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4472 3 42

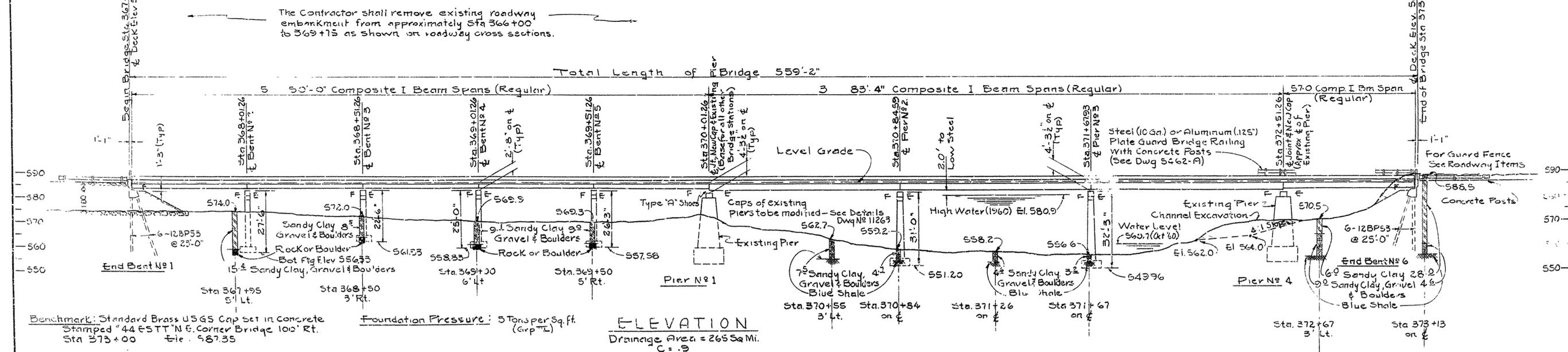
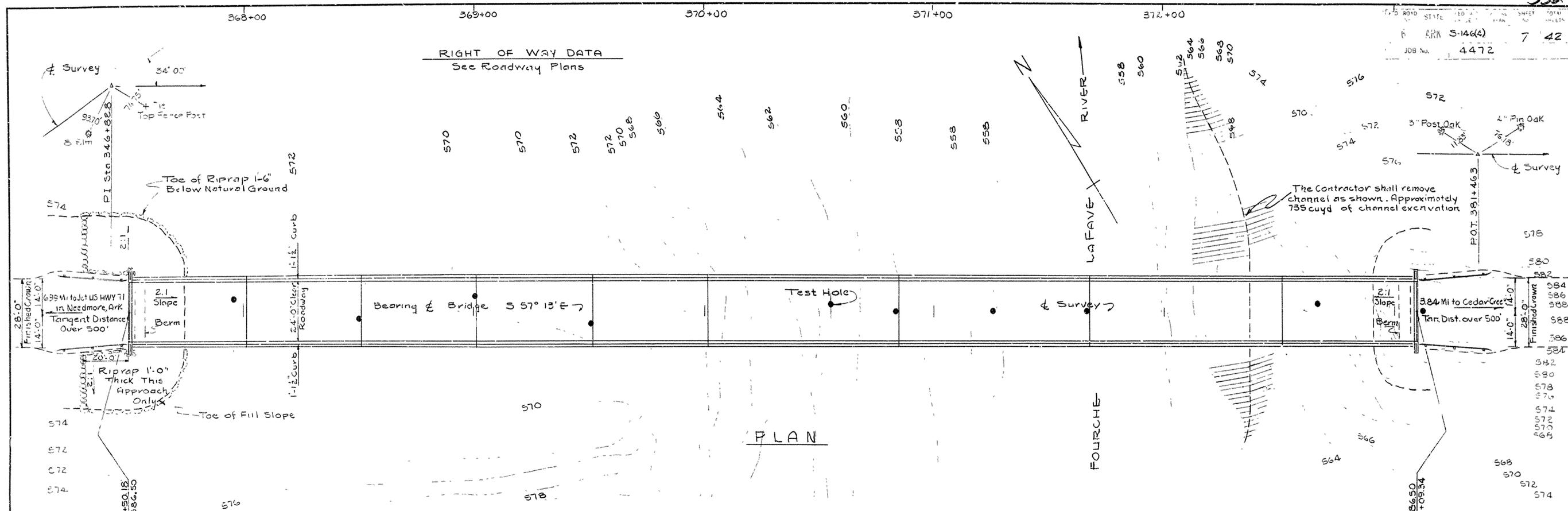
SCHEDULE OF BRIDGE QUANTITIES-JOB 4472

BRIDGE No	CODE No	BRIDGE-NAME PLATE-TITLE	ITEM NUMBER	801	801	SP # 802	SP # 802	803	804	SP 805-7	SP # 806	812	817	1006	
			ITEM	COMMON EXCAVATION FOR STRUCTURES	ROCK EXCAVATION FOR STRUCTURES	CLASS A CONCRETE	CLASS S CONCRETE	REINFORCING STEEL	STEEL BEARING PILING (12 BP 53)	STEEL OR ALUMINUM PLATE GUARD BRIDGE RAILING	STRUCTURAL STEEL IN BEAM SPANS	BRIDGE NAME- PLATES (TYPE C)	RIPRAP	REMOVAL OF EXISTING BRIDGE- STRUCTURES	
		UNIT OF BRIDGE	UNIT	CU. YD.	CU. YD.	CU. YD.	CU. YD.	LB.	LIN. FT.	LIN. FT.	LB.	PLATE	CU YD	COMPLETE ITEM	
3447	X051	FOURCHE LA FAVE RIVER	END BENT No 1	36			12.04	1487	138		597	1	105		
			BENT No. 2, 3 4 & 5	290	39	63.34	30.98	9510							
			PIER No. 1 & 4					12.40	980						
			PIER No. 2 & 3	69	33	76.26	15.63	8232							
			END BENT No. 6	37			12.27	1487	150			597			
			FIVE 80'-0" I-BEAM SPANS				163.50	29,178		500.1	128,682				
			THREE 83'-4" I-BEAM SPANS				161.14	29,436		500.0	224,846				
			ONE 57'-0" I-BEAM SPAN				37.14	6650		114.2	33,768				
			TOTALS FOR BRIDGE 3447	432	72	139.60	445.10	86,960	288	1,114.3	388,490	1	105	100%	
TOTALS FOR JOB 4472				432	72	139.60	445.10	86,960	288	1,114.3	388,490	1	105	100%	

SCHEDULE OF BRIDGE QUANTITIES
BRIDGE OVER
FOURCHE LAFAVE RIVER
SCOTT COUNTY
ROUTE 28 SEC. 2
ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.
DRAWN BY: *EM* DATE: 1 Feb 61
TRACED BY: _____ DATE: _____
CHECKED BY: *W* DATE: 2-1-61
BRIDGE NO. 3447 DRAWING NO. 11260

L.P. Wilson
BRIDGE ENGINEER



Benchmark: Standard Brass USGS Cap set in concrete stamped "44 ESTT N E. Corner Bridge 100' Rt. Sta 373+00 Elev. 587.35

GENERAL NOTES

All concrete to be poured in the dry. All exposed corners to be chamfered 3/4" unless otherwise noted.

Rock excavation shall be made to neat lines of concrete footings. Care shall be exercised to avoid shattering of rock faces by excessive blasting. Concrete in footings shall be poured directly against excavated surfaces of rock. Excavate a minimum of 2'0" into solid rock or shale.

In general all construction joints in bents and piers shall be horizontal and shall be provided with keys not less than 1 1/2" high covering the middle third of both dimensions.

Filling in End Bents to be 12" 6P @ 53# driven to a minimum capacity of 36 tons per pile and to the material designated as rock or shale in the sounding logs. Order pile lengths shown. Cut-off or build-up, if necessary, to be paid for in accordance with Section 504 of the Specifications.

Filling in both End Bents to be driven with the embankment in place.

Omit end drains on each side of roadway in both end spans.

The Contractor shall remove, in accordance with Section 1006 of the Specifications, the existing bridge consisting of three

90-ft. reinforced concrete arch spans, 16' roadway, located approximately 100' upstream from new bridge.

For details of End Bents see Dwg. No. 5460A.

For Interior Bents, Piers and Pier Remodeling, see Dwg. No. 11262, 11263, and 11264.

For Superstructure details see Dwg. No. 5460, 5462, & 5462A.

Specifications: Arkansas State Highway Commission Standard Specifications for Highway Construction, Edition of 1959, and designated Special Provisions.

Work of remodeling Piers 1 & 4 in accordance with Dwg. No. 11263 will not be paid for directly, but will be considered subsidiary to the other items of the contract.

DESIGN SPECIFICATIONS:

Live Loading	AASPC 1957 H-15
Unit Stresses	
Class A Concrete (n=15)	£40 psi
Class S Concrete (n=10)	1,200 psi
Reinforcing Steel	20,000 psi
Structural Steel	18,000 psi

LAYOUT OF BRIDGE OVER FOURCHE LA FAVE RIVER

SCOTT COUNTY

ROUTE 28 SEC. 2

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

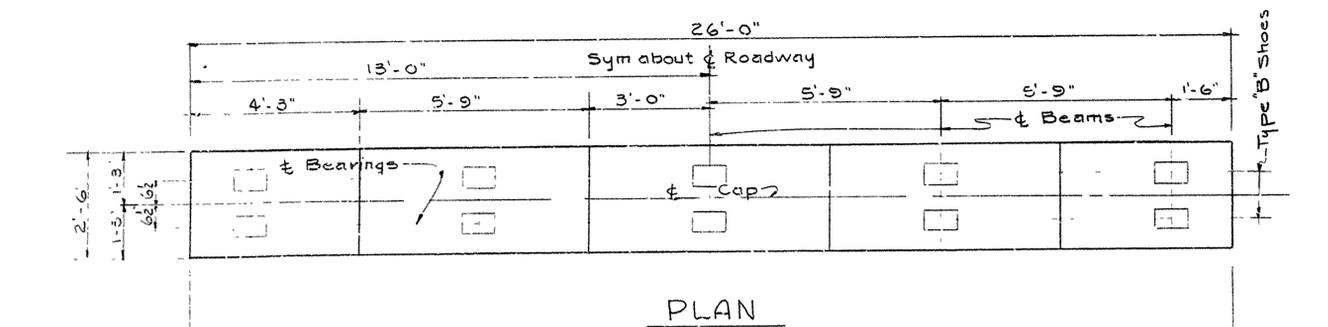
DRAWN BY: *JEM* DATE: 19 Dec 60

TRACED BY: *JEM* DATE: 1-3-61

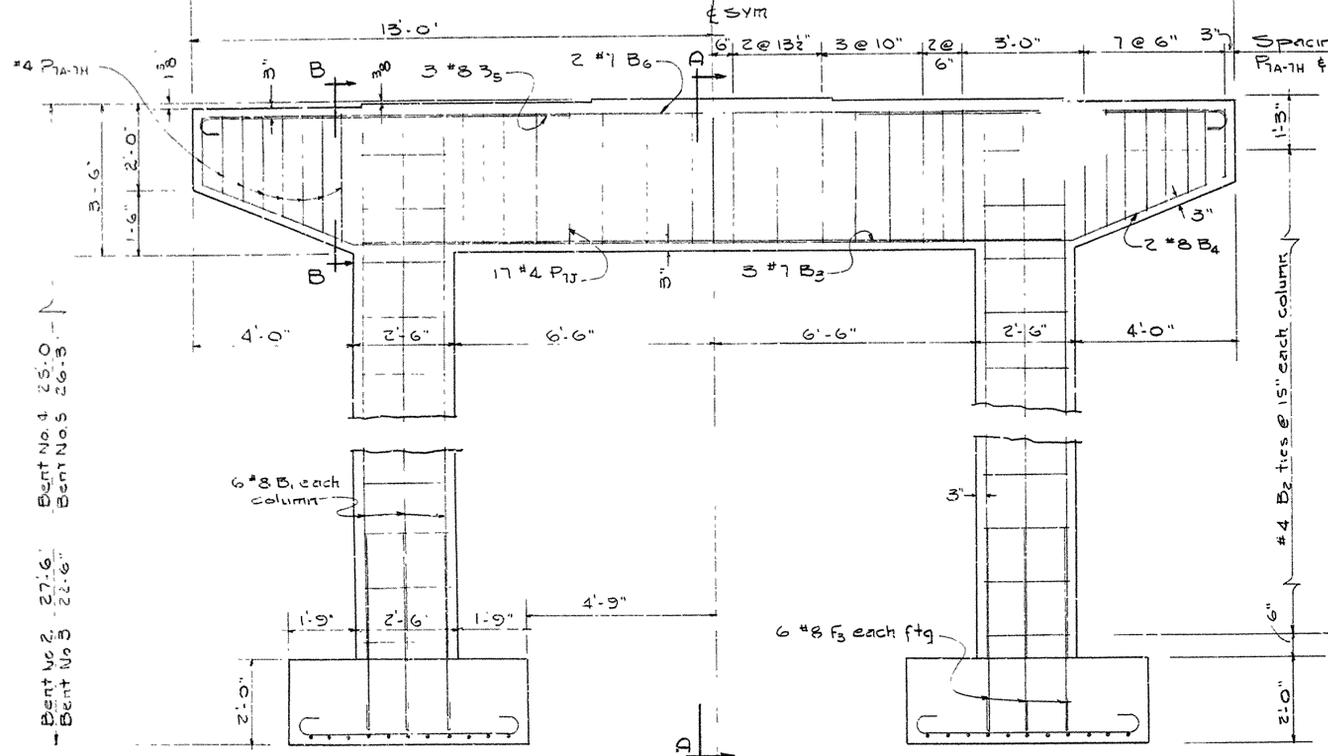
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BRIDGE NO. 3447 DRAWING NO. 11261

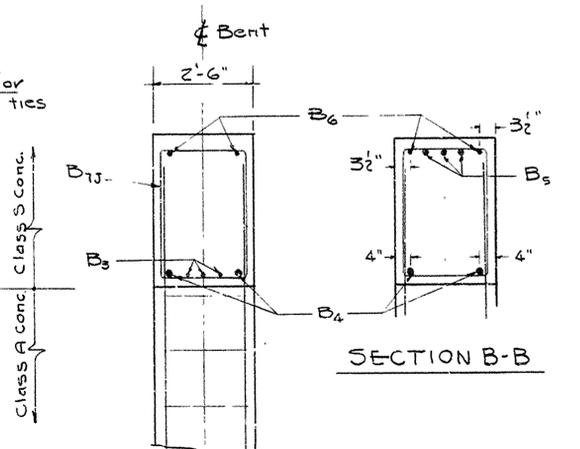
J. P. Carlson
BRIDGE ENGINEER



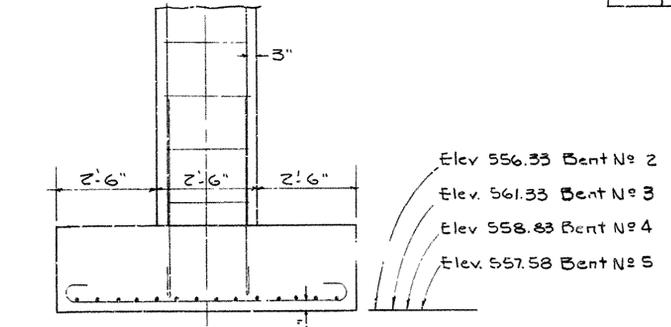
PLAN



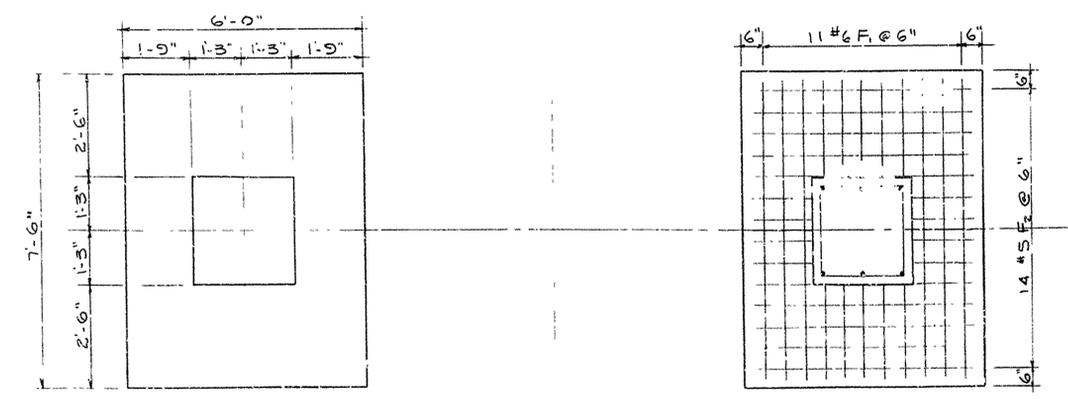
ELEVATION



SECTION B-B



SECTION A-A



FOOTING PLAN

BAR LIST FOR EACH BENT

BENT NO.	MARK	SIZE	No. REQ'D	LENGTH	A	B	K	"P" Dia	BENDING DIAGRAM
ALL BENTS	F ₁	#6	22	8.5	7.0	-	6	4 1/2	
	F ₂	#5	28	6.9	5.6	-	5	3 3/4	
	F ₃	#8	12	6.3	5.1 1/2	-	9	8	
2	B ₁	#8	12	24.10	-	-	Str.	-	
	B ₂	#4	40	9.3	2.2	2.2	4	1 1/2	
3	B ₁	#8	12	19.10	-	-	Str.	-	
	B ₂	#4	32	9.3	2.2	2.2	4	1 1/2	
4	B ₁	#8	12	22.4	-	-	Str.	-	
	B ₂	#4	36	9.3	2.2	2.2	4	1 1/2	
5	B ₁	#8	12	23.7	-	-	Str.	-	
	B ₂	#4	38	9.3	2.2	2.2	4	1 1/2	
ALL BENTS	P _{1A}	#7	3	17.6	-	-	Str.	-	
	P _{1H}	#8	2	26.0	3.8	1.5	-	8	
	P _{1J}	#4	6	10.9	3.7 1/2	-	9	8	
ALL BENTS	B ₆	#7	2	27.2	25.6	-	7	5 1/4	
	P _{1A}	#4	16	8.0 + 10.8	1.7 1/2 + 2.1 1/2	2.1	4	1 1/2	
	P _{1J}	#4	17	10.9	3.0	2.1	4	1 1/2	

~ Dimensions are in inches center to center of bars ~

GENERAL NOTES

Concrete in footings and columns to be Class A. Concrete in caps to be Class B. All concrete to be poured in the dry. Exposed corners to be chamfered 3/4" unless otherwise noted.

Reinforcing steel to be of deformed bars of Intermediate or Hard Grade steel. Shop lists and bending diagrams shall be submitted and approval secured before fabrication of reinforcing steel is begun.

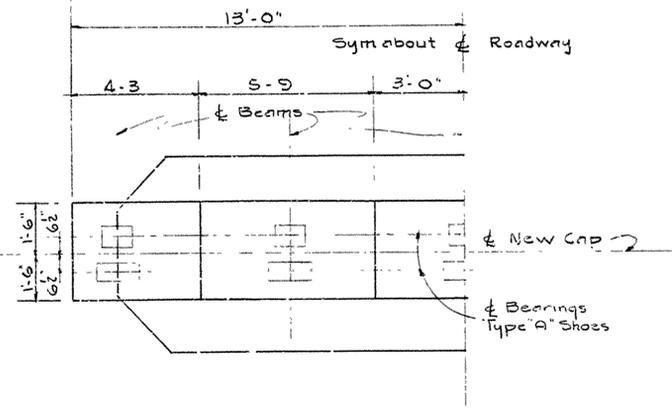
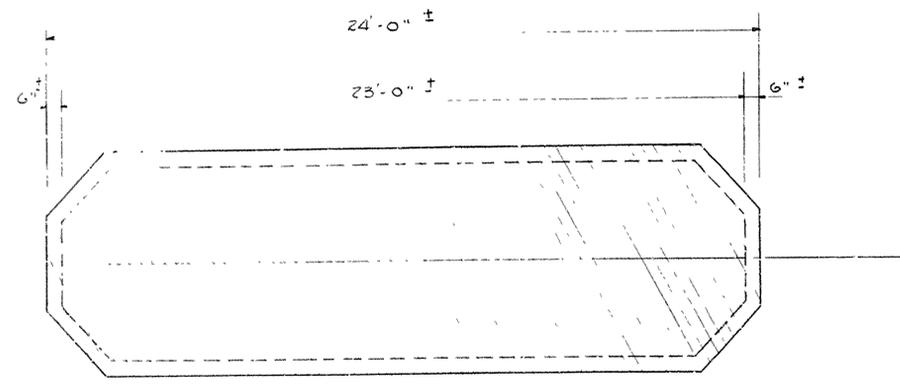
For additional notes and details, see Drawing Nos. 11261, 5450, and 5452.

DETAILS OF BENTS NO. 2, 3, 4 & 5
BRIDGE OVER
FOURCHE-LAFAVE RIVER
 SCOTT COUNTY
 ROUTE 28 SEC. 2
ARKANSAS STATE HIGHWAY COMMISSION
 LITTLE ROCK, ARK.

DRAWN BY: *E.M.* DATE: 12-20-61
 TRACED BY: *J.E.M.* DATE: 1-25-62
 CHECKED BY: *J.E.M.* DATE: 1-25-62

SCALE: 1/2" = 1'-0"
 BRIDGE NO. 34.47 DRAWING NO. 11262

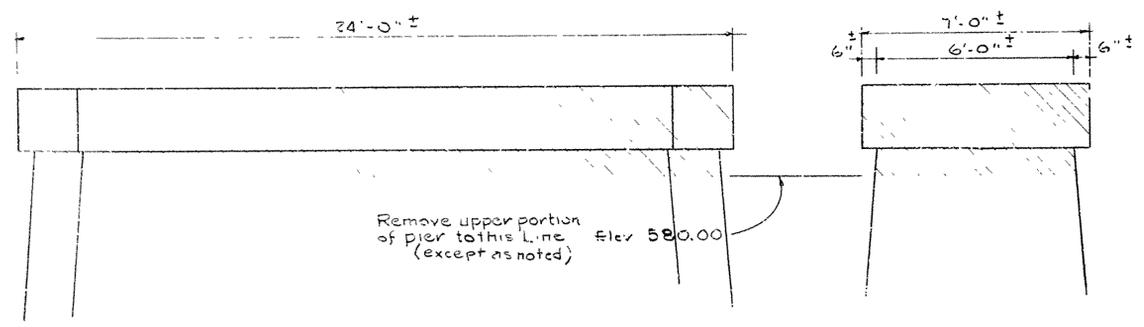
L. P. Wilson
 BRIDGE ENGINEER



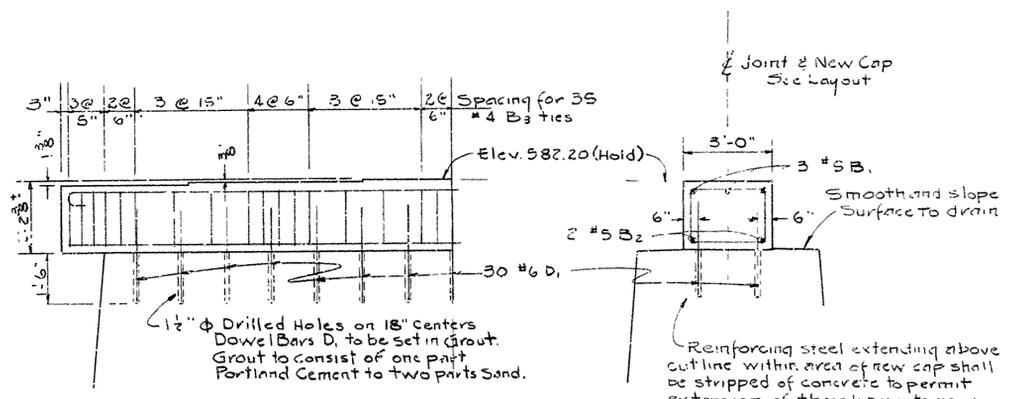
BAR LIST FOR EACH PIER

MARK	SIZE	No.	LENGTH	A	K	Pin. Dia.	BENDING DIAGRAM
B ₁	#5	3	26-9	25-6	5	3/4	
B ₂	#5	2	25-6	-	Str.		
B ₃	#4	35	9-4	2-8	4	1/2	
D ₁	#6	30	3-0	-	Str.		

~ Dimensions are center to center of bars ~



EXISTING PIER
Showing Portion to be Removed



REMODELED PIER
Showing Details of New Cap

Reinforcing steel extending above outline within area of new cap shall be stripped of concrete to permit extension of these bars into new work. All other bars are to be removed to 1" min. below outline. Fill all depressions with grout to form smooth surface.

GENERAL NOTES

Concrete in remodeled Cap to be Class 5. The concrete to be poured in the fry. All exposed corners shall be chamfered 3/4".

Reinforcing steel to be deformed bars of Intermediate or Hard Grade steel.

For additional notes and details see Drawing Nos. 11261, 5460 and 5462.

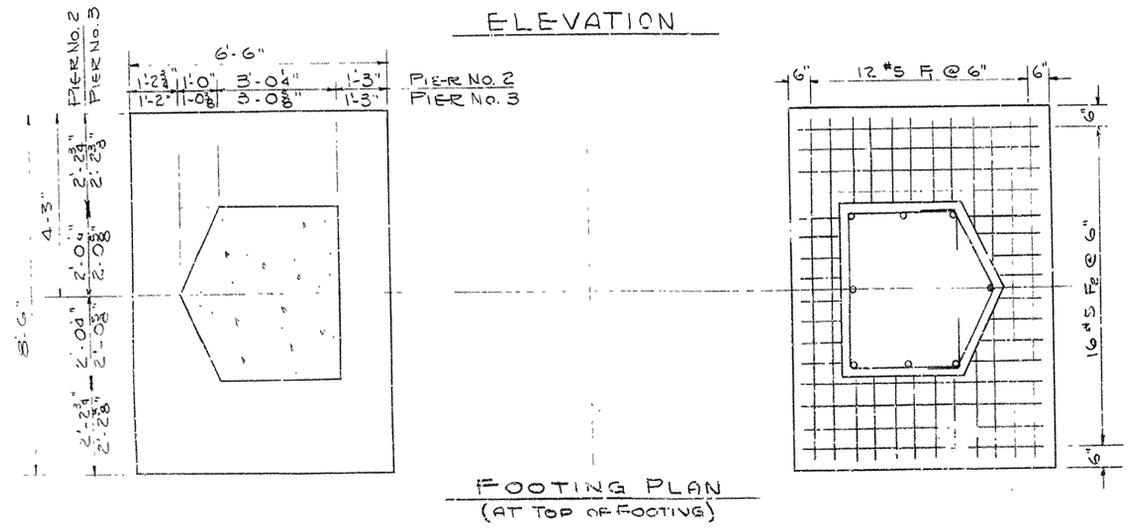
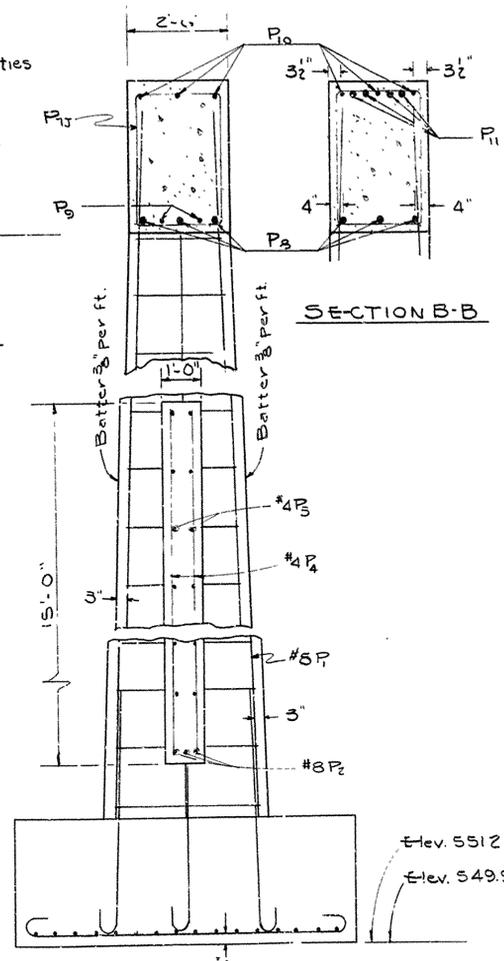
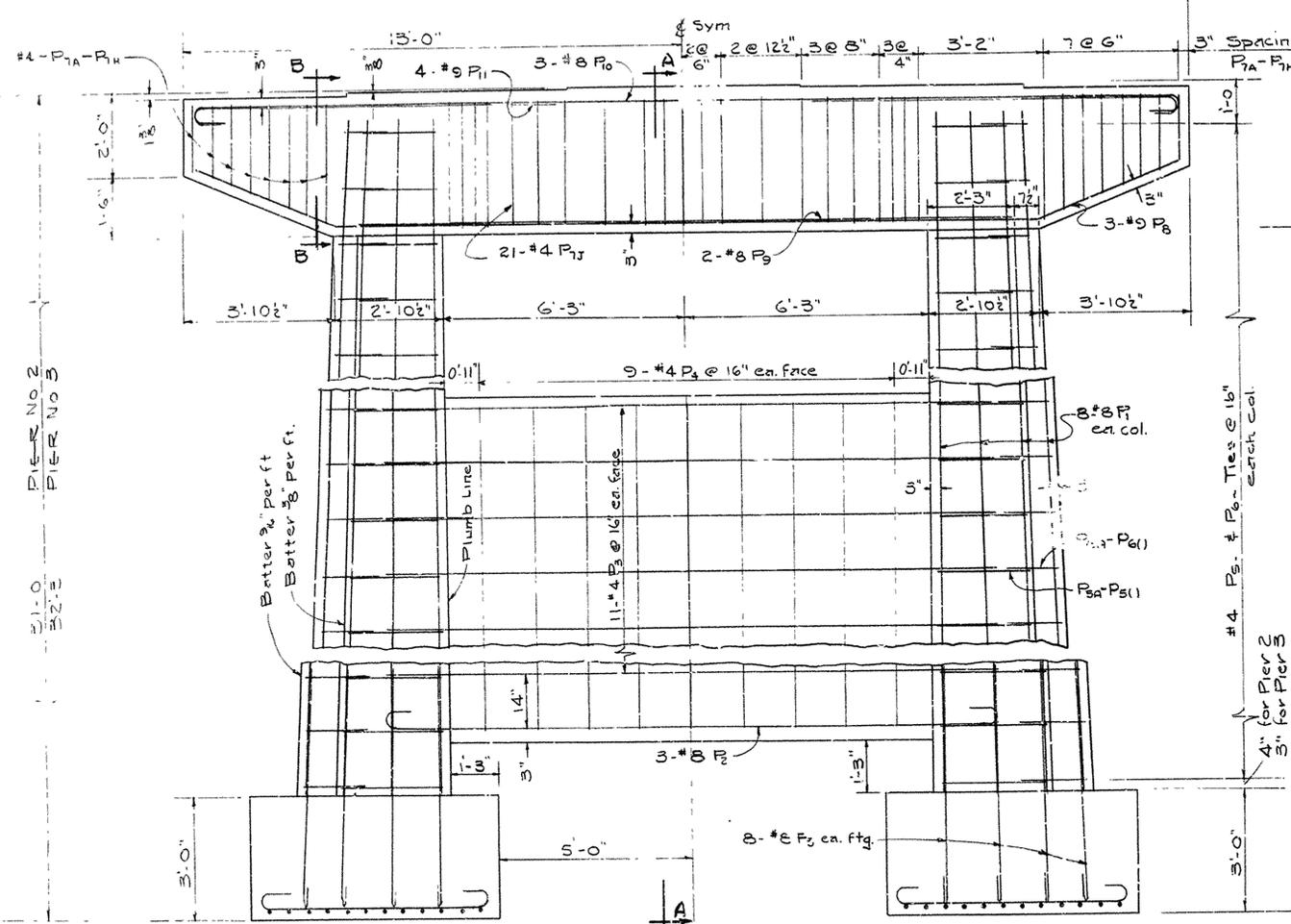
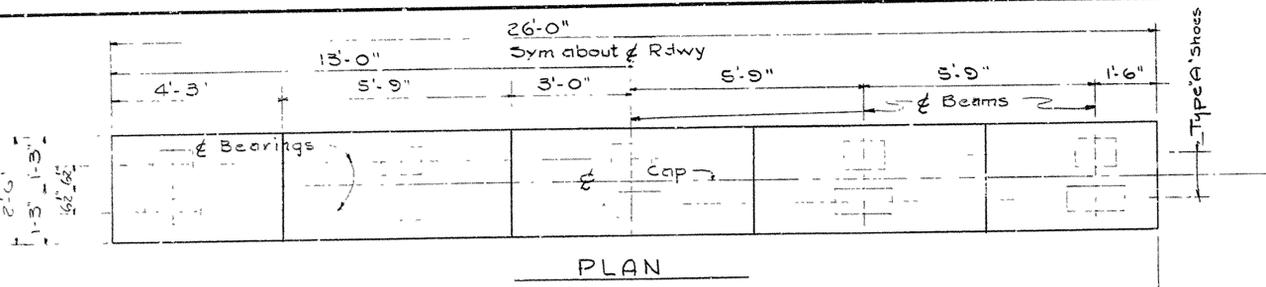
Remodeling of Piers 1 and 4 consisting of the removal of the upper portion of piers, drilling holes and grouting down bars into place, and finishing of surfaces as shown, shall not be paid for directly, but will be considered subsidiary to the other items of the contract.

**DETAILS FOR REMODELING
EXISTING PIERS NO 1 & 4
BRIDGE OVER
FOURCHE LAFAYE RIVER,
SCOTT COUNTY
ROUTE 28 SEC. 2
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.**

DRAWN BY: *J.M.* DATE: 12/26/61
TRACED BY: _____ DATE: _____ SCALE: 3/8" = 1'-0"
CHECKED BY: J.E.M. DATE: 1-25-62
BRIDGE NO. 3447 DRAWING NO. 11263

J.P. Wilson
BRIDGE ENGINEER

FED. ROAD No.	STATE	FED. AID PROJECT	FISCAL YEAR	SHEET No.	TOTAL SHEETS
6	ARK.	S-146(4)		10	42
JOB No.		4472			



BAR LIST FOR EACH PIER

Mark	SIZE	No. R. PIER 2	No. R. PIER 3	LENGTH	A	B	K	P' Piv. Dia.	BENDING DIAGRAM
F ₁	#5	24	24	9-3	8-0		5	3 3/4	
F ₂	#5	32	32	7-3	6-0		5	3 3/4	
F ₃	#8	16	16	7-1	5-1 1/2		9	8	
P ₁	#8			27-4				Str.	
P ₂	#8			28-7				Str.	
P ₃	#8	3	3	18-1	15-10		9	8	
P ₄	#8	18	18	14-6				Str.	
P _{5A} -P _{5U}	#4	42		8-5 to 11-9	1-11 3/8 to 3-7 3/8	1-10 1/2 to 2-8 1/2		1 1/2	
P _{5A} -P _{5V}	#4		44	8-5 to 11-11	1-11 3/8 to 3-8 3/8	1-10 1/2 to 2-9		1 1/2	
P _{6A} -P _{6U}	#4	42		5-5 to 7-3 1/2	1-1 to 2-0 3/8			1 1/2	
P _{6A} -P _{6V}	#4		44	5-5 to 7-4 1/2	1-1 to 2-1			1 1/2	
P _{7A} -P _{7H}	#4	16	16	8-0 to 10-8	1-7 1/2 to 2-1 1/2	2-1	4	1 1/2	
P _{7J}	#4	21	21	10-9	3-0	2-1	4	1 1/2	
P ₈	#9	3	3	26-0	3-8	1-5		9	
P ₉	#8	2	2	17-0				Str.	
P ₁₀	#8	3	3	27-10	25-7		9	8	
P ₁₁	#9	8	8	10-8	9-5		10	9	

~ Dimensions are center to center of bars ~

GENERAL NOTES

Concrete in footings, columns and web wall to be Class A. Concrete in caps to be Class S. All concrete to be poured in the dry. Exposed corners to be chamfered 3/4" unless otherwise noted.

Reinforcing steel to be of deformed bars of Intermediate or Hard Grade Steel. Shop lists and bending diagrams, including bar supports, shall be submitted and approval secured before fabrication of reinforcing steel is begun.

For additional notes and details see Dwg Nos. 11261, 5460 & 5462.

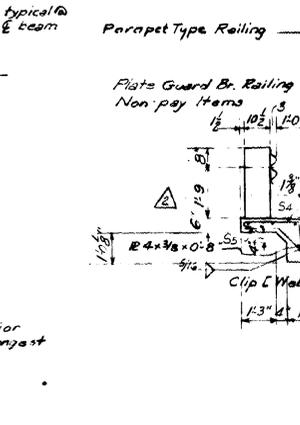
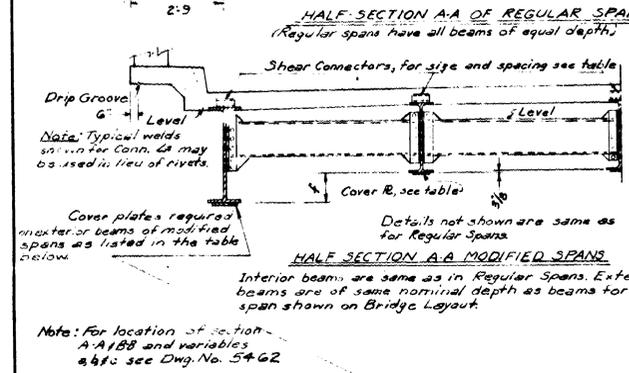
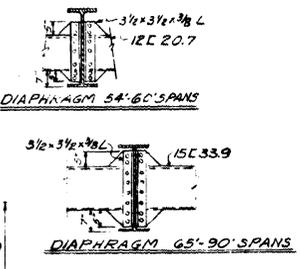
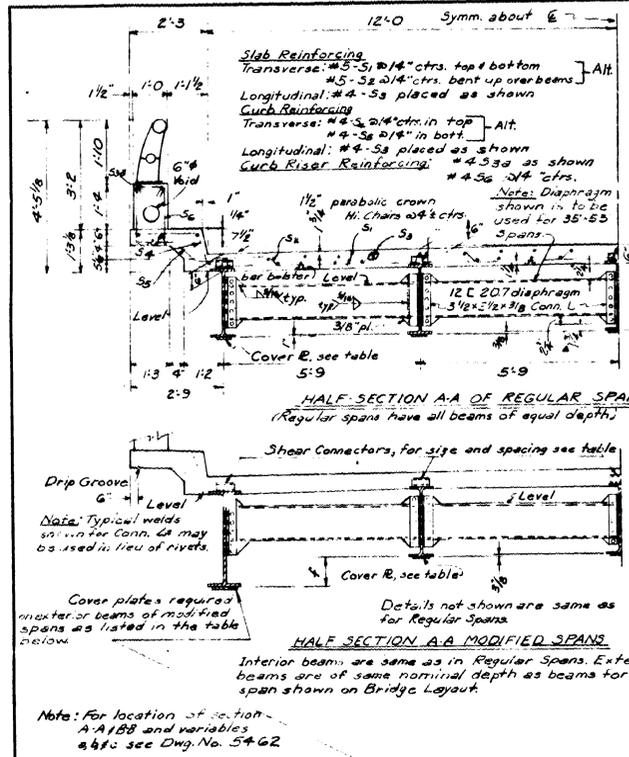
DETAILS OF PIERS NO 2 & 3
 BRIDGE-OVER
 FOURCHE LAFAVER RIVER
 SCOTT COUNTY
 ROUTE 28 SEC. 2
 ARKANSAS STATE HIGHWAY COMMISSION
 LITTLE ROCK, ARK.

DRAWN BY: DM DATE: 20 Dec 60
 TRACED BY: DATE:
 CHECKED BY: J.E.M. DATE: 1-6-61

SCALE: 1/2" = 1'-0"

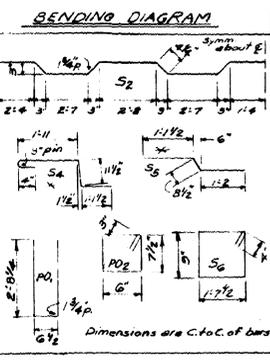
BRIDGE NO. 3447 DRAWING NO. 11264

L.P. Carlson
 BRIDGE DESIGN ENGINEER

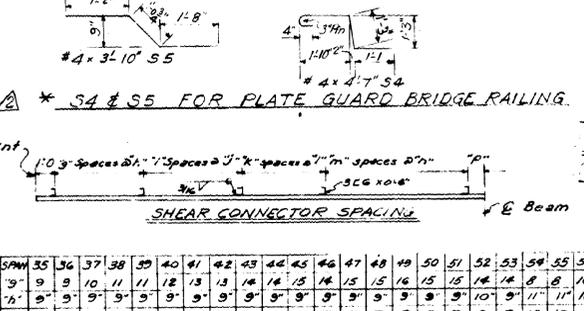
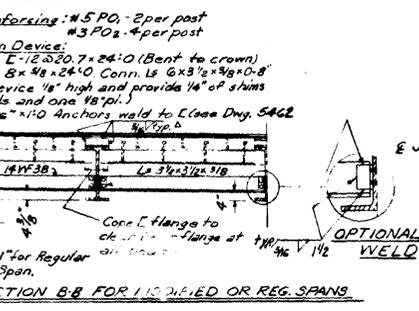


BAR LIST - ONE SPAN

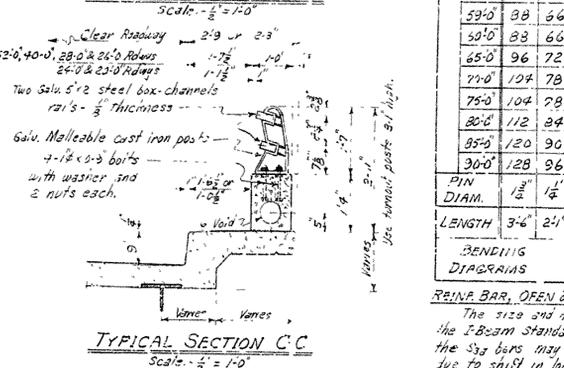
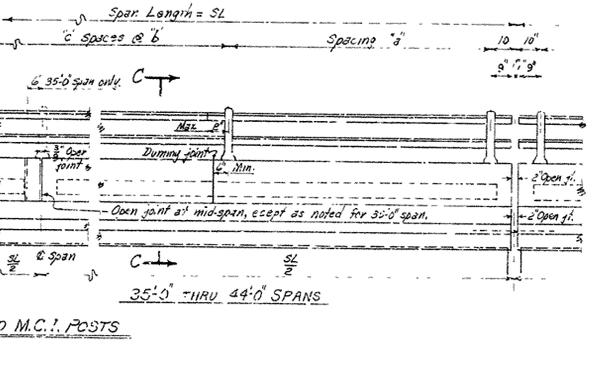
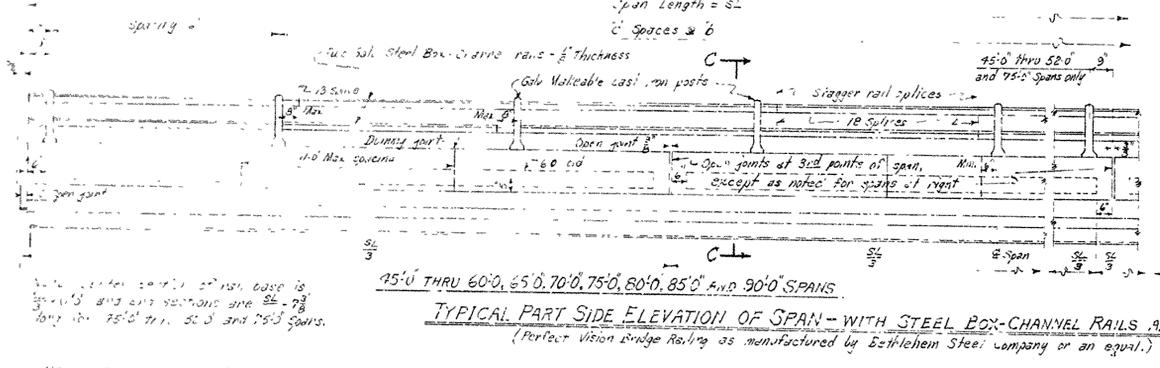
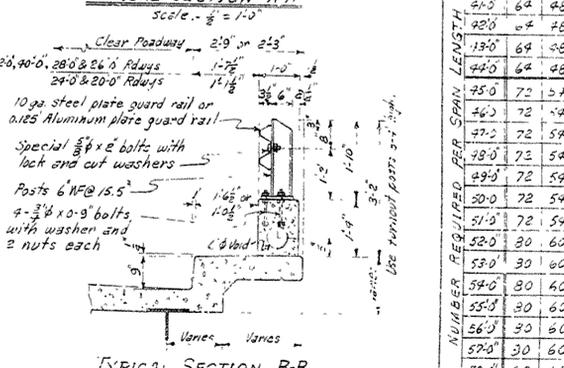
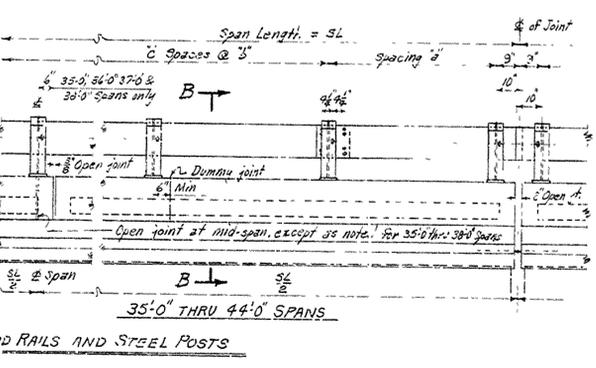
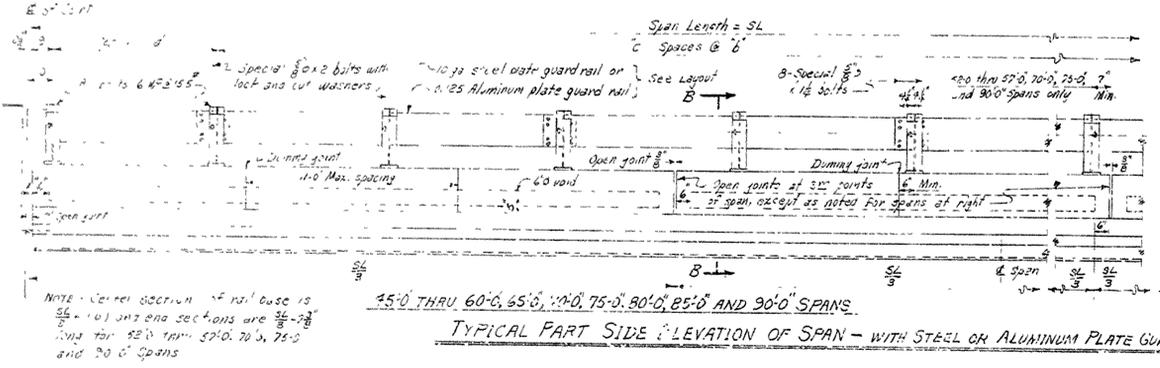
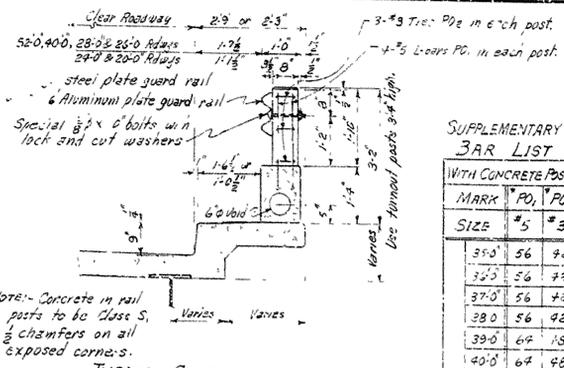
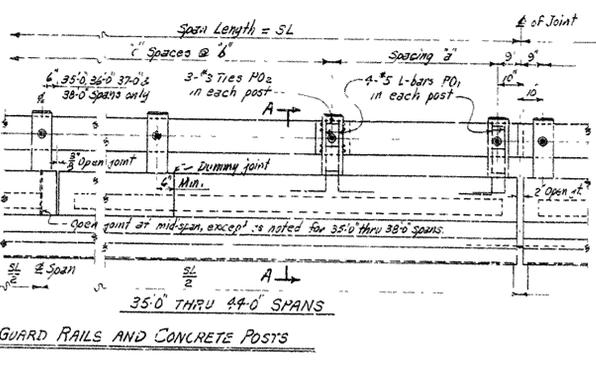
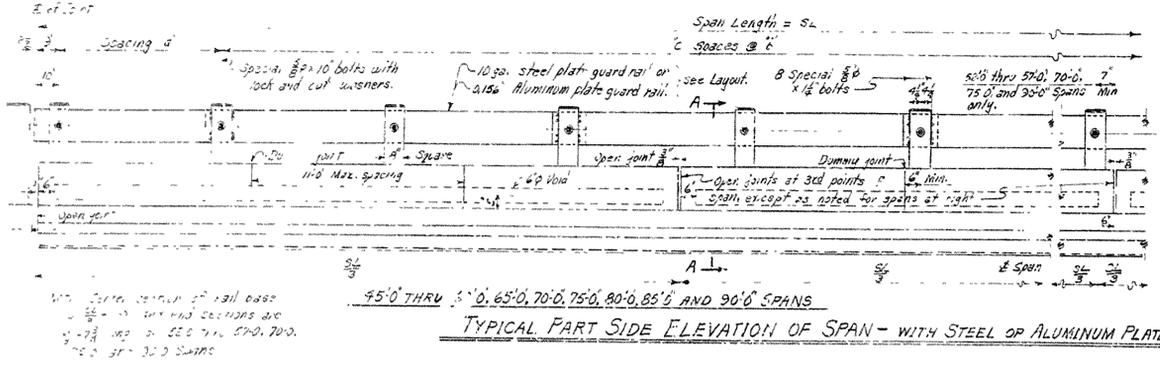
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S ₁	5	25'-0"	5/8"	22	22	24	24	26	26	28	28	30	30	32	32	34	34	36	36	38	38	40	40	42	42	44	44	46	46	48	48	50	50	52	52	54	54	56	56	58	58	60	60	65	70	75	80	85	90
S ₂	5	25'-8"	3/4"	30	30	31	32	33	34	35	36	36	37	38	39	40	41	42	42	43	44	45	46	47	48	48	49	50	51	52	53	54	55	55	56	57	58	59	60	104	104	112	120	130	138	146	154		
S ₃	4	3'-6"	5/8"	47																																													
S ₄	4	5'-2"	3/4"	94																																													
S ₅	4	5'-11"	3/4"	141																																													
S ₆	4	4'-6"	1/2"	62	62	64	64	66	66	68	68	70	70	72	72	74	74	76	76	78	78	80	80	82	82	84	84	86	86	88	88	90	92	94	96	96	98	98	100	102	104	104	112	120	130	138	146	154	
S ₇	4	3'-0"	1/2"	60	60	62	64	66	68	70	72	74	76	78	80	82	84	86	88	90	92	94	96	96	98	98	100	102	104	104	112	120	130	138	146	154													
S ₈	4	5'-4"	1/2"	62	62	64	66	68	70	72	74	76	78	80	82	84	86	88	90	92	94	96	98	98	100	102	104	104	112	120	130	138	146	154															
S ₉	4	3'-0"	3/4"	12																																													
P ₀₁	5	5'-10"	3/4"	28																																													
P ₀₂	5	2'-8"	1/4"	56																																													



SPAN	BEAM	REGULAR SPANS			COVER PLATE	RAILING	DEAD LOAD DEFLECTION	STUD SHEAR CONNECTORS	VALUES OF F AND DEAD LOAD DEFLECTION FOR 24" SIDE BEAMS OF MODIFIED SPANS																																																																																														
		Interior	Exterior	Beam					Interior	Exterior	Beam	Interior	Exterior	Beam	Interior	Exterior	Beam																																																																																						
35	21WF62	4x3/8x14-0	7x8	8-4	8-4	2	5:7	5:8	4	2:2 3/8	3/8	1/2	2:0	2:1	2:2	2:3	2:4	2:5	2:6	2:7	2:8	2:9	2:10	2:11	2:12	2:13	2:14	2:15	2:16	2:17	2:18	2:19	2:20	2:21	2:22	2:23	2:24	2:25	2:26	2:27	2:28	2:29	2:30	2:31	2:32	2:33	2:34	2:35	2:36	2:37	2:38	2:39	2:40	2:41	2:42	2:43	2:44	2:45	2:46	2:47	2:48	2:49	2:50	2:51	2:52	2:53	2:54	2:55	2:56	2:57	2:58	2:59	2:60	2:61	2:62	2:63	2:64	2:65	2:66	2:67	2:68	2:69	2:70	2:71	2:72	2:73	2:74	2:75	2:76	2:77	2:78	2:79	2:80	2:81	2:82	2:83	2:84	2:85	2:86	2:87	2:88	2:89	2:90



SPAN	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	65	70	75	80	85	90																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																
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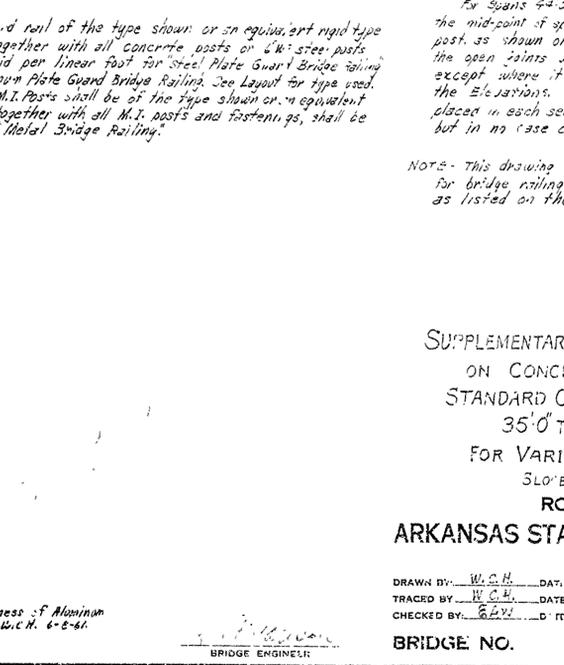
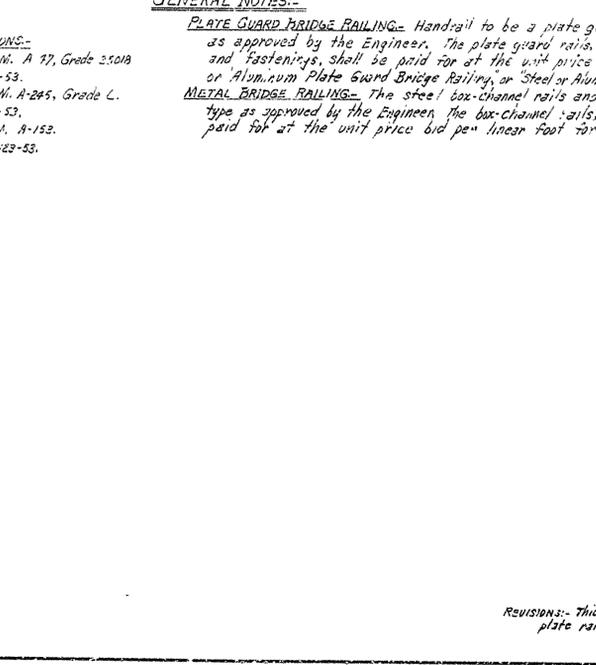
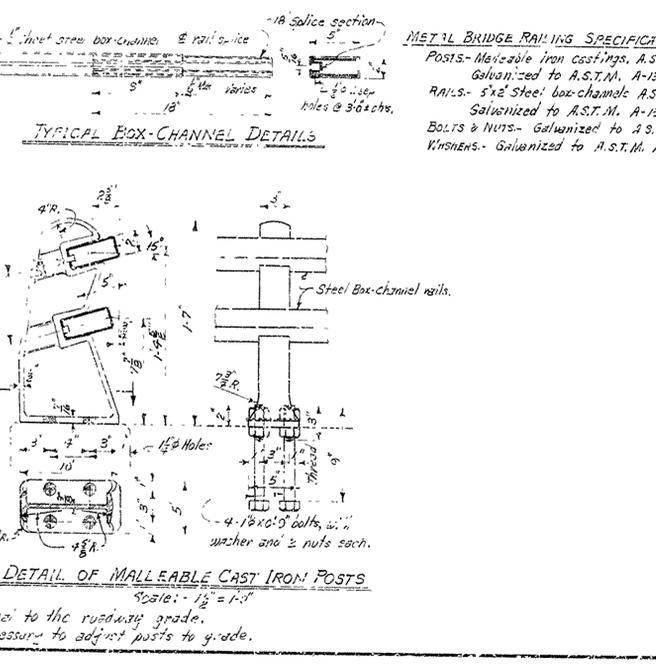
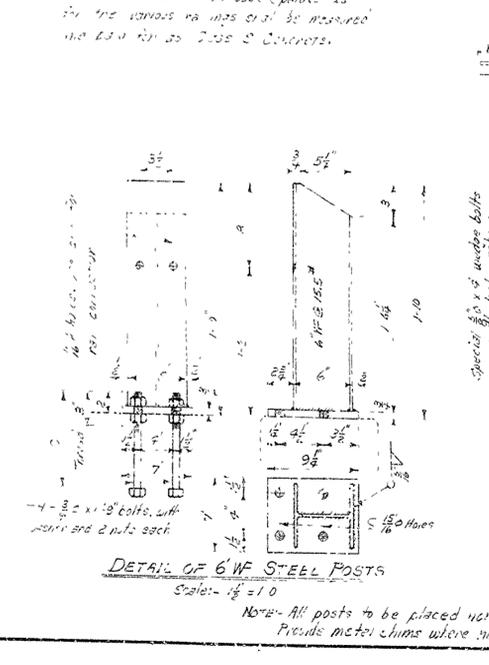


SUPPLEMENTARY 3AR LIST

VARIABLE DIMENSIONS- RAIL POST SPACING

SPAN LENGTH	WITH CONCRETE POSTS		POST SPACING WITH PLATE GUARD RAILS		POST SPACING WITH BOX CHANNEL RAILS	
	MARK	SIZE	END	INTERIOR	END	INTERIOR
35'-0"	56	42	35'-0"	5'-5"	4	5'-3"
40'-0"	56	42	36'-0"	5'-7"	4	5'-0"
45'-0"	56	42	37'-0"	5'-9"	4	4'-8"
50'-0"	56	42	38'-0"	5'-11"	4	4'-6"
55'-0"	56	42	39'-0"	5'-2"	5	5'-5"
60'-0"	56	42	40'-0"	5'-6"	5	5'-4"
65'-0"	56	42	41'-0"	5'-8"	5	5'-2"
70'-0"	56	42	42'-0"	5'-8"	5	5'-0"
75'-0"	56	42	43'-0"	5'-9"	5	4'-8"
80'-0"	56	42	44'-0"	5'-10"	5	4'-8"
85'-0"	56	42	45'-0"	5'-10"	5	4'-6"
90'-0"	56	42	46'-0"	5'-11"	5	4'-6"
95'-0"	56	42	47'-0"	5'-11"	5	4'-4"
100'-0"	56	42	48'-0"	5'-11"	5	4'-2"
105'-0"	56	42	49'-0"	5'-11"	5	4'-0"
110'-0"	56	42	50'-0"	5'-11"	5	3'-8"
115'-0"	56	42	51'-0"	5'-11"	5	3'-6"
120'-0"	56	42	52'-0"	5'-11"	5	3'-4"
125'-0"	56	42	53'-0"	5'-11"	5	3'-2"
130'-0"	56	42	54'-0"	5'-11"	5	3'-0"
135'-0"	56	42	55'-0"	5'-11"	5	2'-8"
140'-0"	56	42	56'-0"	5'-11"	5	2'-6"
145'-0"	56	42	57'-0"	5'-11"	5	2'-4"
150'-0"	56	42	58'-0"	5'-11"	5	2'-2"
155'-0"	56	42	59'-0"	5'-11"	5	2'-0"
160'-0"	56	42	60'-0"	5'-11"	5	1'-8"
165'-0"	56	42	61'-0"	5'-11"	5	1'-6"
170'-0"	56	42	62'-0"	5'-11"	5	1'-4"
175'-0"	56	42	63'-0"	5'-11"	5	1'-2"
180'-0"	56	42	64'-0"	5'-11"	5	1'-0"
185'-0"	56	42	65'-0"	5'-11"	5	0'-8"
190'-0"	56	42	66'-0"	5'-11"	5	0'-6"
195'-0"	56	42	67'-0"	5'-11"	5	0'-4"
200'-0"	56	42	68'-0"	5'-11"	5	0'-2"
205'-0"	56	42	69'-0"	5'-11"	5	0'-0"
210'-0"	56	42	70'-0"	5'-11"	5	0'-0"

REIN. BARS, OPEN & DUMMY JOINT NOTES FOR RAIL BASE:-
 The size and number of #3 bars will be the same as for the I-Beam standard used. The number and length of the #3 bars may vary from that shown on the I-Beam standard to shift in location of open joints in the rail base.
 For spans 44'-0" and under the open joint will occur at the mid-point of span, except where it is shifted to clear post as shown on Elevation. For spans 45'-0" thru 90'-0" the open joints will occur at the third-points of span, except where it is shifted to clear posts, as shown in the Elevation. One or more dummy joints shall be placed in each section of the rail base, (1/10 max. spacing) but in no case closer than 6' from E of post.



REVISIONS: Thickness of Aluminum plate rail, W.C.H. 6-2-61

GENERAL NOTES:-
PLATE GUARD BRIDGE RAILING- Handrail to be a plate guard rail of the type shown or an equivalent rigid type as approved by the Engineer. The plate guard rails, together with all concrete posts or steel posts and fastenings, shall be paid for at the unit price bid per linear foot for steel plate guard bridge railing or Aluminum Plate Guard Bridge Railing, See Layout for type used.
METAL BRIDGE RAILING- The steel box-channel rails and M.I. Posts shall be of the type shown or an equivalent type as approved by the Engineer. The box-channel rails, together with all M.I. posts and fastenings, shall be paid for at the unit price bid per linear foot for Metal Bridge Railing.

METAL BRIDGE RAILING SPECIFICATIONS:-
 Posts- Malleable iron castings, A.S.T.M. A 47, Grade 35018 Galvanized to A.S.T.M. A-123-53.
 Rails- 5 x 2 Steel box-channel, A.S.T.M. A-295, Grade L. Galvanized to A.S.T.M. A-123-53.
 Bolts & NUTS- Galvanized to A.S.T.M. A-152.
 Washers- Galvanized to A.S.T.M. A-123-53.

NOTE: This drawing to be used as a supplementary drawing for bridge railing to the standard or special drawings as listed on the Layout sheet of each bridge.

SUPPLEMENTARY DETAILS OF BRIDGE RAILING ON CONCRETE PARAPET WALL FOR STANDARD COMPOSITE I-BEAM SPANS 35'-0" TO 90'-0" SPANS INCL. FOR VARIOUS WIDTHS OF ROADWAYS SLOPED AND CROWNED ROADWAY

ROUTE SEC.

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: W.C.H. DATE: 11-30-60
 TRACED BY: W.C.H. DATE: 12-6-60
 CHECKED BY: G.W.V. DATE: 1-2-61

BRIDGE NO. DRAWING NO. 5462-4.

ALUMINUM RAILINGS
 20'-0" 24'-0" 26'-0" 28'-0" 30'-0"
 34'-0" 36'-0" 38'-0" 40'-0" 42'-0"
 44'-0" 46'-0" 48'-0" 50'-0" 52'-0"
 54'-0" 56'-0" 58'-0" 60'-0" 62'-0"
 64'-0" 66'-0" 68'-0" 70'-0" 72'-0"
 74'-0" 76'-0" 78'-0" 80'-0" 82'-0"
 84'-0" 86'-0" 88'-0" 90'-0" 92'-0"
 94'-0" 96'-0" 98'-0" 100'-0" 102'-0"
 104'-0" 106'-0" 108'-0" 110'-0" 112'-0"
 114'-0" 116'-0" 118'-0" 120'-0" 122'-0"
 124'-0" 126'-0" 128'-0" 130'-0" 132'-0"
 134'-0" 136'-0" 138'-0" 140'-0" 142'-0"
 144'-0" 146'-0" 148'-0" 150'-0" 152'-0"
 154'-0" 156'-0" 158'-0" 160'-0" 162'-0"
 164'-0" 166'-0" 168'-0" 170'-0" 172'-0"
 174'-0" 176'-0" 178'-0" 180'-0" 182'-0"
 184'-0" 186'-0" 188'-0" 190'-0" 192'-0"
 194'-0" 196'-0" 198'-0" 200'-0" 202'-0"
 204'-0" 206'-0" 208'-0" 210'-0" 212'-0"
 214'-0" 216'-0" 218'-0" 220'-0" 222'-0"
 224'-0" 226'-0" 228'-0" 230'-0" 232'-0"
 234'-0" 236'-0" 238'-0" 240'-0" 242'-0"
 244'-0" 246'-0" 248'-0" 250'-0" 252'-0"
 254'-0" 256'-0" 258'-0" 260'-0" 262'-0"
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 304'-0" 306'-0" 308'-0" 310'-0" 312'-0"
 314'-0" 316'-0" 318'-0" 320'-0" 322'-0"
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