

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
|--|--------------|--------------------|-------|---------|-----------|--------------|
| | | 6 | ARK. | 012479 | 1 | 23 |
| HWYS. 100 & 167 CABLE MEDIAN BARRIER IMPVTS. (S) | | | | | | |

ARKANSAS DEPARTMENT OF TRANSPORTATION
CONSTRUCTION PLANS FOR STATE HIGHWAY

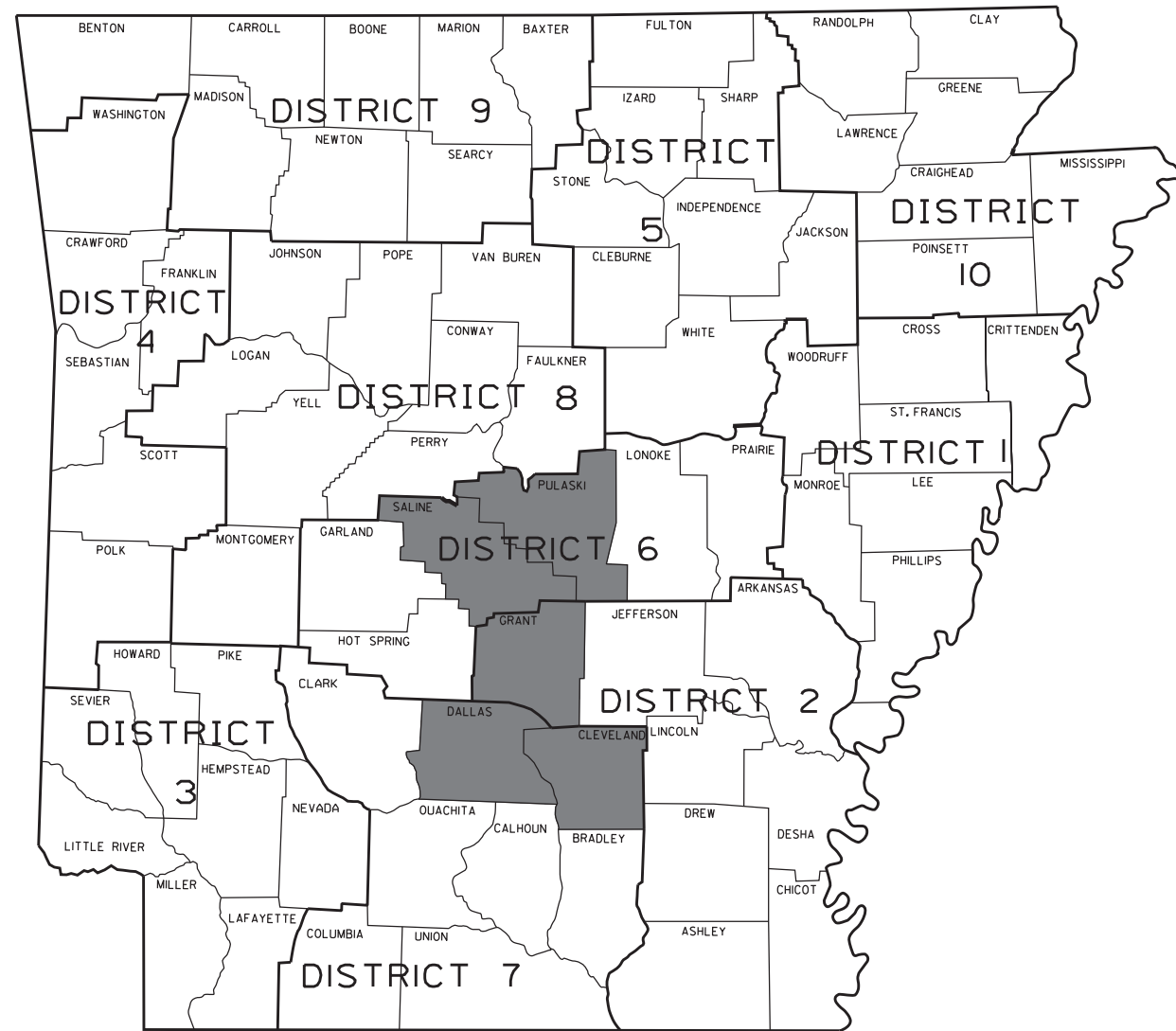


HWYS. 100 & 167 CABLE
MEDIAN BARRIER IMPVTS. (S)

VARIOUS COUNTIES

JOB 012479

FED. AID PROJ. HSIP-0076(335)



ARK. HWY. DIST. NO. 2, 6 & 7

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Date: 2024.03.15 05:59:37 -05'00'

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
|---------------------------------------|--------------|--------------------|-------|---------|-----------|--------------|
| | | 6 | ARK. | 012479 | 2 | 23 |
| INDEX OF SHEETS AND STANDARD DRAWINGS | | | | | | |



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Date: 2024.05.24

INDEX OF SHEETS

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| 22 | GRANT AND SALINE COUNTY PLAN |
| 23 | PULASKI COUNTY PLAN |

ROADWAY STANDARD DRAWINGS

| | | |
|-------|--|----------|
| CDP-1 | CONCRETE DITCH PAVING | 12-08-16 |
| TC-1 | STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION | 11-07-19 |
| TC-2 | STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION | 05-20-21 |
| TC-3 | STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION | 08-12-21 |
| TEC-1 | TEMPORARY EROSION CONTROL DEVICES | 11-16-17 |
| TEC-2 | TEMPORARY EROSION CONTROL DEVICES | 06-02-94 |
| TEC-3 | TEMPORARY EROSION CONTROL DEVICES | 11-03-94 |
| TEC-4 | TEMPORARY EROSION CONTROL DEVICES | 07-26-12 |

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| | | 6 | ARK. | 012479 | 3 | 23 |
| GOVERNING SPECIFICATIONS AND GENERAL NOTES | | | | | | |



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Date: 2024.06.20

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

| NUMBER | TITLE |
|------------|--|
| ERRATA | ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS |
| FHWA-1273 | REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS |
| FHWA-1273 | SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS |
| FHWA-1273 | SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140) |
| FHWA-1273 | SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES |
| FHWA-1273 | SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS |
| FHWA-1273 | SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS |
| FHWA-1273 | SUPPLEMENT - WAGE RATE DETERMINATION |
| 100-3 | CONTRACTOR'S LICENSE |
| 100-4 | DEPARTMENT NAME CHANGE |
| 102-2 | ISSUANCE OF PROPOSALS |
| 102-3 | PREQUALIFICATION OF BIDDERS |
| 103-2 | CONTACT INFORMATION FOR MOTORIST DAMAGE CLAIMS |
| 105-4 | MAINTENANCE DURING CONSTRUCTION |
| 107-2 | RESTRAINING CONDITIONS |
| 108-1 | LIQUIDATED DAMAGES |
| 108-2 | WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER |
| 110-1 | PROTECTION OF WATER QUALITY AND WETLANDS |
| 306-1 | QUALITY CONTROL AND ACCEPTANCE |
| 603-1 | LANE CLOSURE NOTIFICATION |
| 604-1 | RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES |
| 604-3 | TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES (MASH) |
| 605-1 | CONCRETE DITCH PAVING |
| 620-1 | MULCH COVER |
| 621-1 | FILTER SOCKS |
| 802-4 | CEMENT |
| JOB 012479 | ASSESSMENT OF WORKING DAYS - MAINTENANCE OF TRAFFIC |
| JOB 012479 | BIDDING REQUIREMENTS AND CONDITIONS |
| JOB 012479 | BUY AMERICA - CONSTRUCTION MATERIALS |
| JOB 012479 | CARGO PREFERENCE ACT REQUIREMENTS |
| JOB 012479 | CONCRETE DITCH PAVING |
| JOB 012479 | DISADVANTAGED BUSINESS ENTERPRISE BIDDER'S RESPONSIBILITIES |
| JOB 012479 | GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION |
| JOB 012479 | LIQUIDATED DAMAGES PROCEDURE FOR BID LETTINGS |
| JOB 012479 | MAINTENANCE OF TRAFFIC |
| JOB 012479 | MANDATORY ELECTRONIC CONTRACT |
| JOB 012479 | MANDATORY ELECTRONIC DOCUMENT SUBMITTAL |
| JOB 012479 | PARTNERING REQUIREMENTS |
| JOB 012479 | PRICE ADJUSTMENT FOR FUEL |
| JOB 012479 | PROHIBITION OF CERTAIN TELECOMMUNICATIONS AND VIDEO SURVEILLANCE SERVICES OR EQUIPMENT |
| JOB 012479 | SEQUENCE OF CONSTRUCTION |
| JOB 012479 | STORM WATER POLLUTION PREVENTION PLAN - DISTRICT 2 |
| JOB 012479 | STORM WATER POLLUTION PREVENTION PLAN - DISTRICT 6 |
| JOB 012479 | STORM WATER POLLUTION PREVENTION PLAN - DISTRICT 7 |
| JOB 012479 | TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES |
| JOB 012479 | UTILITY ADJUSTMENTS |
| JOB 012479 | VALUE ENGINEERING |
| JOB 012479 | WIRE ROPE SAFETY FENCE (WRSF) SPECIFICATIONS |
| JOB 012479 | WIRE ROPE SAFETY FENCE (POST REPAIR) |
| JOB 012479 | WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS - DISTRICT 2 |
| JOB 012479 | WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS - DISTRICT 6 |
| JOB 012479 | WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS - DISTRICT 7 |
| JOB 012479 | WRSF TRAINING WORKSHOP |

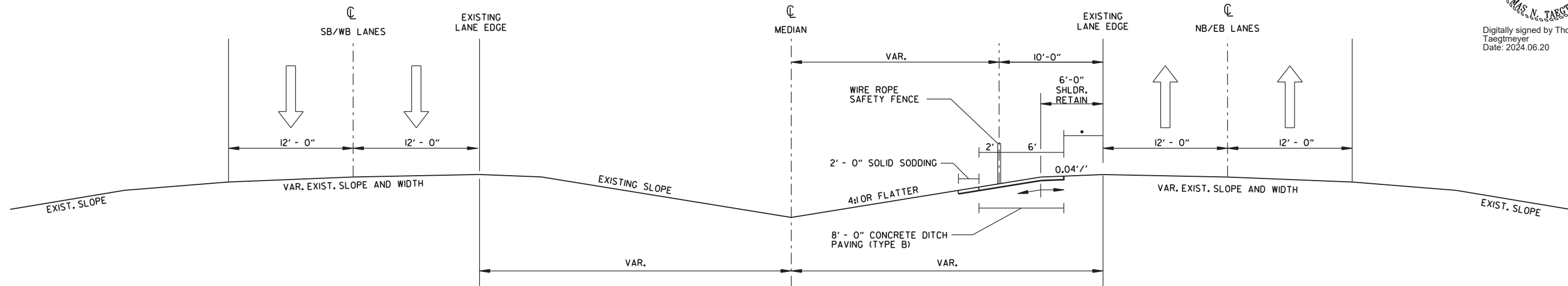
GENERAL NOTES

- ALL PIPE LINES, POWER, TELEPHONE, AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO ENSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATIONS.
- THE SEQUENCE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS IS A GENERAL OUTLINE FOR THE CONSTRUCTION OF THIS PROJECT, AND IN NO WAY IS IT INTENDED TO COVER EVERY ITEM IN THE PROJECT. ITEMS NOT CRITICAL TO THE CONSTRUCTION SEQUENCE MAY BE CONSTRUCTED IN ANY STAGE AS APPROVED BY THE RESIDENT ENGINEER.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE OF THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- AGGREGATE BASE COURSE OUTSIDE THE EXISTING SHOULDERS SHALL BE UNIFORMLY COMPACTED, STABLE, AND FREE OF SEGREGATION. THE DENSITY REQUIREMENTS OF SECTION 303 ARE HEREBY WAIVED.
- PREPARATORY WORK, SUCH AS CLIPPING THE GRASS AND DEBRIS FROM THE EDGE OF THE EXISTING ROADWAY, WILL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED A PART OF THE OTHER ITEMS OF WORK.

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
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| | | 6 | ARK. | 012479 | 4 | 23 |
| TYPICAL SECTIONS OF IMPROVEMENT | | | | | | |

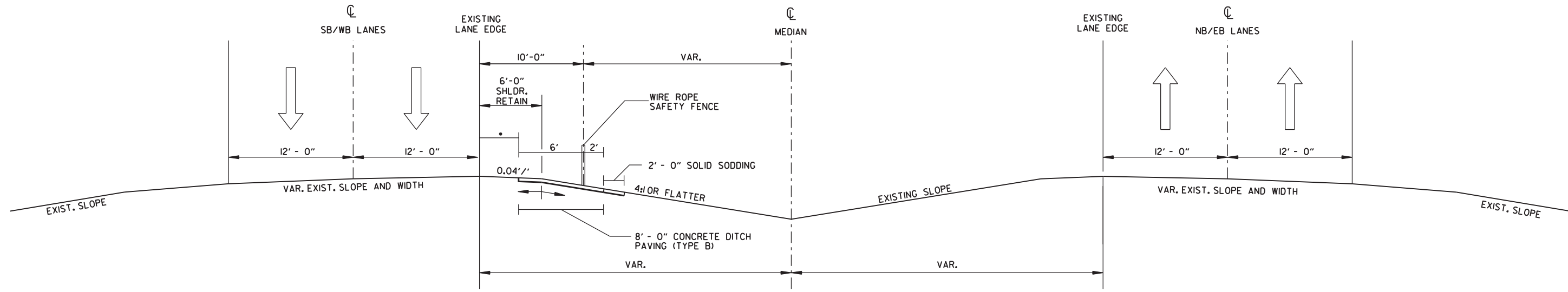


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Date: 2024.06.20



TYPICAL SECTION OF IMPROVEMENT
FOR WIRE ROPE SAFETY FENCE RIGHT OF CENTERLINE

• 4'-0" EXISTING PAVED SHOULDER TO REMAIN

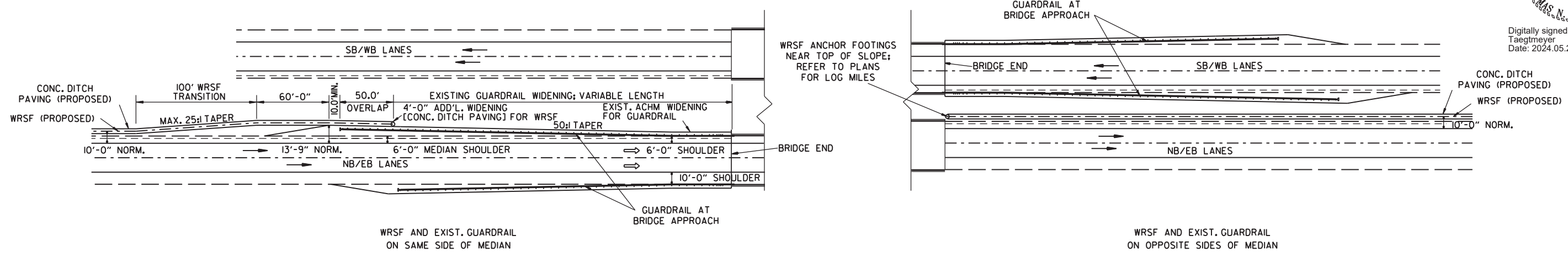


TYPICAL SECTION OF IMPROVEMENT
FOR WIRE ROPE SAFETY FENCE LEFT OF CENTERLINE

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
|-----------------|--------------|--------------------|-------|---------|-----------|--------------|
| | | 6 | ARK. | 012479 | 5 | 23 |
| SPECIAL DETAILS | | | | | | |

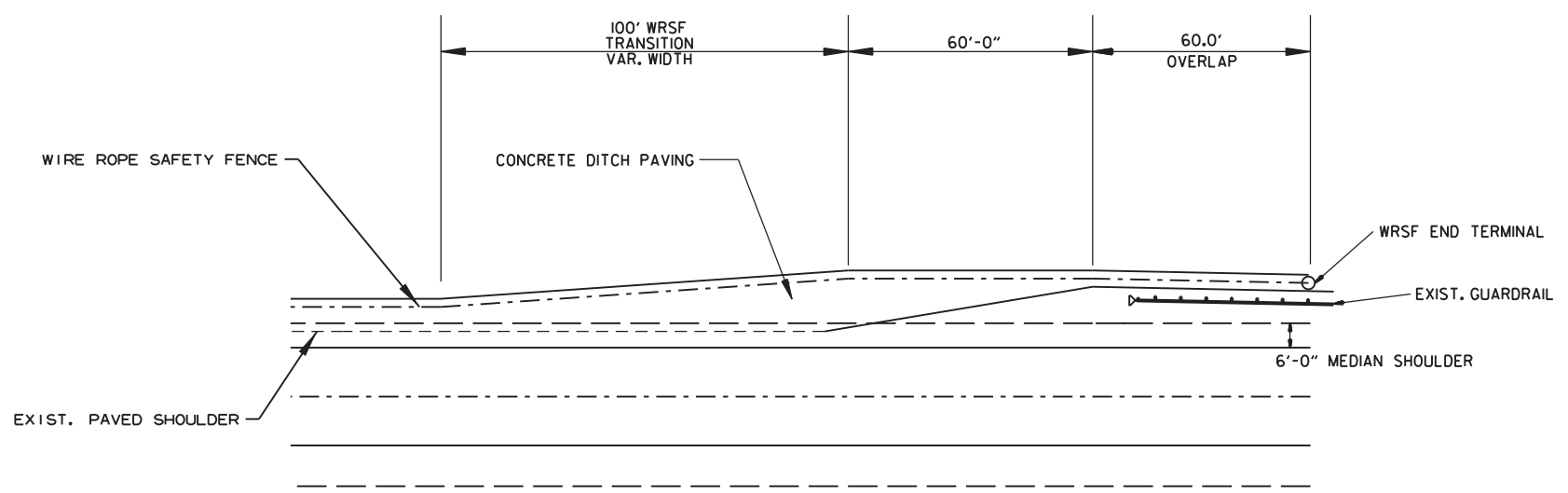


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Date: 2024.05.24



DETAIL OF WIRE ROPE SAFETY FENCE AT EXISTING BRIDGE ENDS

REFER TO PLANS FOR RELATIVE PLACEMENT
OF GUARDRAIL AND WIRE ROPE SAFETY FENCE
AT EACH BRIDGE END



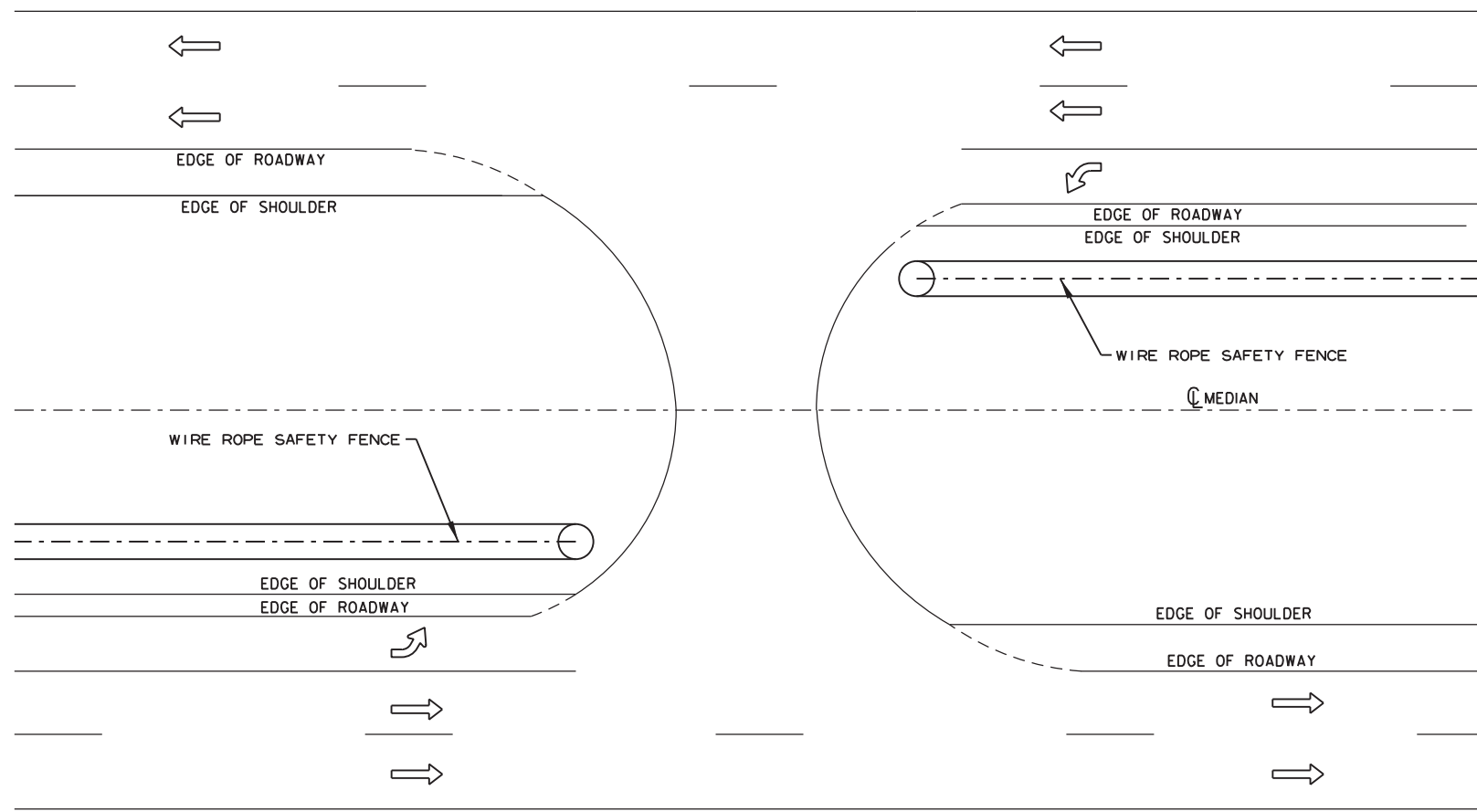
DETAIL OF CONCRETE DITCH PAVING AT GUARDRAIL LOCATIONS

DATE & TIME: 5/24/2024 8:55:50 AM
FILE: J:\25846.16\012479 - Special Details Sheets.dgn

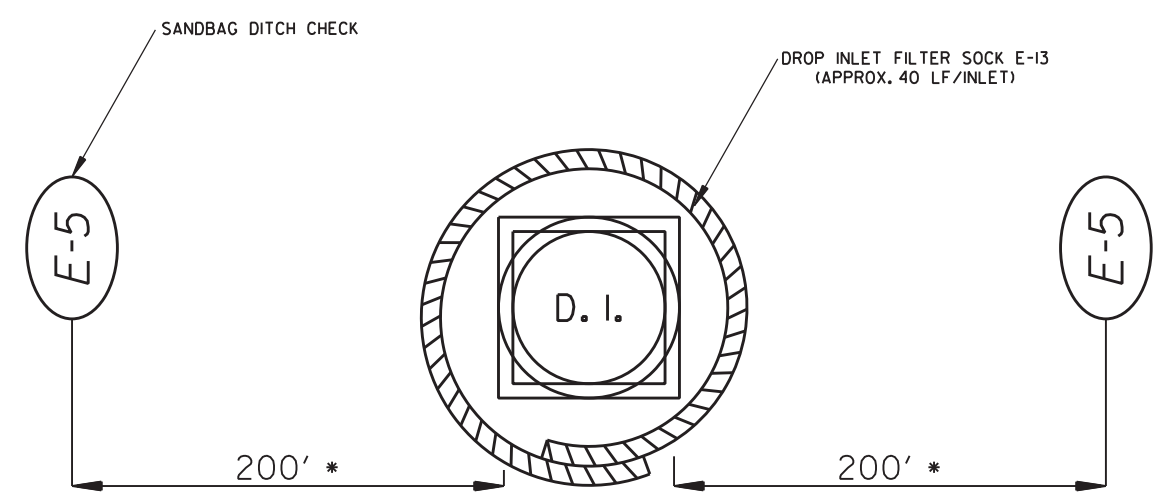
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| | | 6 | ARK. | 012479 | 6 | 23 |
| SPECIAL DETAILS | | | | | | |



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DETAIL OF EXISTING MEDIAN CROSSING

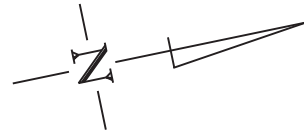


*200' SHOWN FOR INFORMATIONAL PURPOSES ONLY.
CONTRACTOR SHALL ADJUST DISTANCE AS DIRECTED
BY THE ENGINEER.

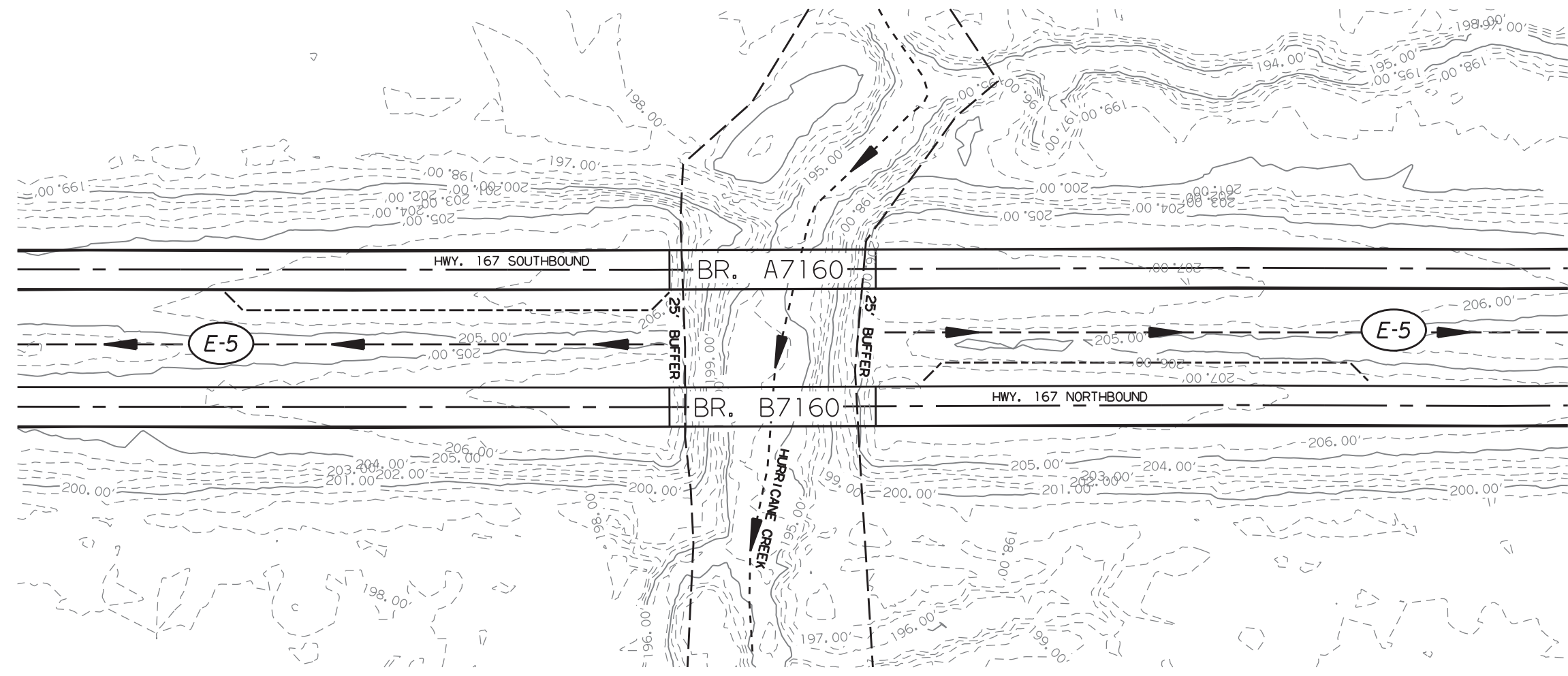
TEMPORARY EROSION CONTROL DETAIL
AT MEDIAN INLET

DATE & TIME: 5/24/2024 8:51:28 AM
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| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
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| | | 6 | ARK. | 012479 | 7 | 23 |
| SPECIAL DETAILS | | | | | | |



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Date: 2024.05.24



L.M. 3.557 HWY. 167, SEC. 7

| | |
|----------------------|--|
| SAND BAG DITCH CHECK | |
| FILTER SOCK | |

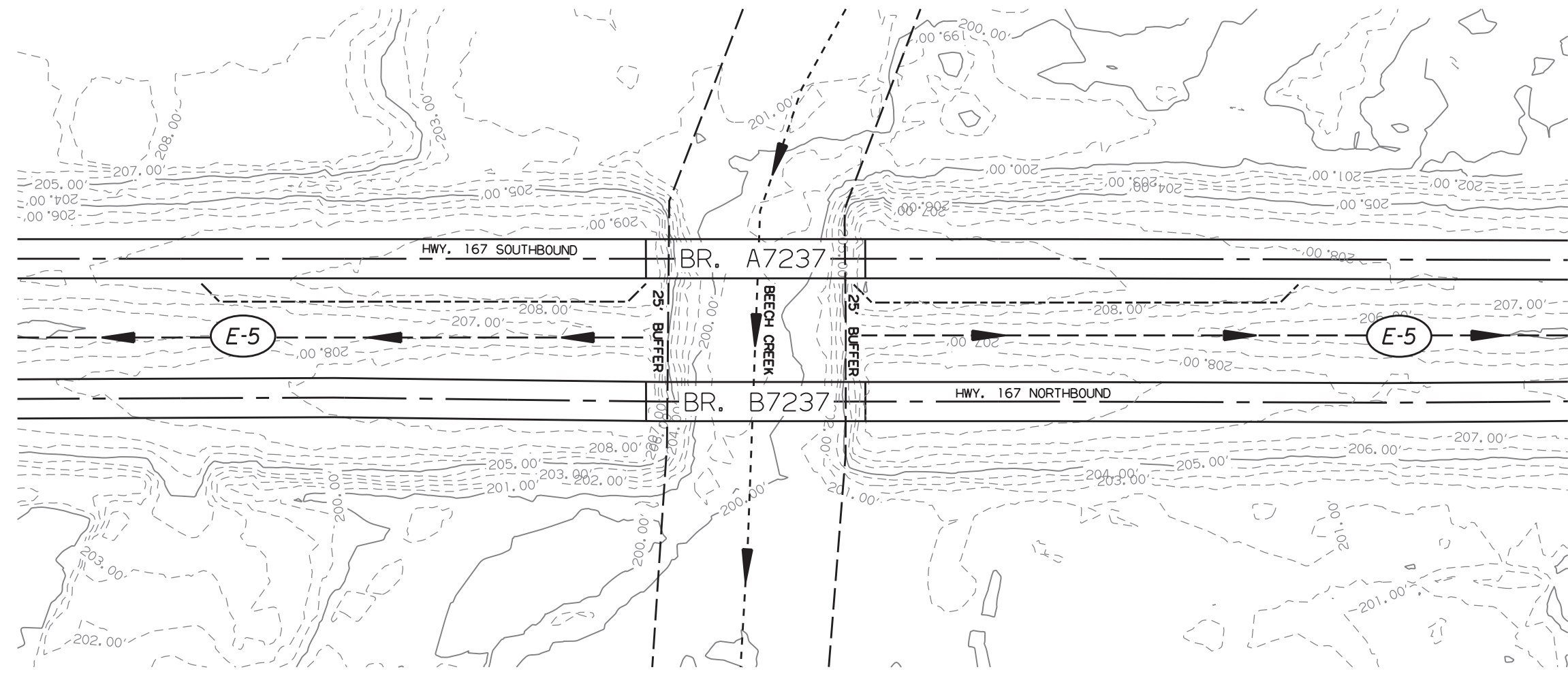
SITE 1 - DALLAS COUNTY
SPECIAL DETAILS

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| | | 6 | ARK. | 012479 | 8 | 23 |
| SPECIAL DETAILS | | | | | | |



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Date: 2024.05.24



L.M. 6.314 HWY. 167, SEC. 7

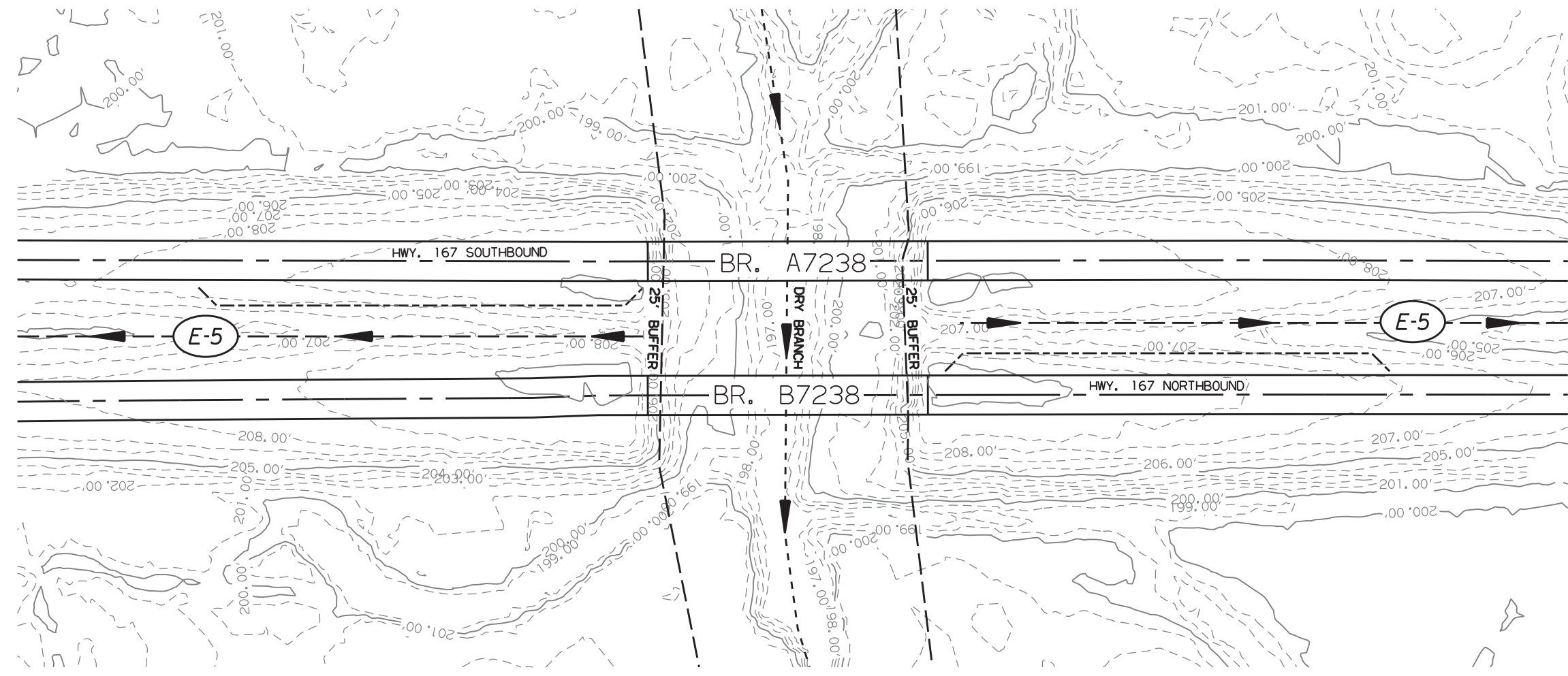
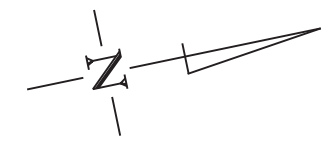
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|----------------------|--|
| SAND BAG DITCH CHECK | |
| FILTER SOCK | |

SITE 1 - DALLAS COUNTY
SPECIAL DETAILS

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
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| | | 6 | ARK. | 012479 | 9 | 23 |
| SPECIAL DETAILS | | | | | | |



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Date: 2024.05.24

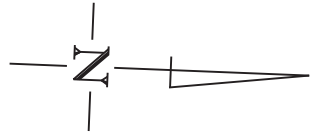


L.M. 6.488 HWY. 167, SEC. 7

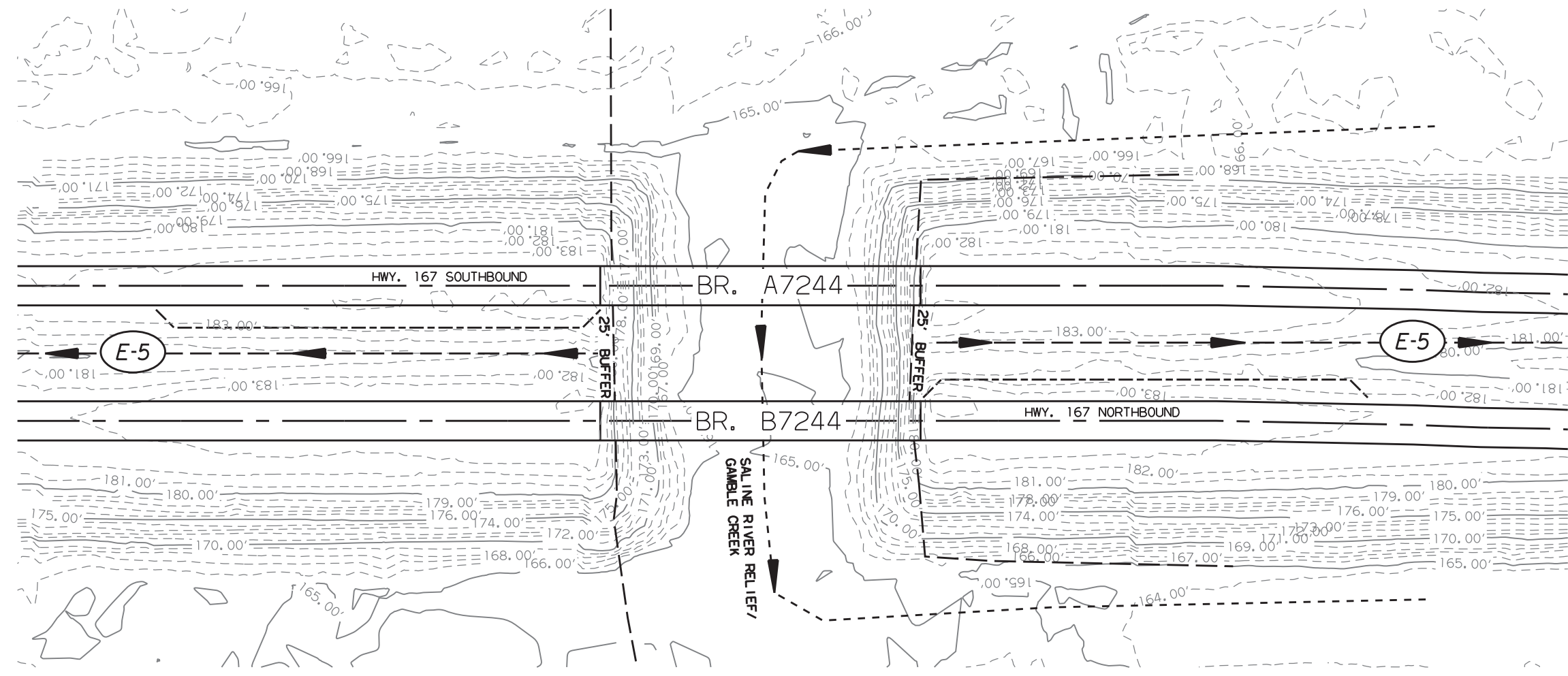
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|----------------------|--|
| SAND BAG DITCH CHECK | |
| FILTER SOCK | |

SITE 1 - DALLAS COUNTY
SPECIAL DETAILS

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
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| | | 6 | ARK. | 012479 | 10 | 23 |
| SPECIAL DETAILS | | | | | | |



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L.M. 2.625 HWY. 167, SEC. 9

| | |
|----------------------|--|
| SAND BAG DITCH CHECK | |
| FILTER SOCK | |

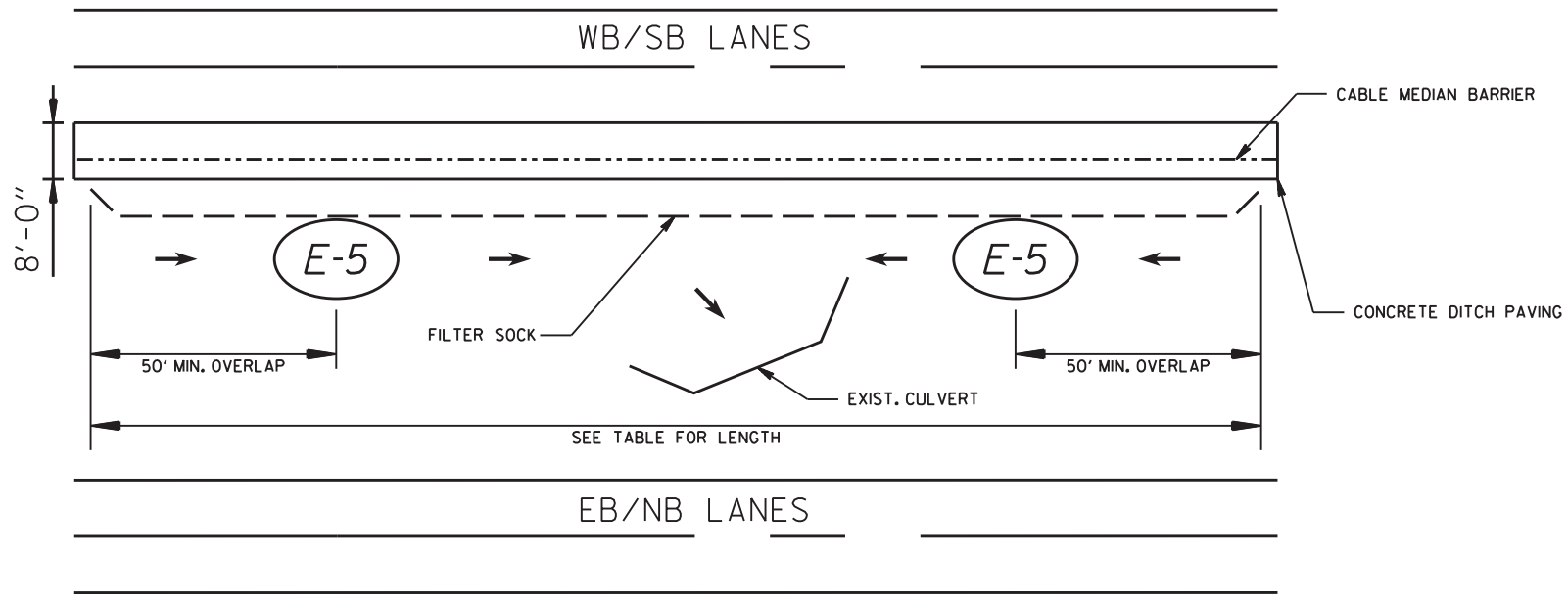
SITE 2 - DALLAS COUNTY
SPECIAL DETAILS

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Date: 2024.05.24

| FILTER SOCK LOCATIONS E-3 | | | | | | |
|---------------------------|--------------|-------------------|------|-----------|-----------------------|--------|
| BEGIN LOG MILE | END LOG MILE | ROUTE AND SECTION | SITE | COUNTY | DESCRIPTION | LENGTH |
| 3.510 | 3.557 | HWY. 167, SEC. 7 | 1 | DA.LAS | BRIDGE END | 250' |
| 3.578 | 3.625 | HWY. 167, SEC. 7 | 1 | DA.LAS | BRIDGE END | 250' |
| 6.267 | 6.314 | HWY. 167, SEC. 7 | 1 | DA.LAS | BRIDGE END | 250' |
| 6.371 | 6.418 | HWY. 167, SEC. 7 | 1 | DA.LAS | BRIDGE END | 250' |
| 6.441 | 6.488 | HWY. 167, SEC. 7 | 1 | DA.LAS | BRIDGE END | 250' |
| 6.519 | 6.567 | HWY. 167, SEC. 7 | 1 | DA.LAS | BRIDGE END | 250' |
| 0.615 | 0.710 | HWY. 167, SEC. 8 | 2 | CLEVELAND | CONCRETE DITCH PAVING | 500' |
| 1.604 | 1.723 | HWY. 167, SEC. 8 | 2 | CLEVELAND | CONCRETE DITCH PAVING | 630' |
| 1.868 | 1.926 | HWY. 167, SEC. 8 | 2 | CLEVELAND | CONCRETE DITCH PAVING | 305' |
| 3.591 | 3.655 | HWY. 167, SEC. 8 | 2 | CLEVELAND | CONCRETE DITCH PAVING | 340' |
| 2.578 | 2.625 | HWY. 167, SEC. 9 | 2 | DA.LAS | BRIDGE END | 250' |
| 2.665 | 2.712 | HWY. 167, SEC. 9 | 2 | DA.LAS | BRIDGE END | 250' |
| 4.035 | 4.082 | HWY. 100, SEC. 0 | 5 | PULASKI | EXIST. CULVERT | 250' |
| 4.100 | 4.147 | HWY. 100, SEC. 0 | 5 | PULASKI | EXIST. CULVERT | 250' |
| 4.610 | 4.657 | HWY. 100, SEC. 0 | 5 | PULASKI | EXIST. CULVERT | 250' |
| 4.732 | 4.780 | HWY. 100, SEC. 0 | 5 | PULASKI | EXIST. CULVERT | 250' |
| 5.237 | 5.284 | HWY. 100, SEC. 0 | 5 | PULASKI | EXIST. CULVERT | 250' |
| 5.303 | 5.350 | HWY. 100, SEC. 0 | 5 | PULASKI | EXIST. CULVERT | 250' |
| 5.450 | 5.497 | HWY. 100, SEC. 0 | 5 | PULASKI | EXIST. CULVERT | 250' |
| 5.515 | 5.562 | HWY. 100, SEC. 0 | 5 | PULASKI | EXIST. CULVERT | 250' |
| 5.790 | 5.837 | HWY. 100, SEC. 0 | 5 | PULASKI | EXIST. CULVERT | 250' |



FILTER SOCK DETAIL
N.T.S.

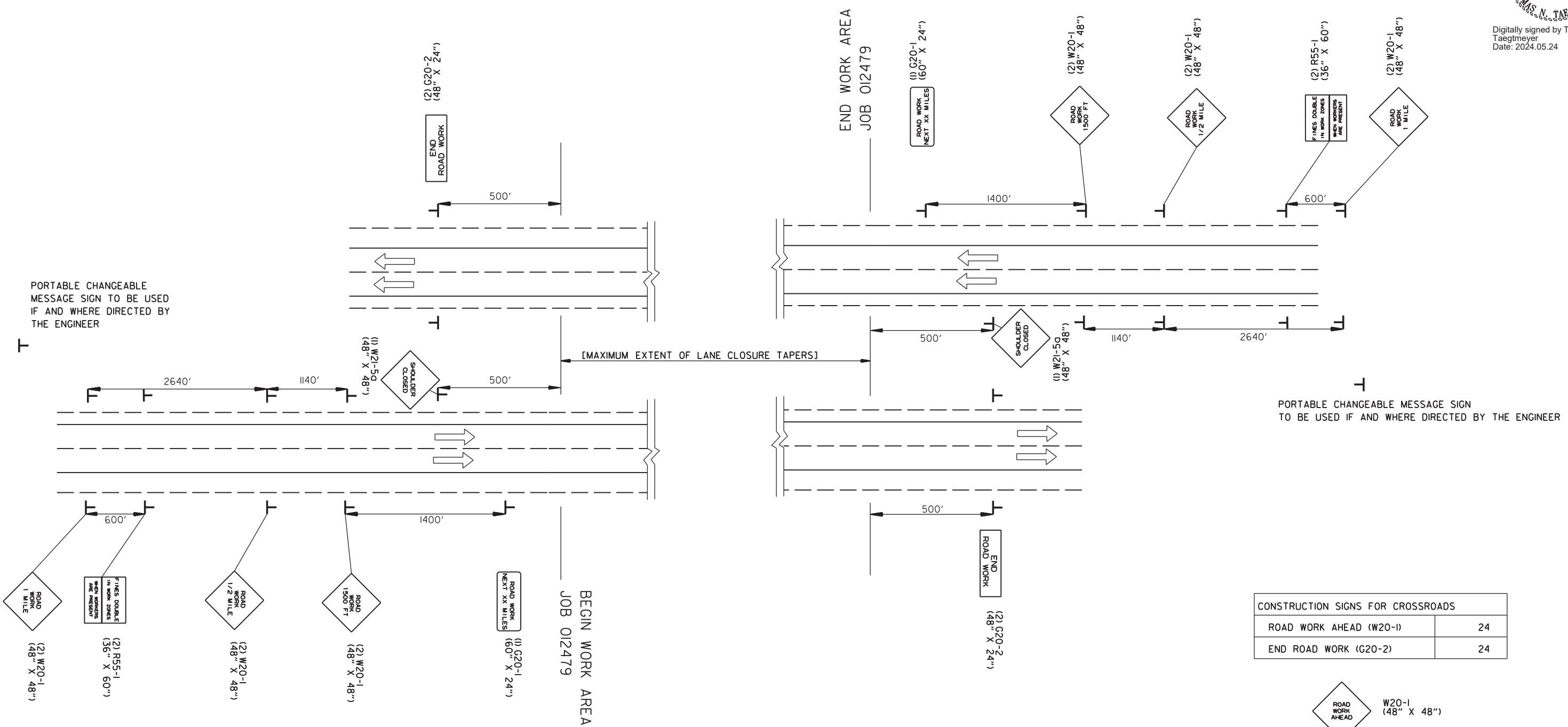


| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
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| | | 6 | ARK. | 012479 | 12 | 23 |
| MAINTENANCE OF TRAFFIC DETAILS | | | | | | |



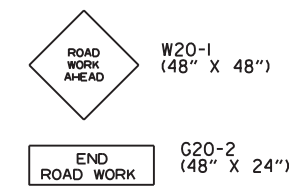
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Date: 2024.05.24

NOTE : W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS AS WORKING AREA SHIFTS.



NOTE : W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS AS WORKING AREA SHIFTS.

| CONSTRUCTION SIGNS FOR CROSSROADS | |
|-----------------------------------|----|
| ROAD WORK AHEAD (W20-1) | 24 |
| END ROAD WORK (G20-2) | 24 |



ADVANCE SIGNS AT BEGINNING AND END OF JOB ALL STAGES

ADVANCE SIGNS AT SITE ENDS MAINTENANCE OF TRAFFIC DETAILS

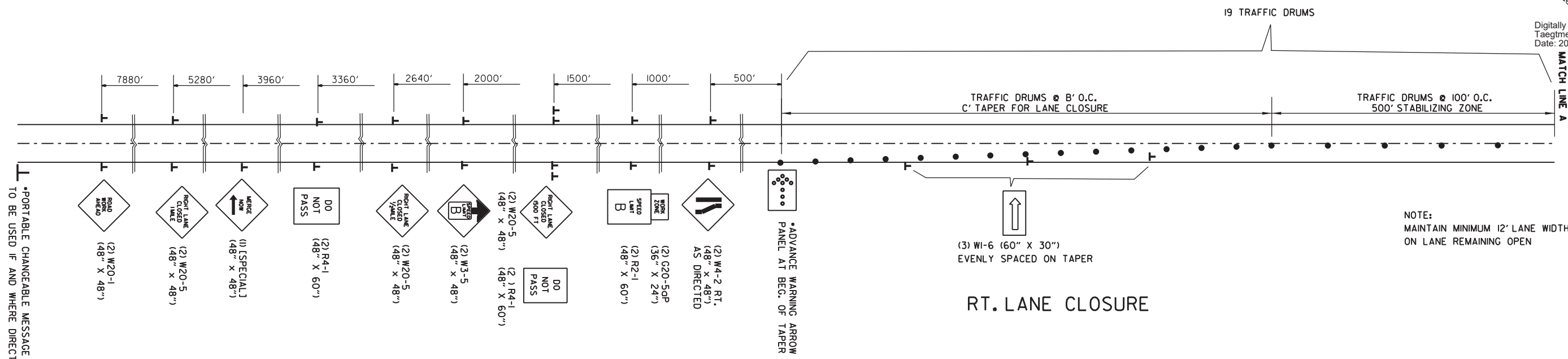
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| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
| | | 6 | ARK. | 012479 | 13 | 23 |
| MAINTENANCE OF TRAFFIC DETAILS | | | | | | |

TABLE OF VARIABLES

| DESIGN SPEED "A" | B | C | D | E |
|------------------|----|-----|------|-----|
| 60 | 50 | 720 | 1100 | 600 |
| 65 | 55 | 780 | 1210 | 660 |
| 70 | 60 | 840 | 1320 | 720 |
| 75 | 65 | 900 | 1430 | 780 |



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*PORTABLE CHANGEABLE MESSAGE SIGN TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER

NOTE:
MAINTAIN MINIMUM 12' LANE WIDTH ON LANE REMAINING OPEN

RT. LANE CLOSURE

*TO BE PLACED AT MINIMUM OFFSET OF 12' FROM EDGE OF THROUGH LANE OF TRAFFIC, OR FURTHER IF PRACTICAL.

SPEED LIMIT SIGNS ARE ALSO PROVIDED FOR PLACEMENT PAST ENTRANCE RAMP WITHIN THE WORK ZONE.

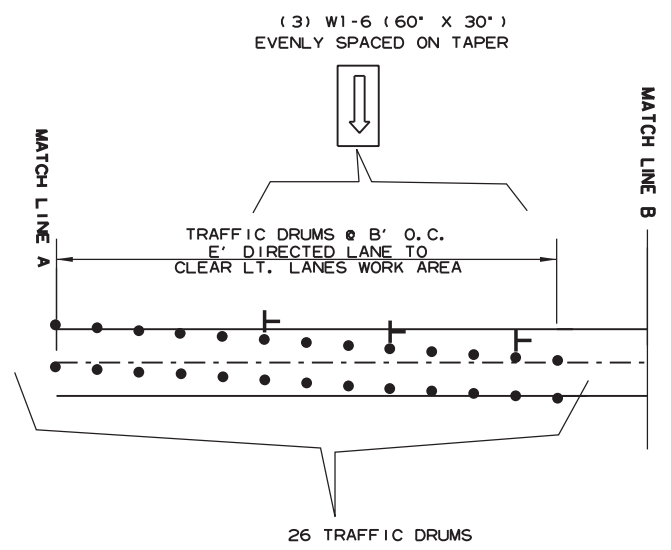
**SPEED LIMIT SIGN SHALL MATCH PERMANENT SPEED LIMIT.



NOTE:
SPEED LIMIT 50 MPH TRAVEL SPEED
HWY. 100, SEC. 0 LOG MILE 3.816 TO LOG MILE 6.550

SPEED LIMIT 60 MPH TRAVEL SPEED
HWY. 167, SEC. 10 LOG MILE 9.310 TO LOG MILE 14.348
HWY. 167, SEC. 11 LOG MILE 0.000 TO LOG MILE 3.000

ALL OTHER LOCATIONS SPEED LIMIT 65 MPH TRAVEL SPEED.

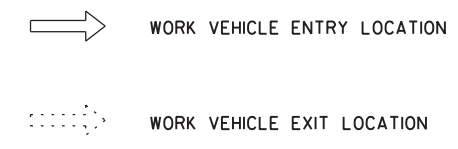
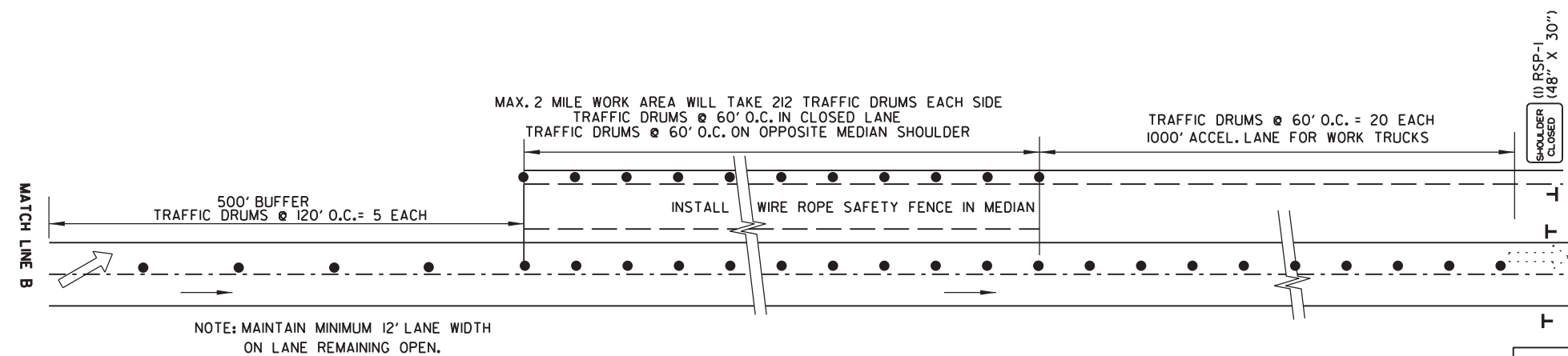


DIVERSION FOR LT. LANE WORK ZONE

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
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| MAINTENANCE OF TRAFFIC DETAILS | | | | | | |



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NOTE: REFER TO SP-MAINTENANCE OF TRAFFIC FOR LANE CLOSURE LIMITATIONS AND RESTRICTIONS. QUANTITY OF TRAFFIC DRUMS PROVIDED IN THE CONTRACT IS THE MAXIMUM NUMBER REQUIRED FOR ONE LANE CLOSURE.

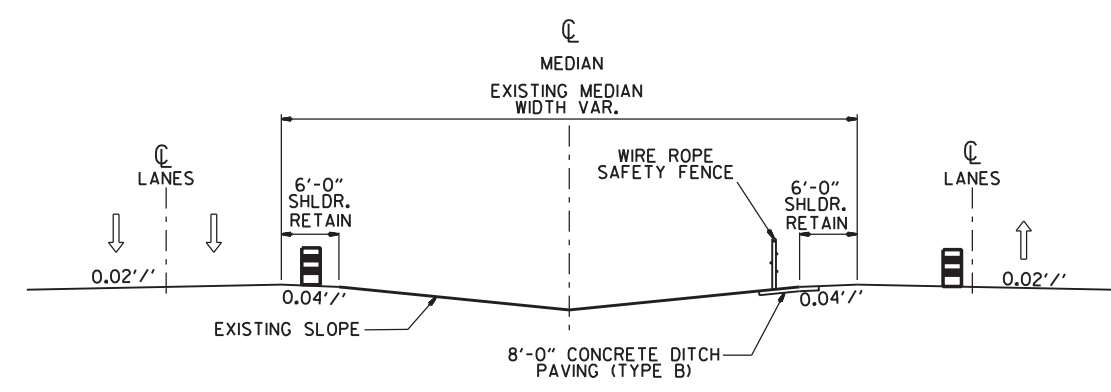
*SPEED LIMIT SIGNS SHALL MATCH PERMANENT SPEED LIMIT.



NOTE:
 SPEED LIMIT 50 MPH TRAVEL SPEED
 HWY. 100, SEC. 0 LOG MILE 3.816 TO LOG MILE 6.550

SPEED LIMIT 60 MPH TRAVEL SPEED
 HWY. 167, SEC. 10 LOG MILE 9.310 TO LOG MILE 14.348
 HWY. 167, SEC. 11 LOG MILE 0.000 TO LOG MILE 3.000

ALL OTHER LOCATIONS SPEED LIMIT 65 MPH TRAVEL SPEED.



MOVABLE WORK ZONE FOR WRSF INSTALLATION

NOTE: CONTRACTOR MUST UTILIZE ENTRY/EXIT LOCATION AS SHOWN ON THE PLANS.

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
|--------------|--------------|--------------------|-------|---------|-----------|--------------|
| | | 6 | ARK. | 012479 | 15 | 23 |
| QUANTITIES | | | | | | |



Digitally signed by Thomas N. Taegtmeier
Date: 2024.06.24

ADVANCE WARNING SIGNS AND DEVICES

| SIGN NUMBER | DESCRIPTION | SIGN SIZE | ENTIRE PROJECT | MAXIMUM NUMBER REQUIRED | TOTAL SIGNS REQUIRED | | TRAFFIC DRUMS | * ADVANCE WARNING ARROW PANEL | * PORTABLE CHANGEABLE MESSAGE SIGN |
|----------------|--|-----------|----------------|-------------------------|----------------------|---------------|---------------|-------------------------------|------------------------------------|
| | | | EACH | | NO. | SQ. FT. | | EACH | DAY |
| W20-1 | ROAD WORK 1 MILE | 48"x48" | 4 | 4 | 4 | 64.0 | | | |
| W20-1 | ROAD WORK 1/2 MLE | 48"x48" | 4 | 4 | 4 | 64.0 | | | |
| W20-1 | ROAD WORK 1500 FT. | 48"x48" | 4 | 4 | 4 | 64.0 | | | |
| * W20-1 | ROAD WORK AHEAD | 48"x48" | 26 | 26 | 26 | 416.0 | | | |
| * G20-2 | END ROAD WORK | 48"x24" | 28 | 28 | 28 | 224.0 | | | |
| G20-1 | ROAD WORK NEXT xx MILES | 60"x24" | 2 | 2 | 2 | 20.0 | | | |
| G20-5aP | WORK ZONE | 36"x24" | 2 | 2 | 2 | 12.0 | | | |
| W1-6 | LARGE ARROW | 60"x30" | 6 | 6 | 6 | 75.0 | | | |
| W3-5 | REDUCED SPEED LIMIT AHEAD | 48"x48" | 2 | 2 | 2 | 32.0 | | | |
| W4-2 | RIGHT LANE ENDS | 48"x48" | 2 | 2 | 2 | 32.0 | | | |
| * R2-1 | SPEED LIMIT | 48"x60" | 14 | 14 | 14 | 280.0 | | | |
| R4-1 | DO NOT PASS | 48"x60" | 4 | 4 | 4 | 80.0 | | | |
| R55-1 | FINE DOUBLE IN WORK ZONES WHEN WORKERS ARE PRESENT | 36"x60" | 4 | 4 | 4 | 60.0 | | | |
| W20-5 | RIGHT LANE CLOSED 1 MILE | 48"x48" | 2 | 2 | 2 | 32.0 | | | |
| W20-5 | RIGHT LANE CLOSED 1/2 MILE | 48"x48" | 2 | 2 | 2 | 32.0 | | | |
| W20-5 | RIGHT LANE CLOSED 1500 FT. | 48"x48" | 2 | 2 | 2 | 32.0 | | | |
| W21-5a | SHOULDER CLOSED | 48"x48" | 2 | 2 | 2 | 32.0 | | | |
| RSP-1 | SHOULDER CLOSED | 48"x30" | 1 | 1 | 1 | 10.0 | | | |
| SPECIAL | MERGE NOW ARROW | 48"x48" | 2 | 2 | 2 | 32.0 | | | |
| | TRAFFIC DRUMS | | 494 | 494 | | | 494 | | |
| | ADVANCE WARNING ARROW PANEL | | 1 | 1 | | | 290 | | |
| | PORTABLE CHANGEABLE MESSAGE SIGN | | 2 | 2 | | | | 120 | |
| TOTALS: | | | | | | 1593.0 | 494 | 290 | 120 |

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

QUANTITIES ABOVE ARE LISTED FOR 1 SITE USE ONLY. QUANTITIES TO BE RE-USED PER EACH SITE, AS NECESSARY.

* QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.
TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

| | | | | | | |
|--------------|--------------|--------------------|-------|---------|-----------|--------------|
| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
| | | 6 | ARK. | 012479 | 16 | 23 |
| QUANTITIES | | | | | | |



Digitally signed by Thomas N. Taegtmeier
Date: 2024.06.24

EROSION CONTROL (BOX 1 OF 2)

| LOG MILE | LOG MILE | SITE NUMBER | COUNTY | LOCATION | PERMANENT EROSION CONTROL | | | | | TEMPORARY EROSION CONTROL | | | | | | | | |
|--|----------|-------------|------------------------------|------------------------------|---------------------------|-------------|-------------|--------------|----------------------------|---------------------------|--------------|--------------|-------------------|-----------------------|------------------------|------------------------------|--|--|
| | | | | | SEEDING | LIME | MULCH COVER | WATER | SECOND SEEDING APPLICATION | TEMPORARY SEEDING | MULCH COVER | WATER | 18" FILTER SOCK | SAND BAG DITCH CHECKS | DROP INLET FILTER SOCK | *SEDIMENT REMOVAL & DISPOSAL | | |
| | | | | | ACRE | TON | ACRE | M.GAL. | ACRE | ACRE | ACRE | M.GAL. | (E-3) LIN. FT. | (E-5) BAG | (E-13) LIN. FT. | CU. YD. | | |
| 1.970 | 2.445 | SITE 1 | DALLAS | LT. OF C.L. HWY. 167, SEC. 7 | | | | | | 0.46 | 0.46 | 9.4 | | | | | | |
| 2.455 | 3.027 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.55 | 0.55 | 11.2 | | | | | |
| 3.040 | 3.459 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.41 | 0.41 | 8.4 | | | | | |
| 3.469 | 3.557 | SITE 1 | | RT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.09 | 0.09 | 1.8 | 250 | | | | |
| 3.578 | 3.892 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.30 | 0.30 | 6.1 | 250 | | | | |
| 3.905 | 4.233 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.32 | 0.32 | 6.5 | | | | | |
| 4.267 | 4.774 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.49 | 0.49 | 10.0 | | | | | |
| 4.785 | 5.257 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.46 | 0.46 | 9.4 | | | | | |
| 5.268 | 5.684 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.40 | 0.40 | 8.2 | | | | | |
| 5.694 | 6.024 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.32 | 0.32 | 6.5 | | | | | |
| 6.036 | 6.314 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.27 | 0.27 | 5.5 | 250 | | | | |
| 6.371 | 6.488 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.11 | 0.11 | 2.2 | 500 | | | | |
| 6.519 | 6.743 | SITE 1 | | RT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.22 | 0.22 | 4.5 | 250 | | | | |
| 6.754 | 6.940 | SITE 1 | | RT. OF C.L. HWY. 167, SEC. 7 | | | | | | | 0.18 | 0.18 | 3.7 | | | | | |
| *ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER | | | | | 1.00 | 2.00 | 1.00 | 102.0 | 1.00 | 0.22 | 0.22 | 4.5 | | 1100 | 1000 | 62 | | |
| COUNTY SUBTOTALS: | | | | | 1.00 | 2.00 | 1.00 | 102.0 | 1.00 | 4.80 | 4.80 | 97.9 | 1500 | 1100 | 1000 | 62 | | |
| 0.420 | 0.615 | SITE 2 | CLEVELAND | LT. OF C.L. HWY. 167, SEC. 8 | | | | | | 0.19 | 0.19 | 3.9 | | | | | | |
| 0.605 | 0.755 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.15 | 0.15 | 3.1 | 500 | | | | |
| 0.765 | 1.256 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.48 | 0.48 | 9.8 | | | | | |
| 1.267 | 1.786 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.50 | 0.50 | 10.2 | 630 | | | | |
| 1.797 | 2.317 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.50 | 0.50 | 10.2 | 305 | | | | |
| 2.328 | 2.799 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.46 | 0.46 | 9.4 | | | | | |
| 2.810 | 3.143 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.32 | 0.32 | 6.5 | | | | | |
| 3.152 | 3.524 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.36 | 0.36 | 7.3 | | | | | |
| 3.534 | 3.947 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.40 | 0.40 | 8.2 | 340 | | | | |
| 3.956 | 4.325 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.36 | 0.36 | 7.3 | | | | | |
| 4.335 | 4.824 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.47 | 0.47 | 9.6 | | | | | |
| 4.834 | 5.334 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.48 | 0.48 | 9.8 | | | | | |
| 5.343 | 5.802 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.45 | 0.45 | 9.2 | | | | | |
| 5.811 | 6.294 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.47 | 0.47 | 9.6 | | | | | |
| 6.303 | 6.678 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.36 | 0.36 | 7.3 | | | | | |
| 6.688 | 7.165 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.46 | 0.46 | 9.4 | | | | | |
| 7.175 | 7.657 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.47 | 0.47 | 9.6 | | | | | |
| 7.667 | 8.107 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.43 | 0.43 | 8.8 | | | | | |
| 8.117 | 8.604 | SITE 2 | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.47 | 0.47 | 9.6 | | | | | | |
| 8.614 | 8.730 | SITE 2 | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.11 | 0.11 | 2.2 | | | | | | |
| | 0.260 | SITE 2 | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.25 | 0.25 | 5.1 | | | | | | |
| 0.270 | 0.695 | SITE 2 | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.41 | 0.41 | 8.4 | | | | | | |
| 0.705 | 1.092 | SITE 2 | LT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.38 | 0.38 | 7.8 | | | | | | |
| 1.103 | 1.464 | SITE 2 | LT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.35 | 0.35 | 7.1 | | | | | | |
| 1.474 | 1.907 | SITE 2 | LT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.42 | 0.42 | 8.6 | | | | | | |
| 1.917 | 2.112 | SITE 2 | LT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.19 | 0.19 | 3.9 | | | | | | |
| 2.122 | 2.407 | SITE 2 | LT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.28 | 0.28 | 5.7 | | | | | | |
| 2.420 | 2.625 | SITE 2 | LT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.20 | 0.20 | 4.1 | 250 | | | | | |
| 2.665 | 2.950 | SITE 2 | RT. OF C.L. HWY. 167, SEC. 8 | | | | | | | 0.28 | 0.28 | 5.7 | 250 | | | | | |
| *ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER | | | | | 1.00 | 2.00 | 1.00 | 102.0 | 1.00 | 0.52 | 0.52 | 10.6 | | 2860 | 2600 | 161 | | |
| COUNTY SUBTOTALS: | | | | | 1.00 | 2.00 | 1.00 | 102.0 | 1.00 | 11.17 | 11.17 | 228.0 | 2275 | 2860 | 2600 | 161 | | |
| SUBTOTALS (BOX 1 OF 2): | | | | | 2.00 | 4.00 | 2.00 | 204.0 | 2.00 | 15.97 | 15.97 | 325.9 | 3775 | 3960 | 3600 | 223 | | |

BASIS OF ESTIMATE:

- LIME 2 TONS / ACRE OF SEEDING
- WATER..... 102.0 M.G. / ACRE OF SEEDING
- WATER..... 20.4 M.G. / ACRE OF TEMPORARY SEEDING
- WATER..... 12.6 GAL. / SQ. YD. OF SOLID SODDING
- SAND BAG DITCH CHECKS..... 22 BAGS / LOCATION

NOTE: THE TEMPORARY EROSION CONTROL DEVICES SHOWN ABOVE AND ON THE PLANS SHALL BE INSTALLED IN SUCH A SEQUENCE AS TO DETER EROSION AND SEDIMENTATION ON U.S. WATERWAYS AS EXPLAINED BY THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMIT.

*QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

| | | | | | | |
|--------------|--------------|--------------------|-------|---------|-----------|--------------|
| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
| | | 6 | ARK. | 012479 | 17 | 23 |
| QUANTITIES | | | | | | |



Digitally signed by Thomas N. Taegtmeyer
Date: 2024.06.24

EROSION CONTROL (BOX 2 OF 2)

| LOG MILE | LOG MILE | SITE NUMBER | COUNTY | LOCATION | PERMANENT EROSION CONTROL | | | | | TEMPORARY EROSION CONTROL | | | | | | |
|--|----------|-------------|------------------------------|-------------------------------|---------------------------|--------------|-------------|--------------|----------------------------|---------------------------|--------------|--------------|-------------------|-----------------------|------------------------------|------------------------------|
| | | | | | SEEDING | LIME | MULCH COVER | WATER | SECOND SEEDING APPLICATION | TEMPORARY SEEDING | MULCH COVER | WATER | 18" FILTER SOCK | SAND BAG DITCH CHECKS | DROP INLET FILTER SOCK (18") | *SEDIMENT REMOVAL & DISPOSAL |
| | | | | | ACRE | TON | ACRE | M.GAL. | ACRE | ACRE | ACRE | M.GAL. | (E-3) LIN. FT. | (E-5) BAG | (E-13) LIN. FT. | CU. YD. |
| 9.361 | 9.828 | SITE 3 | GRANT | LT. OF C.L. HWY. 167, SEC. 10 | | | | | | 0.45 | 0.45 | 9.2 | | | | |
| 9.838 | 10.360 | SITE 3 | | LT. OF C.L. HWY. 167, SEC. 10 | | | | | | 0.51 | 0.51 | 10.4 | | | | |
| 10.370 | 10.889 | SITE 3 | | LT. OF C.L. HWY. 167, SEC. 10 | | | | | | 0.50 | 0.50 | 10.2 | | | | |
| 10.906 | 11.433 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | | | | | | 0.51 | 0.51 | 10.4 | | | | |
| 11.445 | 11.999 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | | | | | | 0.54 | 0.54 | 11.0 | | | | |
| 12.009 | 12.554 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | | | | | | 0.53 | 0.53 | 10.8 | | | | |
| 12.589 | 13.174 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | | | | | | 0.57 | 0.57 | 11.6 | | | | |
| 13.184 | 13.779 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | | | | | | 0.58 | 0.58 | 11.8 | | | | |
| 13.793 | 14.348 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | | | | | | 0.54 | 0.54 | 11.0 | | | | |
| 0.061 | 0.593 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 11 | | | | | | 0.52 | 0.52 | 10.6 | | | | |
| 0.607 | 1.202 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 11 | | | | | | 0.58 | 0.58 | 11.8 | | | | |
| 1.297 | 2.014 | SITE 3 | | LT. OF C.L. HWY. 167, SEC. 11 | | | | | | 0.70 | 0.70 | 14.3 | | | | |
| 2.026 | 2.892 | SITE 3 | | LT. OF C.L. HWY. 167, SEC. 11 | | | | | | 0.84 | 0.84 | 17.1 | | | | |
| *ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER | | | | | 1.00 | 2.00 | 1.00 | 102.0 | 1.00 | 0.37 | 0.37 | 7.5 | | 1760 | 1600 | 99 |
| COUNTY SUBTOTALS: | | | | | 1.00 | 2.00 | 1.00 | 102.0 | 1.00 | 7.74 | 7.74 | 157.7 | | 1760 | 1600 | 99 |
| 2.476 | 2.992 | SITE 4 | SALINE | LT. OF C.L. HWY. 167, SEC. 12 | | | | | | 0.50 | 0.50 | 10.2 | | | | |
| 3.006 | 3.493 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | | | | | | 0.47 | 0.47 | 9.6 | | | | |
| 3.509 | 4.070 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | | | | | | 0.54 | 0.54 | 11.0 | | | | |
| 4.082 | 4.464 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | | | | | | 0.37 | 0.37 | 7.5 | | | | |
| 4.475 | 4.863 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | | | | | | 0.36 | 0.36 | 7.8 | | | | |
| 4.881 | 5.248 | SITE 4 | | RT. OF C.L. HWY. 167, SEC. 12 | | | | | | 0.36 | 0.36 | 7.3 | | | | |
| 5.259 | 5.952 | SITE 4 | | RT. OF C.L. HWY. 167, SEC. 12 | | | | | | 0.67 | 0.67 | 13.7 | | | | |
| 5.964 | 6.554 | SITE 4 | | RT. OF C.L. HWY. 167, SEC. 12 | | | | | | 0.57 | 0.57 | 11.6 | | | | |
| 6.575 | 6.893 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | | | | | | 0.31 | 0.31 | 6.3 | | | | |
| 6.904 | 7.007 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | | | | | | 0.10 | 0.10 | 2.0 | | | | |
| *ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER | | | | | 1.00 | 2.00 | 1.00 | 102.0 | 1.00 | 0.21 | 0.21 | 4.3 | | 1408 | 1280 | 79 |
| COUNTY SUBTOTALS: | | | | | 1.00 | 2.00 | 1.00 | 102.0 | 1.00 | 4.46 | 4.48 | 91.3 | | 1408 | 1280 | 79 |
| 3.860 | 3.993 | SITE 5 | PULASKI | LT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.13 | 0.13 | 2.7 | 250 | | | |
| 3.983 | 4.085 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.10 | 0.10 | 2.0 | 250 | | | |
| 4.098 | 4.205 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.10 | 0.10 | 2.0 | 250 | | | |
| 4.195 | 4.397 | SITE 5 | | LT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.20 | 0.20 | 4.1 | 250 | | | |
| 4.418 | 4.693 | SITE 5 | | LT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.27 | 0.27 | 5.5 | 250 | | | |
| 4.707 | 4.967 | SITE 5 | | LT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.25 | 0.25 | 5.1 | 250 | | | |
| 4.990 | 5.288 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.29 | 0.29 | 5.9 | 250 | | | |
| 5.302 | 5.501 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.19 | 0.19 | 3.9 | 500 | | | |
| 5.513 | 5.754 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.23 | 0.23 | 4.7 | 250 | | | |
| 5.770 | 6.016 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.24 | 0.24 | 4.9 | 250 | | | |
| 6.029 | 6.256 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.22 | 0.22 | 4.5 | | | | |
| 6.270 | 6.550 | SITE 5 | LT. OF C.L. HWY. 100, SEC. 0 | | | | | | 0.27 | 0.27 | 5.5 | | | | | |
| *ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER | | | | | 1.00 | 2.00 | 1.00 | 102.0 | 1.00 | 0.12 | 0.12 | 2.4 | | 638 | 400 | 25 |
| COUNTY SUBTOTALS: | | | | | 1.00 | 2.00 | 1.00 | 102.0 | 1.00 | 2.61 | 2.61 | 53.2 | 2750 | 638 | 400 | 25 |
| SUBTOTALS (BOX 2 OF 2): | | | | | 3.00 | 6.00 | 3.00 | 306.0 | 3.00 | 14.83 | 14.83 | 302.2 | 2750 | 3806 | 3280 | 203 |
| SUBTOTALS (BOX 1 OF 2): | | | | | 2.00 | 4.00 | 2.00 | 204.0 | 2.00 | 15.97 | 15.97 | 325.9 | 3775 | 3960 | 3600 | 223 |
| TOTALS: | | | | | 5.00 | 10.00 | 5.00 | 510.0 | 5.00 | 30.80 | 30.80 | 628.1 | 6525 | 7766 | 6880 | 426 |

BASIS OF ESTIMATE:
LIME2 TONS / ACRE OF SEEDING
WATER.....102.0 M.G. / ACRE OF SEEDING
WATER.....20.4 M.G. / ACRE OF TEMPORARY SEEDING
WATER.....12.6 GAL. / SQ. YD. OF SOLID SODDING
SAND BAG DITCH CHECKS.....22 BAGS / LOCATION

NOTE: THE TEMPORARY EROSION CONTROL DEVICES SHOWN ABOVE AND ON THE PLANS SHALL BE INSTALLED IN SUCH A SEQUENCE AS TO DETER EROSION AND SEDIMENTATION ON U.S. WATERWAYS AS EXPLAINED BY THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMIT.

*QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
|--------------|--------------|--------------------|-------|---------|-----------|--------------|
| | | 6 | ARK. | 012479 | 18 | 23 |
| QUANTITIES | | | | | | |

WIRE ROPE SAFETY FENCE (BOX 1 OF 2)

| LOG MILE | LOG MILE | SITE NUMBER | COUNTY | LOCATION | WIRE ROPE SAFETY FENCE | *WRSF ANCHOR | WRSF MAINTENANCE MATERIALS | **WRSF (POST REPAIR) |
|-------------------------------|----------|-------------|------------------------------|-------------------------------|------------------------|--------------|----------------------------|----------------------|
| | | | | | LIN. FT | EACH | LUMP SUM | EACH |
| 1.970 | 2.445 | SITE 1 | DALLAS | LT. OF C.L. HWY. 167, SEC. 7 | 2508 | 2 | | |
| 2.455 | 3.027 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | 3020 | 2 | | |
| 3.040 | 3.459 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | 2212 | 2 | | |
| 3.409 | 3.557 | SITE 1 | | RT. OF C.L. HWY. 167, SEC. 7 | 465 | 2 | | |
| 3.578 | 3.892 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | 1658 | 2 | | |
| 3.905 | 4.233 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | 1732 | 2 | | |
| 4.267 | 4.774 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | 2677 | 2 | | |
| 4.785 | 5.257 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | 2492 | 2 | | |
| 5.268 | 5.684 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | 2196 | 2 | | |
| 5.694 | 6.024 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | 1742 | 2 | | |
| 6.036 | 6.314 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | 1468 | 2 | | |
| 6.371 | 6.488 | SITE 1 | | LT. OF C.L. HWY. 167, SEC. 7 | 618 | 2 | | |
| 6.519 | 6.743 | SITE 1 | | RT. OF C.L. HWY. 167, SEC. 7 | 1183 | 2 | | |
| 6.754 | 6.940 | SITE 1 | | RT. OF C.L. HWY. 167, SEC. 7 | 982 | 2 | | |
| COUNTY SUBTOTALS: | | | | | 24953 | 28 | | |
| 0.420 | 0.615 | SITE 2 | CLEVELAND | LT. OF C.L. HWY. 167, SEC. 8 | 1030 | 2 | | |
| 0.605 | 0.755 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 792 | 2 | | |
| 0.765 | 1.256 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 2592 | 2 | | |
| 1.267 | 1.786 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 2740 | 2 | | |
| 1.797 | 2.317 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 2746 | 2 | | |
| 2.328 | 2.799 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 2487 | 2 | | |
| 2.810 | 3.143 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 1758 | 2 | | |
| 3.152 | 3.524 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 1964 | 2 | | |
| 3.534 | 3.947 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 2181 | 2 | | |
| 3.956 | 4.325 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 1948 | 2 | | |
| 4.335 | 4.824 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 2582 | 2 | | |
| 4.834 | 5.334 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 2640 | 2 | | |
| 5.343 | 5.802 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 2424 | 2 | | |
| 5.811 | 6.294 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 2550 | 2 | | |
| 6.303 | 6.678 | SITE 2 | RT. OF C.L. HWY. 167, SEC. 8 | 1980 | 2 | | | |
| 6.688 | 7.165 | SITE 2 | RT. OF C.L. HWY. 167, SEC. 8 | 2519 | 2 | | | |
| 7.175 | 7.657 | SITE 2 | RT. OF C.L. HWY. 167, SEC. 8 | 2545 | 2 | | | |
| 7.667 | 8.107 | SITE 2 | RT. OF C.L. HWY. 167, SEC. 8 | 2323 | 2 | | | |
| 8.117 | 8.604 | SITE 2 | RT. OF C.L. HWY. 167, SEC. 8 | 2571 | 2 | | | |
| 8.614 | 8.730 | SITE 2 | RT. OF C.L. HWY. 167, SEC. 8 | 612 | 2 | | | |
| 0.000 | 0.260 | SITE 2 | DALLAS | RT. OF C.L. HWY. 167, SEC. 8 | 1373 | 2 | | |
| 0.270 | 0.695 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 2244 | 2 | | |
| 0.705 | 1.092 | SITE 2 | | LT. OF C.L. HWY. 167, SEC. 8 | 2043 | 2 | | |
| 1.103 | 1.464 | SITE 2 | | LT. OF C.L. HWY. 167, SEC. 8 | 1906 | 2 | | |
| 1.474 | 1.907 | SITE 2 | | LT. OF C.L. HWY. 167, SEC. 8 | 2286 | 2 | | |
| 1.917 | 2.112 | SITE 2 | | LT. OF C.L. HWY. 167, SEC. 8 | 1030 | 2 | | |
| 2.122 | 2.407 | SITE 2 | | LT. OF C.L. HWY. 167, SEC. 8 | 1505 | 2 | | |
| 2.420 | 2.625 | SITE 2 | | LT. OF C.L. HWY. 167, SEC. 8 | 1082 | 2 | | |
| 2.605 | 2.950 | SITE 2 | | RT. OF C.L. HWY. 167, SEC. 8 | 1505 | 2 | | |
| COUNTY SUBTOTALS: | | | | | 57958 | 58 | | |
| 9.361 | 9.828 | SITE 3 | GRANT | LT. OF C.L. HWY. 167, SEC. 10 | 2466 | 2 | | |
| 9.838 | 10.360 | SITE 3 | | LT. OF C.L. HWY. 167, SEC. 10 | 2756 | 2 | | |
| 10.370 | 10.889 | SITE 3 | | LT. OF C.L. HWY. 167, SEC. 10 | 2740 | 2 | | |
| 10.906 | 11.433 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | 2783 | 2 | | |
| 11.445 | 11.999 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | 2925 | 2 | | |
| 12.009 | 12.554 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | 2878 | 2 | | |
| 12.589 | 13.174 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | 3089 | 2 | | |
| 13.184 | 13.779 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | 3142 | 2 | | |
| 13.793 | 14.348 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 10 | 2930 | 2 | | |
| 0.061 | 0.593 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 11 | 2809 | 2 | | |
| 0.607 | 1.202 | SITE 3 | | RT. OF C.L. HWY. 167, SEC. 11 | 3142 | 2 | | |
| 1.297 | 2.014 | SITE 3 | | LT. OF C.L. HWY. 167, SEC. 11 | 3786 | 2 | | |
| 2.026 | 2.892 | SITE 3 | | LT. OF C.L. HWY. 167, SEC. 11 | 4572 | 2 | | |
| COUNTY SUBTOTALS: | | | | | 40018 | 26 | | |
| SUBTOTALS (BOX 1 OF 2) | | | | | 122929 | 112 | | |

WIRE ROPE SAFETY FENCE (BOX 2 OF 2)

| LOG MILE | LOG MILE | SITE NUMBER | COUNTY | LOCATION | WIRE ROPE SAFETY FENCE | *WRSF ANCHOR | WRSF MAINTENANCE MATERIALS | **WRSF (POST REPAIR) |
|-------------------------------|----------|-------------|------------------------------|-------------------------------|------------------------|--------------|----------------------------|----------------------|
| | | | | | LIN. FT | EACH | LUMP SUM | EACH |
| 2.476 | 2.992 | SITE 4 | SALINE | LT. OF C.L. HWY. 167, SEC. 12 | 2724 | 2 | | |
| 3.006 | 3.493 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | 2571 | 2 | | |
| 3.509 | 4.070 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | 2962 | 2 | | |
| 4.082 | 4.464 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | 2017 | 2 | | |
| 4.475 | 4.863 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | 2049 | 2 | | |
| 4.881 | 5.248 | SITE 4 | | RT. OF C.L. HWY. 167, SEC. 12 | 1938 | 2 | | |
| 5.259 | 5.952 | SITE 4 | | RT. OF C.L. HWY. 167, SEC. 12 | 3659 | 2 | | |
| 5.964 | 6.554 | SITE 4 | | RT. OF C.L. HWY. 167, SEC. 12 | 3115 | 2 | | |
| 6.575 | 6.893 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | 1679 | 2 | | |
| 6.904 | 7.007 | SITE 4 | | LT. OF C.L. HWY. 167, SEC. 12 | 544 | 2 | | |
| COUNTY SUBTOTALS: | | | | | 23258 | 20 | | |
| 3.860 | 3.993 | SITE 5 | PULASKI | LT. OF C.L. HWY. 100, SEC. 0 | 702 | 2 | | |
| 3.983 | 4.085 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | 539 | 2 | | |
| 4.098 | 4.205 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | 565 | 2 | | |
| 4.195 | 4.397 | SITE 5 | | LT. OF C.L. HWY. 100, SEC. 0 | 1067 | 2 | | |
| 4.418 | 4.693 | SITE 5 | | LT. OF C.L. HWY. 100, SEC. 0 | 1452 | 2 | | |
| 4.707 | 4.967 | SITE 5 | | LT. OF C.L. HWY. 100, SEC. 0 | 1373 | 2 | | |
| 4.990 | 5.288 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | 1573 | 2 | | |
| 5.302 | 5.501 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | 1051 | 2 | | |
| 5.513 | 5.754 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | 1272 | 2 | | |
| 5.770 | 6.016 | SITE 5 | | RT. OF C.L. HWY. 100, SEC. 0 | 1299 | 2 | | |
| 6.029 | 6.256 | SITE 5 | RT. OF C.L. HWY. 100, SEC. 0 | 1199 | 2 | | | |
| 6.270 | 6.550 | SITE 5 | LT. OF C.L. HWY. 100, SEC. 0 | 1478 | 2 | | | |
| COUNTY SUBTOTALS: | | | | | 13570 | 24 | | |
| ENTIRE PROJECT | | | | | | | 1.00 | 50 |
| SUBTOTALS (BOX 2 OF 2) | | | | | 36828 | 44 | | |
| SUBTOTALS (BOX 1 OF 2) | | | | | 122929 | 112 | | |
| TOTALS: | | | | | 159757 | 156 | 1.00 | 50 |

* THIS ITEM IS SHOWN FOR INFORMATION ONLY.
 ** QUANTITY ESTIMATED.
 SEE SECTION 104.03 OF THE STD. SPECS.
 TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.



Digitally signed by Thomas N. Taegtmeier
 Date: 2024.06.24

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
|-------------------------------------|--------------|--------------------|-------|---------|-----------|--------------|
| | | 6 | ARK. | 012479 | 20 | 23 |
| SUMMARY OF QUANTITIES AND REVISIONS | | | | | | |



Digitally signed by Thomas N. Taegtmeier
Date: 2024.06.24

SUMMARY OF QUANTITIES

| ITEM NUMBER | ITEM | QUANTITY | UNIT |
|---------------|--|----------|----------|
| 601 | MOBILIZATION | 1.00 | LUMP SUM |
| SP, SS, & 603 | MAINTENANCE OF TRAFFIC | 1.00 | LUMP SUM |
| SS & 604 | SIGNS | 1593 | SQ. FT. |
| SS & 604 | TRAFFIC DRUMS | 494 | EACH |
| SS & 604 | ADVANCE WARNING ARROW PANEL | 290 | DAY |
| SP, SS, & 604 | FORTABLE CHANGEABLE MESSAGE SIGN | 120 | WEEK |
| SP, SS, & 605 | CONCRETE DITCH PAVING (TYPE B) | 142002 | SQ. YD. |
| SP | WIRE ROPE SAFETY FENCE | 159757 | LIN. FT. |
| SP | WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS | 1.00 | LUMP SUM |
| SP | WIRE ROPE SAFETY FENCE (POST REPAIR) | 50 | EACH |
| 620 | LIME | 10 | TON |
| 620 | SEEDING | 5.00 | ACRE |
| SS & 620 | MULCH COVER | 35.80 | ACRE |
| 620 | WATER | 1585.4 | M. GAL. |
| 621 | TEMPORARY SEEDING | 30.80 | ACRE |
| 621 | SAND BAG DITCH CHECKS | 7766 | BAG |
| 621 | SEDIMENT REMOVAL AND DISPOSAL | 426 | CU. YD. |
| SS & 621 | FILTER SOCK (18") | 13405 | LIN. FT. |
| 623 | SECOND SEEDING APPLICATION | 5.00 | ACRE |
| 624 | SOLID SODDING | 35502 | SQ. YD. |
| 635 | ROADWAY CONSTRUCTION CONTROL | 1.00 | LUMP SUM |
| | | | |

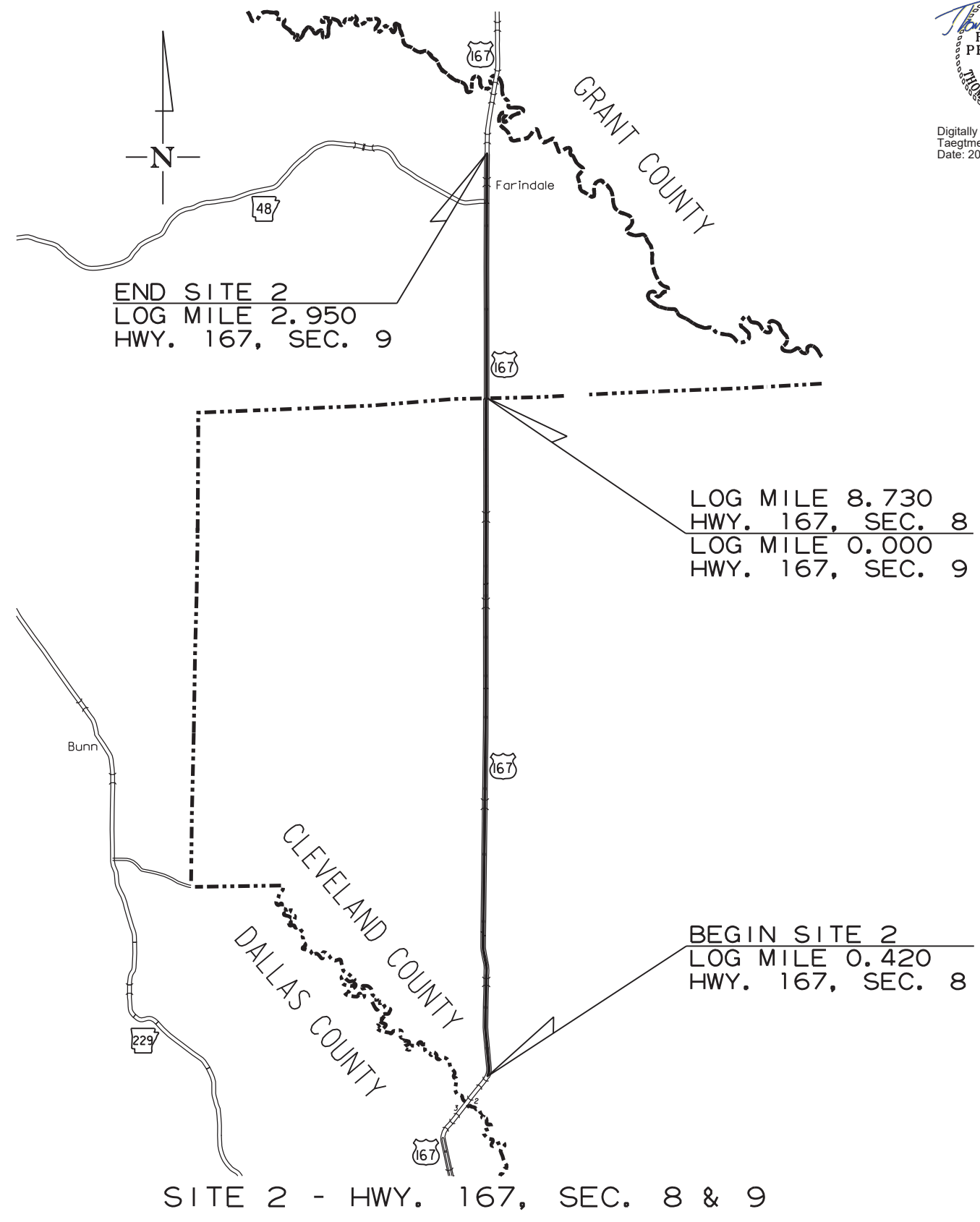
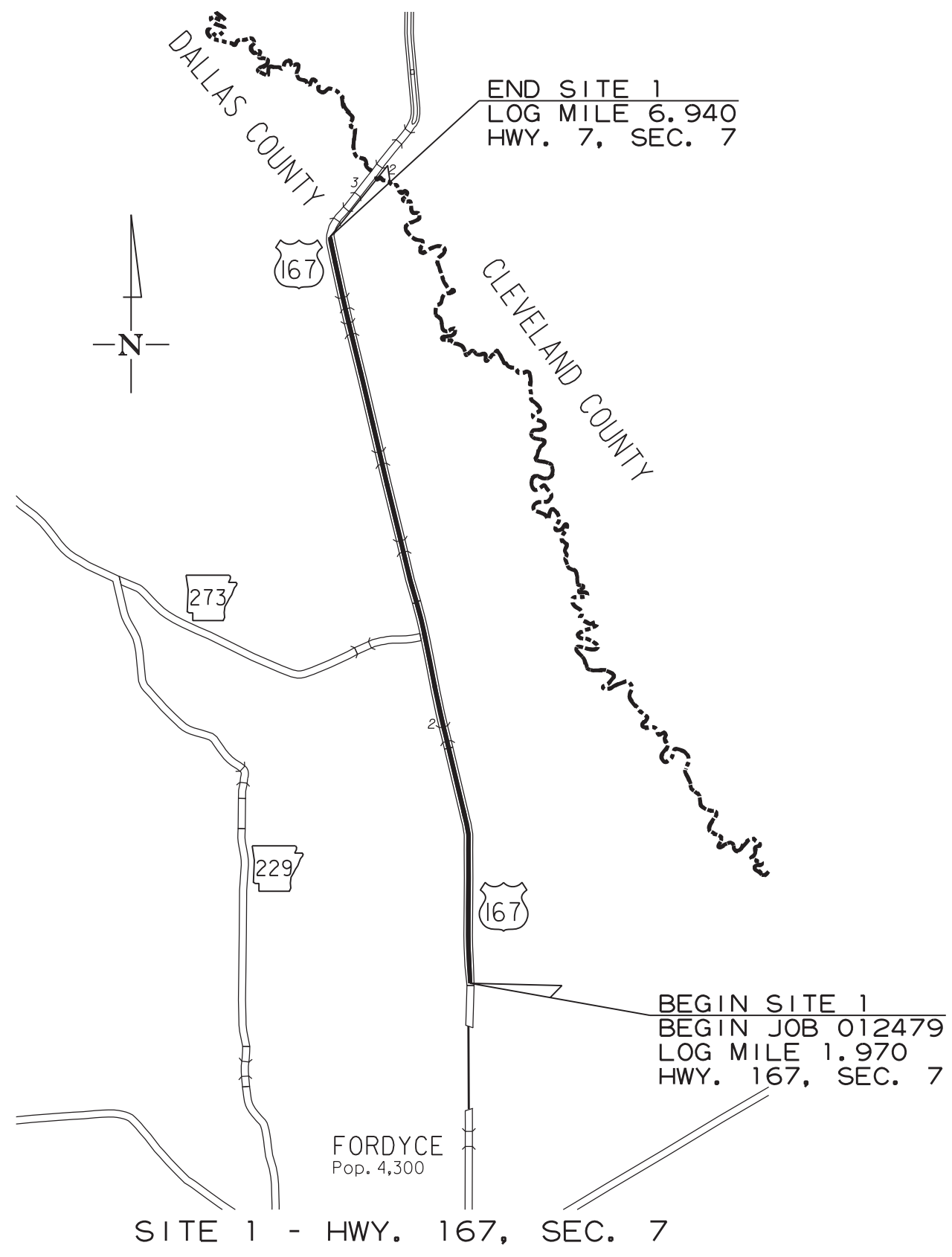
REVISIONS

| DATE | REVISION | SHEET NUMBER |
|------|----------|--------------|
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| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
|----------------------------------|--------------|--------------------|-------|---------|-----------|--------------|
| | | 6 | ARK. | 012479 | 21 | 23 |
| DALLAS AND CLEVELAND COUNTY PLAN | | | | | | |



Digitally signed by Thomas N. Taegtmeier
Date: 2024.05.24

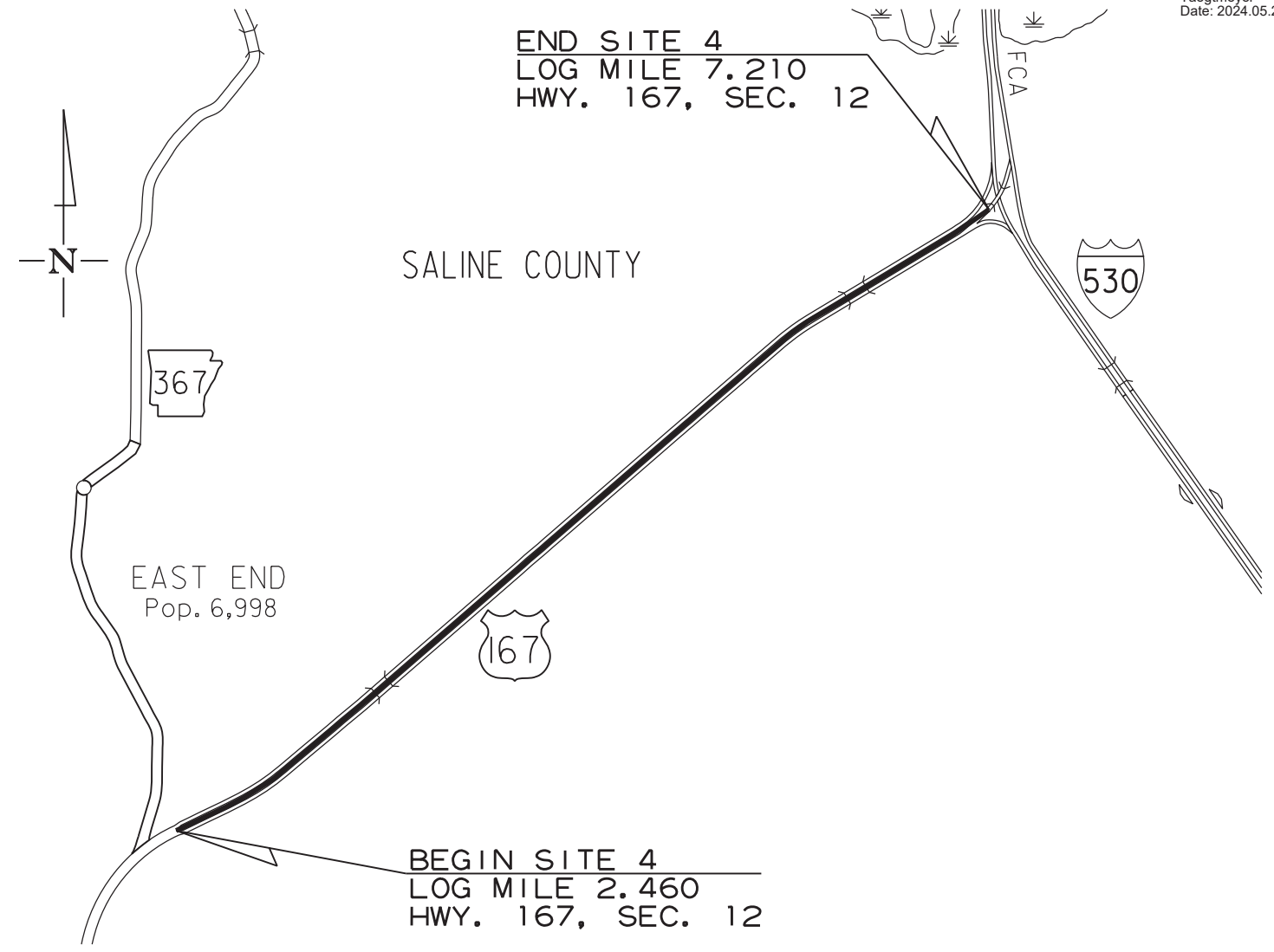
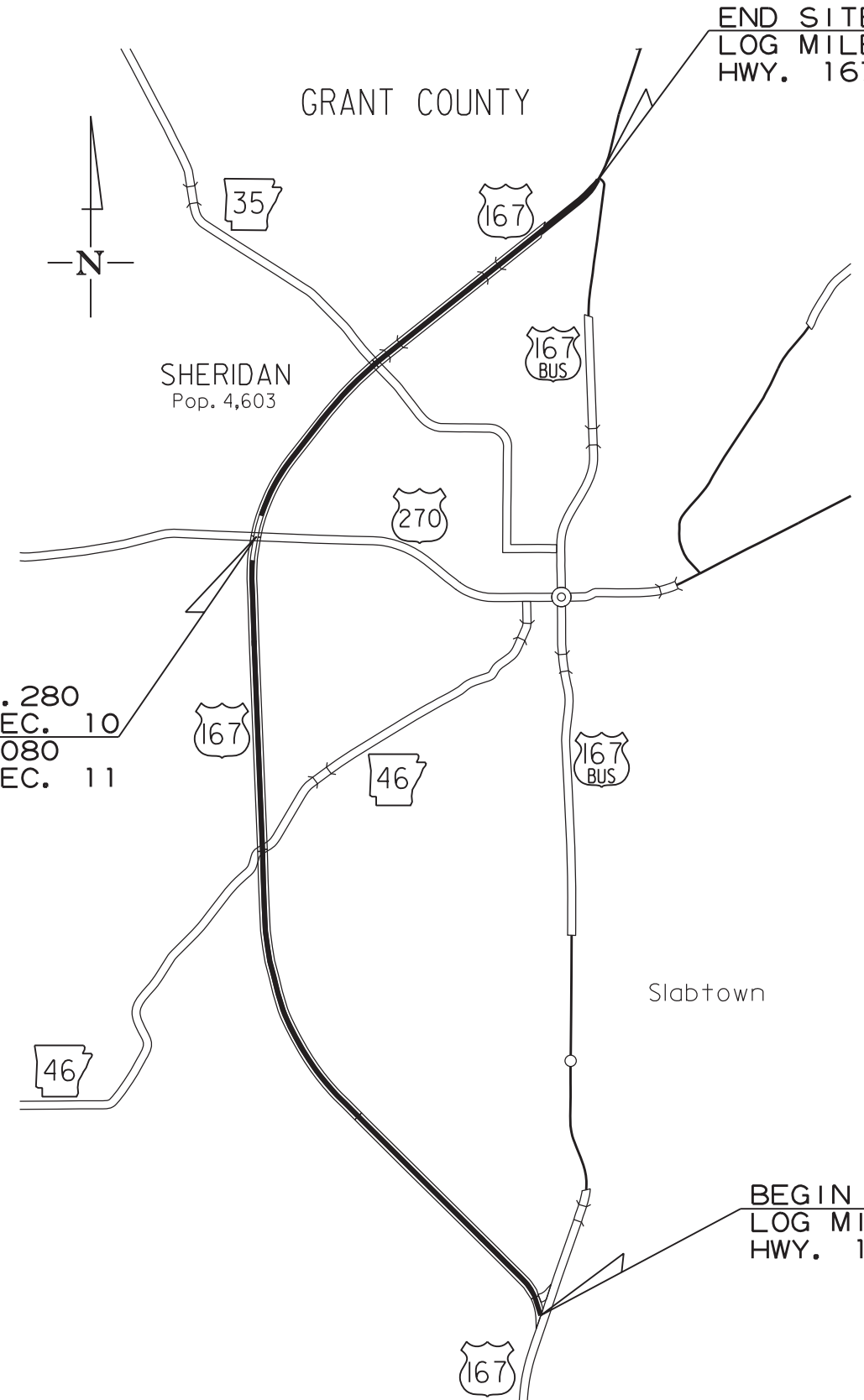


DATE & TIME: 5/23/2024 2:06:27 PM
FILE: J:\25846.16\012479 - Plan Sheets.dgn

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
|------------------------------|--------------|--------------------|-------|---------|-----------|--------------|
| | | 6 | ARK. | 012479 | 22 | 23 |
| GRANT AND SALINE COUNTY PLAN | | | | | | |



Digitally signed by Thomas N. Taegtmeier
Date: 2024.05.24



END SITE 3
LOG MILE 3.000
HWY. 167, SEC. 11

END SITE 4
LOG MILE 7.210
HWY. 167, SEC. 12

LOG MILE 14.280
HWY. 167, SEC. 10
LOG MILE 0.080
HWY. 167, SEC. 11

BEGIN SITE 4
LOG MILE 2.460
HWY. 167, SEC. 12

BEGIN SITE 3
LOG MILE 9.310
HWY. 167, SEC. 10

SITE 3 - HWY. 167, SEC. 10 & 11

SITE 4 - HWY. 167, SEC. 12

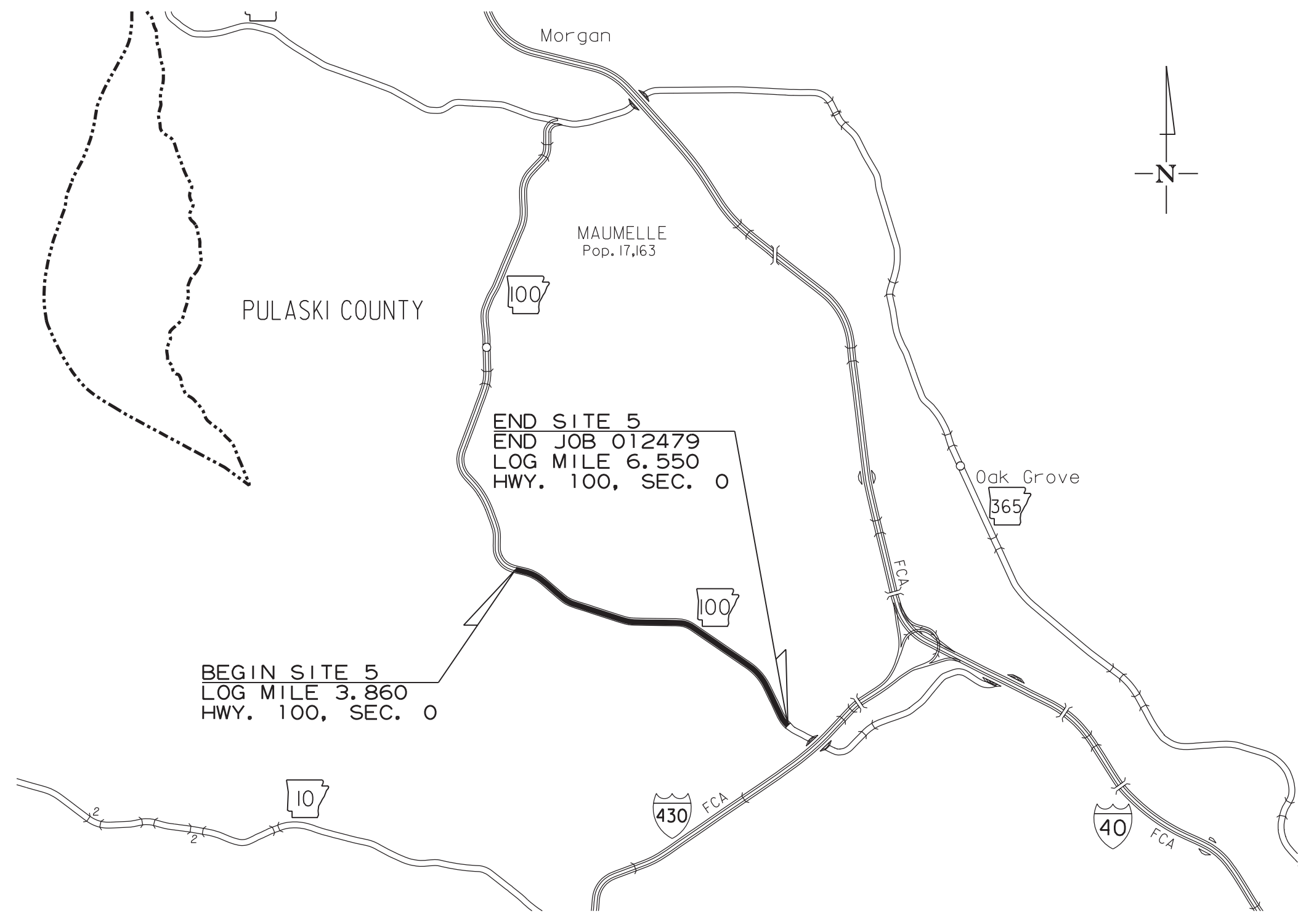
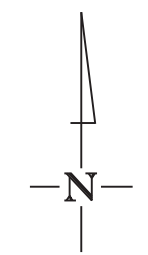
SITES 3 AND 4
GRANT AND SALINE COUNTY PLAN

DATE & TIME: 5/23/2024 2:00:27 PM
FILE: J:\25846.16\012479 - Plan Sheets.dgn

| DATE REVISED | DATE REVISED | FED. RD. DIST. NO. | STATE | JOB NO. | SHEET NO. | TOTAL SHEETS |
|---------------------|--------------|--------------------|-------|---------|-----------|--------------|
| | | 6 | ARK. | 012479 | 23 | 23 |
| PULASKI COUNTY PLAN | | | | | | |



Digitally signed by Thomas N. Taegtmeier
Date: 2024.05.24



END SITE 5
END JOB 012479
LOG MILE 6.550
HWY. 100, SEC. 0

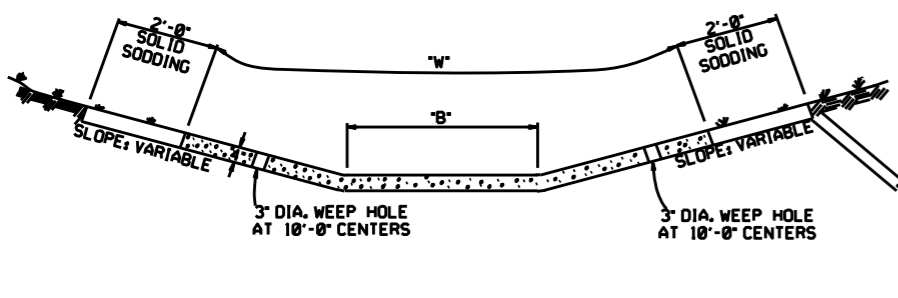
BEGIN SITE 5
LOG MILE 3.860
HWY. 100, SEC. 0

SITE 5 - HWY. 100, SEC. 0

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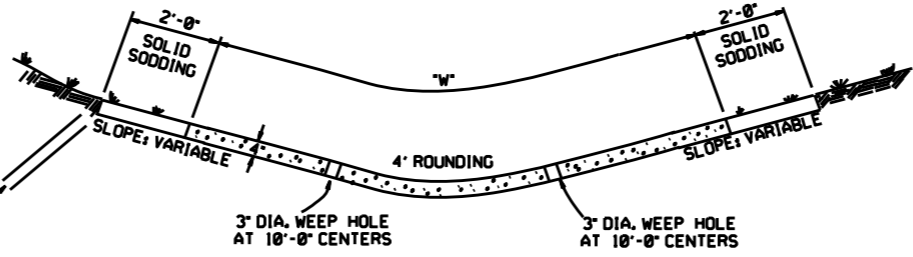
SITE 5
PULASKI COUNTY PLAN

REFER TO TABULATION OF QUANTITIES FOR "W" & "B" DIMENSIONS



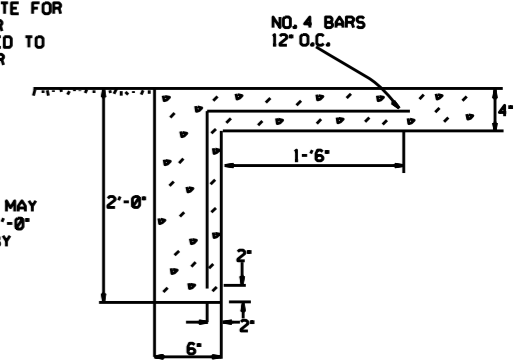
TYPE A

REFER TO TABULATION OF QUANTITIES FOR "W" DIMENSIONS



TYPE B

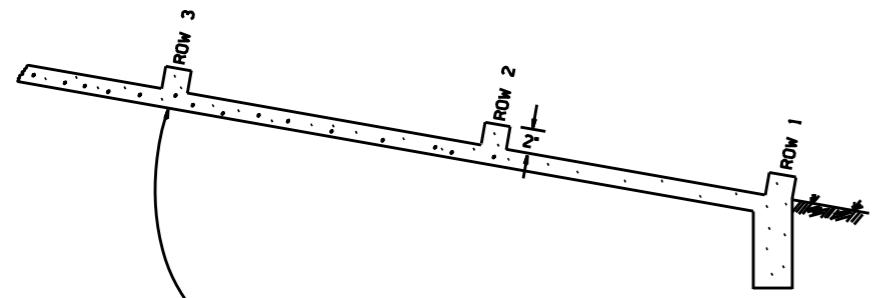
THE STEEL AND ADDITIONAL CONCRETE FOR THE WALLS SHALL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR "CONCRETE DITCH PAVING."



TOE WALL DETAIL FOR CONCRETE DITCH PAVING

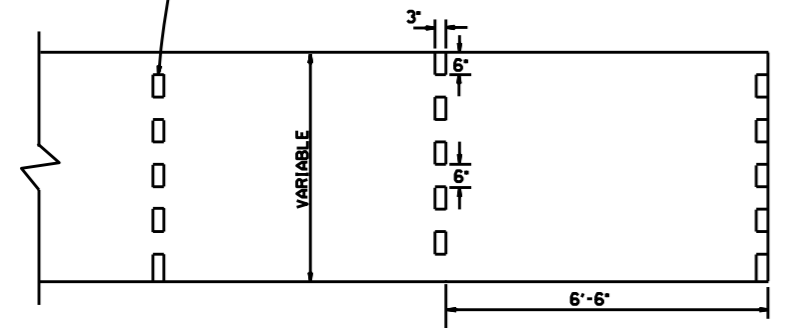
GENERAL NOTES:

- THE FULL WIDTH OF EACH SECTION SHALL BE POURED MONOLITHICALLY.
- TOE WALLS TO BE CONSTRUCTED FULL WIDTH AT EACH END OF DITCH PAVING, AND POURED MONOLITHICALLY.
- SOLID SOD ALONG DITCH PAVING TO BE PLACED WITHIN 14 DAYS OF DITCH PAVING CONSTRUCTION.
- 1' WIDE TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE DITCH PAVING AT 45' INTERVALS. THE SPACE SHALL BE FILLED WITH APPROVED JOINT FILLER COMPLYING WITH AASHTO M213.



NUMBER OF ELEMENTS PER ROW VARIES WITH WIDTH OF PAVING SPECIFIED

ENERGY DISSIPATORS TO BE USED FOR THE ENTIRE LENGTH OF DITCH WHEN SLOPE OF DITCH PAVING EXCEEDS 7%. THE DISSIPATORS WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR CONCRETE DITCH PAVING.




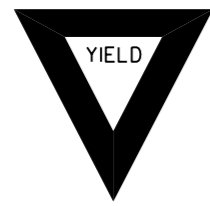







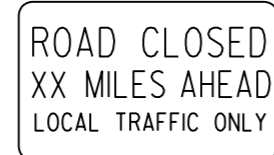
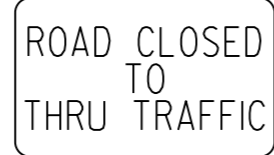

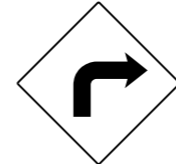

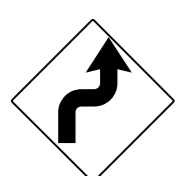

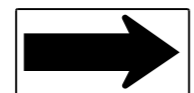

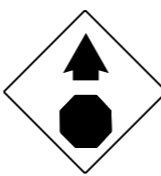

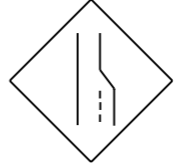

















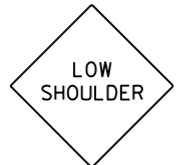
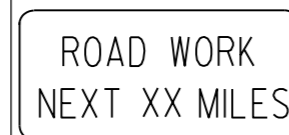
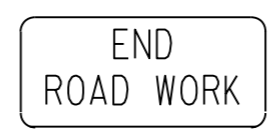
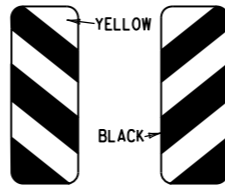


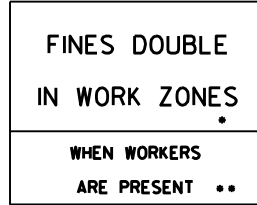
ENERGY DISSIPATORS (NO SCALE)

| DATE | REVISION | DATE FILM'D |
|----------|--|-------------|
| 12-8-16 | CORRECTED ENERGY DISSIPATOR DRAWING AND NOTE | |
| 11-17-10 | ADDED GENERAL NOTE | |
| 6-2-94 | ADDED GENERAL NOTE ABOUT SOLID SODDING | |
| 11-30-88 | ELIMINATED MIN. ROWS OF ELEMENTS | 111-30-89 |
| 7-15-88 | REVISED DISSIPATOR NOTE | 653-7-15-88 |
| 4-3-87 | REVISED ENERGY DISSIPATOR | 871-4-3-87 |
| 1-9-87 | MODIFIED NOTE ON ENERGY DISS. | 832-1-9-87 |
| 11-3-86 | ADDED NOTE TO ENERGY DISS. | 839-12-1-86 |
| 11-1-84 | ENERGY DISSIPATOR DETAILS ADDED | 508-11-1-84 |
| 11-1-84 | EXCAVATION DETAILS ADDED | |
| | TYPED A & B | |
| 10-2-72 | REVISED AND REDRAWN | 508-10-2-72 |
| | DATE | REVISION |
| | | DATE FILM'D |

ARKANSAS STATE HIGHWAY COMMISSION

CONCRETE DITCH PAVING

STANDARD DRAWING CDP-1

| | | | | | | | |
|--|---|--|---|--|---|---|---|
| <p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p> | <p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p> | <p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p> | <p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p> | <p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p> | <p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p> | <p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p> | |
| <p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p> | <p>R11-2</p>  <p>48"x30"</p> | <p>R11-3A</p>  <p>60"x30"</p> | <p>R11-4</p>  <p>60"x30"</p> | <p>W21-5a</p>  <p>STD. 36"x36" FWY. 48"x48"</p> | <p>WI-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p> | <p>WI-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p> | |
| <p>WI-3</p>  <p>STD. 48"x48"</p> | <p>WI-4</p>  <p>STD. 48"x48"</p> | <p>WI-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p> | <p>WI-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p> | <p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p> | <p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p> | <p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p> | |
| <p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p> | <p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p> | <p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p> | <p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p> | <p>W13-1</p>  <p>STD. 24"x24"</p> | <p>W20-1</p>  <p>STD. 48"x48"</p> | <p>W20-2</p>  <p>STD. 48"x48"</p> | <p>W20-3</p>  <p>STD. 48"x48"</p> |
| <p>W20-4</p>  <p>STD. 48"x48"</p> | <p>W20-5</p>  <p>STD. 48"x48"</p> | <p>W20-7a</p>  <p>18" 500 FEET 24" W16-2</p> <p>STD. 36"x36" FWY. 48"x48"</p> | <p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p> | <p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p> | <p>W24-1</p>  <p>STD. 36"x36"</p> | <p>WI-4b</p>  <p>STD. 48"x48"</p> | <p>R56-1</p>  <p>STD. 18"x18"</p> |
| <p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p> | <p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p> | <p>G20-1</p>  <p>60"x24"</p> | <p>G20-2</p>  <p>48"x24"</p> | <p>OM-3L OM-3R</p>  <p>12"x36"</p> | <p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p> | <p>M4-10</p>  <p>48"x18"</p> | <p>R55-1</p>  <p>36"x60"</p> <p>• USE 6" C LETTERS •• USE 4" D LETTERS</p> |

ADVANCE DISTANCES (XXXX)

| | |
|---------|--------------|
| 500 FT | 1/2 MILE |
| 1000 FT | 3/4 MILE |
| 1500 FT | 1 MILE AHEAD |

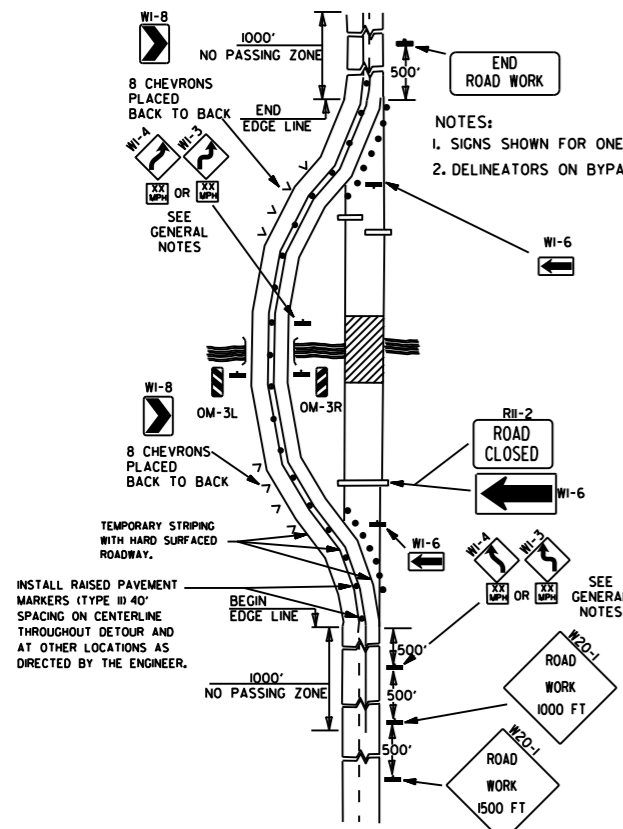
GENERAL NOTES:

- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

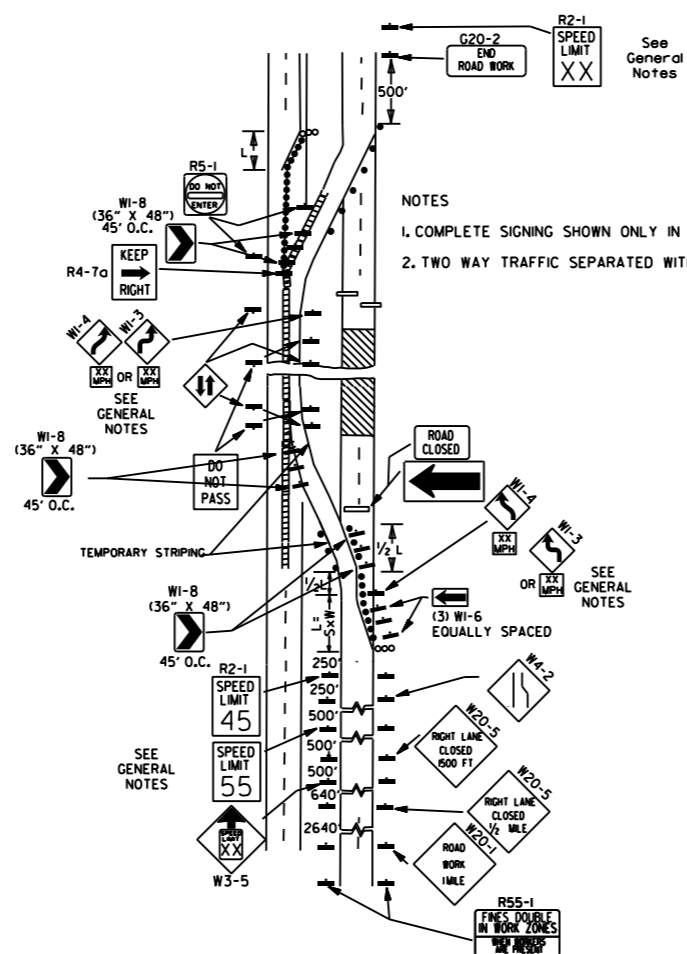
• NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

| DATE | REVISION | FILMED |
|----------|--|--------|
| 11-07-19 | REVISED FOR MASH | |
| 4-13-17 | DELETED RSP-1 & ADDED W21-5a | |
| 9-2-15 | REVISED REDUCED SPEED LIMIT AHEAD SIGNS REVISED ROAD WORK NEXT XX MILES | |
| 12-15-11 | REVISED W24-1 | |
| 11-17-10 | DELETED W8-9a & ADDED W8-9 | |
| 10-15-09 | ADDED REFERENCE TO MASH & ADDED SIGN W24-1 | |
| 4-17-08 | REVISED SIGN DESIGNATIONS | |
| 11-18-04 | REVISED NOTES | |
| 10-9-03 | REVISED NOTE 1 | |
| 11-16-01 | REVISED NOTE 7 | |
| 9-28-00 | REVISED NOTE | |
| 11-18-98 | ADDED NOTE | |
| 6-26-97 | REVISED NOTE 5 | |
| 4-03-97 | REVISED NOTE 5 | |
| 10-18-96 | ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7 | |
| 10-12-95 | ADDED R55-1 | |
| 6-8-95 | REVISED TO CORRECT SIGN ILLUSTRATIONS | 6-8-95 |
| 2-2-95 | REVISED PER PART VI, MUTCD SEPT. 3, 1993 | |
| 8-15-91 | DRAWN AND PLACED IN USE | |

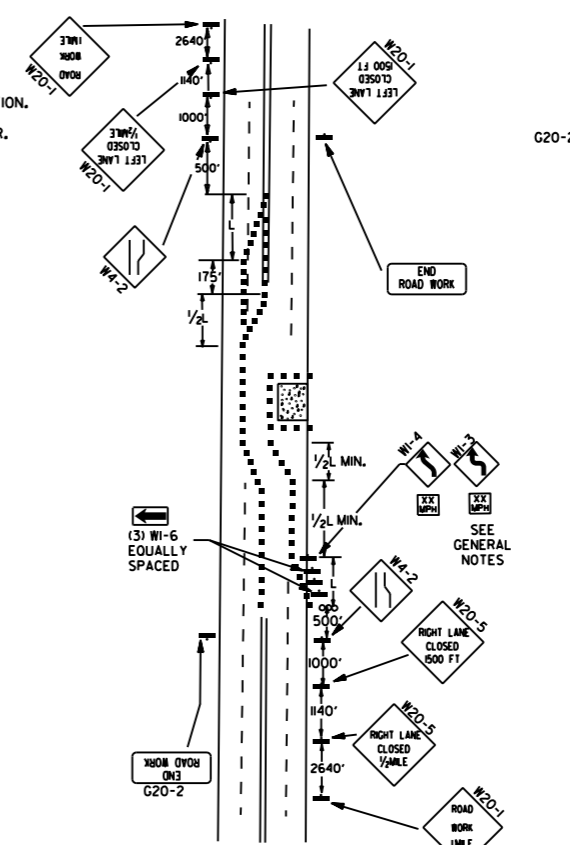
ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-1



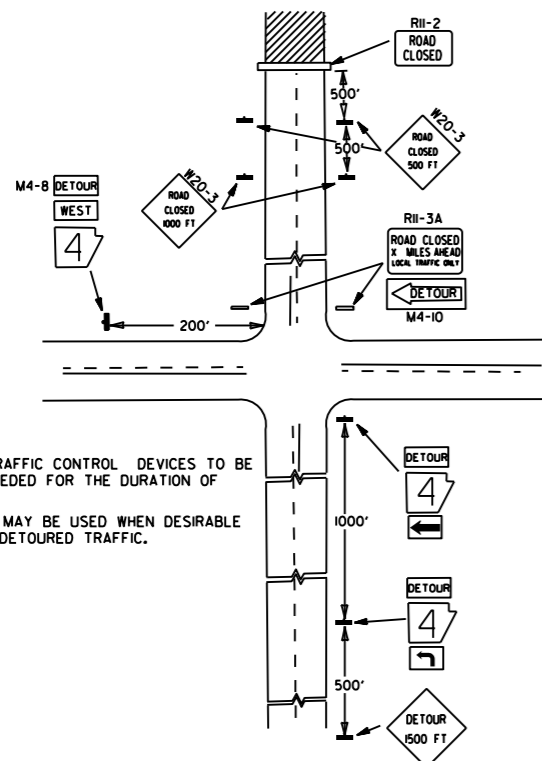
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



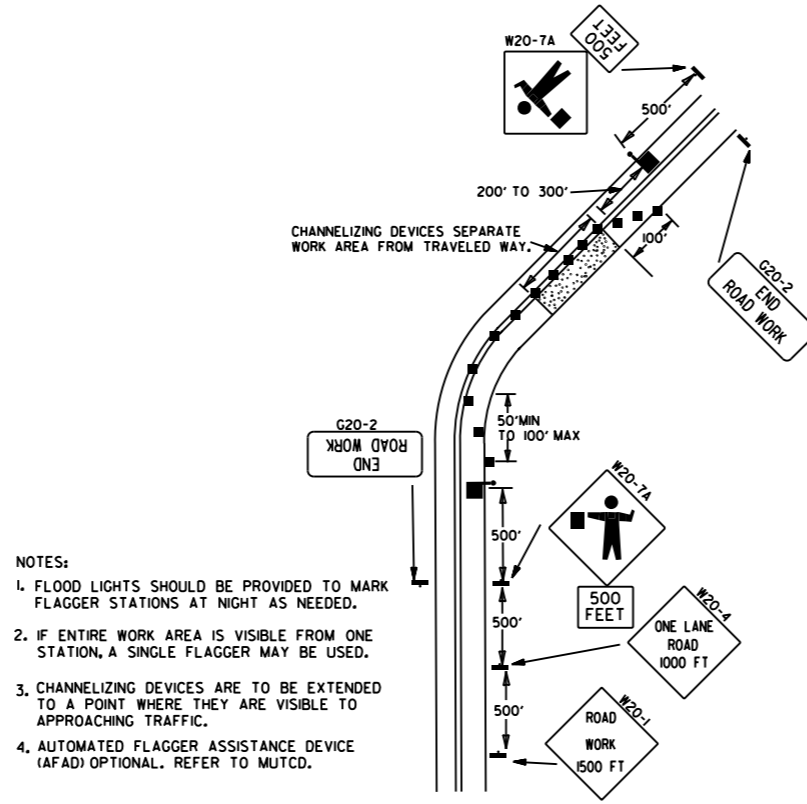
(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



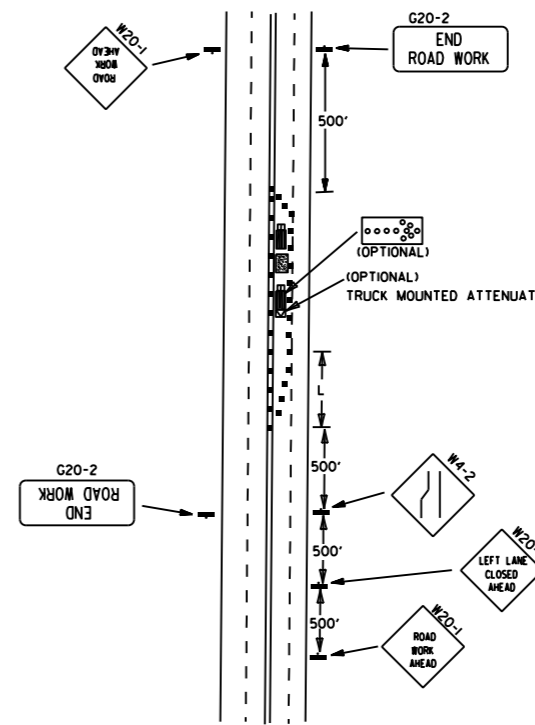
(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.

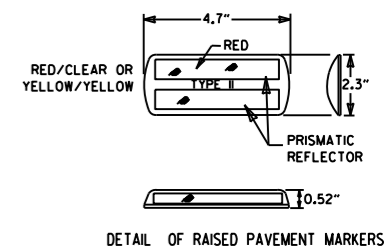


(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

- KEY:
- FLAGGER
 - ▬ POSITIVE BARRIER
 - ∞ ARROW PANEL (IF REQUIRED)
 - ▬ TYPE III BARRICADE
 - CHANNELIZING DEVICE
 - TRAFFIC DRUM
 - RAISED PAVEMENT MARKER



TYPICAL ADVANCE WARNING SIGN PLACEMENT

TAPER FORMULAE:

$L = SXW$ FOR SPEEDS OF 45MPH OR MORE.

$L = \frac{WS^2}{60}$ FOR SPEEDS OF 40MPH OR LESS.

WHERE:

L = MINIMUM LENGTH OF TAPER.

S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.

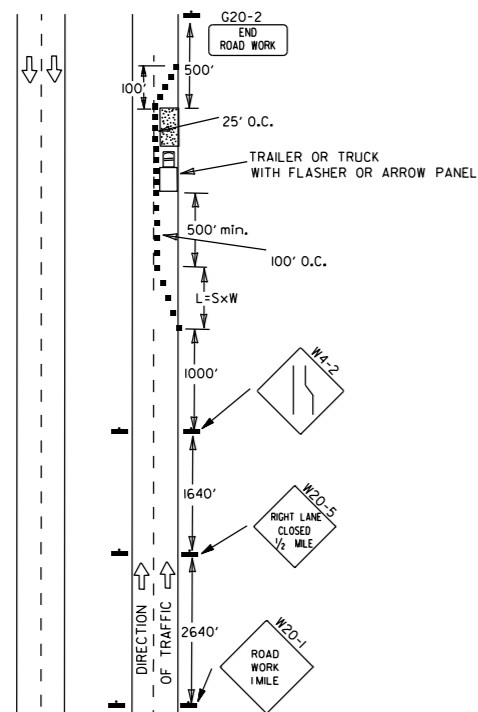
W = WIDTH OF OFFSET.

GENERAL NOTES:

1. THE MAINTENANCE DIVISION SHALL CONDUCT A BALL BANK STUDY TO DETERMINE THE ADVISORY SPEED LIMIT PRIOR TO OPENING TO TRAFFIC. THE ADVISORY SPEED WILL BE POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
2. WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-1(55)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
7. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
8. DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ARDOT QUALIFIED PRODUCTS LIST.
9. ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

| DATE | REVISION | FILED |
|----------|--|--------|
| 05-20-21 | REVISED NOTE 7 | |
| 11-07-19 | REVISED NOTE 1, ADDED NOTE 9 | |
| 9-2-15 | REVISED NOTE 2, ADDED NOTE 8, REVISED DRAWING (A) & REPLACED R2-5A WITH W3-5 | |
| 9-12-13 | REVISED DETAIL OF RAISED PAVEMENT MARKERS | |
| 3-11-10 | ADDED (AFAD) | |
| 11-20-08 | REVISED SIGN DESIGNATIONS | |
| 11-18-04 | ADDED GENERAL NOTE | |
| 10-18-96 | ADDED R55-1 | |
| 4-26-96 | CORRECTED (a) BEHIND G20-2 | |
| 6-8-95 | CORRECTED SIGN IDENT. ON W1-4A | 6-8-95 |
| 2-2-95 | REVISED PER PART VI, MUTCD, SEPT. 3, 1993 | |
| 8-15-91 | DRAWN AND PLACED IN USE | |

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION



(A) TYPICAL APPLICATION - DAYTIME MAINTENANCE OPERATIONS OF SHORT DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

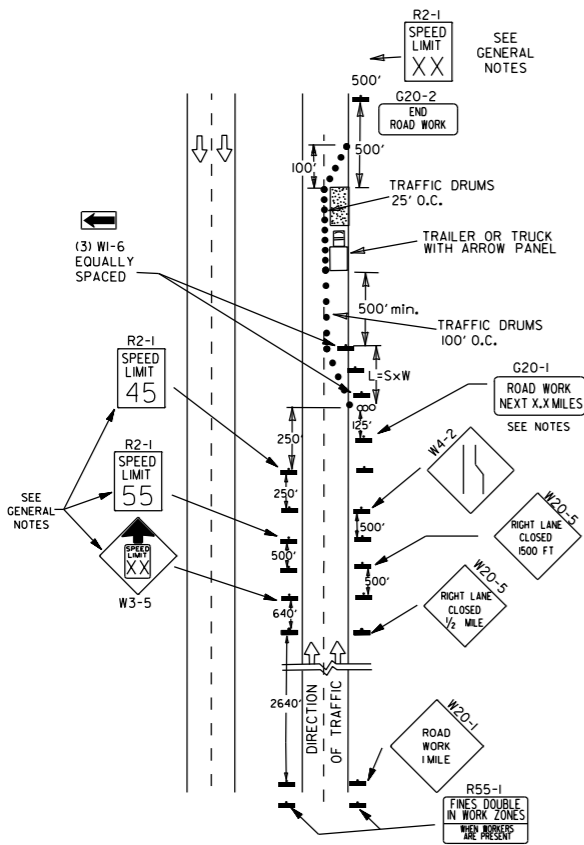
(B) TYPICAL APPLICATION - 3-LANE ONEWAY ROADWAY WHERE CENTER LANE IS CLOSED.

KEY:

- ○ ○ ARROW PANEL (IF REQUIRED)
- CHANNELIZING DEVICE
- TRAFFIC DRUM

GENERAL NOTES:

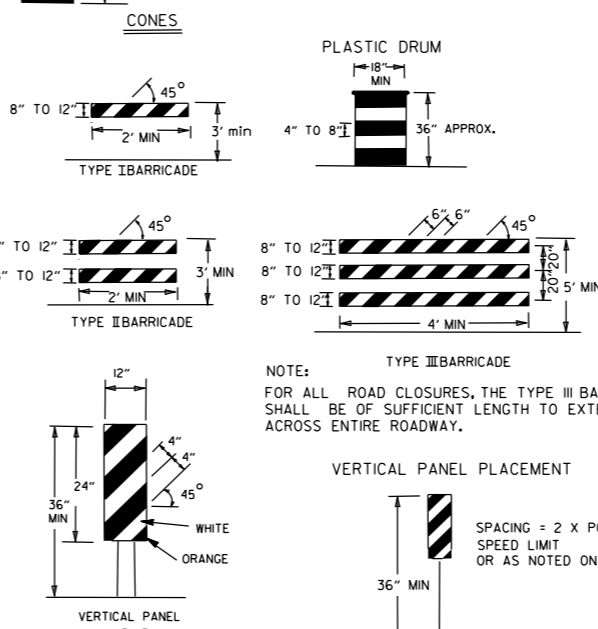
1. A SPEED LIMIT REDUCTION MAY BE IMPLEMENTED ONLY WHEN DESIGNATED IN THE PLAN OR WHEN RECOMMENDED BY THE ROADWAY DESIGN DIVISION.
2. WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-(55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-(65) SHALL BE OMITTED. ADDITIONAL R2-1(55)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/2 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT OR AS DIRECTED BY THE ENGINEER.
5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHOULD BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
7. THE G20-1 SIGN WILL BE REQUIRED ON JOBS OF OVER TWO MILES IN LENGTH. WHEN THE LANE CLOSURE IS NOT AT THE BEGINNING OF THE PROJECT, THE G20-1 SIGN SHALL BE ERECTED 125' IN ADVANCE OF THE JOB LIMIT. ADDITIONAL W20-1(1/2 MILE) SIGNS ARE NOT REQUIRED IN ADVANCE OF LANE CLOSURES THAT BEGIN INSIDE THE PROJECT LIMITS.
8. FLAGGERS SHALL USE STOP/SLOW PADDLES FOR CONTROLLING TRAFFIC THROUGH WORK ZONES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
9. ALL PLASTIC DRUMS AND CONES SHALL MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
10. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
11. ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).



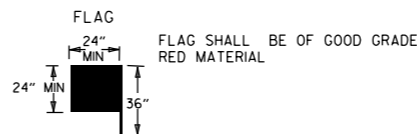
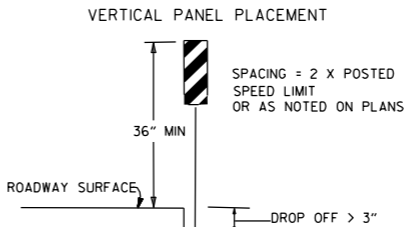
(C) TYPICAL APPLICATION - CONSTRUCTION OPERATIONS OF INTERMEDIATE TO LONG TERM DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

CHANNELIZING DEVICES

WHEN CONES ARE USED ON FREEWAYS AND MULTI-LANE HIGHWAYS, THEY SHALL BE 28" MIN. DURING HOURS OF DARKNESS, 28" CONES SHALL BE USED ON ALL ROADWAYS, AND SHALL BE REFLECTORIZED IN ACCORDANCE WITH THE M.U.T.C.D.



NOTE: FOR ALL ROAD CLOSURES, THE TYPE III BARRICADES SHALL BE OF SUFFICIENT LENGTH TO EXTEND ACROSS ENTIRE ROADWAY.



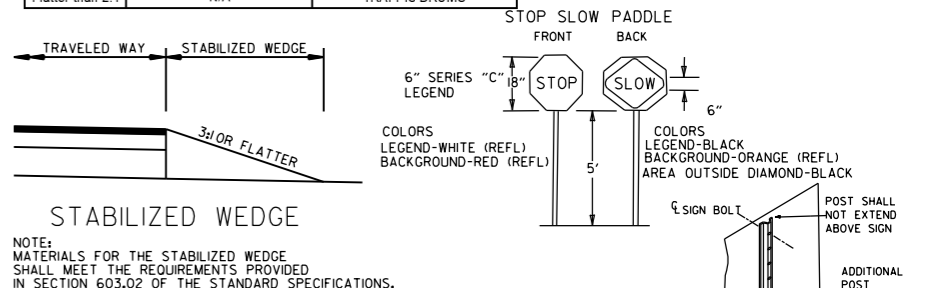
(D) TYPICAL APPLICATION - CLOSING MULTIPLE LANES OF A MULTILANE HIGHWAY.

| TRAFFIC CONTROL DEVICES | | | |
|-------------------------|---|---|--|
| VERTICAL DIFFERENTIAL | LOCATION | TRAFFIC CONTROL | |
| | | ≤ 45 MPH | > 45 MPH |
| ≤ 1" | CENTERLINE | W8-11 | W8-11 |
| > 1" ≤ 3" | CENTERLINE | W8-11 AND CENTERLINE LANE STRIPING | W8-11 AND CENTERLINE LANE STRIPING |
| > 3" | CENTERLINE | STANDARD LANE CLOSURE ⁽⁶⁾ | STANDARD LANE CLOSURE ⁽⁶⁾ |
| ≤ 3" | EDGE OF TRAVELED LANE OR EDGE OF SHOULDER | W8-9 AND TRAFFIC DRUMS ⁽¹⁾ | W8-9 AND TRAFFIC DRUMS ⁽¹⁾ |
| > 3" | EDGE OF TRAVELED LANE OR EDGE OF SHOULDER | W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾ | W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾ |
| > 6" | EDGE OF TRAVELED LANE OR EDGE OF SHOULDER | W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾ | W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾ |
| > 18" | EDGE OF TRAVELED LANE OR EDGE OF SHOULDER | W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾ | A STABILIZED WEDGE, W8-17, EDGE LINE STRIPING AND TRAFFIC DRUMS ⁽³⁾ |
| > 24" | EDGE OF TRAVELED LANE OR EDGE OF SHOULDER | PRECAST CONCRETE BARRIER ⁽⁴⁾ & EDGE LINES | PRECAST CONCRETE BARRIER ⁽⁴⁾ & EDGE LINES |

| INTERSTATE | | |
|-----------------------|---|---|
| VERTICAL DIFFERENTIAL | LOCATION | TRAFFIC CONTROL |
| ≤ 3" | CENTERLINE | W8-11 AND LANE STRIPING |
| ≤ 3" | EDGE OF TRAVELED LANE OR EDGE OF SHOULDER | W8-9, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾ |
| > 3" | EDGE OF TRAVELED LANE OR EDGE OF SHOULDER | W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾ |
| > 6" | EDGE OF TRAVELED LANE OR EDGE OF SHOULDER | PRECAST CONCRETE BARRIER & EDGE LINES |

| INTERSTATE AND NON-INTERSTATE | | |
|-------------------------------|--------|--------------------------|
| FORESLOPE | HEIGHT | TRAFFIC CONTROL |
| 1:1 | > 2 FT | PRECAST CONCRETE BARRIER |
| 2:1 | ≤ 5 FT | TRAFFIC DRUMS |
| 2:1 | > 5 FT | PRECAST CONCRETE BARRIER |
| Flatter than 2:1 | N/A | TRAFFIC DRUMS |

- GENERAL NOTES:
1. WHEN THE SHOULDER AREA IS USED AS PART OF THE TRAVELED LANE AND THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, THEN VERTICAL PANELS SHALL BE USED.
 2. WHEN THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, A STABILIZED WEDGE SHALL BE USED.
 3. PRECAST CONCRETE BARRIER WALL CAN BE USED IN LIEU OF A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS, IF AND WHERE DIRECTED BY THE ENGINEER.
 4. A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS CAN BE USED IN LIEU OF PRECAST CONCRETE BARRIER WALL, IF AND WHERE DIRECTED BY THE ENGINEER.
 5. W21-5, W21-50, AND/OR W21-5b SIGNS SHALL BE USED WHERE THE ROADWAY IS UNOBSTRUCTED IF AND WHERE DIRECTED BY THE ENGINEER. TIME LIMITATIONS MUST CONFORM TO SECTION 603 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION).

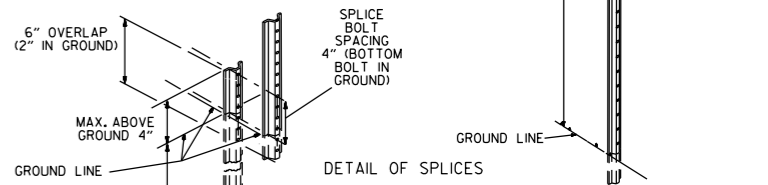


NOTE: MATERIALS FOR THE STABILIZED WEDGE SHALL MEET THE REQUIREMENTS PROVIDED IN SECTION 603.02 OF THE STANDARD SPECIFICATIONS.

NOTES: USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. 5HS-2)

NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS.

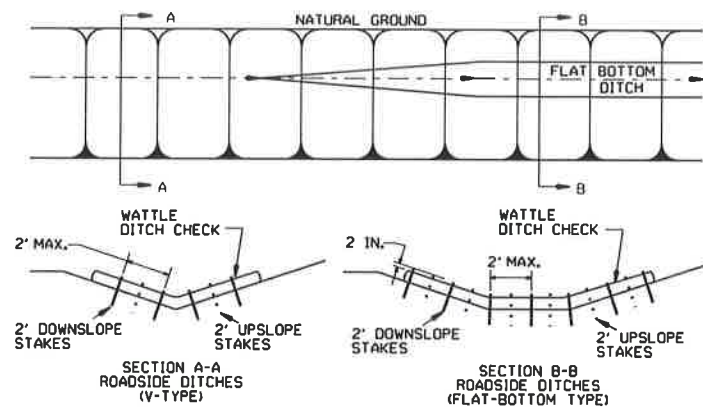
SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.



| DATE | REVISION | FILED |
|----------|--|--------|
| 08-12-21 | REVISED TRAFFIC CONTROL DEVICES AND NOTES | |
| 05-20-21 | REVISED NOTE 10 | |
| 2-27-20 | REVISED TRAFFIC CONTROL DEVICES DETAILS | |
| 11-07-19 | REVISED NOTE 9, ADDED NOTE II | |
| 7-25-19 | REVISED TRAFFIC CONTROL DEVICES DETAILS | |
| 9-2-15 | REVISED NOTE 2 & REPLACED R2-5A WITH W3-5 | |
| 10-15-09 | ADDED REFERENCE TO MASH | |
| 11-20-08 | REVISED SIGN DESIGNATIONS | |
| 11-18-04 | ADDED NOTE | |
| 10-1-98 | ADDED NOTE | |
| 4-03-97 | ADDED (SP) TO W6-18 & REVISED TRAFFIC CONTROL DEVICES NOTE | |
| 10-18-96 | ADDED R55-1 | |
| 10-12-95 | MOVED UPPER SPLICE | |
| 6-8-95 | REVISED SPLICE DETAIL, TEXT | 6-8-95 |
| 2-2-95 | REVISED PER PART VI, MUTCD, SEPT. 3, 1993 | |
| 8-15-91 | DRAWN AND PLACED IN USE | |

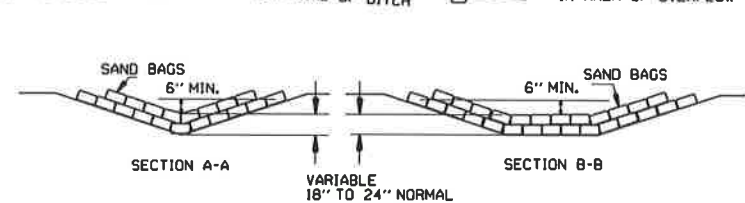
GENERAL NOTES

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

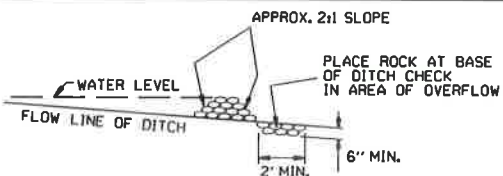


WATTLE DITCH CHECK (E-1)

NUMBER OF SAND BAGS AND ARRANGEMENT VARIABLE WITH ON-SITE CONDITIONS. PLACE SAND BAGS AT BASE OF DITCH CHECK IN AREA OF OVERFLOW.

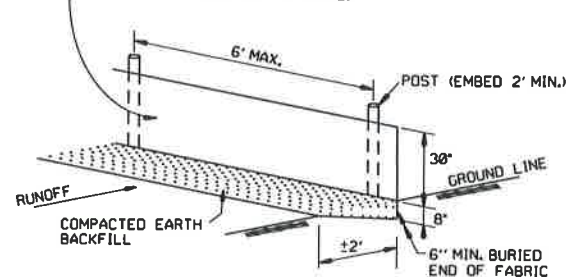


SAND BAG DITCH CHECK (E-5)

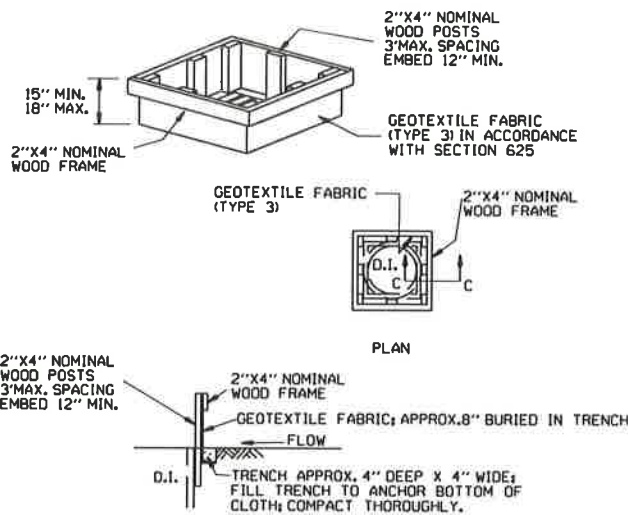


ROCK DITCH CHECK (E-6)

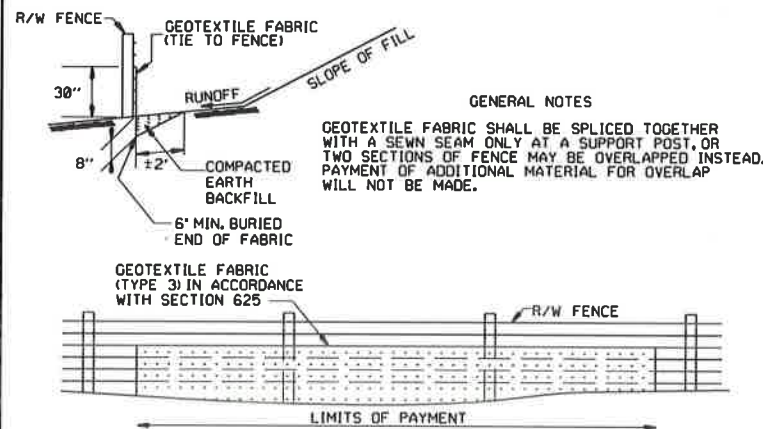
GENERAL NOTES
 GEOTEXTILE FABRIC (TYPE 4) IN ACCORDANCE WITH SECTION 625
 GEOTEXTILE FABRIC SHALL BE SPICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST OR TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.



SILT FENCE (E-11)

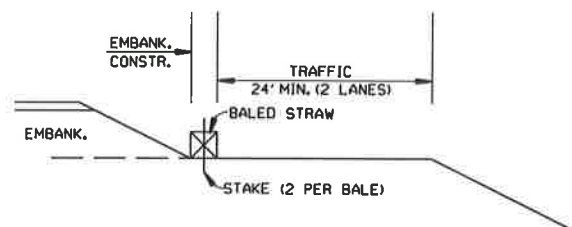


DROP INLET SILTY FENCE (E-7)

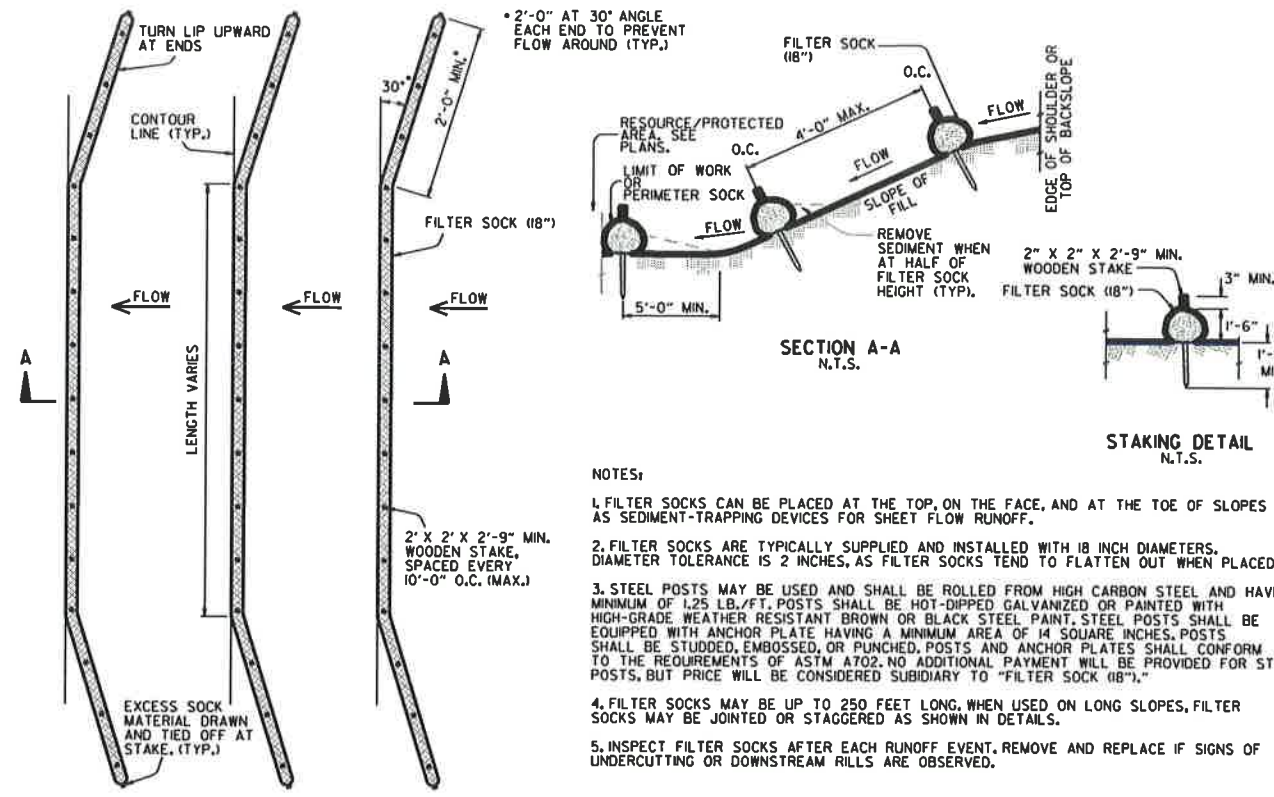


SILT FENCE ON R/W FENCE (E-4)

GENERAL NOTES
 1. STRAW BALES SHALL BE INSTALLED SO THAT THE BINDINGS ARE ORIENTED AROUND THE SIDES RATHER THAN ALONG THE TOPS AND BOTTOMS OF THE BALES. THE BALES SHALL BE A MINIMUM OF 30 INCHES IN LENGTH.
 2. NO GAPS SHALL BE LEFT BETWEEN BALES.
 3. BALED STRAW FILTER BARRIERS COMPLETED AND ACCEPTED WILL BE MEASURED BY THE BALE IN PLACE AS AUTHORIZED BY THE ENGINEER AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID PER BALE FOR BALED STRAW DITCH CHECKS.

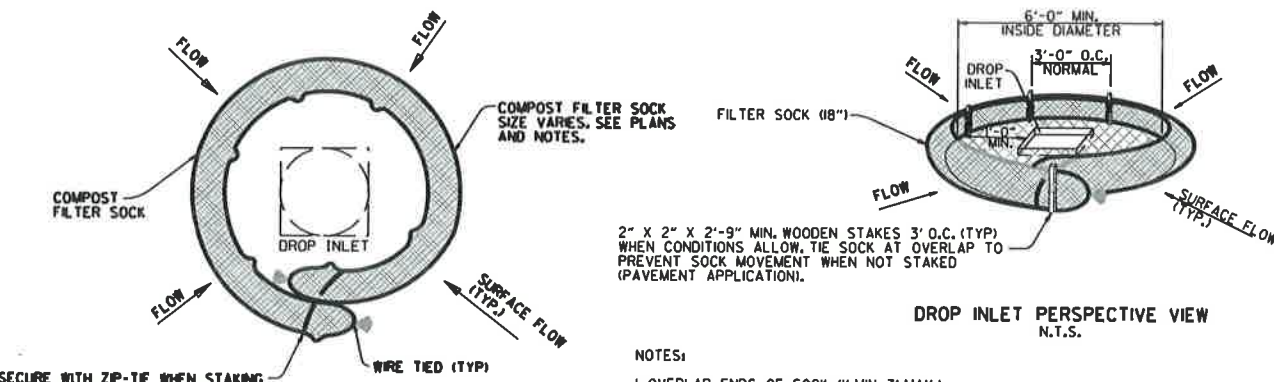


BALED STRAW FILTER BARRIER (E-2)



FILTER SOCK ALONG SLOPE (E-3)

NOTES:
 1. FILTER SOCKS CAN BE PLACED AT THE TOP, ON THE FACE, AND AT THE TOE OF SLOPES AS SEDIMENT-TRAPPING DEVICES FOR SHEET FLOW RUNOFF.
 2. FILTER SOCKS ARE TYPICALLY SUPPLIED AND INSTALLED WITH 18 INCH DIAMETERS. DIAMETER TOLERANCE IS 2 INCHES, AS FILTER SOCKS TEND TO FLATTEN OUT WHEN PLACED.
 3. STEEL POSTS MAY BE USED AND SHALL BE ROLLED FROM HIGH CARBON STEEL AND HAVE A MINIMUM OF 1.25 LB./FT. POSTS SHALL BE HOT-DIPPED GALVANIZED OR PAINTED WITH HIGH-GRADE WEATHER RESISTANT BROWN OR BLACK STEEL PAINT. STEEL POSTS SHALL BE EQUIPPED WITH ANCHOR PLATE HAVING A MINIMUM AREA OF 14 SQUARE INCHES. POSTS SHALL BE STUDDED, EMBOSSED, OR PUNCHED. POSTS AND ANCHOR PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A702. NO ADDITIONAL PAYMENT WILL BE PROVIDED FOR STEEL POSTS, BUT PRICE WILL BE CONSIDERED SUBSIDIARY TO "FILTER SOCK (18")."
 4. FILTER SOCKS MAY BE UP TO 250 FEET LONG. WHEN USED ON LONG SLOPES, FILTER SOCKS MAY BE JOINTED OR STAGGERED AS SHOWN IN DETAILS.
 5. INSPECT FILTER SOCKS AFTER EACH RUNOFF EVENT. REMOVE AND REPLACE IF SIGNS OF UNDERCUTTING OR DOWNSTREAM RILLS ARE OBSERVED.

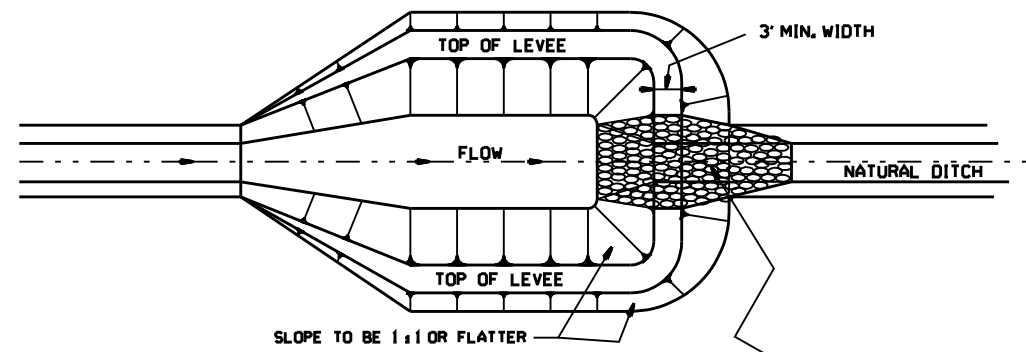


COMPOST FILTER SOCK DROP INLET PROTECTION (E-13)

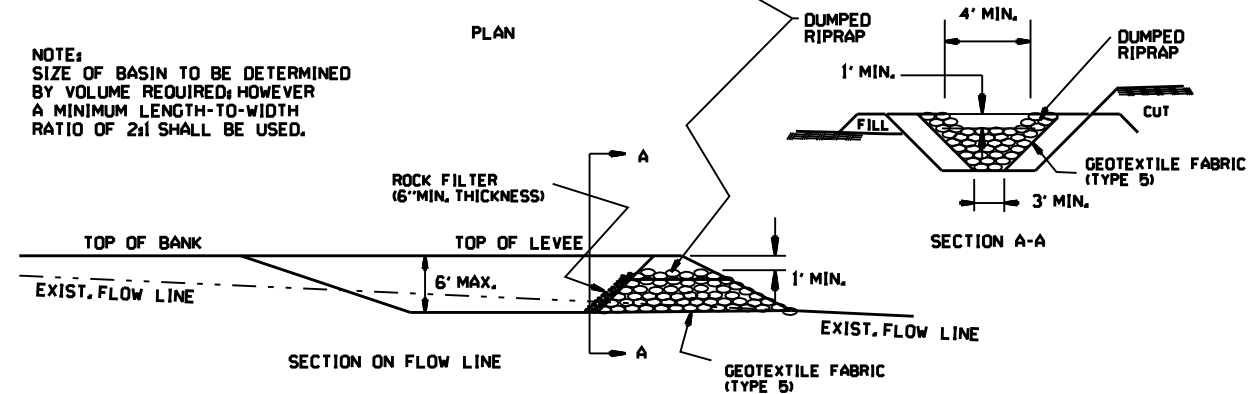
NOTES:
 1. OVERLAP ENDS OF SOCK (1\"/>

| | | |
|----------|--|-------------|
| 11-16-17 | ADDED FILTER SOCK E-3 AND E-13 | |
| 12-15-11 | DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK | |
| 1-18-98 | ADDED NOTES | |
| 07-02-98 | ADDED BALED STRAW FILTER BARRIER (E-2) | 7-20-95 |
| 07-20-95 | REVISED SILTY FENCE E-4 AND E-11 | |
| 07-15-94 | REV. E-4 & E-11 MIN. 13\"/> | |
| 06-02-94 | REVISED E-1, 4, 7 & 11 DELETED E-2 & 3 | 6-2-94 |
| 04-01-93 | REDRAWN | |
| 10-01-92 | REDRAWN | |
| 08-02-76 | ISSUED R.D.M. | 298-7-28-76 |
| DATE | REVISION | FILMED |

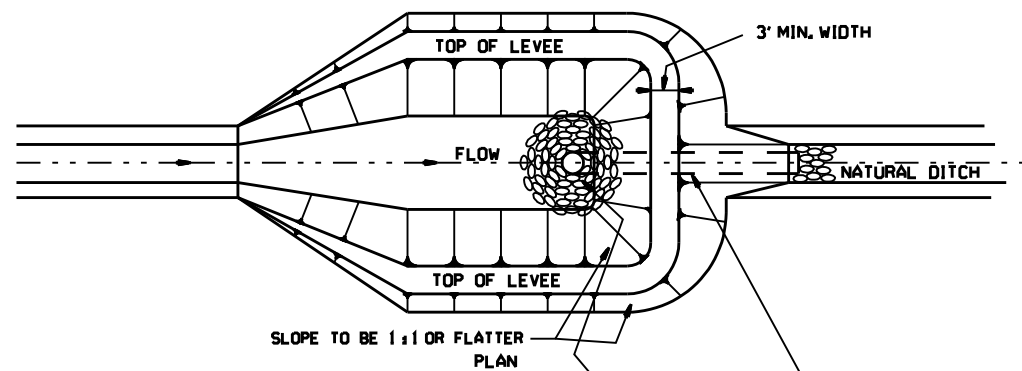
ARKANSAS STATE HIGHWAY COMMISSION
 TEMPORARY EROSION CONTROL DEVICES
 STANDARD DRAWING TEC-1



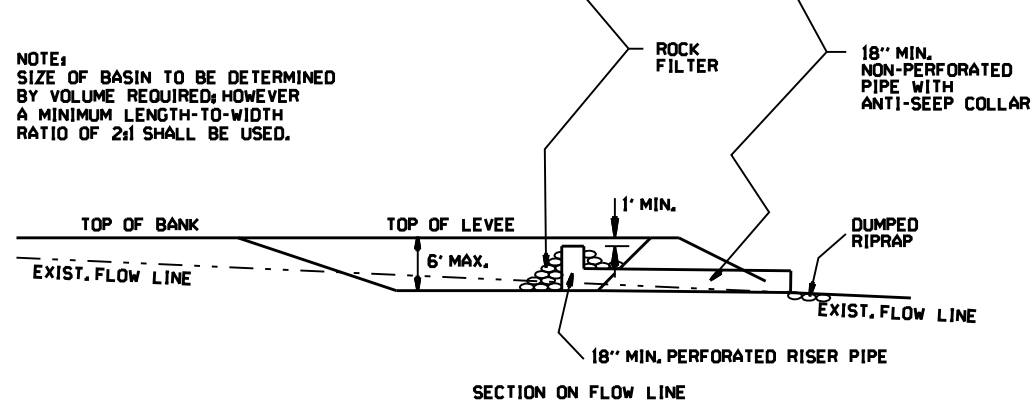
NOTE:
SIZE OF BASIN TO BE DETERMINED
BY VOLUME REQUIRED; HOWEVER
A MINIMUM LENGTH-TO-WIDTH
RATIO OF 2:1 SHALL BE USED.



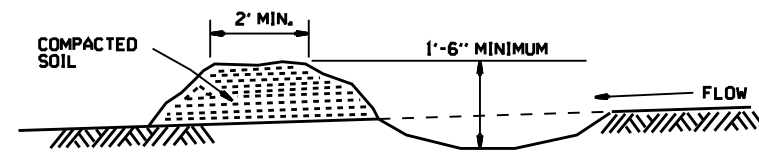
SEDIMENT BASIN WITH RIPRAP OUTLET (E-9)



NOTE:
SIZE OF BASIN TO BE DETERMINED
BY VOLUME REQUIRED; HOWEVER
A MINIMUM LENGTH-TO-WIDTH
RATIO OF 2:1 SHALL BE USED.

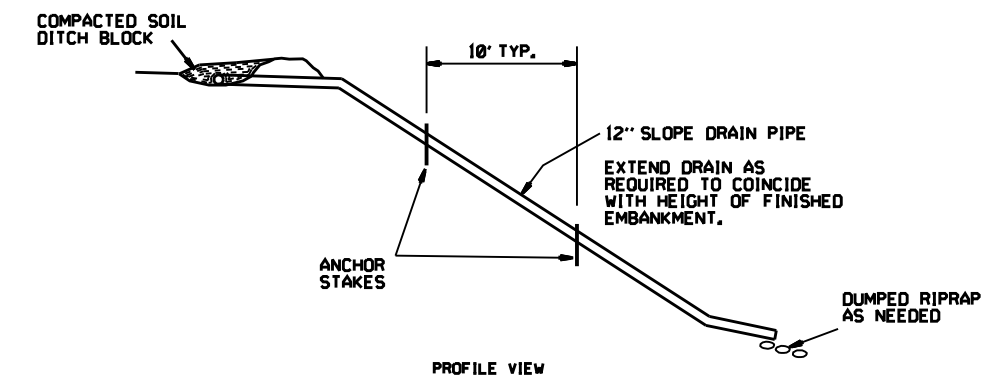
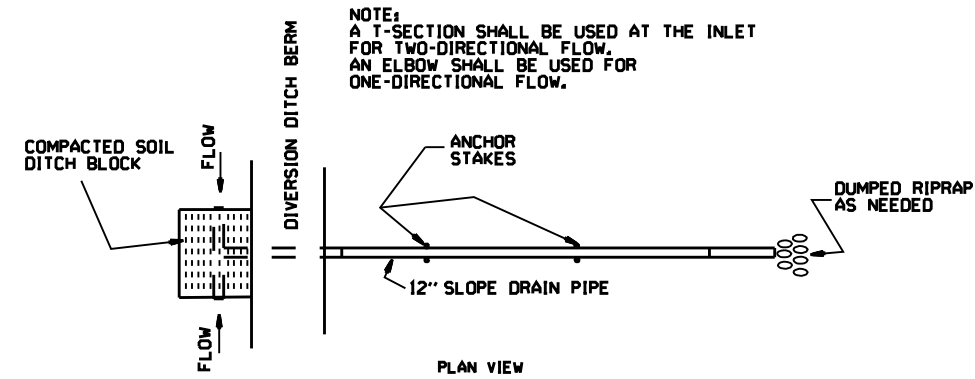


SEDIMENT BASIN WITH PIPE OUTLET (E-10)

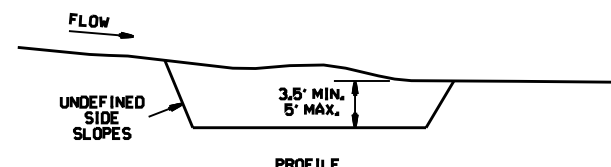
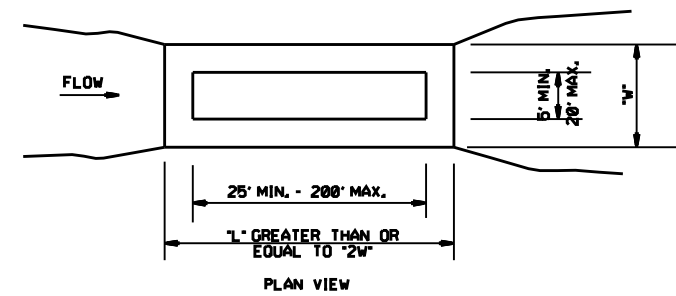


DIVERSION DITCH (E-8)

NOTE:
A T-SECTION SHALL BE USED AT THE INLET
FOR TWO-DIRECTIONAL FLOW.
AN ELBOW SHALL BE USED FOR
ONE-DIRECTIONAL FLOW.



SLOPE DRAIN (E-12)



SEDIMENT BASIN (E-14)

| | |
|--------|---|
| 6-2-94 | Revised E-8 & E-12r Added E-14 & Deleted E-13 |
| 4-1-93 | ISSUED |
| DATE | REVISION |
| | FILMED |

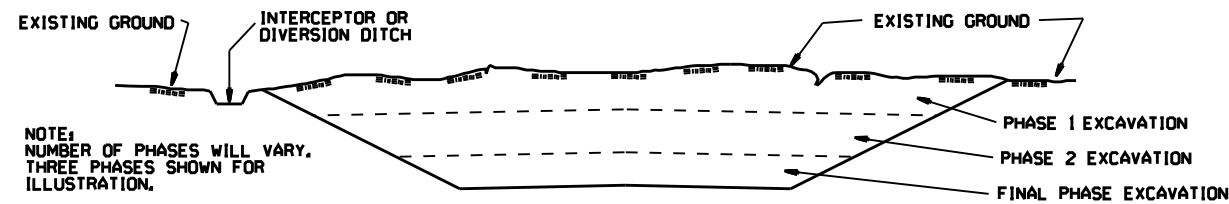
ARKANSAS STATE HIGHWAY COMMISSION
TEMPORARY EROSION
CONTROL DEVICES
STANDARD DRAWING TEC-2

CLEARING AND GRUBBING

CONSTRUCTION SEQUENCE

1. PLACE PERIMETER CONTROLS (I.E. SILT FENCES, DIVERSION DITCHES, SEDIMENT BASINS, ETC.)
2. PERFORM CLEARING AND GRUBBING OPERATION.

EXCAVATION



NOTE:
NUMBER OF PHASES WILL VARY.
THREE PHASES SHOWN FOR
ILLUSTRATION.

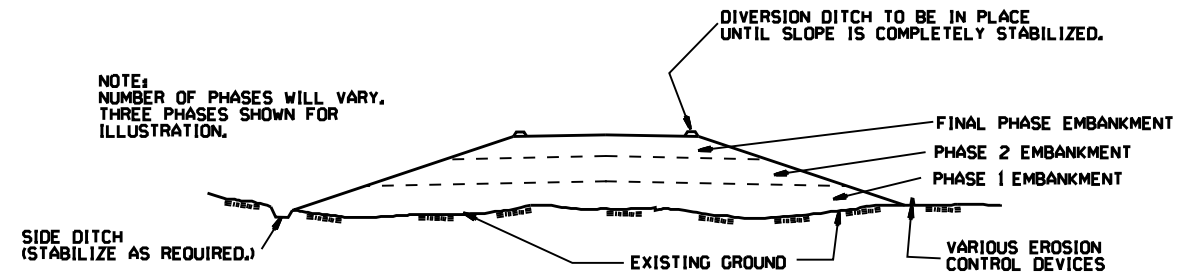
GENERAL NOTE

ALL CUT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE EXCAVATED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

CONSTRUCTION SEQUENCE

1. EXCAVATE AND STABILIZE INTERCEPTOR AND/OR DIVERSION DITCHES.
2. PERFORM PHASE 1 EXCAVATION, PLACE PERMANENT OR TEMPORARY SEEDING.
3. PERFORM PHASE 2 EXCAVATION, PLACE PERMANENT OR TEMPORARY SEEDING.
4. PERFORM FINAL PHASE OF EXCAVATION, PLACE PERMANENT OR TEMPORARY SEEDING, STABILIZE DITCHES, CONSTRUCT DITCH CHECKS, DIVERSION DITCHES, SEDIMENT BASINS, OR OTHER EROSION CONTROL DEVICES AS REQUIRED.

EMBANKMENT



NOTE:
NUMBER OF PHASES WILL VARY.
THREE PHASES SHOWN FOR
ILLUSTRATION.

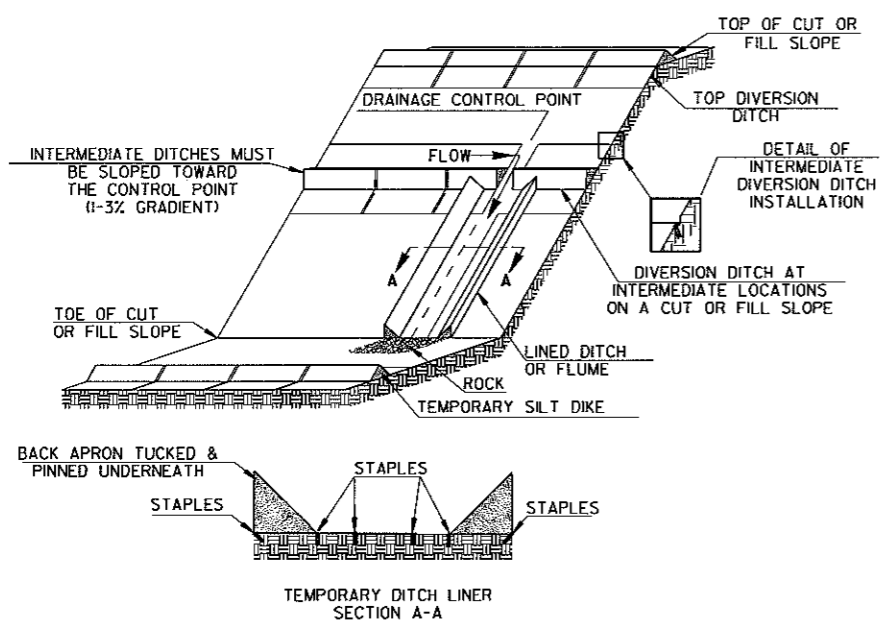
GENERAL NOTE

ALL EMBANKMENT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE CONSTRUCTED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

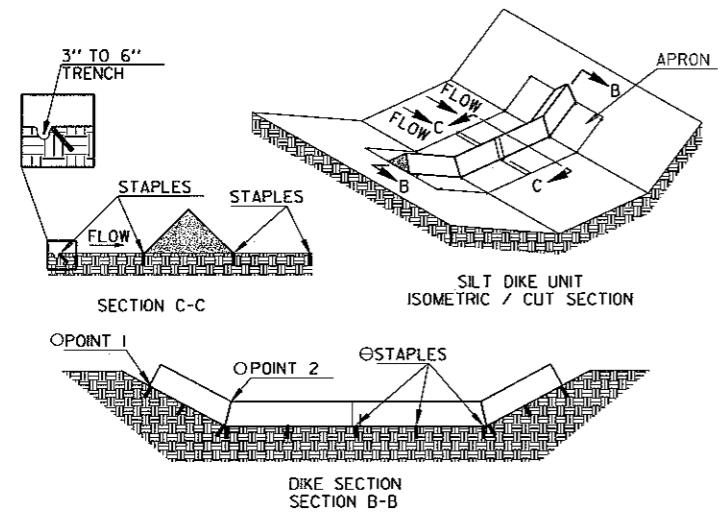
CONSTRUCTION SEQUENCE

1. CONSTRUCT DIVERSION DITCHES, DITCH CHECKS, SEDIMENT BASINS, SILT FENCES, OR OTHER EROSION CONTROL DEVICES AS SPECIFIED.
2. PLACE PHASE 1 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING, PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
3. PLACE PHASE 2 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING, PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
4. PLACE FINAL PHASE OF EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING, PLACE DIVERSION DITCHES AND SLOPE DRAINS AND MAINTAIN UNTIL ENTIRE SLOPE IS STABILIZED.

| | | | |
|----------|--------------------|-----------------------------------|--------|
| | | ARKANSAS STATE HIGHWAY COMMISSION | |
| | | TEMPORARY EROSION CONTROL DEVICES | |
| | | STANDARD DRAWING TEC-3 | |
| 11-03-94 | CORRECTED SPELLING | | |
| 6-2-94 | Drawn & Issued | 6-2-94 | FILMED |
| DATE | REVISION | | |

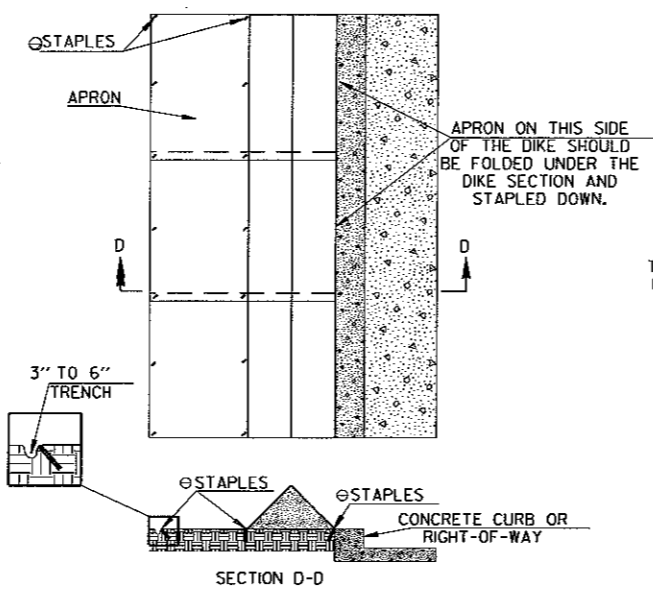


TRIANGULAR SILT DIKE INSTALLATION FOR DIVERSION DITCH AND/OR DITCH LINER

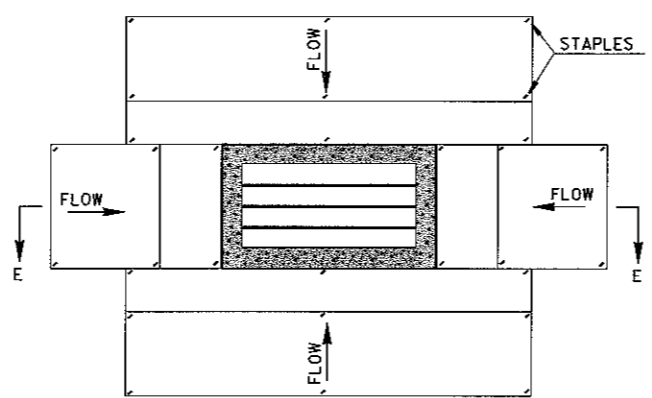


TRIANGULAR SILT DIKE INSTALLATION FOR ROADWAY DITCH OR DRAINAGE DITCH

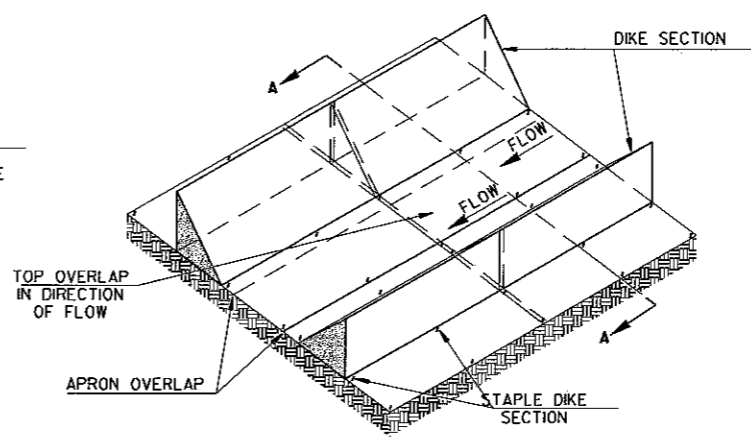
○ POINT "1" MUST BE HIGHER THAN POINT "2" TO ENSURE THAT WATER FLOWS OVER THE DIKE AND NOT AROUND THE ENDS.
 ⊗ STAPLES SHALL BE PLACED WHERE THE UNITS OVERLAP AND IN THE CENTER OF THE UNIT AS SHOWN ON THE DIAGRAM.



TRIANGULAR SILT DIKE INSTALLATION FOR CONTINUOUS BARRIER



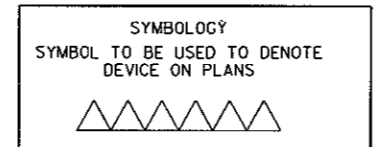
TRIANGULAR SILT DIKE INSTALLATION FOR DROP INLETS



TRIANGULAR SILT DIKE INSTALLATION FOR TEMPORARY DITCH LINER

GENERAL NOTES

1. THIS WORK SHALL CONSIST OF FURNISHING, INSTALLING, AND MAINTAINING THE TRIANGULAR SILT DIKE. THE DIKES SHALL BE USED AS A CONTINUOUS LINE BARRIER AT THE TOE OF SLOPE OR ACROSS THE ROADWAY DITCH TO CONTAIN SEDIMENT AND MINIMIZE EROSION, OR AS DIRECTED BY THE ENGINEER. THESE DIKES SHALL BE INSTALLED AND LOCATED AS SOON AS CONSTRUCTION WILL ALLOW OR AS DIRECTED BY THE ENGINEER.
2. TRIANGULAR SILT DIKE SHALL BE TRIANGULAR SHAPED HAVING A HEIGHT OF AT LEAST 8" TO 10" IN THE CENTER WITH EQUAL SIDES AND A 16" TO 20" BASE. THE TRIANGULAR SHAPED INNER MATERIAL SHALL BE URETHANE FOAM. THE OUTER COVER SHALL BE A WOVEN GEOTEXTILE FABRIC PLACED AROUND THE INNER MATERIAL & ALLOWED TO EXTEND BEYOND BOTH SIDES OF THE TRIANGLE 24" TO 36". THIS FABRIC SHOULD BE MILDEW RESISTANT, ROT-PROOF AND RESISTANT TO HEAT AND ULTRAVIOLET RADIATION MEETING REQUIREMENTS FOR SEDIMENT CONTROL IN AASHTO M288. THE DIKES SHALL BE ATTACHED TO THE GROUND WITH WIRE STAPLES. THE STAPLES SHALL BE NO. 11 GAUGE WIRE AND BE AT LEAST 6" TO 8" LONG. STAPLES SHALL BE PLACED AS SHOWN ON THESE DETAILS.
3. ACCEPTED TRIANGULAR SILT DIKE, MEASURED AS PROVIDED ABOVE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID FOR TRIANGULAR SILT DIKE. PRICE BID WILL INCLUDE THE COST OF FURNISHING THE DIKES, INSTALLING, MAINTAINING AND REMOVAL WHEN DIRECTED BY THE ENGINEER.



NOTE: SILT DIKE SHOULD ONLY BE USED FOR DROP INLETS IN SUMP LOCATIONS.

| | | | |
|----------|-------------------------|--------|-----------------------------------|
| | | | ARKANSAS STATE HIGHWAY COMMISSION |
| | | | TEMPORARY EROSION CONTROL DEVICES |
| 7-26-12 | REVISED GENERAL NOTE 2. | | STANDARD DRAWING TEC-4 |
| 12-15-11 | ISSUED | | |
| DATE | REVISION | FILMED | |