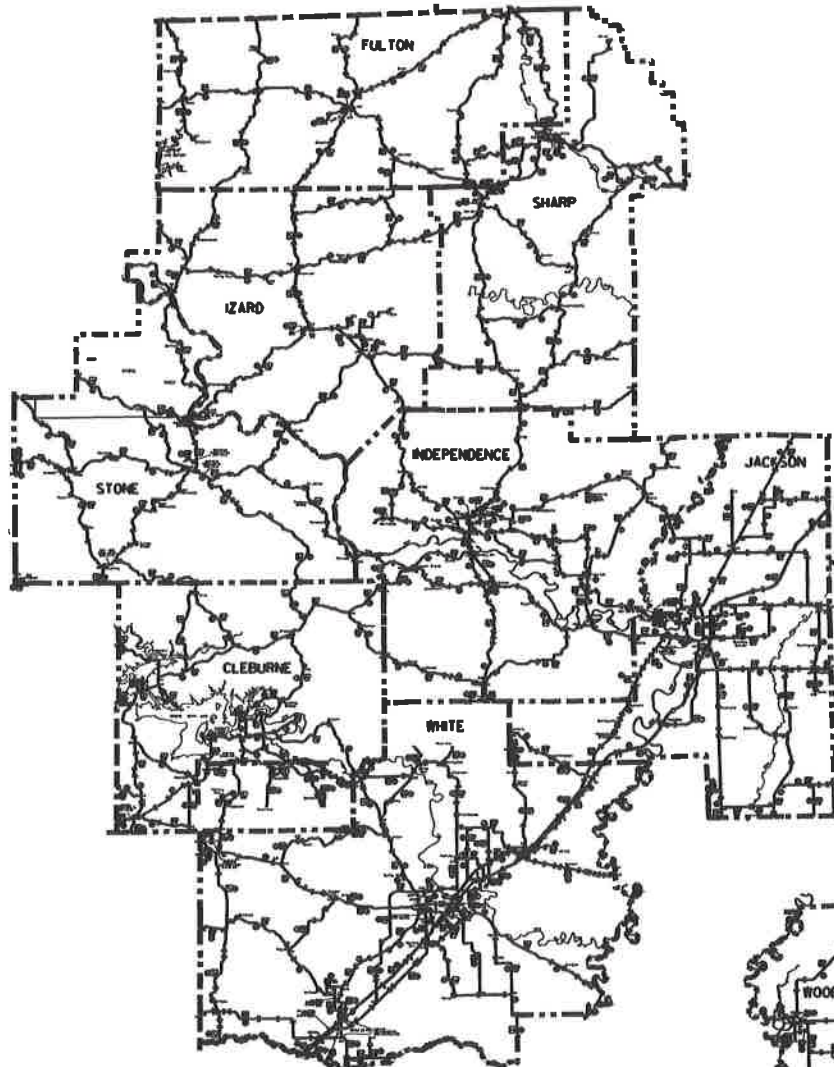
**DISTRICTS 1, 5 & 10 RAISED
PAVEMENT MARKERS (2024) (S)**

VARIOUS COUNTIES

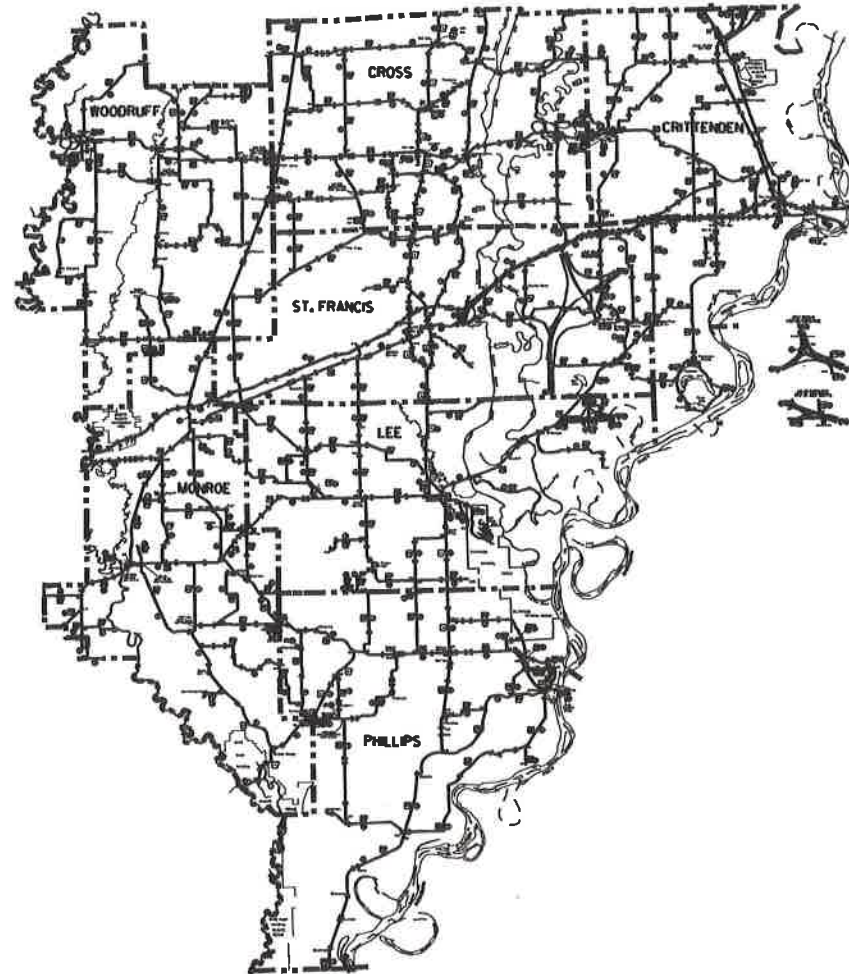
JOB 012469

FED. AID PROJ. STPF-0076(322)

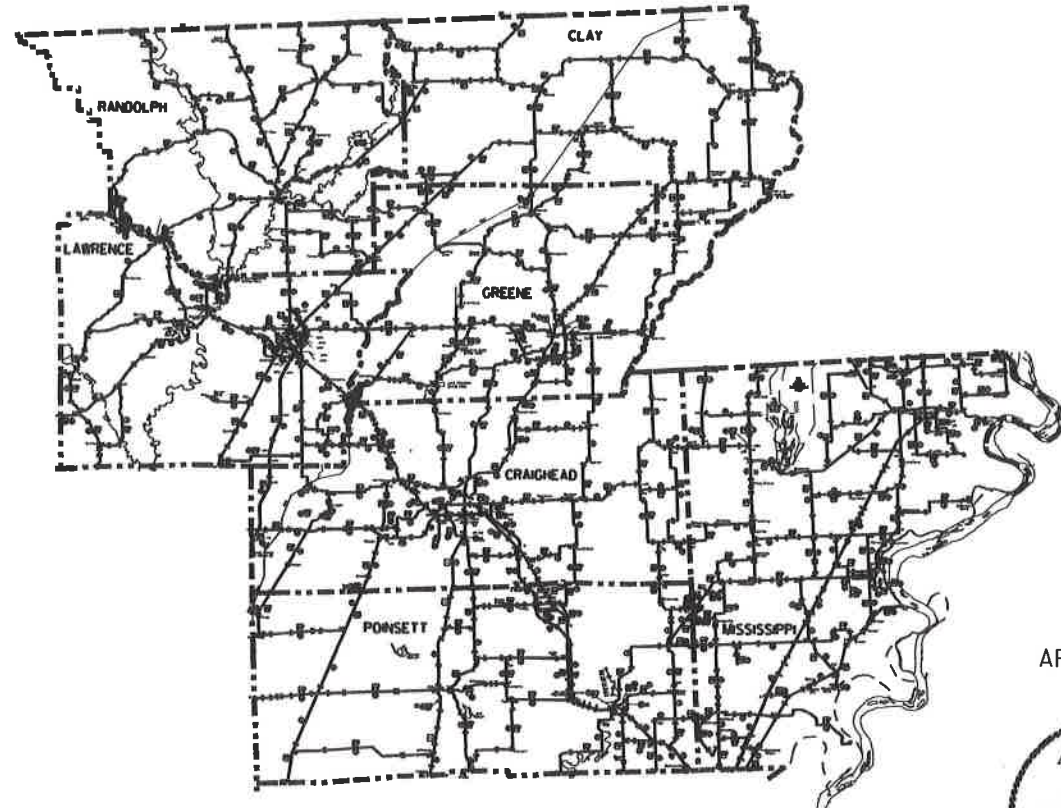
NOT TO SCALE



DISTRICT 5



DISTRICT 1



DISTRICT 10



ARK. HWY. DIST. NOS. 1, 5 & 10



APPROVED



CHIEF ENGINEER - PRECONSTRUCTION

9/11/2023

NO LENGTH INVOLVED

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
11/20/2023		6	ARK.	012469	2	9
12/4/2023						

INDEX OF SHEETS, STD. DRAWINGS, GOV. SPECS., AND GEN. NOTES



INDEX OF SHEETS

SHEET NO.	TITLE
1	TITLE SHEET
2	INDEX OF SHEETS, STANDARD DRAWINGS, GOVERNING SPECIFICATIONS, AND GENERAL NOTES
3 - 8	QUANTITIES
9	SUMMARY OF QUANTITIES AND REVISIONS

ROADWAY STANDARD DRAWINGS

DRWG.NO.	TITLE	DATE
PM-1	PAVEMENT MARKING DETAILS	02-27-20
PM-2	PERMANENT PAVEMENT MARKING ON ACCESS CONTROLLED ROADWAYS	05-14-20
TC-1	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	11-07-19
TC-2	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	05-20-21
TC-3	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-12-21

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-3	CONTRACTOR'S LICENSE
100-4	DEPARTMENT NAME CHANGE
102-2	ISSUANCE OF PROPOSALS
103-2	CONTACT INFORMATION FOR MOTORIST DAMAGE CLAIMS
105-4	MAINTENANCE DURING CONSTRUCTION
107-2	RESTRAINING CONDITIONS
108-1	LIQUIDATED DAMAGES
108-2	WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER
108-3	WORK ORDER FOR FIXED COMPLETION DATE CONTRACTS
306-1	QUALITY CONTROL AND ACCEPTANCE
603-1	LANE CLOSURE NOTIFICATION
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
604-3	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES (MASH)
JOB 012469	ASSESSMENT OF WORKING DAYS - MAINTENANCE OF TRAFFIC
JOB 012469	BIDDING REQUIREMENTS AND CONDITIONS
JOB 012469	BUY AMERICA - CONSTRUCTION MATERIALS
JOB 012469	CARGO PREFERENCE ACT REQUIREMENTS
JOB 012469	COORDINATION OF WORK
JOB 012469	DISADVANTAGED BUSINESS ENTERPRISE BIDDER'S RESPONSIBILITIES
JOB 012469	GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION
JOB 012469	LIQUIDATED DAMAGES PROCEDURE FOR BID LETTINGS
JOB 012469	MAINTENANCE OF TRAFFIC
JOB 012469	MANDATORY ELECTRONIC CONTRACT
JOB 012469	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB 012469	PROHIBITION OF CERTAIN TELECOMMUNICATIONS AND VIDEO SURVEILLANCE SERVICES OR EQUIPMENT
JOB 012469	SEQUENCE OF WORK
JOB 012469	TOTAL SOLAR ECLIPSE

GENERAL NOTES

- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- THE SEQUENCE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS IS A GENERAL OUTLINE FOR THE CONSTRUCTION OF THIS PROJECT, AND IN NO WAY IS IT INTENDED TO COVER EVERY ITEM IN THE PROJECT. ITEMS NOT CRITICAL TO THE CONSTRUCTION SEQUENCE MAY BE CONSTRUCTED IN ANY STAGE AS APPROVED BY THE RESIDENT ENGINEER.
- PLAN LOCATIONS AND QUANTITIES FOR RAISED PAVEMENT MARKERS ARE SUBJECT TO CHANGE IN THE FIELD IF AND WHERE DIRECTED BY THE ENGINEER.

INDEX OF SHEETS, STD. DRAWINGS, GOV. SPECS., AND GEN. NOTES



Sep 11 2023 12:00 PM

RAISED PAVEMENT MARKERS (1 OF 12)

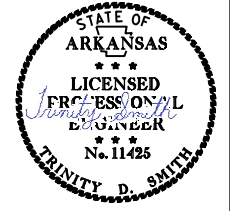
COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
CRITTENDEN	2 LANES	38	8	0.00	2.17	2.17		144
CRITTENDEN	5 LANES - TURN LANE/PAINTED MEDIAN	38	10	0.00	0.84	0.84	112	112
CRITTENDEN	4 LANES - DIVIDED	40	52	264.50	281.20	16.70	2206	
CRITTENDEN	17 - ENTRANCE RAMPS	40	52				646	
CRITTENDEN	18 - EXIT RAMPS	40	52				1548	
CRITTENDEN	2 LANES	42	4	0.00	5.67	5.67		375
CRITTENDEN	2 LANES	42	5	0.00	9.85	9.85		651
CRITTENDEN	2 LANES	50	2	0.00	4.33	4.33		286
CRITTENDEN	2 LANES	50	3	0.00	5.51	5.51		364
CRITTENDEN	2 LANES	50	4	0.00	10.11	10.11		668
CRITTENDEN	4 LANES - DIVIDED	55	11	0.00	4.76	4.76	630	
CRITTENDEN	4 LANES - DIVIDED	55	11	7.09	7.47	0.38	52	
CRITTENDEN	4 LANES - DIVIDED	55	11	7.47	8.41	0.94	189	
CRITTENDEN	4 LANES - DIVIDED	55	11	8.41	27.87	19.46	2570	
CRITTENDEN	20 - ENTRANCE RAMPS	55	11				760	
CRITTENDEN	21 - EXIT RAMPS	55	11				1806	
CRITTENDEN	2 LANES	61	1	0.00	5.02	5.02		332
CRITTENDEN	3 LANES - PASSING LANE	64	17	0.00	0.32	0.32	22	22
CRITTENDEN	2 LANES	64	17	0.32	3.88	3.56		235
CRITTENDEN	3 LANES - PASSING LANE	64	17	3.88	5.40	1.52	101	101
CRITTENDEN	2 LANES	64	17	5.40	8.20	2.80		185
CRITTENDEN	5 LANES - TURN LANE/PAINTED MEDIAN	64	17	8.20	19.00	10.80	1426	1426
CRITTENDEN	4 LANES - DIVIDED	64	17	19.00	19.06	0.06	8	
CRITTENDEN	4 LANES - UNDIVIDED	64	17	19.06	19.23	0.17	24	12
CRITTENDEN	3 LANES - PASSING LANE	64	17	19.23	19.30	0.07	5	5
CRITTENDEN	2 LANES	64	17B	0.00	2.29	2.29		152
CRITTENDEN	2 LANES	64	17C	0.00	1.28	1.28		85
CRITTENDEN	2 LANES	70	20	0.00	9.20	9.20		608
CRITTENDEN	4 LANES - UNDIVIDED	70	20	9.20	9.97	0.77	102	51
CRITTENDEN	5 LANES - TURN LANE/PAINTED MEDIAN	70	20	9.97	10.10	0.13	18	18
CRITTENDEN	5 LANES - TURN LANE/PAINTED MEDIAN	70	20	10.10	12.21	2.11	280	280
CRITTENDEN	5 LANES - TURN LANE/PAINTED MEDIAN	70	20	12.21	15.60	3.39	448	448
CRITTENDEN	2 LANES	77	4	0.00	5.24	5.24		346
CRITTENDEN	5 LANES - TURN LANE/PAINTED MEDIAN	77	5	12.74	15.62	2.88	382	382
CRITTENDEN	4 LANES - UNDIVIDED	77	5	15.62	15.81	0.19	26	13
CRITTENDEN	3 LANES - TURN LANE	77	5	15.81	16.36	0.55		74
CRITTENDEN	5 LANES - TURN LANE/PAINTED MEDIAN	77	5	16.36	17.84	1.48	196	196
CRITTENDEN	2 LANES	79	17	10.24	10.29	0.05		4
CRITTENDEN	2 LANES	79	18	0.00	9.63	9.63		636
CRITTENDEN	2 LANES	118	3	0.00	14.42	14.42		952
CRITTENDEN	2 LANES	131	3	0.00	3.11	3.11		206
CRITTENDEN	2 LANES	131	2	0.00	8.81	8.81		582
CRITTENDEN	2 LANES	131	4	0.00	1.29	1.29		86
CRITTENDEN	2 LANES	147	0	0.00	5.67	5.67		375
CRITTENDEN	2 LANES	147	1	0.00	5.20	5.20		344
CRITTENDEN	2 LANES	147	2	0.00	4.66	4.66		308
CRITTENDEN	2 LANES	149	4	0.00	10.14	10.14		670
CRITTENDEN	2 LANES	149	5	0.00	13.02	13.02		860
CRITTENDEN	2 LANES	184	2	0.00	2.45	2.45		162
CRITTENDEN	2 LANES	218	1	0.00	7.25	7.25		479
CRITTENDEN	4 LANES - DIVIDED	555	1	1.77	4.51	2.74	436	
CRITTENDEN	3- ENTRANCE RAMPS	555	1				114	
CRITTENDEN	3- EXIT RAMPS	555	1				258	
CROSS	5 LANES - TURN LANE/PAINTED MEDIAN	1	13	0.00	7.36	7.36	972	972
CROSS	2 LANES	1	14	0.10	2.73	2.63		174
CROSS	3 LANES - PASSING LANE	1	14	2.73	4.14	1.41	94	94
CROSS	2 LANES	1	14	4.14	5.89	1.75		116
CROSS	3 LANES - PASSING LANE	1	14	5.89	7.09	1.20	80	80
CROSS	2 LANES	1	14	7.09	8.31	1.22		81
CROSS	3 LANES - PASSING LANE	1	14	8.31	10.02	1.71	113	113
CROSS	2 LANES	1	14	10.02	12.30	2.28		151
CROSS	3 LANES - PASSING LANE	1	14	12.30	13.50	1.20	68	68
CROSS	2 LANES	1	14	13.50	14.34	0.84		56
CROSS	2 LANES	1	14B	0.00	0.87	0.87		58
CROSS	2 LANES	1	14C	0.00	1.04	1.04		69
CROSS	2 LANES	42	2	0.00	2.10	2.10		139
CROSS	2 LANES	42	2	5.70	15.91	10.21		674
CROSS	2 LANES	42	3	0.00	17.02	17.02		1124
CROSS	2 LANES	49	6	0.00	13.40	13.40		885
CROSS	2 LANES	64	15S	0.00	1.05	1.05		70
CROSS	2 LANES	64	16B	0.00	1.13	1.13		75
CROSS	2 LANES	64	16C	0.00	4.31	4.31		285
CROSS	2 LANES	75	2	0.00	9.03	9.03		596
CROSS	2 LANES	75	2S	0.00	0.86	0.86		57
CROSS	2 LANES	75	3	0.00	7.30	7.30		482
SUBTOTALS (BOX 1 OF 12):							15692	19668

NOTE:
SEE PM-2 (ONLY ADDED 19 RAISED PAVEMENT MARKERS FOR THE 1520' LEADING UP TO AN EXIT RAMP. THE OTHER 19 ARE INCLUDED IN MAIN LANE CALCULATIONS)
RAISED PAVEMENT MARKERS PER ENTRANCE RAMP = 38 EACH
RAISED PAVEMENT MARKERS PER EXIT RAMP = 86 EACH

** QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

RAISED PAVEMENT MARKERS (2 OF 12)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
CROSS	2 LANES	75	4	0.00	7.90	7.90		522
CROSS	2 LANES	163	1	0.00	2.21	2.21		146
CROSS	2 LANES	163	2	0.00	14.44	14.44		954
CROSS	2 LANES	184	1	0.00	4.84	4.84		320
CROSS	2 LANES	193	1	0.00	3.70	3.70		245
CROSS	2 LANES	193	1	3.70	7.24	3.54		234
CROSS	2 LANES	193	2	0.00	10.30	10.30		680
CROSS	2 LANES	259	5	0.00	3.18	3.18		210
CROSS	2 LANES	259	7	0.00	5.85	5.85		387
CROSS	2 LANES	284	1	0.00	16.10	16.10		1063
CROSS	2 LANES	284	2	0.00	6.83	6.83		451
CROSS	2 LANES	306	5	0.00	0.11	0.11		8
CROSS	2 LANES	306	5	0.19	3.73	3.54		234
CROSS	2 LANES	306	5	3.83	4.57	0.74		49
CROSS	2 LANES	306	5	4.67	5.98	1.31		87
CROSS	2 LANES	350	1	0.00	6.08	6.08		402
CROSS	2 LANES	350	2	0.00	2.13	2.13		141
CROSS	2 LANES	364	1	0.00	13.88	13.88		917
CROSS	2 LANES	364	2	0.00	5.22	5.22		345
LEE	4 LANES - DIVIDED	1	10	6.40	7.85	1.45	192	
LEE	4 LANES - UNDIVIDED	1	10	7.85	8.59	0.74	98	49
LEE	4 LANES - DIVIDED	1	10	8.59	9.41	0.82	110	
LEE	2 LANES	78	2	0.00	12.49	12.49		825
LEE	2 LANES	78	2S	0.00	0.80	0.80		53
LEE	2 LANES	78	3	0.00	4.27	4.27		282
LEE	2 LANES	79	15	1.00	4.30	3.30		218
LEE	2 LANES	79	15	6.80	11.70	4.90		324
LEE	5 LANES - TURN LANE/PAINTED MEDIAN	79	16	0.00	0.23	0.23	32	32
LEE	2 LANES	79	16	0.23	1.89	1.66		110
LEE	2 LANES	79	16	3.99	17.14	13.15		868
LEE	2 LANES	121	1	0.00	9.49	9.49		627
LEE	2 LANES	121	2	0.00	12.98	12.98		857
LEE	2 LANES	121	3	0.00	16.98	16.98		1121
LEE	2 LANES	131	0	0.00	5.27	5.27		348
LEE	2 LANES	131	1	0.00	9.57	9.57		632
LEE	2 LANES	185	0	0.00	2.64	2.64		175
LEE	2 LANES	44	5	0.00	6.76	6.76		447
LEE	2 LANES	238	2	0.00	9.20	9.20		608
LEE	2 LANES	243	2S	0.00	0.66	0.66		44
LEE	2 LANES	243	2	0.00	4.06	4.06		268
LEE	2 LANES	243	3	0.00	7.07	7.07		467
LEE	2 LANES	243	4	0.00	1.47	1.47		98
LEE	2 LANES	259	0	0.00	4.57	4.57		302
LEE	2 LANES	259	1	0.00	5.00	5.00		330
LEE	2 LANES	261	1	0.00	9.43	9.43		623
LEE	2 LANES	316	3	0.00	1.00	1.00		66
LEE	2 LANES	334	3	0.00	3.26	3.26		216
MONROE	2 LANES	1	6	0.00	0.29	0.29		20
MONROE	2 LANES	1	6	9.66	9.95	0.29		20
MONROE	2 LANES	17	1	0.00	21.62	21.62		1427
MONROE	2 LANES	17	2	0.00	10.50	10.50		693
MONROE	2 LANES	17	3	0.00	7.11	7.11		470
MONROE	2 LANES	33	3	0.00	2.78	2.78		184
MONROE	2 LANES	33	4	0.00	4.56	4.56		301
MONROE	2 LANES	39	10	0.00	5.07	5.07		335
MONROE	2 LANES	39	9S	0.00	0.53	0.53		35
MONROE	2 LANES	39	9	0.00	4.90	4.90		324



Sep 11 2023 12:00 PM

RAISED PAVEMENT MARKERS (3 OF 12)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
MONROE	2 LANES	146	3	0.00	1.96	1.96		130
MONROE	2 LANES	146	4	0.00	10.11	10.11		668
MONROE	2 LANES	238	1S	0.00	0.67	0.67		45
MONROE	2 LANES	238	1	0.00	8.37	8.37		553
MONROE	2 LANES	241	1	0.00	2.21	2.21		146
MONROE	2 LANES	241	2	0.00	4.88	4.88		323
MONROE	2 LANES	302	2	0.00	10.93	10.93		722
MONROE	2 LANES	316	1	0.00	0.15	0.15		10
MONROE	2 LANES	362	1	0.00	1.70	1.70		113
MONROE	2 LANES	366	1	0.00	3.20	3.20		212
MONROE	2 LANES	366	2	0.00	8.05	8.05		532
PHILLIPS	2 LANES	1	7	5.40	8.08	2.68		177
PHILLIPS	2 LANES	20	0	0.00	8.26	8.26		546
PHILLIPS	2 LANES	20	1	0.00	23.45	23.45		1548
PHILLIPS	2 LANES	20	1S	0.00	1.85	1.85		123
PHILLIPS	2 LANES	39	11	0.00	8.94	8.94		591
PHILLIPS	2 LANES	39	11S	0.00	0.38	0.38		26
PHILLIPS	2 LANES	41	3	23.70	39.70	16.00		1066
PHILLIPS	2 LANES	49	10	0.00	4.40	4.40		291
PHILLIPS	2 LANES	49	10	5.60	6.39	0.79		53
PHILLIPS	2 LANES	49	10	6.39	8.47	2.08		138
PHILLIPS	5 LANES - TURN LANE/PAINTED MEDIAN	49	10	8.47	17.37	8.90	1176	1176
PHILLIPS	4 LANES - UNDIVIDED	49	10	17.37	23.50	6.13	810	405
PHILLIPS	4 LANES - DIVIDED	49	10	23.50	28.75	5.25	694	
PHILLIPS	5 LANES - TURN LANE/PAINTED MEDIAN	49	10B	0.00	1.38	1.38	184	184
PHILLIPS	4 LANES - DIVIDED	49	10B	1.38	1.91	0.53	70	
PHILLIPS	4 LANES - UNDIVIDED	49	10B	1.91	2.32	0.41	56	28
PHILLIPS	5 LANES - TURN LANE/PAINTED MEDIAN	49	10B	2.32	2.84	0.52	70	70
PHILLIPS	4 LANES - UNDIVIDED	49	10B	2.84	3.02	0.18	24	12
PHILLIPS	5 LANES - TURN LANE/PAINTED MEDIAN	49	10B	3.02	3.14	0.12	16	16
PHILLIPS	4 LANES - UNDIVIDED	49	10B	3.14	4.00	0.86	114	57
PHILLIPS	4 LANES - DIVIDED	49	10B	4.00	4.50	0.50	66	
PHILLIPS	2 LANES	49	10B	4.50	6.92	2.42		160
PHILLIPS	4 LANES - UNDIVIDED	49	10X	0.00	0.08	0.08	12	6
PHILLIPS	2 LANES	49	10X	0.08	0.58	0.50		33
PHILLIPS	3 LANES - TURN LANE	49	10X	0.58	0.60	0.02		4
PHILLIPS	2 LANES	85	1	0.00	9.71	9.71		641
PHILLIPS	2 LANES	86	3	19.10	19.27	0.17		12
PHILLIPS	2 LANES	146	5	0.00	5.36	5.36		354
PHILLIPS	2 LANES	185	1	0.00	2.55	2.55		169
PHILLIPS	2 LANES	185	2	0.00	2.31	2.31		153
PHILLIPS	2 LANES	242	0	0.00	2.60	2.60		172
PHILLIPS	2 LANES	242	1	0.00	2.20	2.20		146
PHILLIPS	2 LANES	242	1S	0.00	0.16	0.16		11
PHILLIPS	2 LANES	243	1	0.00	6.27	6.27		414
PHILLIPS	2 LANES	316	1	0.00	13.82	13.82		913
PHILLIPS	2 LANES	316	2	0.00	6.04	6.04		399
PHILLIPS	2 LANES	318	3	0.00	14.63	14.63		966
PHILLIPS	2 LANES	318	3Y	0.00	0.48	0.48		32
ST. FRANCIS	4 LANES - DIVIDED	1	11	0.00	3.30	3.30	436	
ST. FRANCIS	2 LANES	1	11	3.30	5.81	2.51		168
ST. FRANCIS	3 LANES - TURN LANE	1	11	5.81	5.98	0.17		24
ST. FRANCIS	2 LANES	1	11	5.98	7.05	1.07		71
ST. FRANCIS	2 LANES	1	12	0.00	1.92	1.92		127
ST. FRANCIS	3 LANES - TURN LANE	1	12	1.92	2.10	0.18		24
ST. FRANCIS	2 LANES	1	12	2.10	2.29	0.19		13
ST. FRANCIS	3 LANES - TURN LANE	1	12	2.29	2.40	0.11		16
ST. FRANCIS	2 LANES	1	12	2.40	4.63	2.23		148
ST. FRANCIS	5 LANES - TURN LANE/PAINTED MEDIAN	1	12	4.63	9.90	5.27	696	696
ST. FRANCIS	5 LANES - TURN LANE/PAINTED MEDIAN	1	12	9.90	11.16	1.26	168	168
ST. FRANCIS	4 LANES - DIVIDED	1	11B	0.00	0.75	0.75	100	
ST. FRANCIS	5 LANES - TURN LANE/PAINTED MEDIAN	1	11B	0.75	1.95	1.20	160	160
ST. FRANCIS	4 LANES - UNDIVIDED	1	11B	1.95	4.14	2.19	290	145
ST. FRANCIS	4 LANES - UNDIVIDED	1	12B	0.00	0.97	0.97	130	65
ST. FRANCIS	5 LANES - TURN LANE/PAINTED MEDIAN	1	12B	0.97	1.70	0.73	98	98
ST. FRANCIS	4 LANES - DIVIDED	1	12B	1.70	1.85	0.15	20	
ST. FRANCIS	6 LANES - DIVIDED	1	12B	1.85	1.97	0.12	32	
ST. FRANCIS	4 LANES - DIVIDED	1	12B	1.97	2.09	0.12	16	
ST. FRANCIS	5 LANES - TURN LANE/PAINTED MEDIAN	1	12B	2.09	3.83	1.74	230	230
ST. FRANCIS	2 LANES	38	5	0.00	2.45	2.45		162
ST. FRANCIS	2 LANES	38	6	0.00	11.27	11.27		744
ST. FRANCIS	2 LANES	38	7	0.00	5.59	5.59		369
ST. FRANCIS	4 LANES - DIVIDED	40	51	218.30	228.21	9.91	1310	
ST. FRANCIS	4 LANES - DIVIDED	40	51	228.21	239.97	11.76	1554	
ST. FRANCIS	20 - ENTRANCE RAMPS	40	51				760	
SUBTOTALS (BOX 3 OF 12):							9292	17962

NOTE:
SEE PM-2 (ONLY ADDED 19 RAISED PAVEMENT MARKERS FOR THE 1520' LEADING UP TO AN EXIT RAMP. THE OTHER 19 ARE INCLUDED IN MAIN LANE CALCULATIONS)
RAISED PAVEMENT MARKERS PER ENTRANCE RAMP = 38 EACH
RAISED PAVEMENT MARKERS PER EXIT RAMP = 86 EACH

** QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

RAISED PAVEMENT MARKERS (4 OF 12)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
ST. FRANCIS	20 - EXIT RAMPS	40	51				1720	
ST. FRANCIS	4 LANES - DIVIDED	40	51	239.97	264.49	24.52	3238	
ST. FRANCIS	2 LANES	50	0	0.00	2.21	2.21		146
ST. FRANCIS	2 LANES	50	1	0.00	21.44	21.44		1416
ST. FRANCIS	2 LANES	70	18	0.00	18.72	18.72		1236
ST. FRANCIS	3 LANES - TURN LANE	70	18	18.72	19.28	0.56		74
ST. FRANCIS	2 LANES	70	18	19.28	19.37	0.09		6
ST. FRANCIS	3 LANES - PASSING LANE	70	18	19.37	19.47	0.10	7	7
ST. FRANCIS	4 LANES - UNDIVIDED	70	18	19.47	20.15	0.68	90	45
ST. FRANCIS	2 LANES	70	18	20.15	20.40	0.25		17
ST. FRANCIS	2 LANES	70	19	0.00	25.03	25.03		1652
ST. FRANCIS	2 LANES	75	0	0.00	5.64	5.64		373
ST. FRANCIS	2 LANES	75	1	0.00	1.93	1.93		128
ST. FRANCIS	2 LANES	78	1	0.00	7.44	7.44		492
ST. FRANCIS	2 LANES	79	17	0.00	10.24	10.24		676
ST. FRANCIS	2 LANES	149	1	0.00	4.51	4.51		298
ST. FRANCIS	2 LANES	149	2	0.00	7.11	7.11		470
ST. FRANCIS	2 LANES	140	3	0.00	1.04	1.04		60
ST. FRANCIS	2 LANES	193	0	0.00	0.34	0.34		23
ST. FRANCIS	2 LANES	259	2	0.00	2.51	2.51		166
ST. FRANCIS	2 LANES	259	4	0.00	3.72	3.72		246
ST. FRANCIS	2 LANES	261	2	0.00	5.76	5.76		381
ST. FRANCIS	2 LANES	261	3	0.00	4.37	4.37		289
ST. FRANCIS	2 LANES	284	3	0.00	11.02	11.02		728
ST. FRANCIS	2 LANES	306	2	0.00	14.76	14.76		975
ST. FRANCIS	2 LANES	306	3	0.00	4.74	4.74		313
ST. FRANCIS	2 LANES	334	1	0.00	5.54	5.54		366
ST. FRANCIS	2 LANES	357	1	0.00	9.18	9.18		606
WOODRUFF	2 LANES	17	4	0.00	22.31	22.31		1473
WOODRUFF	2 LANES	17	5	0.00	6.89	6.89		455
WOODRUFF	2 LANES	33	7C	0.00	3.02	3.02		200
WOODRUFF	2 LANES	33	7	0.00	17.20	17.20		1136
WOODRUFF	2 LANES	33	8	0.00	10.99	10.99		726
WOODRUFF	2 LANES	37	1	0.00	5.61	5.61		371
WOODRUFF	2 LANES	38	3	0.00	4.49	4.49		297
WOODRUFF	2 LANES	49	7	0.00	8.70	8.70		575
WOODRUFF	3 LANES - TURN LANE	64	13C	0.00	1.00	1.00		132
WOODRUFF	2 LANES	64	14B	0.00	5.14	5.14		340
WOODRUFF	2 LANES	78	0	0.00	4.02	4.02		266
WOODRUFF	2 LANES	145	3	0.00	5.27	5.27		348
WOODRUFF	2 LANES	145	4	0.00	15.34	15.34		1013
WOODRUFF	2 LANES	260	1	0.00	6.33	6.33		418
WOODRUFF	2 LANES	260	2	0.00	6.79	6.79		449
WOODRUFF	2 LANES	262	1	0.00	11.60	11.60		766
WOODRUFF	2 LANES	269	1	0.00	8.00	8.00		528
WOODRUFF	2 LANES	269	2	0.00	8.86	8.86		585
WOODRUFF	2 LANES	284	0	0.00	0.56	0.56		37
WOODRUFF	2 LANES	306	0	0.00	4.97	4.97		329
WOODRUFF	2 LANES	306	1	0.00	4.76	4.76		315
WOODRUFF	2 LANES	339	1	0.00	0.75	0.75		50
CLEBURNE	2 LANES	5	15A	0.00	4.75	4.75		314
CLEBURNE	2 LANES	16	12	0.00	5.06	5.06		334
CLEBURNE	3 LANES - PASSING LANE	16	12	5.06	6.03	0.97	65	65
CLEBURNE	2 LANES	16	12	6.03	6.44	0.41		28
CLEBURNE	3 LANES - PASSING LANE</							



Sep 11 2023 12:00 PM

RAISED PAVEMENT MARKERS (5 OF 12)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
CLEBURNE	2 LANES	25	3	15.41	19.36	3.95		261
CLEBURNE	3 LANES - PASSING LANE	25	3	19.36	20.23	0.87	58	58
CLEBURNE	2 LANES	25	3	20.23	28.78	8.55		565
CLEBURNE	3 LANES - PASSING LANE	25	3B	0.00	0.72	0.72	48	48
CLEBURNE	2 LANES	25	3B	0.72	1.68	0.96		64
CLEBURNE	4 LANES - UNDIVIDED	25	3B	1.68	1.99	0.31	42	21
CLEBURNE	3 LANES - TURN LANE	25	3B	1.99	3.84	1.85	12	122
CLEBURNE	2 LANES	25	3B	3.84	5.51	1.67	3	110
CLEBURNE	2 LANES	110	5	0.00	4.23	4.23		280
CLEBURNE	4 LANES - UNDIVIDED	110	5	4.23	4.29	0.06	8	4
CLEBURNE	2 LANES	110	6	1.83	4.17	2.34	19	156
CLEBURNE	2 LANES	210	0	0.00	1.68	1.68		111
FULTON	2 LANES	62	12	0.00	5.21	5.21		344
FULTON	3 LANES - PASSING LANE	62	12	5.21	5.95	0.74	49	49
FULTON	4 LANES - UNDIVIDED	62	12	5.95	6.13	0.18	24	12
FULTON	3 LANES - PASSING LANE	62	12	6.13	6.86	0.73	49	49
FULTON	2 LANES	62	12	6.86	10.37	3.51		232
FULTON	3 LANES - TURN LANE	62	12	10.37	10.93	0.56	6	74
FULTON	2 LANES	62	12	10.93	11.58	0.65		43
FULTON	3 LANES - PASSING LANE	62	12	11.58	17.04	5.46	361	361
FULTON	2 LANES	62	12	17.04	17.55	0.51		34
FULTON	3 LANES - PASSING LANE	62	12	17.55	17.86	0.31	21	21
FULTON	3 LANES - TURN LANE	62	12	17.86	18.36	0.50		66
FULTON	3 LANES - PASSING LANE	62	12	18.36	19.57	1.21	80	80
FULTON	2 LANES	62	12	19.57	20.59	1.02		68
FULTON	2 LANES	62	13	0.00	3.91	3.91		258
FULTON	3 LANES - PASSING LANE	62	13	3.91	6.18	2.27	150	150
FULTON	2 LANES	62	13	6.18	8.78	2.60		172
FULTON	3 LANES - PASSING LANE	62	13	8.78	9.75	0.97	64	64
FULTON	2 LANES	62	13	9.75	12.78	3.03		200
FULTON	3 LANES - PASSING LANE	62	13	12.78	13.79	1.01	67	67
FULTON	2 LANES	62	13	13.79	15.49	1.70		113
FULTON	5 LANES - TURN LANE/PAINTED MEDIAN	62	16	0.00	1.24	1.25	164	164
FULTON	5 LANES - TURN LANE/PAINTED MEDIAN	63	1	0.00	2.06	2.06	272	272
FULTON	2 LANES	9	14	0.00	9.91	9.91		855
FULTON	2 LANES	9	15	0.00	19.84	19.84		1310
INDEPENDENCE	2 LANES	14	8	0.00	6.08	6.08		402
INDEPENDENCE	2 LANES	25	4	0.00	0.95	0.95		63
INDEPENDENCE	3 LANES - PASSING LANE	25	4	0.95	3.39	2.44	167	167
INDEPENDENCE	2 LANES	25	4	4.00	4.37	0.37		25
INDEPENDENCE	2 LANES	25	4	4.45	4.68	0.23	3	16
INDEPENDENCE	2 LANES	25	4	10.62	10.97	0.35		24
INDEPENDENCE	3 LANES - TURN LANE	25	4	10.97	11.06	0.09		12
INDEPENDENCE	4 LANES - UNDIVIDED	25	4	11.06	11.07	0.01	6	1
INDEPENDENCE	2 LANES	25	5	0.00	25.39	25.39	6	1676
INDEPENDENCE	3 LANES - PASSING LANE	69	2	0.00	0.19	0.19	13	13
INDEPENDENCE	2 LANES	69	2	0.19	0.60	0.41		28
INDEPENDENCE	3 LANES - PASSING LANE	69	2	0.60	2.07	1.47	98	98
INDEPENDENCE	2 LANES	69	2	2.07	2.65	0.58		39
INDEPENDENCE	3 LANES - PASSING LANE	69	2	2.65	5.44	2.79	185	185
INDEPENDENCE	2 LANES	69	2	5.44	6.20	0.76		51
INDEPENDENCE	3 LANES - PASSING LANE	69	2	6.20	7.36	1.16	77	77
INDEPENDENCE	2 LANES	69	2	7.36	10.51	3.15		208
INDEPENDENCE	3 LANES - PASSING LANE	69	2	10.51	14.21	3.70	245	245
INDEPENDENCE	4 LANES - UNDIVIDED	69	2	14.21	14.36	0.15	20	10
INDEPENDENCE	2 LANES	69	2	14.36	15.46	1.10	5	73
INDEPENDENCE	2 LANES	69	2B	0.00	2.25	2.25	8	149
INDEPENDENCE	2 LANES	69	2S	0.00	0.35	0.35		24
INDEPENDENCE	2 LANES	69	3	4.62	15.42	10.80	18	713
INDEPENDENCE	5 LANES - TURN LANE/PAINTED MEDIAN	167	17	0.00	15.40	15.40	2032	2032
INDEPENDENCE	4 LANES - UNDIVIDED	167	17	15.40	15.85	0.45	60	30
INDEPENDENCE	5 LANES - TURN LANE/PAINTED MEDIAN	167	17	15.85	16.12	0.28	36	36
INDEPENDENCE	4 LANES - UNDIVIDED	167	17	16.12	16.66	0.54	72	36
INDEPENDENCE	5 LANES - TURN LANE/PAINTED MEDIAN	167	17	16.66	16.96	0.30	40	40
INDEPENDENCE	4 LANES - UNDIVIDED	167	17	16.96	17.34	0.38	50	25
INDEPENDENCE	5 LANES - TURN LANE/PAINTED MEDIAN	167	17	17.34	17.87	0.53	70	70
INDEPENDENCE	5 LANES - TURN LANE/PAINTED MEDIAN	167	18	0.00	1.76	1.76	232	232
INDEPENDENCE	5 LANES - TURN LANE/PAINTED MEDIAN	167	18	3.76	13.86	10.10	1334	1334
INDEPENDENCE	3 LANES - TURN LANE	167	18	13.86	14.12	0.26		36
INDEPENDENCE	2 LANES	233	1	0.00	1.16	1.16		77
INDEPENDENCE	2 LANES	233	2	0.00	1.27	1.27	3	84
INDEPENDENCE	3 LANES - TURN LANE	233	2	1.27	1.49	0.22	3	30
INDEPENDENCE	2 LANES	233	2	1.49	1.68	0.19	6	13
INDEPENDENCE	3 LANES - TURN LANE	233	2	1.68	1.80	0.12	3	16
INDEPENDENCE	2 LANES	367	20	0.00	1.62	1.62		107
SUBTOTALS (BOX 5 OF 12):							6289	15155

** QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

RAISED PAVEMENT MARKERS (6 OF 12)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
INDEPENDENCE	2 LANES	394	1	0.00	1.37	1.37		91
IZARD	2 LANES	5	17	0.00	6.42	6.42	10	424
IZARD	2 LANES	56	1	2.80	14.30	11.50		759
IZARD	2 LANES	69	1	0.00	0.12	0.12		8
IZARD	3 LANES - TURN LANE	69	1	0.12	2.79	2.67	6	354
IZARD	3 LANES - PASSING LANE	69	1	2.79	3.53	0.74	50	50
IZARD	2 LANES	69	1	3.53	3.90	0.37		25
IZARD	3 LANES - TURN LANE	69	1	3.90	4.08	0.18	11	24
IZARD	2 LANES	69	1	4.08	6.00	1.92		127
IZARD	3 LANES - TURN LANE	69	1	6.00	6.17	0.17	6	24
IZARD	2 LANES	69	1	6.17	6.70	0.53		35
IZARD	3 LANES - PASSING LANE	69	1	6.70	8.35	1.65	109	109
IZARD	2 LANES	69	1	8.35	10.40	2.05		136
IZARD	3 LANES - PASSING LANE	69	1	10.40	11.07	0.67	45	45
IZARD	3 LANES - TURN LANE	69	1	11.07	12.79	1.72		228
IZARD	2 LANES	69	1	12.79	13.33	0.54		36
IZARD	2 LANES	9	12	0.00	8.10	8.10		535
IZARD	2 LANES	9	13	6.40	14.77	8.37		563
JACKSON	2 LANES	14	12	1.70	2.33	0.63		42
JACKSON	2 LANES - LEFT TURN LANE	14	12	6.77	6.87	0.10	3	13
JACKSON	2 LANES	14	12	6.87	11.37	4.50		297
JACKSON	2 LANES	14	12	13.24	17.36	4.12		272
JACKSON	2 LANES	14	12Y	0.00	0.30	0.30		20
JACKSON	2 LANES	17	6	14.26	17.41	3.15		208
JACKSON	3 LANES - TURN LANE	18	1	0.76	1.72	0.96	12	128
JACKSON	2 LANES	18	1	1.72	2.52	0.80		53
JACKSON	4 LANES - DIVIDED	67	14	0.00	15.67	15.67	2088	
JACKSON	RAMPS	67	14	0.00	15.67	15.67	868	
JACKSON	4 LANES - DIVIDED	67	15	0.00	19.39	19.39	2560	
JACKSON	RAMPS	67	15	0.00	19.39	19.39	1116	
JACKSON	5 LANES - TURN LANE/PAINTED MEDIAN	167	15	0.00	1.50	1.50	198	198
JACKSON	2 LANES	367	19	0.00	7.14	7.14		472
JACKSON	2 LANES	367	21	0.00	7.21	7.21		476
JACKSON	4 LANES - UNDIVIDED	367	21	7.21	7.25	0.04	6	3
JACKSON	5 LANES - TURN LANE/PAINTED MEDIAN	367	21	7.25	11.62	4.37	576	576
JACKSON	4 LANES - UNDIVIDED	367	21	11.62	11.76	0.14	18	9
JACKSON	2 LANES	367	21	11.76	23.42	11.66		770
JACKSON	5 LANES - TURN LANE/PAINTED MEDIAN	367	21	23.42	23.86	0.44	60	60
JACKSON	2 LANES	367	21	23.86	30.02	6.16		407
JACKSON	2 LANES - LEFT TURN LANE	384	3	0.00	0.06	0.06	3	8
JACKSON	2 LANES	384	3	0.06	0.52	0.46		31
JACKSON	3 LANES - TURN LANE	384	3	0.52	0.88	0.36	6	48
JACKSON	2 LANES	384	3	0.88	0.99	0.11		8
SHARP	3 LANES - TURN LANE	167	19	0.00	1.66	1.66		220
SHARP	2 LANES	167	19	1.66	7.92	6.26		414
SHARP	3 LANES - PASSING LANE	167	19	7.92	8.81	0.89	59	59
SHARP	2 LANES	167	19	8.81	12.83	4.02		266
SHARP	3 LANES - PASSING LANE	167	19	12.83	14.57	1.74	115	115
SHARP	2 LANES	167	19	14.57	19.92	5.35		354
SHARP	3 LANES - PASSING LANE	167	19	19.92	20.88	0.96	64	64
SHARP	2 LANES	167	19	20.88	22.14	1.26		84
SHARP	5 LANES - TURN LANE/PAINTED MEDIAN	167	19	22.14	23.20	1.06	140	140
SHARP	2 LANES	62	14	0.00	0.84	0.84		56
SHARP	3 LANES - TURN LANE	62	14	0.84	1.61	0.77		



Sep 11 2023 12:00 PM
Docusign

RAISED PAVEMENT MARKERS (7 OF 12)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
STONE	3 LANES - TURN LANE	5	16	16.15	16.89	0.74	3	98
STONE	2 LANES	5	16	16.69	33.37	16.48		1088
STONE	3 LANES - PASSING LANE	66	2	11.95	12.92	0.97	65	65
STONE	2 LANES	66	2	12.92	14.49	1.57		104
STONE	3 LANES - PASSING LANE	66	2	14.49	15.44	0.95	63	63
STONE	2 LANES	66	2	15.44	19.90	4.46		295
STONE	2 LANES	9	11	0.00	18.04	18.04		1191
STONE	3 LANES - TURN LANE	9	11	18.04	19.64	1.60	19	212
STONE	2 LANES	9	11	19.64	22.98	3.34		221
STONE	3 LANES - PASSING LANE	9	11	22.98	24.11	1.13	75	75
STONE	2 LANES	9	11	24.11	24.28	0.17		12
WHITE	2 LANES	5	13	0.00	0.52	0.52		35
WHITE	3 LANES - PASSING LANE	5	13	0.52	1.52	1.00	66	66
WHITE	2 LANES	5	13	1.52	1.72	0.20		14
WHITE	2 LANES - LEFT TURN LANE	5	13	1.72	1.88	0.16	6	21
WHITE	2 LANES	5	13	1.88	2.92	1.04		69
WHITE	2 LANES	5	14	0.00	1.46	1.46		97
WHITE	3 LANES - PASSING LANE	5	14	1.46	1.65	0.19	13	13
WHITE	2 LANES - LEFT TURN LANE	5	14	1.65	1.84	0.19	3	25
WHITE	2 LANES	5	14	1.84	3.36	1.52		101
WHITE	3 LANES - PASSING LANE	5	14	3.36	3.99	0.66	44	44
WHITE	2 LANES - LEFT TURN LANE	5	14	9.56	9.68	0.12	3	16
WHITE	2 LANES	5	14	9.68	9.89	0.21		14
WHITE	3 LANES - PASSING LANE	5	14	9.89	11.00	1.11	74	74
WHITE	2 LANES	5	14	11.00	14.61	3.61		239
WHITE	3 LANES - PASSING LANE	5	14	14.61	15.70	1.09	72	72
WHITE	2 LANES	5	14	15.70	16.54	0.84		56
WHITE	3 LANES - TURN LANE	5	14	16.54	16.60	0.06		8
WHITE	2 LANES	11	11	0.00	3.40	3.40		224
WHITE	HWY 267 INTERSECTION	13	13	4.98	5.06	0.08	3	11
WHITE	2 LANES	13	13	5.06	9.86	4.80		317
WHITE	2 LANES - LEFT TURN LANE	13	13	9.00	9.16	0.16	10	21
WHITE	2 LANES	13	13	9.16	9.78	0.62		41
WHITE	HWY 36 INTERSECTION	13	13	9.78	9.86	0.08	5	11
WHITE	HWY 36 INTERSECTION	13	13	9.86	9.95	0.09	3	13
WHITE	2 LANES	13	13	9.95	13.47	3.52		233
WHITE	HWY 16 INTERSECTION	13	13	13.47	13.60	0.13	6	12
WHITE	HWY 16 INTERSECTION	13	13	13.60	13.73	0.13	7	12
WHITE	2 LANES	13	13	13.73	17.74	4.01		265
WHITE	2 LANES	16	13	0.00	3.29	3.29		218
WHITE	3 LANES - PASSING LANE	16	13	3.29	4.12	0.83	55	55
WHITE	2 LANES	16	13	4.12	6.72	2.60		172
WHITE	3 LANES - PASSING LANE	16	13	6.72	8.42	1.70		224
WHITE	2 LANES	16	13	8.42	9.18	0.76		51
WHITE	3 LANES - PASSING LANE	16	13	9.18	10.11	0.93	62	62
WHITE	3 LANES - TURN LANE	16	13	10.11	11.31	1.20		158
WHITE	3 LANES - TURN LANE	16	13	11.31	11.38	0.07	3	10
WHITE	2 LANES	16	13	11.38	12.02	0.64		43
WHITE	3 LANES - TURN LANE	16	13	12.02	12.17	0.15	3	20
WHITE	2 LANES	16	13	12.17	13.13	0.96		64
WHITE	HWY 13 INTERSECTION	16	13	13.13	13.25	0.12	6	16
WHITE	HWY 13 INTERSECTION	16	13	13.25	13.40	0.15	30	20
WHITE	4 LANES - UNDIVIDED	16	13	13.40	13.88	0.48	64	32
WHITE	HOLMES INTERSECTION	16	13	13.88	13.94	0.06	8	8
WHITE	HOLMES INTERSECTION	16	13	13.94	14.00	0.06	12	8
WHITE	4 LANES - UNDIVIDED	16	13	14.00	14.63	0.63	84	42
WHITE	3 LANES - TURN LANE	16	13	14.63	14.66	0.03		4
WHITE	2 LANES	16	13	14.66	14.98	0.32		22
WHITE	2 LANES - LEFT TURN LANE	16	13	14.98	15.04	0.06	4	4
WHITE	3 LANES - TURN LANE	16	13	15.04	15.12	0.08	3	12
WHITE	2 LANES	31	4	4.23	6.50	2.27		150
WHITE	2 LANES	36	2	1.01	2.00	0.99		66
WHITE	2 LANES	36	3	0.00	0.33	0.33		22
WHITE	2 LANES - LEFT TURN LANE	36	3	0.33	0.43	0.10	3	13
WHITE	2 LANES	36	3	0.43	12.26	11.83		781
WHITE	3 LANES - TURN LANE	36	3	12.26	14.12	1.86		246
WHITE	2 LANES	36	3	14.12	16.08	1.96		130
WHITE	3 LANES - TURN LANE	36	3	16.08	16.28	0.20	13	13
WHITE	2 LANES	36	3	16.28	17.08	0.80		53
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	36	3	17.08	18.08	1.00	132	132
WHITE	HWY. 13 INTERSECTION	36	3	18.08	18.20	0.12	24	8
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	36	3	18.20	19.11	0.91	122	122
WHITE	COUNTRY CLUB INTERSECTION	36	3	19.11	19.21	0.10	20	7
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	36	3	19.21	19.62	0.41	56	56
WHITE	SAWMILL INTERSECTION	36	3	19.62	19.71	0.09	18	6
SUBTOTALS (BOX 7 OF 12):							1262	8598

** QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

RAISED PAVEMENT MARKERS (8 OF 12)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	36	3	19.71	20.21	0.50	66	66
WHITE	SKYLINE INTERSECTION	36	3	20.21	20.28	0.07	14	5
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	36	3	20.28	21.18	0.90	120	120
WHITE	ELM INTERSECTION	36	3	21.18	21.23	0.05	10	3
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	36	3	21.23	21.38	0.15	20	20
WHITE	MAIN INTERSECTION	36	3	21.38	21.49	0.11	22	7
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	36	3	21.49	21.81	0.32	44	44
WHITE	DR. JIMMY CARR/REMINGTON INTERSECTION	36	3	21.81	21.90	0.09	18	6
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	36	3	21.90	22.71	0.81	108	108
WHITE	BENTON INTERSECTION	36	3	22.71	22.81	0.10	20	7
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	36	3	22.81	23.44	0.63	84	84
WHITE	3 LANES - TURN LANE	36	3	23.44	23.83	0.39		52
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	36	4	0.00	0.10	0.10	14	14
WHITE	EVANS INTERSECTION	36	4	0.10	0.18	0.08	16	5
WHITE	4 LANES - UNDIVIDED	36	4	0.18	0.35	0.17	24	12
WHITE	2 LANES	36	4	0.35	1.82	1.47		98
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	64	10	0.00	1.74	1.74	230	230
WHITE	HWY 5 INTERSECTION	64	10	1.74	1.85	0.11	22	7
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	64	10	1.85	13.40	11.55	1526	1526
WHITE	3 LANES - TURN LANE	64	10	13.40	13.79	0.39	15	52
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	64	11	0.00	0.35	0.35	48	48
WHITE	2 LANES - LEFT TURN LANE	64	11	0.35	0.45	0.10	4	13
WHITE	2 LANES	64	11	0.45	4.20	3.75		248
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	64	11	4.20	4.42	0.22	30	30
WHITE	4 LANES - UNDIVIDED	64	11	4.42	4.76	0.34	46	23
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	64	11	4.76	4.92	0.16	22	22
WHITE	4 LANES - UNDIVIDED	64	11	4.92	5.27	0.35	48	24
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	64	11	5.27	5.45	0.18	24	24
WHITE	4 LANES - UNDIVIDED	64	11	5.45	5.65	0.20	28	14
WHITE	2 LANES	64	11	5.65	9.46	3.81		252
WHITE	3 LANES - TURN LANE	67	12B	0.00	0.18	0.18		24
WHITE	367 INTERSECTION	67	12B	0.18	0.24	0.06	4	4
WHITE	3 LANES - TURN LANE	67	12B	0.24	0.34	0.10		14
WHITE	CORNERSTONE INTERSECTION	67	12B	0.34	0.42	0.08	16	5
WHITE	2 LANES	67	12B	0.42	0.85	0.43		29
WHITE	3 LANES - TURN LANE	67	12B	0.85	1.09	0.24		32
WHITE	2 LANES	67	12B	1.09	1.94	0.85	3	57
WHITE	HWY 16 INTERSECTION	67	12C	3.50	3.54	0.04	3	3
WHITE	3 LANES - TURN LANE	67	12C	3.54	3.94	0.40		54
WHITE	GRAND INTERSECTION	67	12C	3.94	4.02	0.08	5	5
WHITE	3 LANES - TURN LANE	67	12C	4.02	4.46	0.44		60
WHITE	MOSS INTERSECTION	67	12C	4.46	4.53	0.07	5	5
WHITE	3 LANES - TURN LANE	67	12C	4.53	4.60	0.07		10
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	67	12C	4.60	4.77	0.17	24	24
WHITE	BENTON INTERSECTION	67	12C	4.77	4.80	0.03	6	2
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	67	12C	4.80	5.27	0.47	64	64
WHITE	MARION & CLOVERDALE INTERSECTIONS	67	12C	5.27	5.36	0.09	18	6
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	67	12C	5.36	5.99	0.63	84	84
WHITE	POPLAR AND RAMP INTERSECTION	67	12C	5.99	6.13	0.14	28	9
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	67	12C	6.13	6.17	0.04	6	6
WHITE	RAMP INTERSECTION	67	12C	6.17	6.22	0.05	10	3
WHITE	4 LANES - UNDIVIDED	67	12C	0.46	1.54	1.08	144	72
WHITE	367 INTERSECTION	67	12C	1.54	1.68	0.14	26	19
WHITE	4 LANES - UNDIVIDED	67	12C	1.68	2.13	0.45	60	30
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	67	12C	2.13				



Sep 11 2023 12:01 PM
DataSign

RAISED PAVEMENT MARKERS (9 OF 12)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
							EACH	
WHITE	4 LANES - UNDIVIDED	367	17	7.47	8.21	0.74	98	49
WHITE	5 LANES - TURN LANE/PAINTED MEDIAN	367	17	8.21	8.29	0.08	12	12
WHITE	2 LANES	367	18	0.00	10.77	10.77		711
WHITE	3 LANES - PASSING LANE	367	17Y	0.00	0.18	0.18	12	12
WHITE	2 LANES	367	17Y	0.18	0.28	0.10		7
WHITE	2 LANES	385	1	2.45	6.22	3.77		249
WHITE	4 LANES - DIVIDED	67	12	0.00	20.67	20.67	2728	
WHITE	RAMPS	67	12	0.00	20.67	20.67	1612	
WHITE	4 LANES - DIVIDED	67	13	0.00	20.78	20.78	2742	
WHITE	RAMPS	67	13	0.00	20.78	20.78	1860	
CLAY	2 LANES	1	22	0.00	4.91	4.91		324
CLAY	2 LANES	49	1	0.00	15.56	15.56	10	1037
CLAY	2 LANES	62	20	0.00	0.82	0.82		54
CLAY	3 LANES - TURN LANE	62	20	0.82	1.06	0.24		32
CLAY	2 LANES	62	20	1.06	16.10	15.04	5	1003
CLAY	3 LANES - PASSING LANE	62	20	16.10	17.02	0.92	61	61
CLAY	2 LANES	62	20	17.02	17.47	0.45		30
CLAY	3 LANES - PASSING LANE	62	20	17.47	18.41	0.94	62	62
CLAY	2 LANES	62	20	18.41	20.00	1.59		105
CLAY	3 LANES - PASSING LANE	62	20	20.00	20.83	0.83	55	55
CLAY	2 LANES	62	20	20.83	23.04	2.21		146
CLAY	3 LANES - PASSING LANE	62	20	23.04	23.97	0.93	62	62
CLAY	2 LANES	62	20	23.97	25.81	1.84		121
CLAY	3 LANES - TURN LANE	62	20	25.81	25.95	0.14		18
CLAY	3 LANES - TURN LANE	62	21	0.00	0.90	0.90	5	120
CLAY	2 LANES	62	21	0.90	7.59	6.69		442
CLAY	2 LANES	67	20	0.00	9.91	9.91		654
CLAY	3 LANES - TURN LANE	67	20	9.91	11.81	1.90	20	240
CLAY	2 LANES	67	20	11.81	18.05	6.24	10	422
CLAY	2 LANES	90	1	0.00	7.21	7.21	5	480
CLAY	2 LANES	90	2	0.00	17.04	17.04		1125
CLAY	2 LANES	90	3	0.00	10.18	10.18		681
CLAY	2 LANES	119	6	0.00	3.05	3.05		201
CLAY	2 LANES	135	6	0.00	10.04	10.04		675
CLAY	2 LANES	139	4	0.00	8.47	8.47		559
CLAY	2 LANES	139	5	0.00	4.53	4.53		299
CLAY	2 LANES	211	1	4.59	9.63	5.04		333
CLAY	2 LANES	328	2	0.00	7.83	7.83		517
CRAIGHEAD	2 LANES	1	17	0.00	2.92	2.92	5	203
CRAIGHEAD	3 LANES - PASSING LANE	1	17	2.92	3.59	0.67	55	55
CRAIGHEAD	4 LANES - UNDIVIDED	1	17	3.59	4.23	0.64	84	42
CRAIGHEAD	3 LANES - PASSING LANE	1	17	4.23	4.92	0.69	56	56
CRAIGHEAD	3 LANES - TURN LANE	1	17	4.92	7.17	2.25	25	298
CRAIGHEAD	5 LANES - TURN LANE	1	17	7.17	7.22	0.05	7	7
CRAIGHEAD	2 LANES	1	17B	0.00	2.38	2.38		157
CRAIGHEAD	5 LANES - TURN LANE	1	17B	2.80	3.20	0.40	50	50
CRAIGHEAD	2 LANES	1	17B	3.20	4.03	0.83	15	70
CRAIGHEAD	5 LANES - TURN LANE	18	4	0.00	28.85	28.85	3928	3688
CRAIGHEAD	5 LANES - TURN LANE	49	3	0.00	14.45	14.45	1204	900
CRAIGHEAD	2 LANES	49	3B	0.00	3.66	3.66		242
CRAIGHEAD	5 LANES - TURN LANE	49	4	0.00	6.36	6.36	890	820
CRAIGHEAD	5 LANES - TURN LANE	63	6	0.00	8.96	8.96	1212	1170
CRAIGHEAD	1 - ENTRANCE RAMP	63	6				38	
CRAIGHEAD	1 - EXIT RAMP	63	6				86	
CRAIGHEAD	2 LANES	63	6B	0.00	1.15	1.15		76
CRAIGHEAD	3 LANES - TURN LANE	63	6B	1.15	1.86	0.71		94
CRAIGHEAD	2 LANES	63	6B	1.86	2.74	0.88		58
CRAIGHEAD	4 LANES - DIVIDED	67	16	0.00	7.50	7.50	990	
CRAIGHEAD	2 - ENTRANCE RAMPS	67	16				76	
CRAIGHEAD	2 - EXIT RAMPS	67	16				172	
CRAIGHEAD	2 LANES	69	8	0.00	4.34	4.34		286
CRAIGHEAD	2 LANES	91	2	11.88	11.88	1.34		88
CRAIGHEAD	5 LANES - TURN LANE	91	2	11.88	18.71	6.83	902	902
CRAIGHEAD	2 LANES	135	2	0.00	11.02	11.02		727
CRAIGHEAD	2 LANES	135	3	0.00	10.59	10.59		699
CRAIGHEAD	2 LANES	139	0	0.00	10.36	10.36		684
CRAIGHEAD	2 LANES	141	0	0.00	2.08	2.08		137
CRAIGHEAD	4 LANES - UNDIVIDED	141	1	0.00	3.33	3.33	440	220
CRAIGHEAD	2 LANES	141	1	3.33	9.15	5.82		384
CRAIGHEAD	2 LANES	158	3	0.00	2.99	2.99		197
CRAIGHEAD	2 LANES	158	4	0.00	10.44	10.44		689
CRAIGHEAD	2 LANES	163	5	0.00	5.22	5.22		345
CRAIGHEAD	2 LANES	163	5S	0.00	0.37	0.37		24
CRAIGHEAD	2 LANES	226	2	0.00	0.51	0.51		34
CRAIGHEAD	3 LANES - DECELERATION LANE	226	2	0.51	0.72	0.21	14	14
SUBTOTALS (BOX 9 OF 12):							19608	24314

NOTE:
SEE PM-2 (ONLY ADDED 19 RAISED PAVEMENT MARKERS FOR THE 1520' LEADING UP TO AN EXIT RAMP. THE OTHER 19 ARE INCLUDED IN MAIN LANE CALCULATIONS)
RAISED PAVEMENT MARKERS PER ENTRANCE RAMP = 38 EACH
RAISED PAVEMENT MARKERS PER EXIT RAMP = 86 EACH

** QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

RAISED PAVEMENT MARKERS (10 OF 12)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
							EACH	
CRAIGHEAD	5 LANES - TURN LANE	226	2	0.72	1.15	0.43	56	56
CRAIGHEAD	4 LANES - DIVIDED	226	2	1.15	12.83	11.68	1541	
CRAIGHEAD	5 LANES - TURN LANE	226	2	12.83	12.98	0.15	20	20
CRAIGHEAD	2 LANES	226	3	0.00	0.98	0.98		65
CRAIGHEAD	2 LANES	226	3	1.94	5.82	3.88		256
CRAIGHEAD	2 LANES	226	3S	0.00	1.46	1.46		96
CRAIGHEAD	2 LANES	230	7	0.00	3.91	3.91		258
CRAIGHEAD	2 LANES	230	10	0.00	7.04	7.04		465
CRAIGHEAD	2 LANES	349	1	0.00	5.49	5.49		362
CRAIGHEAD	2 LANES	351	0	0.00	0.95	0.95		63
CRAIGHEAD	2 LANES	351	1	0.00	2.50	2.50		165
CRAIGHEAD	5 LANES - TURN LANE	463	1	0.00	0.31	0.31	16	14
CRAIGHEAD	2 LANES	463	1	0.31	6.88	6.57	15	450
CRAIGHEAD	3 LANES - TURN LANE	463	1	6.88	8.05	1.17		144
CRAIGHEAD	2 LANES	463	1	8.05	10.17	2.12		140
CRAIGHEAD	4 LANES - DIVIDED	555	3	31.30	49.94	18.64	2460	
CRAIGHEAD	19 - ENTRANCE RAMPS	555	3				722	
CRAIGHEAD	19 - EXIT RAMPS	555	3				1634	
GREENE	2 LANES	34	3	0.00	16.60	16.60		1100
GREENE	2 LANES	34	4	0.00	7.35	7.35		485
GREENE	2 LANES	49	2	0.00	11.50	11.50		759
GREENE	5 LANES - TURN LANE	49	2	13.66	24.00	10.34	1514	1214
GREENE	2 LANES	49	2B	0.00	0.83	0.83		55
GREENE	4 LANES - UNDIVIDED	49	2B	0.83	1.11	0.28	37	19
GREENE	2 LANES	49	2B	1.11	2.20	1.09	15	73
GREENE	5 LANES - TURN LANE	49	2Y	0.00	0.38	0.38	51	51
GREENE	2 LANES	69	10	0.00	7.48	7.48	5	494
GREENE	2 LANES	90	4	0.00	9.75	9.75		644
GREENE	2 LANES	135	4	0.00	6.05	6.05		399
GREENE	2 LANES	135	5	0.00	14.58	14.58	15	972
GREENE	2 LANES	139	2	0.00	12.87	12.87		849
GREENE	2 LANES	141	2	0.00	8.27	8.27		546
GREENE	2 LANES	141	3	0.00	16.07	16.07		1061
GREENE	2 LANES	168	0	0.00	6.10	6.10		403
GREENE	2 LANES	168	1	0.00	2.49	2.49		164
GREENE	2 LANES	228	5	0.00	8.30	8.30		548
GREENE	2 LANES	304	2	0.00	5.49	5.49		362
GREENE	2 LANES	351	3	0.00	3.17	3.17		210
GREENE	2 LANES	358	0	0.00	2.11	2.11		140
GREENE	2 LANES	358	1	0.00	11.76	11.76		777
GREENE	5 LANES - TURN LANE	412	8	0.00	13.87	13.87	1830	1830
GREENE	2 LANES	412	8	13.87	19.05	5.18		342
GREENE	5 LANES - TURN LANE	412	9	5.32	12.46	7.14	942	942
GREENE	5 LANES - TURN LANE	412	8B	0.00	4.59	4.59	606	606
GREENE	5 LANES - TURN LANE	412	9B	0.00	2.04	2.04	270	270
LAWRENCE	2 LANES	25	6	0.00	24.29	24.29		1603
LAWRENCE	2 LANES	34	1	0.00	1.43	1.43	5	99
LAWRENCE	2 LANES	34	1	7.35	8.39	1.04		69
LAWRENCE	2 LANES	62	18	0.00	0.25	0.25		17
LAWRENCE	2 LANES	63	3	0.00	5.04	5.04	10	342
LAWRENCE	3 LANES - TURN LANE	63	3	5.04	5.45	0.41		54
LAWRENCE	2 LANES	63	3	5.45	7.26	1.81	5	125
LAWRENCE	3 LANES - PASSING LANE	63	3	7.26	7.72	0.46	30	30
LAWRENCE	2 LANES	63	3	7.72	9.87	2.15		142



Sep 11 2023 12:01 PM

RAISED PAVEMENT MARKERS (11 OF 12)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
LAWRENCE	2 LANES	67	17Y	0.00	0.43	0.43	10	28
LAWRENCE	2 LANES	90	8	0.00	1.95	1.95		129
LAWRENCE	2 LANES	91	1	0.00	2.63	2.63		174
LAWRENCE	2 LANES	115	2	0.00	17.80	17.80		1175
LAWRENCE	2 LANES	117	1	0.00	4.17	4.17		275
LAWRENCE	2 LANES	117	2	0.00	11.00	11.00		726
LAWRENCE	2 LANES	117	3	0.00	1.34	1.34		88
LAWRENCE	2 LANES	228	4	0.00	0.90	0.90		59
LAWRENCE	2 LANES	230	2	0.00	2.06	2.06		136
LAWRENCE	2 LANES	230	6	0.00	4.01	4.01		265
LAWRENCE	2 LANES	361	2	0.00	3.04	3.04		201
LAWRENCE	2 LANES	367	22	0.00	11.62	11.62	10	777
LAWRENCE	3 LANES - TURN LANE	367	22	11.62	12.00	0.38	10	50
LAWRENCE	2 LANES	367	22	12.00	12.44	0.44		29
LAWRENCE	3 LANES - TURN LANE	367	22	12.44	14.59	2.15	20	264
LAWRENCE	2 LANES	412	6	0.00	3.15	3.15		208
LAWRENCE	3 LANES - TURN LANE	412	6	3.15	4.98	1.83	25	235
LAWRENCE	3 LANES - TURN LANE	412	7	0.00	0.50	0.50	5	61
LAWRENCE	2 LANES	412	7	0.50	0.98	0.48		32
LAWRENCE	2 LANES - BUBBLE	412	7	0.98	1.12	0.14		18
LAWRENCE	5 LANES - TURN LANE	412	7	1.12	7.70	6.58	868	868
MISSISSIPPI	2 LANES	14	16	0.00	15.44	15.44		1019
MISSISSIPPI	5 LANES - TURN LANE	18	5	0.00	6.24	6.24	824	824
MISSISSIPPI	5 LANES - TURN LANE	18	6	0.00	17.43	17.43	2300	2300
MISSISSIPPI	4 LANES - UNDIVIDED	18	6	17.43	18.06	0.63	89	47
MISSISSIPPI	5 LANES - TURN LANE	18	7	0.00	6.38	6.38	912	772
MISSISSIPPI	2 LANES	18	7	6.38	9.09	2.71		179
MISSISSIPPI	4 LANES - DIVIDED	55	12	27.87	44.58	16.71	2206	
MISSISSIPPI	4 LANES - DIVIDED	55	12	52.43	58.03	5.60	740	
MISSISSIPPI	4 LANES - DIVIDED	55	12	62.31	72.27	9.96	1314	
MISSISSIPPI	20 - ENTRANCE RAMPS	55	12				760	
MISSISSIPPI	20 - EXIT RAMPS	55	12				1720	
MISSISSIPPI	2 LANES	61	2	0.00	23.67	23.67	15	1578
MISSISSIPPI	5 LANES - TURN LANE	61	2	23.67	24.33	0.66	93	88
MISSISSIPPI	3 LANES - TURN LANE	61	2	24.33	24.60	0.27		36
MISSISSIPPI	2 LANES	61	2	24.60	24.75	0.15	5	10
MISSISSIPPI	3 LANES - TURN LANE	61	2	24.75	24.84	0.09	10	12
MISSISSIPPI	2 LANES	61	3	0.00	13.14	13.14		868
MISSISSIPPI	3 LANES - TURN LANE	61	3	13.14	13.41	0.27		36
MISSISSIPPI	5 LANES - TURN LANE	61	3	13.41	16.46	3.05	211	201
MISSISSIPPI	2 LANES	61	3	16.46	16.96	0.50	15	43
MISSISSIPPI	3 LANES - TURN LANE	61	3	16.96	17.51	0.55	10	74
MISSISSIPPI	2 LANES	61	3	17.51	22.21	4.70		310
MISSISSIPPI	2 LANES	77	1	0.00	12.20	12.20		805
MISSISSIPPI	2 LANES	77	2	0.00	11.67	11.67		770
MISSISSIPPI	2 LANES	118	1	9.70	17.88	8.18		540
MISSISSIPPI	2 LANES	119	2	0.00	2.67	2.67		176
MISSISSIPPI	2 LANES	119	2Y	0.00	0.54	0.54		36
MISSISSIPPI	2 LANES	119	4	0.00	3.27	3.27		216
MISSISSIPPI	2 LANES	137	1	0.00	8.78	8.78		579
MISSISSIPPI	2 LANES	140	2	3.83	14.47	10.64		666
MISSISSIPPI	3 LANES - TURN LANE	140	2	14.42	14.63	0.21	10	28
MISSISSIPPI	5 LANES - TURN LANE	140	2	14.63	15.51	0.88	116	116
MISSISSIPPI	4 LANES	140	2	15.51	16.60	1.09	144	72
MISSISSIPPI	4 LANES - DIVIDED	140	2	16.60	18.47	1.87	246	246
MISSISSIPPI	2 LANES	140	3	0.00	1.89	1.89		125
MISSISSIPPI	2 LANES	150	0	0.00	3.77	3.77		249
MISSISSIPPI	2 LANES	150	1	0.00	10.46	10.46		690
MISSISSIPPI	4 LANES - UNDIVIDED	151	2	0.00	1.61	1.61	222	116
MISSISSIPPI	5 LANES - TURN LANE	151	2	1.62	3.27	1.65	218	218
MISSISSIPPI	2 LANES	151	2	3.27	6.66	3.39		224
MISSISSIPPI	2 LANES	158	1	0.00	14.84	14.84		979
MISSISSIPPI	2 LANES	158	2	0.00	7.53	7.53		497
MISSISSIPPI	2 LANES	181	0	0.00	2.45	2.45		162
MISSISSIPPI	2 LANES	181	1	0.00	6.11	6.11		403
MISSISSIPPI	2 LANES	181	2	0.00	10.24	10.24		676
MISSISSIPPI	3 LANES - TURN LANE	181	4	0.00	0.26	0.26		34
MISSISSIPPI	2 LANES	181	4	0.26	2.87	2.61		172
MISSISSIPPI	2 LANES	198	10	0.00	3.37	3.37		222
MISSISSIPPI	2 LANES	239	0	0.00	1.45	1.45		96
MISSISSIPPI	2 LANES	312	1	0.00	4.93	4.93		326
MISSISSIPPI	2 LANES	312	3	0.00	4.48	4.48		296
MISSISSIPPI	2 LANES	325	0	0.00	1.49	1.49		98
MISSISSIPPI	2 LANES	325	1	0.00	3.51	3.51		232
POINSETT	2 LANES	1	15	0.00	8.42	8.42	5	561
SUBTOTALS (BOX 11 OF 12):							13133	24854

NOTE:
 SEE PM-2 (ONLY ADDED 19 RAISED PAVEMENT MARKERS FOR THE 1520' LEADING UP TO AN EXIT RAMP. THE OTHER 19 ARE INCLUDED IN MAIN LANE CALCULATIONS)
 RAISED PAVEMENT MARKERS PER ENTRANCE RAMP = 38 EACH
 RAISED PAVEMENT MARKERS PER EXIT RAMP = 86 EACH

** QUANTITIES ESTIMATED.
 SEE SECTION 104.03 OF THE STD. SPECS.

RAISED PAVEMENT MARKERS (12 OF 12)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II	TYPE II
							(WHITE/RED)	(YELLOW/YELLOW)
POINSETT	3 LANES - PASSING LANE	1	16	6.05	8.32	2.27	151	151
POINSETT	2 LANES	1	16	8.32	9.95	1.63		108
POINSETT	2 LANES	14	13	0.00	17.76	17.76	5	1177
POINSETT	3 LANES - TURN LANE	14	14	0.00	0.44	0.44	48	68
POINSETT	2 LANES	14	14	0.44	1.34	0.90		59
POINSETT	2 LANES	14	15	0.00	2.25	2.25		149
POINSETT	2 LANES	49	5	0.00	2.12	2.12		140
POINSETT	2 LANES	49	5	10.68	16.69	6.02		397
POINSETT	2 LANES	69	6	0.00	5.87	5.87		387
POINSETT	2 LANES	69	6	5.87	6.10	0.23	15	20
POINSETT	2 LANES	69	7	0.00	3.33	3.33	5	220
POINSETT	2 LANES	69	7S	0.00	1.72	1.72		114
POINSETT	3 LANES - TURN LANE	75	5	10.83	10.95	0.12	5	11
POINSETT	2 LANES	75	5	10.95	11.01	0.06		4
POINSETT	3 LANES - TURN LANE	75	5	11.01	11.18	0.17	11	11
POINSETT	2 LANES	75	5	11.18	11.67	0.49		32
POINSETT	2 LANES	118	2	0.00	6.09	6.09		402
POINSETT	3 LANES - TURN LANE	118	2	6.09	6.42	0.33	59	29
POINSETT	2 LANES	135	1	0.00	17.23	17.23		1137
POINSETT	2 LANES	136	0	0.00	2.19	2.19		150
POINSETT	2 LANES	140	1	0.00	14.18	14.18		936
POINSETT	2 LANES	149	6	0.00	5.87	5.87		355
POINSETT	3 LANES - TURN LANE	149	6	5.87	5.95	0.08	5	12
POINSETT	2 LANES	149	6	5.95	6.08	0.13		9
POINSETT	4 LANES	149	6	6.08	6.87	0.79	52	52
POINSETT	2 LANES	158	6	0.00	2.80	2.80		185
POINSETT	2 LANES	163	3	0.00	8.52	8.52		562
POINSETT	2 LANES	163	4	0.00	11.80	11.80		779
POINSETT	2 LANES	198	0	0.00	1.43	1.43		94
POINSETT	3 LANES - TURN LANE	198	0	1.43	1.78	0.35		46
POINSETT	2 LANES	198	0	1.78	2.44	0.66		44
POINSETT	2 LANES	214	0	0.00	8.35	8.35		551
POINSETT	2 LANES	214	2	0.00	8.95	8.95		591
POINSETT	2 LANES	214	3	0.00	2.23	2.23		147
POINSETT	2 LANES	463	2	0.00	1.57	1.57	10	114
POINSETT	3 LANES - TURN LANE	463	2	1.57	4.49	2.92	25	361
POINSETT	2 LANES	463	2	4.49	11.92	7.43		490
POINSETT	4 LANES - DIVIDED	555	2	4.51	31.30	26.79	3536	
POINSETT	14 - ENTRANCE RAMPS	555	2				532	
POINSETT	14 - EXIT RAMPS	555	2				1204	
RANDOLPH	2 LANES	34	2	0.00	1.99	1.99		131
RANDOLPH	2 LANES	62	19	0.00	11.78	11.78		778
RANDOLPH	5 LANES - TURN LANE	62	19	11.78	12.62	0.84	111	111
RANDOLPH	5 LANES - TURN LANE	67	18	0.00	7.74	7.74	1022	1022
RANDOLPH	5 LANES - TURN LANE	67	19	0.00	0.41	0.41	54	54
RANDOLPH	2 LANES	67	19	0.41	15.51	15.10	15	1012
RANDOLPH	2 LANES	90	6	0.00	11.16	11.16	15	752
RANDOLPH	2 LANES	90	7	0.00	13.24	13.24		874
RANDOLPH	2 LANES	115	1	0.00	18.10	18.10		1195
RANDOLPH	2 LANES	166	0	0.00	7.97	7.97	5	531
RANDOLPH	2 LANES	166	2	0.00	7.10	7.10		469
RANDOLPH	2 LANES	251	1	0.00	7.50	7.50		495
RANDOLPH	2 LANES - TURN LANE	304	1	0.00	0.87	0.87		114
RANDOLPH	2 LANES	304	1					

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
11/20/2023		6	ARK.	012469	9	9
12/4/2023						

SUMMARY OF QUANTITIES & REVISIONS



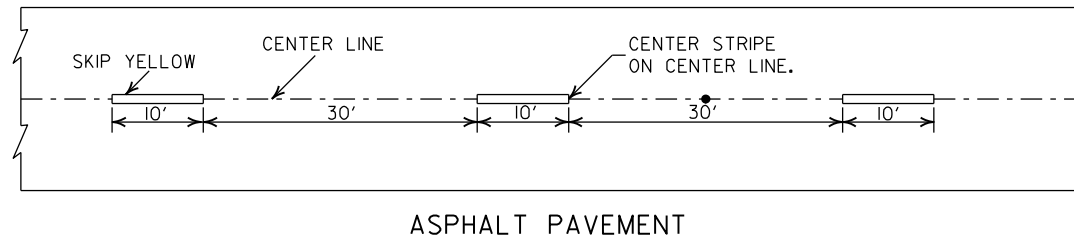
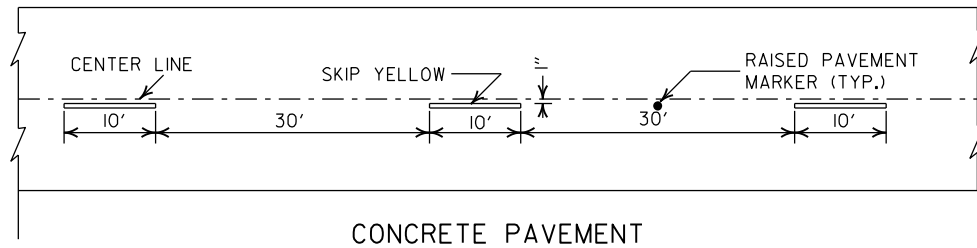
12-05-2023

SUMMARY OF QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
601	MOBILIZATION	1.00	LUMP SUM
SP, SS, & 603	MAINTENANCE OF TRAFFIC	1.00	LUMP SUM
635	ROADWAY CONSTRUCTION CONTROL	1.00	LUMP SUM
721	RAISED PAVEMENT MARKERS (TYPE II)	341811	EACH

REVISIONS

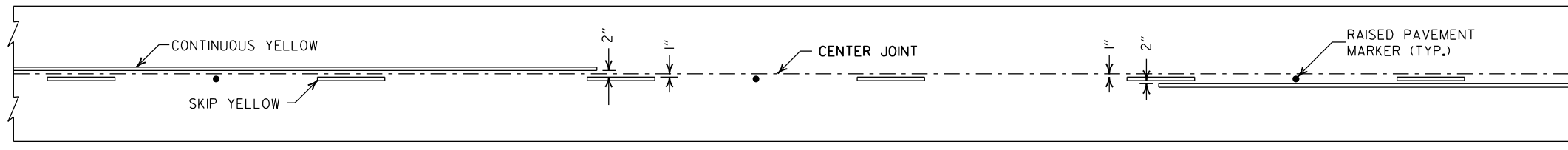
DATE	REVISION	SHEET NUMBER
11/20/2023	ADDED SUPPLEMENTAL SPECIFICATION "CONTACT INFORMATION FOR MOTORIST DAMAGE CLAIMS"	2 & 9
12/4/2023	REVISED "SEQUENCE OF CONSTRUCTION" TO "SEQUENCE OF WORK"	2 & 9



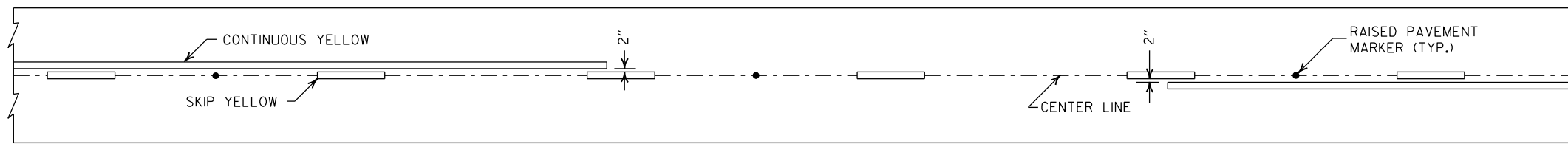
CONCRETE PAVEMENT

ASPHALT PAVEMENT

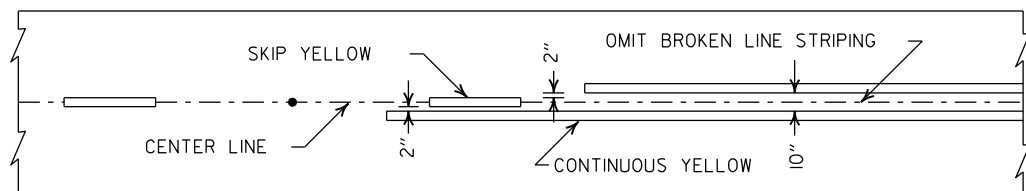
BROKEN LINE STRIPING



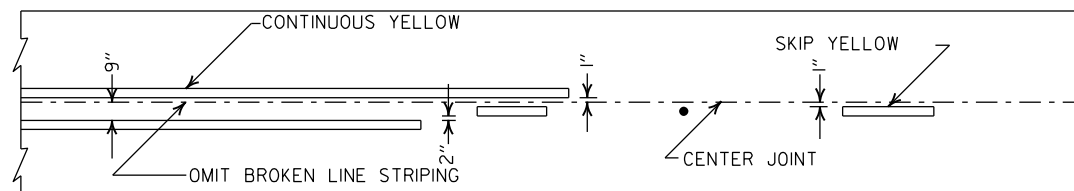
SOLID LINE STRIPING ON CONCRETE PAVEMENT



SOLID LINE STRIPING ON ASPHALT PAVEMENT

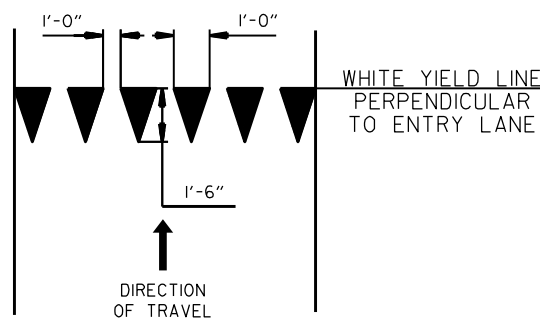


ASPHALT PAVEMENT

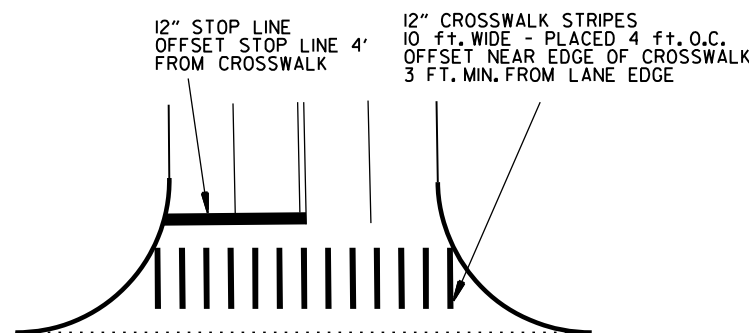


CONCRETE PAVEMENT

STRIPING AT ADJACENT NO PASSING LANES



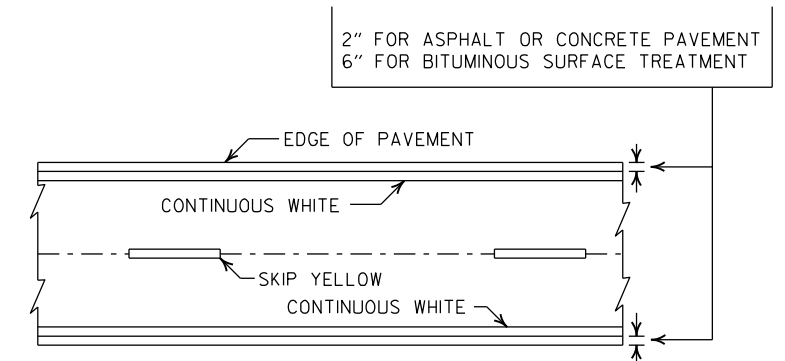
YIELD LINE DETAIL



CROSSWALK AND STOP LINE DETAILS

NOTES:

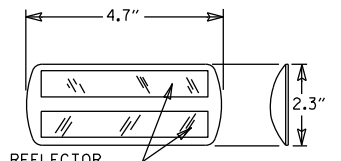
1. REFER TO THE STRIPING DETAILS FOR PAVEMENT MARKING LINE WIDTHS.
2. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
3. RAISED PAVEMENT MARKERS SHALL BE PLACED ON AN 80 FEET SPACING UNLESS OTHERWISE SHOWN IN THE PLANS.



PAVEMENT EDGE LINE MARKING

NOTE: THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.

TYPE II RED/CLEAR OR YELLOW/YELLOW



PRISMATIC REFLECTOR

NOTE: DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ARDOT QUALIFIED PRODUCTS LIST.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

2-27-20	REVISED STOP LINE DETAILS	
6-1-17	ADDED YIELD LINE DETAIL	
5-12-16	REVISED LINE WIDTHS, SPACING, & NOTES	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REVISED GENERAL NOTES & REMOVED PLOWABLE PVMT MRKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTLS.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION

PAVEMENT MARKING DETAILS

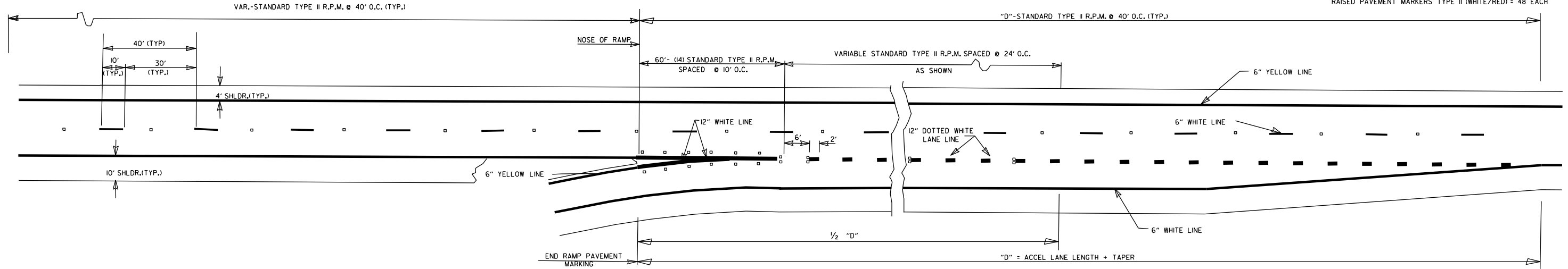
STANDARD DRAWING PM-1

ENTRANCE RAMP

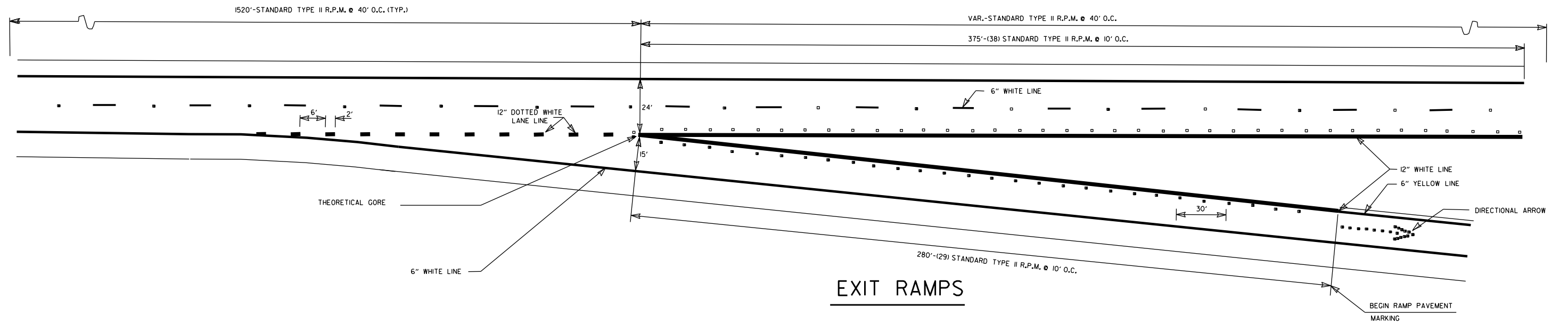
12" WHITE = 370 LIN. FT.
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH

EXIT RAMP

6" WHITE = 280 LIN. FT.
12" WHITE = 815 LIN. FT.
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 48 EACH



ENTRANCE RAMP

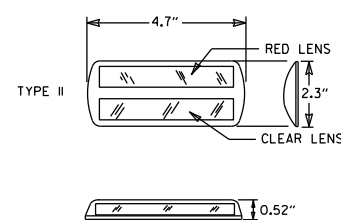


EXIT RAMP

GENERAL NOTES:
THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

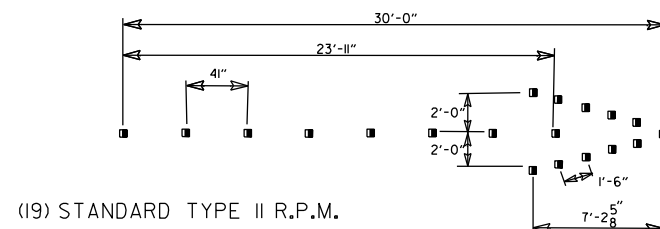
THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ADOT QUALIFIED PRODUCTS LIST.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

NOTE:
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.




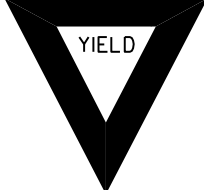
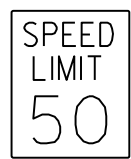
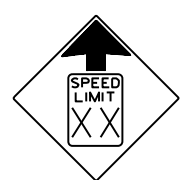





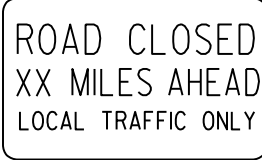


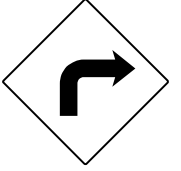


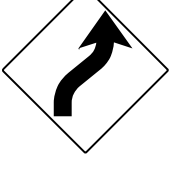

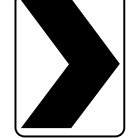
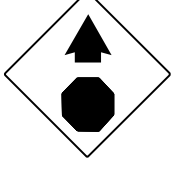
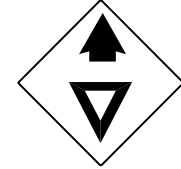
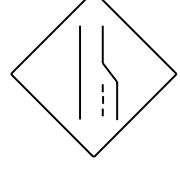



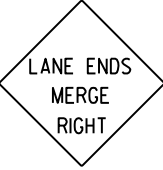









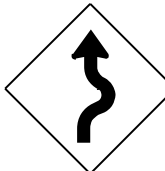



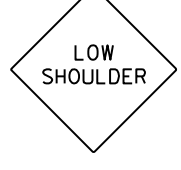

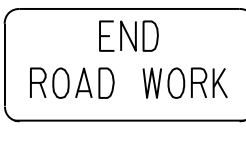
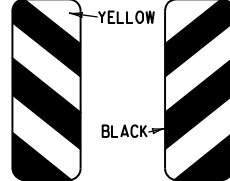
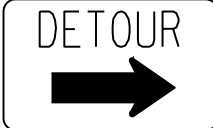

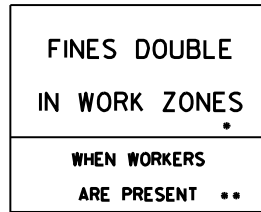
DIRECTIONAL ARROWS

DATE	REVISION	FILMED
05-14-20	REMOVED CROSSHATCH MARKINGS ON EXIT RAMP	
11-07-19	REVISED DOTTED PAV'T MARKINGS; ADDED CROSSHATCH MARKINGS ON EXIT RAMP	
12-8-16	REVISED RAISED PAV'T MARKERS FOR 80' SPACING; REVISED WIDTH OF STRIPING	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
7-26-12	REVISED RPM NOTATION	
12-15-11	REVISED RPMs ACCORDING TO LATEST POLICY	
11-17-10	REMOVED PLOWABLE PAVEMENT MARKERS	
6-3-10	REVISED PER 2009 MUTCD	
11-18-04	REVISED NOTES	
8-22-02	ADDED & REVISED NOTES; REV. ENTRANCE & EXIT RAMP	
5-18-00	REMOVED HASHMARKS	
7-02-98	CHANGED TYPES TO ROMAN NUMERALS	
4-26-96	ADDED DIMENSIONS & QUANTITIES; REVISED LANE WIDTH ON EXIT RAMP	
2-2-95	PLACED IN USE	2-2-95

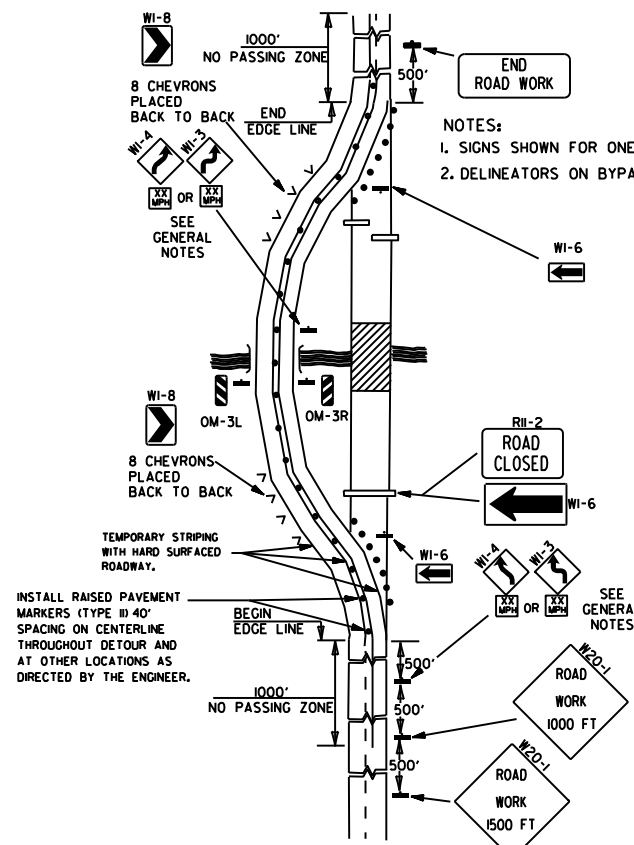
ARKANSAS STATE HIGHWAY COMMISSION

**PAVEMENT MARKING DETAILS
ON
ACCESS CONTROLLED ROADWAYS**

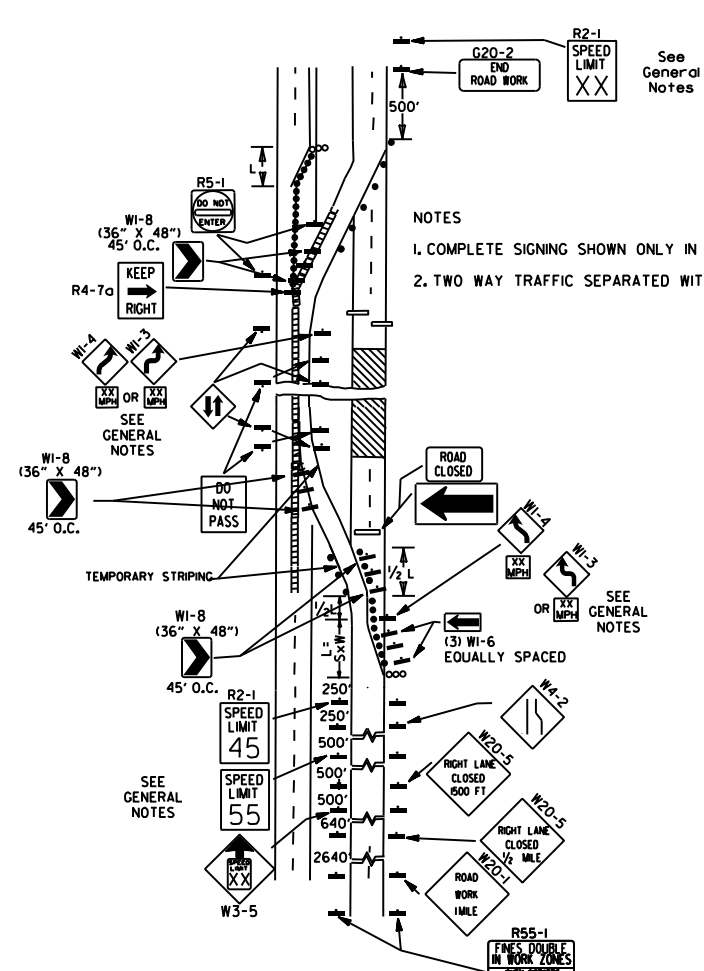
STANDARD DRAWING PM-2

							ADVANCE DISTANCES (XXXX)
<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 36"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>500 FT 1/2 MILE 1000 FT 3/4 MILE 1500 FT 1 MILE AHEAD</p> <p>GENERAL NOTES: 1. ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION. 2. TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER. 3. EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED. 4. SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SO. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE. 5. SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3. 6. POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE. 7. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS. 8. FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS. 9. MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT. 10. R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.</p> <p>NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.</p>
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>W21-5a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W1-3</p>  <p>STD. 48"x48"</p>	<p>W1-4</p>  <p>STD. 48"x48"</p>	<p>W1-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>W1-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>500 FEET W16-2 24" STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>W1-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60" • USE 6" C LETTERS •• USE 4" D LETTERS</p>

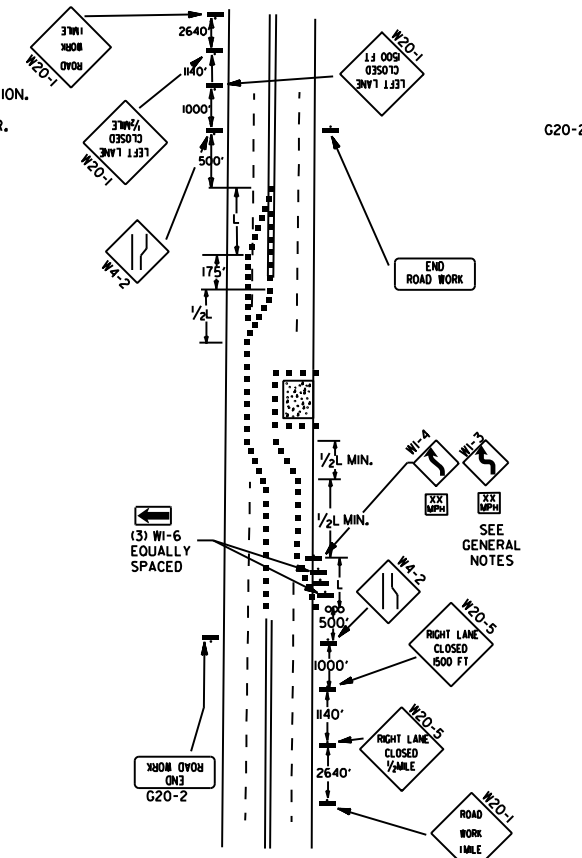
DATE	REVISION	FILMED
11-07-19	REVISED FOR MASH	
4-13-17	DELETED RSP-1 & ADDED W21-5a	
9-2-15	REVISED REDUCED SPEED LIMIT AHEAD SIGNS REVISED ROAD WORK NEXT XX MILES	
12-15-11	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	



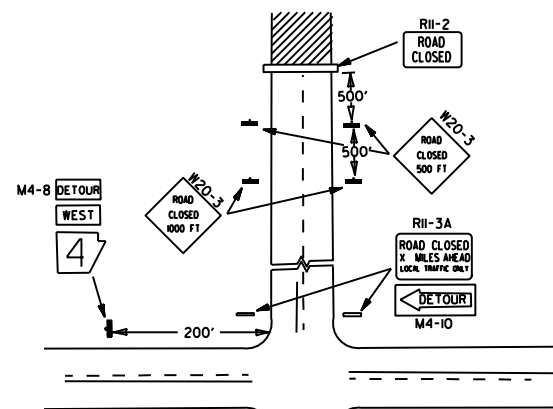
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



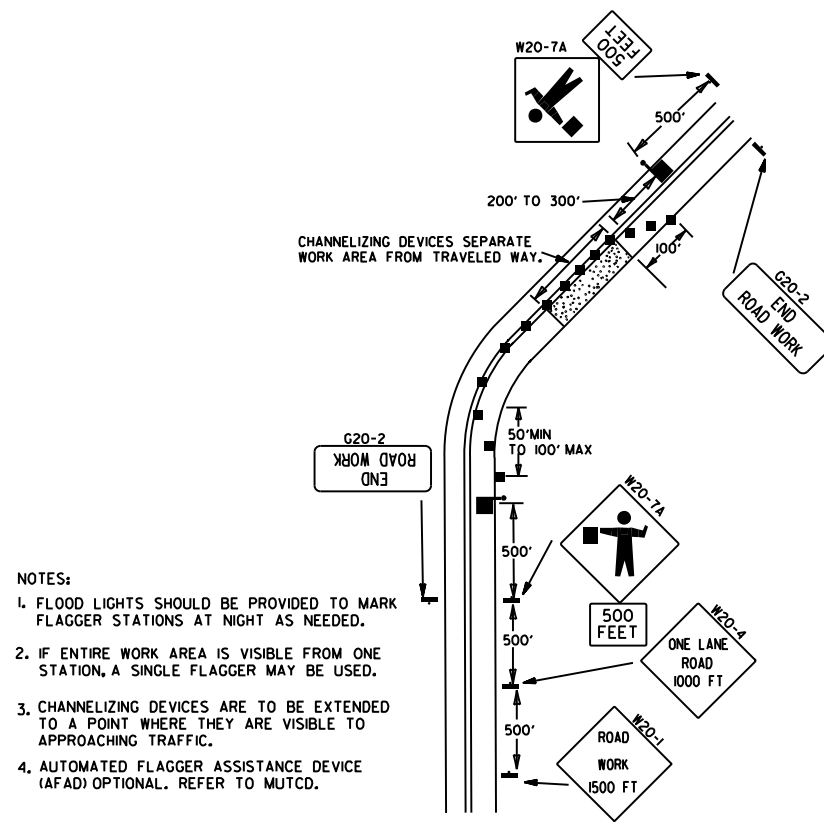
(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



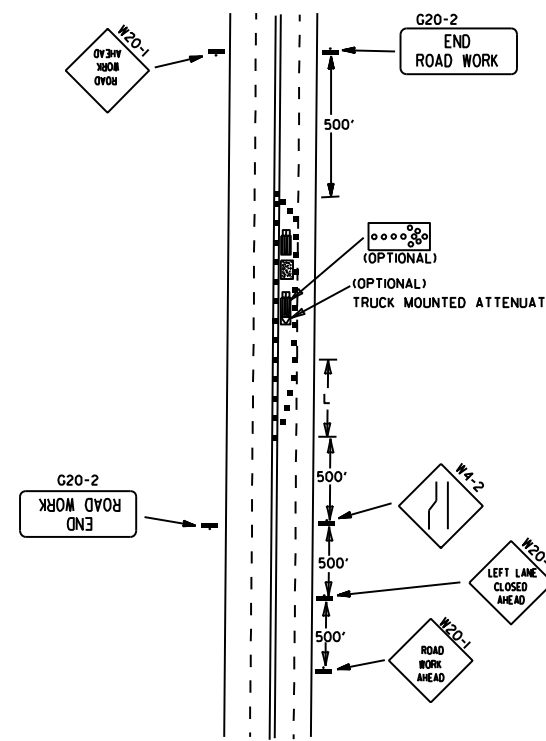
(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.

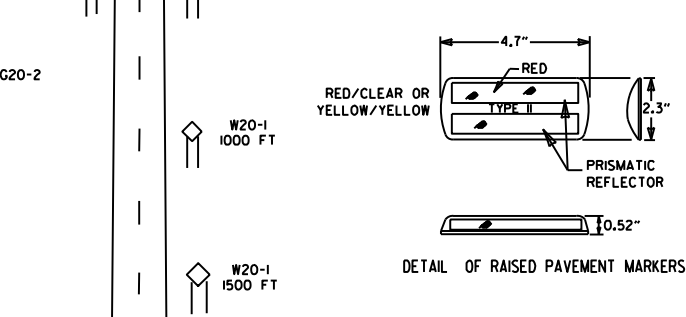


(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

- KEY:
- FLAGGER
 - POSITIVE BARRIER
 - ARROW PANEL (IF REQUIRED)
 - TYPE III BARRICADE
 - CHANNELIZING DEVICE
 - TRAFFIC DRUM
 - RAISED PAVEMENT MARKER



TYPICAL ADVANCE WARNING SIGN PLACEMENT

GENERAL NOTES:
1. THE MAINTENANCE DIVISION SHALL CONDUCT A BALL BANK STUDY TO DETERMINE THE ADVISORY SPEED LIMIT PRIOR TO OPENING TO TRAFFIC. THE ADVISORY SPEED WILL BE POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
2. WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-(155) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-145MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(145) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-(145) SHALL BE OMITTED. ADDITIONAL R2-155MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(155) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
7. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
8. DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ADOT QUALIFIED PRODUCTS LIST.
9. ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

DATE	REVISION	FILED
05-20-21	REVISED NOTE 7	
11-07-19	REVISED NOTE 4, ADDED NOTE 9	
9-2-15	REVISED NOTE 2, ADDED NOTE 8, REVISED DRAWING (A) & REPLACED R2-5A WITH W3-5	
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-11-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

TRAFFIC CONTROL DEVICES

VERTICAL DIFFERENTIAL	LOCATION	TRAFFIC CONTROL	
		≤ 45 MPH	> 45 MPH
≤ 1"	CENTERLINE	W8-11	W8-11
> 1"	CENTERLINE	W8-11 AND CENTERLINE LANE STRIPING	W8-11 AND CENTERLINE LANE STRIPING
≤ 3"	CENTERLINE	STANDARD LANE CLOSURE ⁽⁶⁾	STANDARD LANE CLOSURE ⁽⁶⁾
≤ 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9 AND TRAFFIC DRUMS ⁽¹⁾	W8-9 AND TRAFFIC DRUMS ⁽¹⁾
> 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 18"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	A STABILIZED WEDGE, W8-17, EDGE LINE STRIPING AND TRAFFIC DRUMS ⁽²⁾
> 24"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER ⁽¹⁾ & EDGE LINES	PRECAST CONCRETE BARRIER ⁽¹⁾ & EDGE LINES

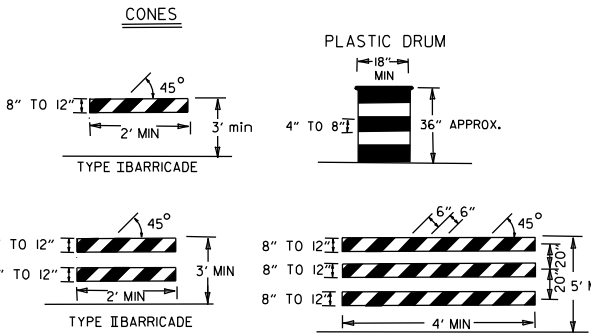
INTERSTATE		
VERTICAL DIFFERENTIAL	LOCATION	TRAFFIC CONTROL
≤ 3"	CENTERLINE	W8-11 AND LANE STRIPING
≤ 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER & EDGE LINES

INTERSTATE AND NON-INTERSTATE		
FORESLOPE	HEIGHT	TRAFFIC CONTROL
1:1	> 2 FT	PRECAST CONCRETE BARRIER
2:1	≤ 5 FT	TRAFFIC DRUMS
2:1	> 5 FT	PRECAST CONCRETE BARRIER
Flatter than 2:1	N/A	TRAFFIC DRUMS

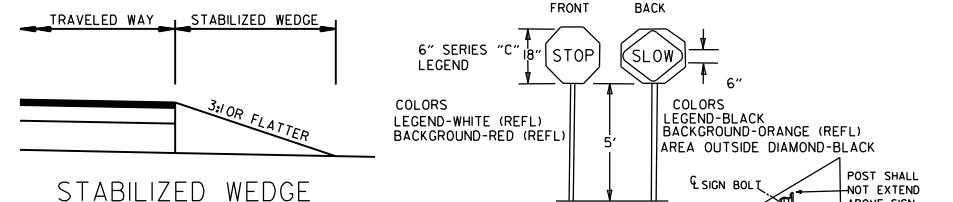
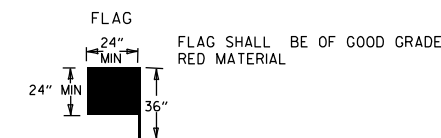
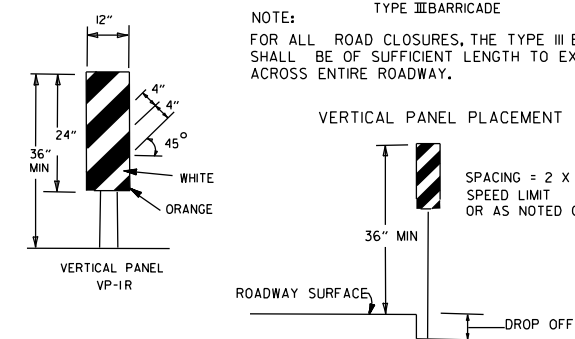
- GENERAL NOTES:
- WHEN THE SHOULDER AREA IS USED AS PART OF THE TRAVELED LANE AND THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, THEN VERTICAL PANELS SHOULD BE USED.
 - WHEN THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, A STABILIZED WEDGE SHALL BE USED. PRECAST CONCRETE BARRIER SHALL CAN BE USED IN LIEU OF A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS.
 - A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS CAN BE USED IN LIEU OF PRECAST CONCRETE BARRIER WALL, IF AND WHERE DIRECTED BY THE ENGINEER.
 - W21-5, W21-5a, AND/OR W21-5b SIGNS SHALL BE USED WHERE THE ROADWAY IS UNOBSTRUCTED IF AND WHERE DIRECTED BY THE ENGINEER.
 - TIME LIMITATIONS MUST CONFORM TO SECTION 603 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION).

CHANNELIZING DEVICES

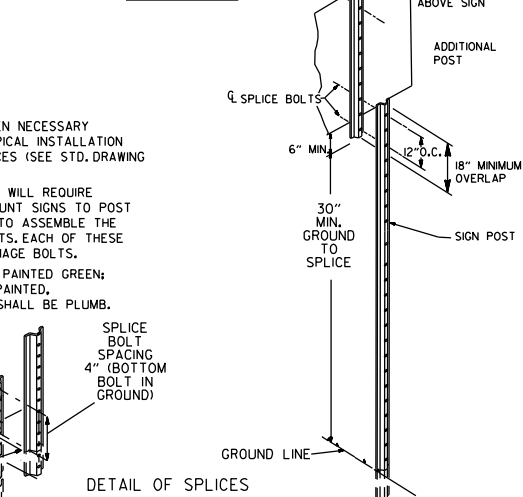
* WHEN CONES ARE USED ON FREEWAYS AND MULTI-LANE HIGHWAYS, THEY SHALL BE 28" MIN. DURING HOURS OF DARKNESS, 28" CONES SHALL BE USED ON ALL ROADWAYS, AND SHALL BE REFLECTORIZED IN ACCORDANCE WITH THE M.U.T.C.D.



NOTE: FOR ALL ROAD CLOSURES, THE TYPE III BARRICADES SHALL BE OF SUFFICIENT LENGTH TO EXTEND ACROSS ENTIRE ROADWAY.

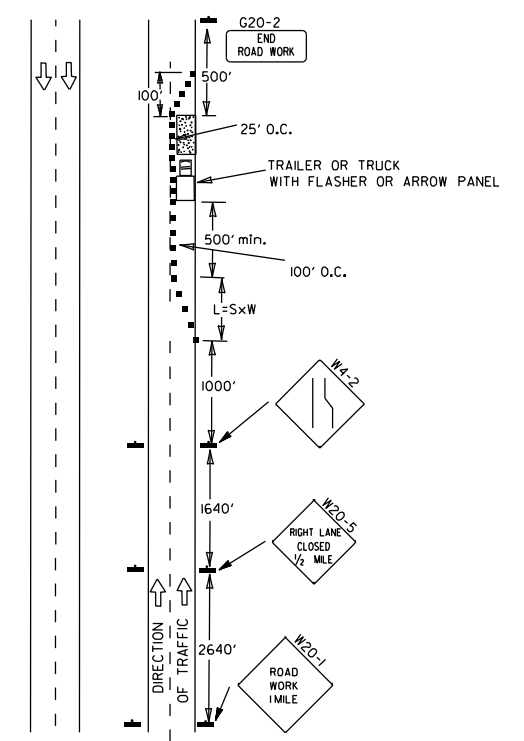


NOTE: MATERIALS FOR THE STABILIZED WEDGE SHALL MEET THE REQUIREMENTS PROVIDED IN SECTION 603.02 OF THE STANDARD SPECIFICATIONS.

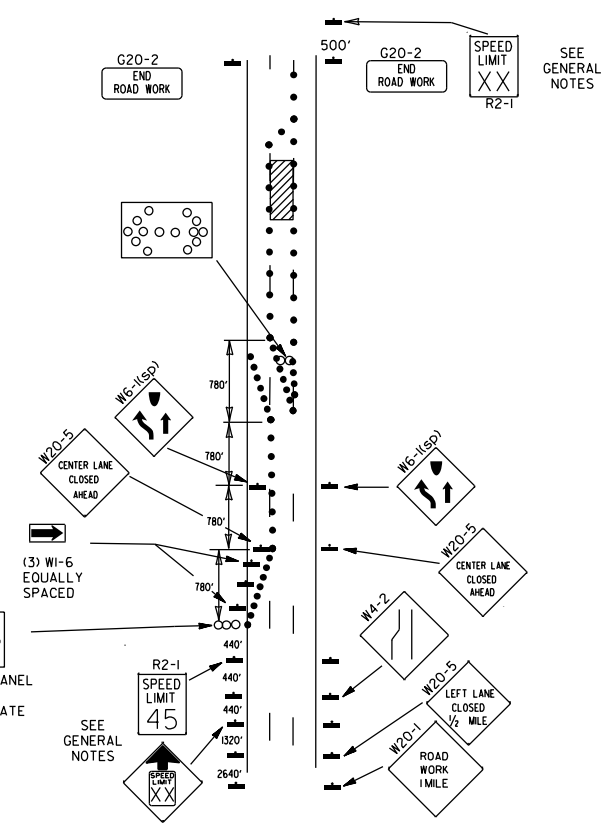


DATE	REVISION	REVISION	FILMED
08-12-21	REVISED TRAFFIC CONTROL DEVICES AND NOTES		
05-20-21	REVISED NOTE 10		
2-27-20	REVISED TRAFFIC CONTROL DEVICES DETAILS		
11-07-19	REVISED NOTE 9, ADDED NOTE II		
7-25-19	REVISED TRAFFIC CONTROL DEVICES DETAILS		
9-2-15	REVISED NOTE 2 & REPLACED R2-5A WITH W3-5		
10-15-09	ADDED REFERENCE TO MASH		
11-20-08	REVISED SIGN DESIGNATIONS		
11-18-04	ADDED NOTE		
10-1-98	ADDED NOTE		
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE		
10-18-96	ADDED R55-1		
10-12-95	MOVED UPPER SPLICE		
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95	
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993		
8-15-91	DRAWN AND PLACED IN USE		

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-3



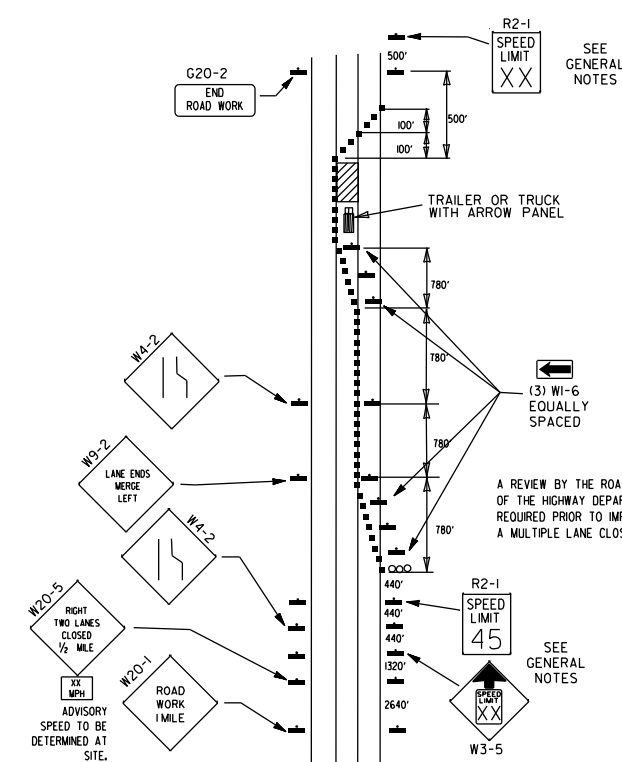
(A) TYPICAL APPLICATION - DAYTIME MAINTENANCE OPERATIONS OF SHORT DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



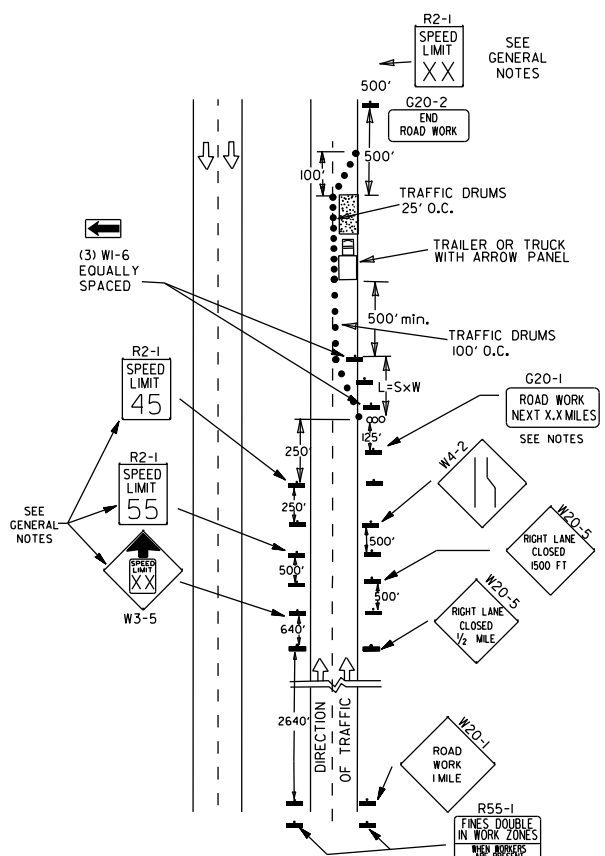
(B) TYPICAL APPLICATION - 3-LANE ONEWAY ROADWAY WHERE CENTER LANE IS CLOSED.

- KEY:
- ○ ○ ○ ARROW PANEL (IF REQUIRED)
 - CHANNELIZING DEVICE
 - TRAFFIC DRUM

- GENERAL NOTES:
- A SPEED LIMIT REDUCTION MAY BE IMPLEMENTED ONLY WHEN DESIGNATED IN THE PLAN OR WHEN RECOMMENDED BY THE ROADWAY DESIGN DIVISION.
 - WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 - WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-1(55)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 - THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT OR AS DIRECTED BY THE ENGINEER.
 - WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
 - PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 - THE G20-1 SIGN WILL BE REQUIRED ON JOBS OF OVER TWO MILES IN LENGTH. WHEN THE LANE CLOSURE IS NOT AT THE BEGINNING OF THE PROJECT, THE G20-1 SIGN SHALL BE ERRECTED 125' IN ADVANCE OF THE JOB LIMIT. ADDITIONAL W20-1(1/4 MILE) SIGNS ARE NOT REQUIRED IN ADVANCE OF LANE CLOSURES THAT BEGIN INSIDE THE PROJECT LIMITS.
 - FLAGGERS SHALL USE STOP/SLOW PADDLES FOR CONTROLLING TRAFFIC THROUGH WORK ZONES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
 - ALL PLASTIC DRUMS AND CONES SHALL MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
 - TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
 - ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).



(D) TYPICAL APPLICATION - CLOSING MULTIPLE LANES OF A MULTILANE HIGHWAY.



(C) TYPICAL APPLICATION - CONSTRUCTION OPERATIONS OF INTERMEDIATE TO LONG TERM DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.