

S-146(4) 3 42
4472 3 42

SCHEDULE OF BRIDGE QUANTITIES-JOB 4472

BRIDGE No.	CODE No	BRIDGE-NAME PLATE-TITLE	ITEM NUMBER		801	801	SP # 802	SP # 802	803	804	SP 805-7	SP # 806	812	817	1006
			ITEM		COMMON EXCAVATION FOR STRUCTURES	ROCK EXCAVATION FOR STRUCTURES	CLASS A CONCRETE	CLASS S CONCRETE	REINFORCING STEEL	STEEL BEARING PILING (12 BP 53)	STEEL OR ALUMINUM PLATE GUARD BRIDGE RAILING	STRUCTURAL STEEL IN BEAM SPANS	BRIDGE- NAME- PLATES (TYPE C)	RIPRAP	REMOVAL OF EXISTING BRIDGE STRUCTURES
					UNIT OF BRIDGE	UNIT	CU. YD.	CU. YD.	CU. YD.	CU. YD.	LB.	LIN. FT.	LIN. FT.	LB.	PLATE
3447	X031	FOURCHE LA FAVE RIVER	END BENT No 1		36			12.04	1487	138		597	1	105	
			BENT No. 2, 3 4 & 5		290	39	63.34	30.98	9510						
			PIER No. 1 & 4					12.40	980						
			PIER No. 2 & 3		69	33	76.26	15.63	8232						
			END BENT No. 6		37			12.27	1487	150		597			
			FIVE 80'-0" I-BEAM SPANS					163.50	29,178		500.1	128,682			
			THREE 83'-4" I-BEAM SPANS					161.14	29,436		500.0	224,846			
			ONE 57'-0" I-BEAM SPAN					37.14	6650		114.2	33,768			
			TOTALS FOR BRIDGE 3447		432	72	139.60	445.10	86,960	288	1,114.3	388,490	1	105	100 %
			TOTALS FOR JOB 4472		432	72	139.60	445.10	86,960	288	1,114.3	388,490	1	105	100 %

SCHEDULE OF BRIDGE QUANTITIES

BRIDGE OVER
FOURCHE LAFAVE RIVER

SCOTT COUNTY
ROUTE 28 SEC. 2

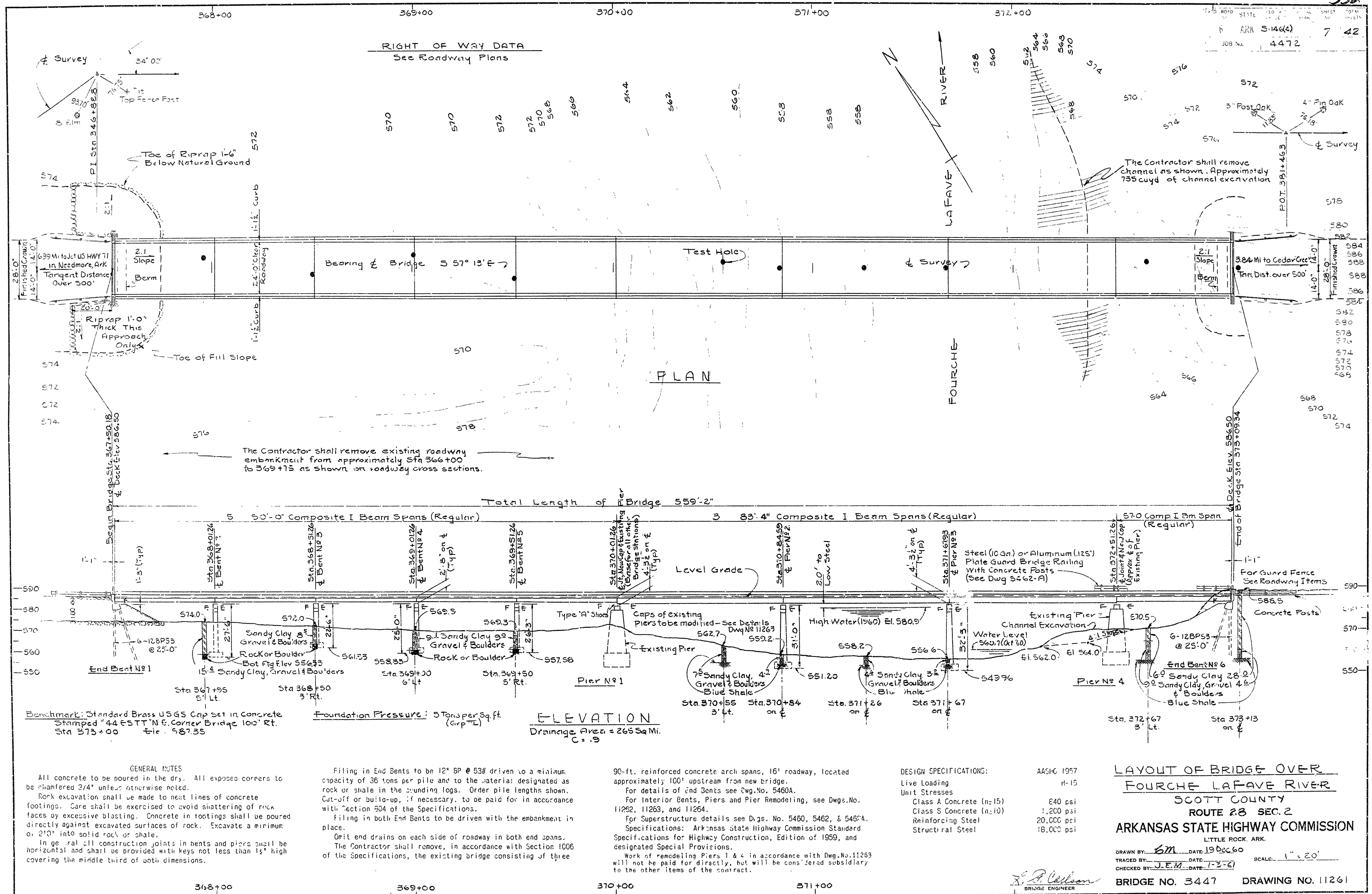
ARKANSAS STATE HIGHWAY COMMISSION

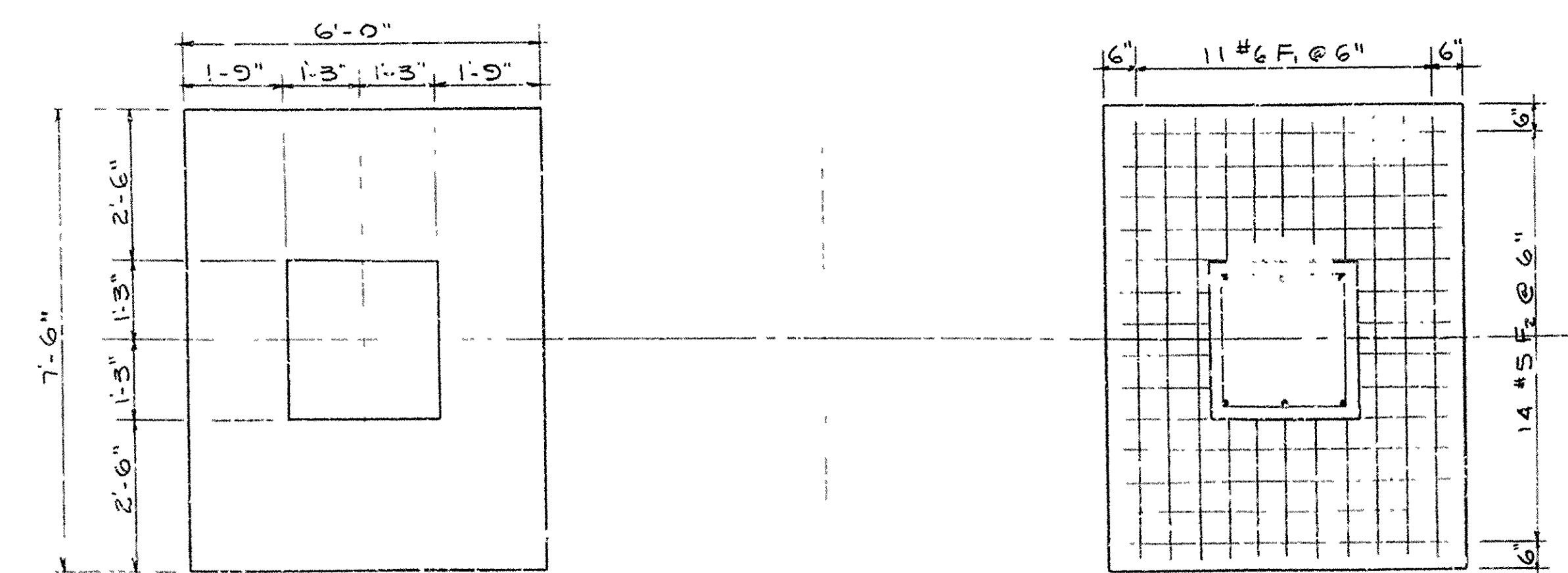
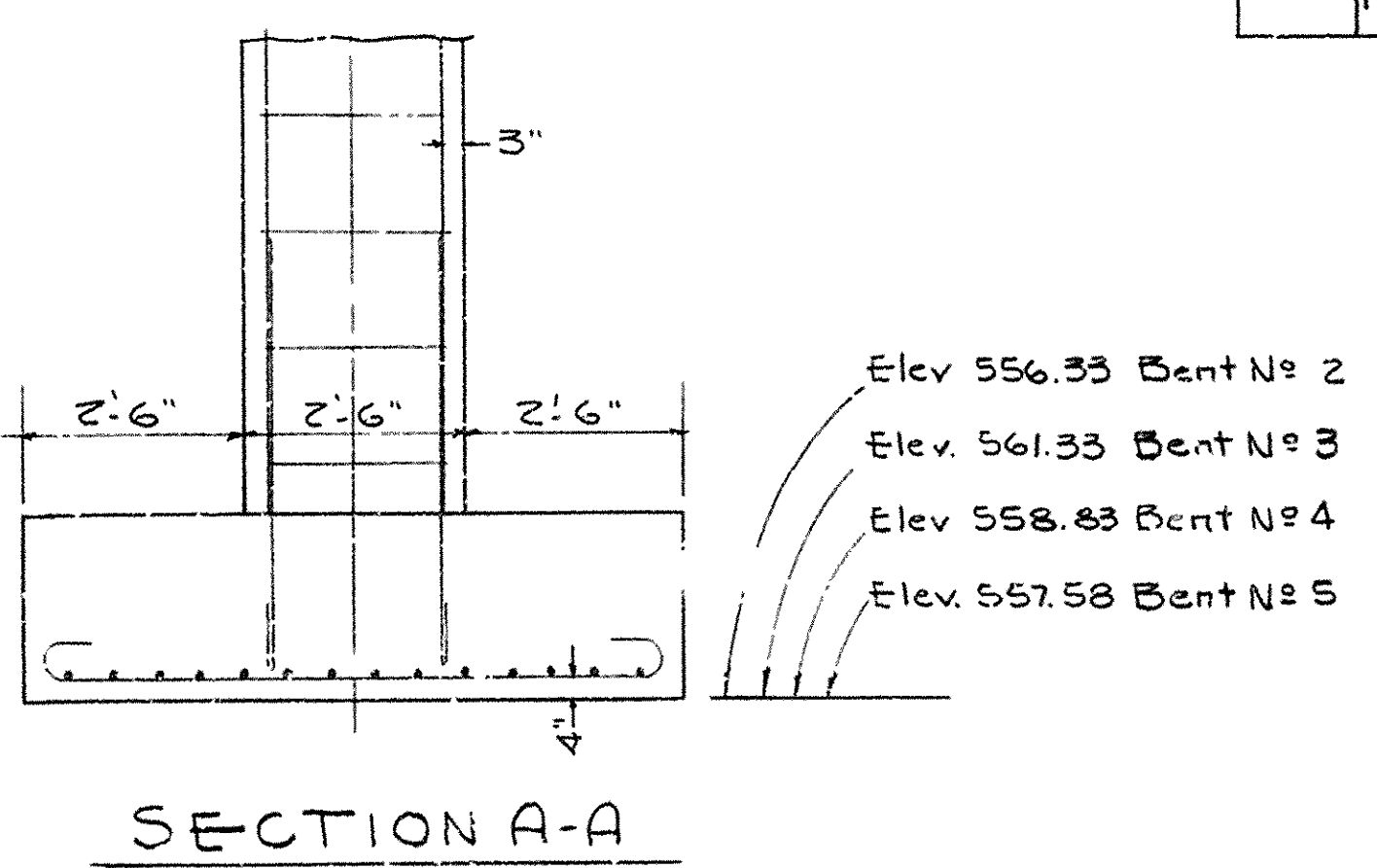
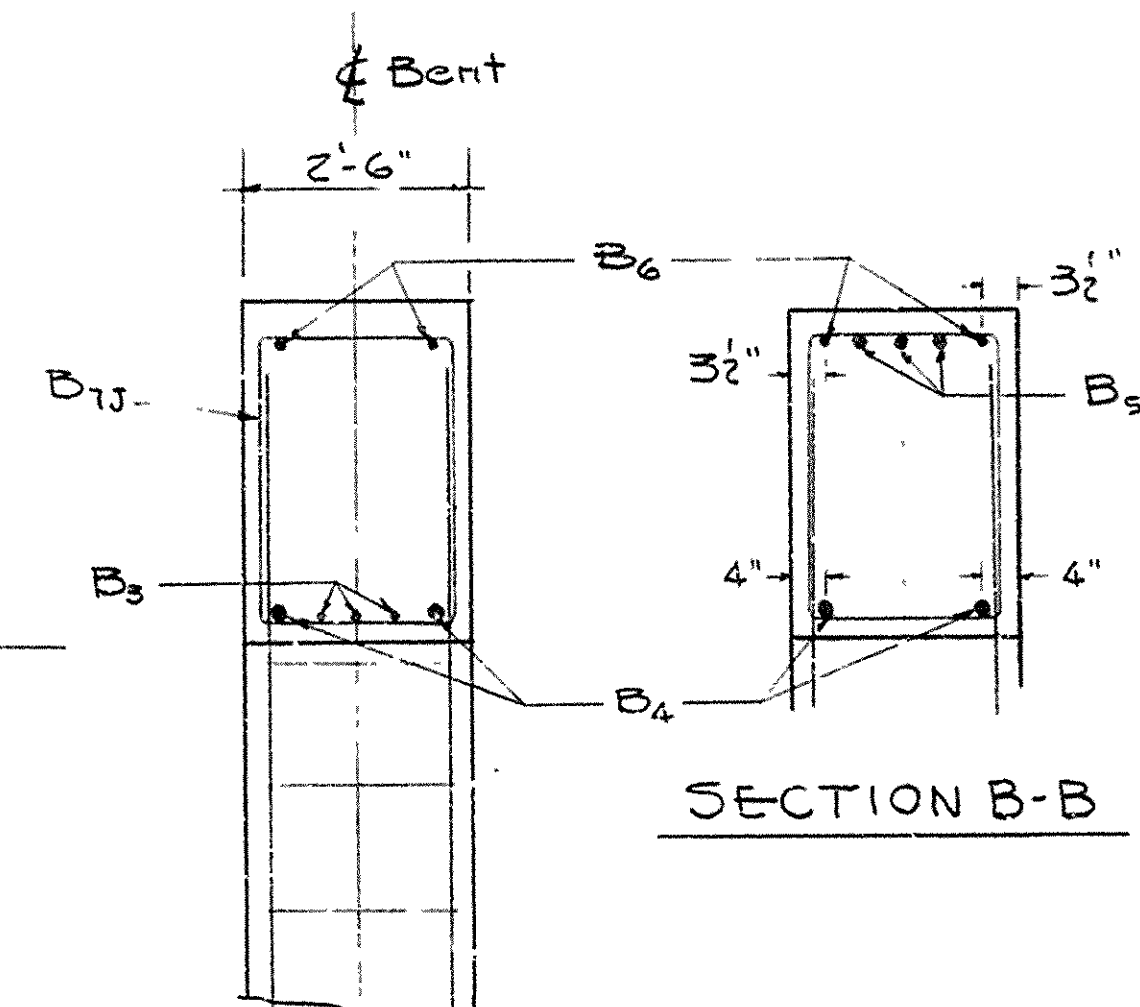
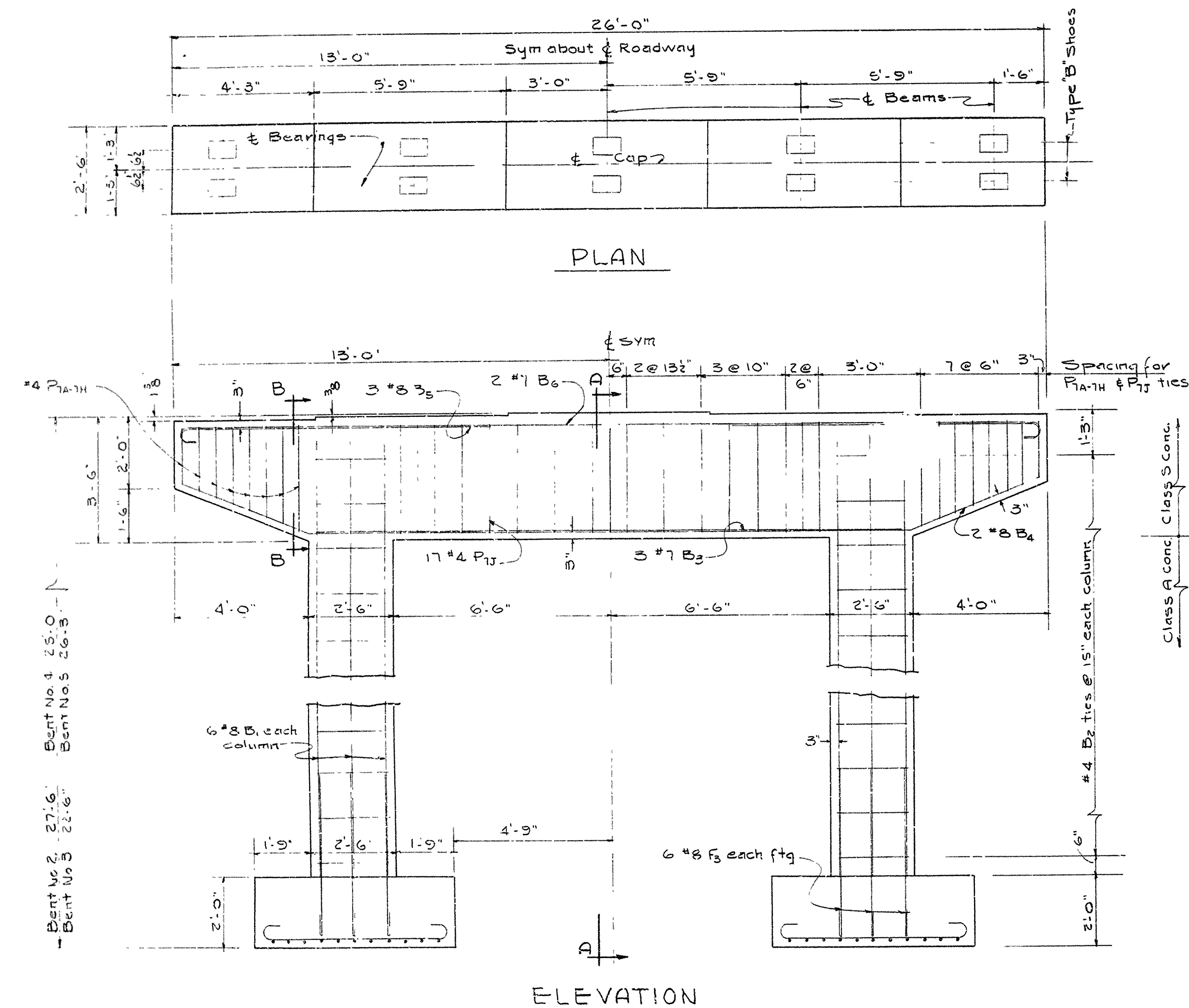
LITTLE ROCK, ARK.

DRAWN BY: SM DATE: 1 Feb 61
TRACED BY: WTS DATE: 2-1-61
CHECKED BY: WTS DATE: 2-1-61

L.P. Williams
BRIDGE ENGINEER

BRIDGE NO. 3447 DRAWING NO. 11260





BAR LIST FOR EACH BENT

BENT NO.	MARK	SIZE	No. REQ'D	LENGTH	A	B	K	"P" Pin Dia	BENDING DIAGRAM
ALL BENTS	F ₁	#6	22	8.5	7.0	—	6	4 1/2	
	F ₂	#6	28	6.9	5.6	—	5	3 3/4	
	F ₃	#8	12	6.3	5.1 1/2	—	9	6	
2	B ₁	#8	12	24.10	—	—	Str.	—	
	B ₂	#4	40	9.3	2.2	2.2	4	1 1/2	
3	B ₁	#8	12	19.10	—	—	Str.	—	
	B ₂	#4	32	9.3	2.2	2.2	4	1 1/2	
4	B ₁	#8	12	22.4	—	—	Str.	—	
	B ₂	#4	36	9.3	2.2	2.2	4	1 1/2	
5	B ₁	#8	12	23.7	—	—	Str.	—	
	B ₂	#4	38	9.3	2.2	2.2	4	1 1/2	
ALL BENTS	P _{1A-H}	#7	3	17.6	—	—	Str.	—	
	B ₄	#8	2	26.0	3.8	1.5	—	8	
	B ₅	#4	6	10.9	3.7 1/2	—	9	8	
	B ₆	#7	2	27.2	25.6	—	7	5 1/4	
ALL BENTS	P _{1A-H}	#4	16	8.0 to 10.8	1.7 1/2 to 2.1 1/2	2.1	4	1 1/2	
	P _{1J}	#4	17	10.9	3.0	2.1	4	1 1/2	

~ Dimensions are in inches center to center of bars ~

GENERAL NOTES

Concrete in footings and columns to be Class A.
Concrete in caps to be Class B. All concrete to be poured in the dry. Exposed corners to be chamfered 3/4" unless otherwise noted.

Reinforcing steel to be of deformed bars of Intermediate or Hard Grade steel. Shop lists and bending diagrams shall be submitted and approval secured before fabrication of reinforcing steel is begun.

For additional notes and details see Drawing Nos. 11261, 5450, and 5452.

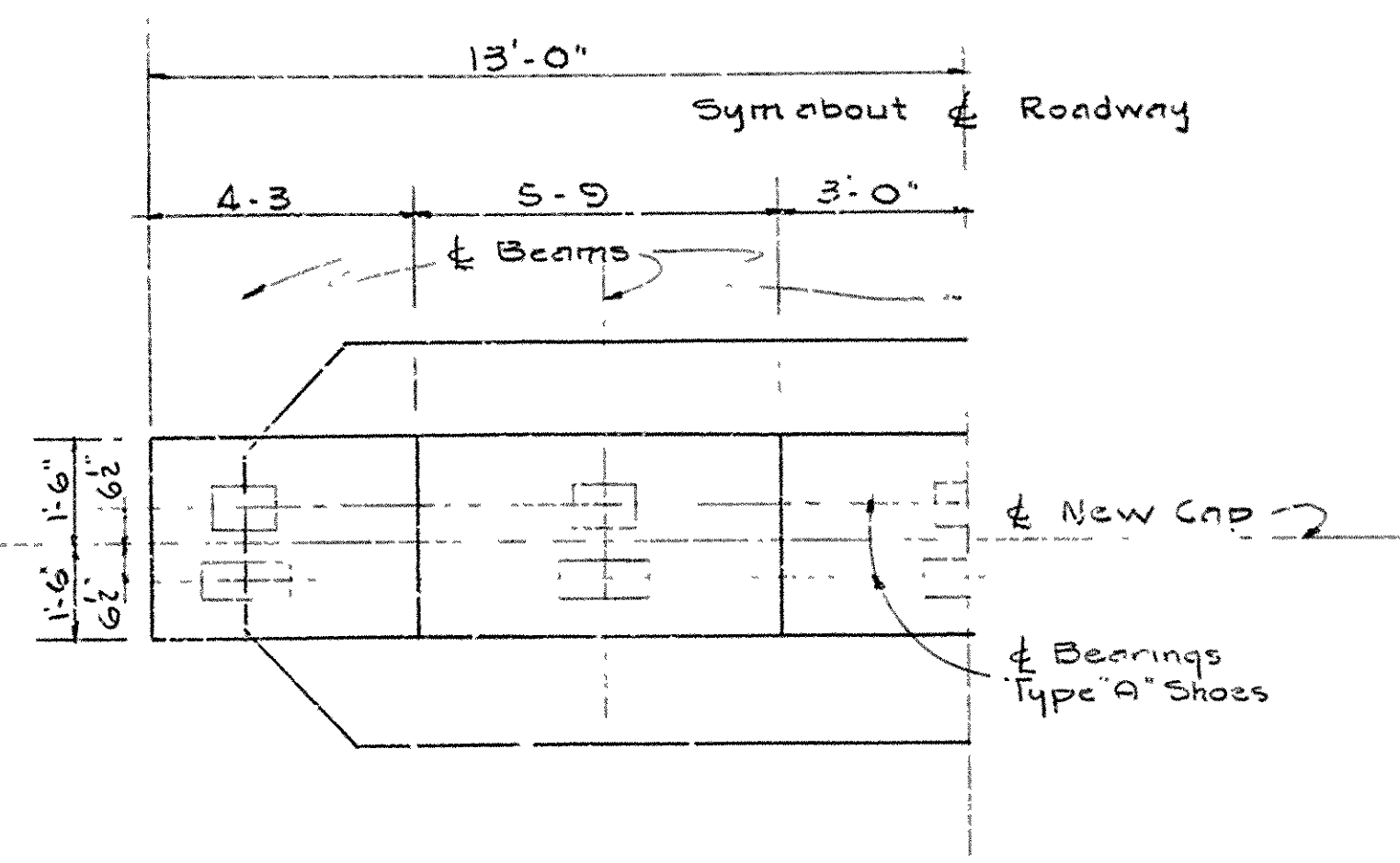
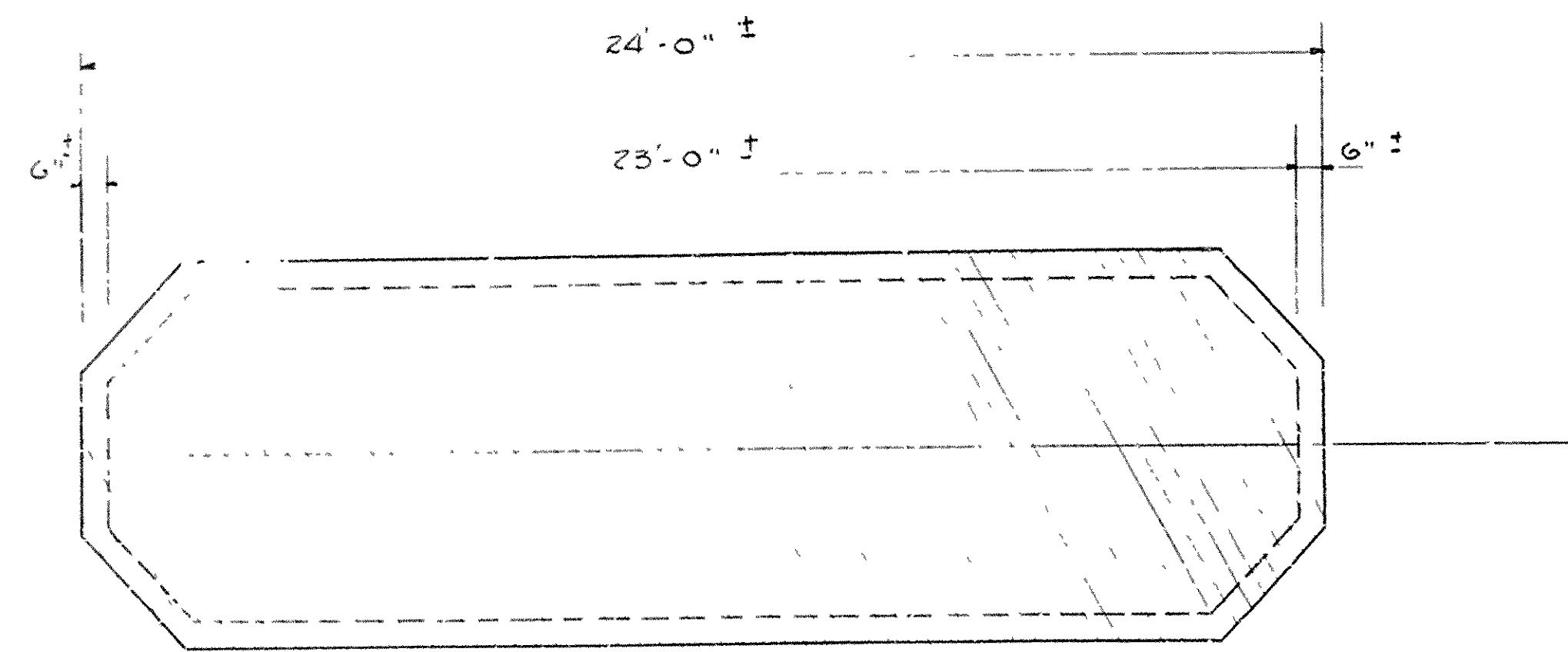
DETAILS OF BENTS NO. 3, 4 & 5
BRIDGE OVER
FOURCHE-LAFAVE RIVER
SCOTT COUNTY
ROUTE 28 SEC. 2
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: *J.M.* DATE: 12-1-61
THROD BY: *J.F.M.* DATE: 1-25-62
CHECKED BY: *J.F.M.* DATE: 1-25-62

SCALE: 1/2" = 1'-0"

BRIDGE NO. 34.47 DRAWING NO. 11262

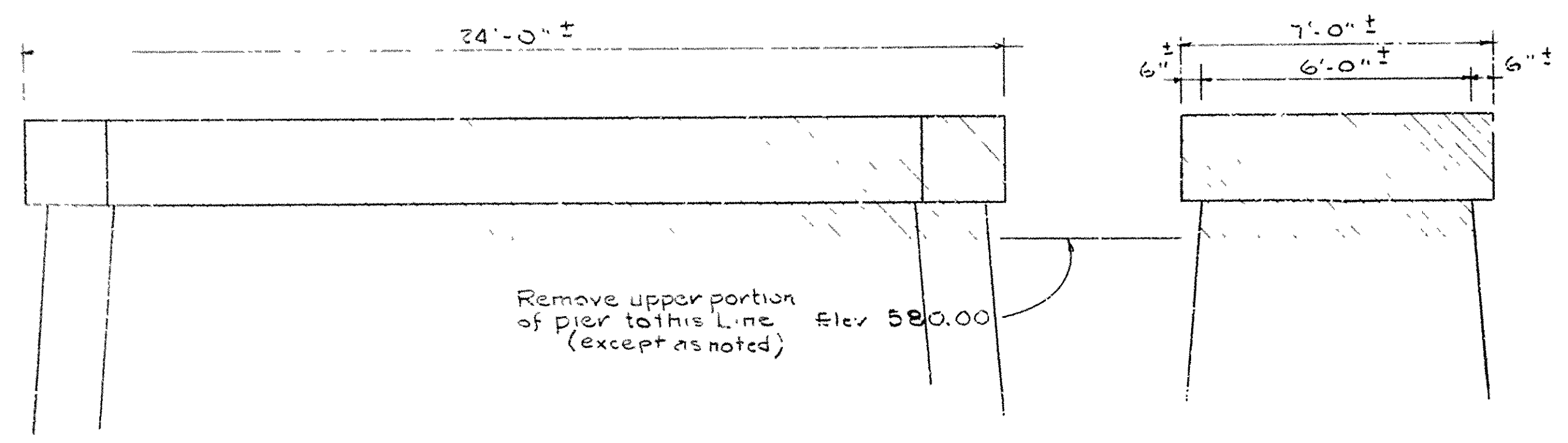
J. M. Carlson
BRIDGE ENGINEER



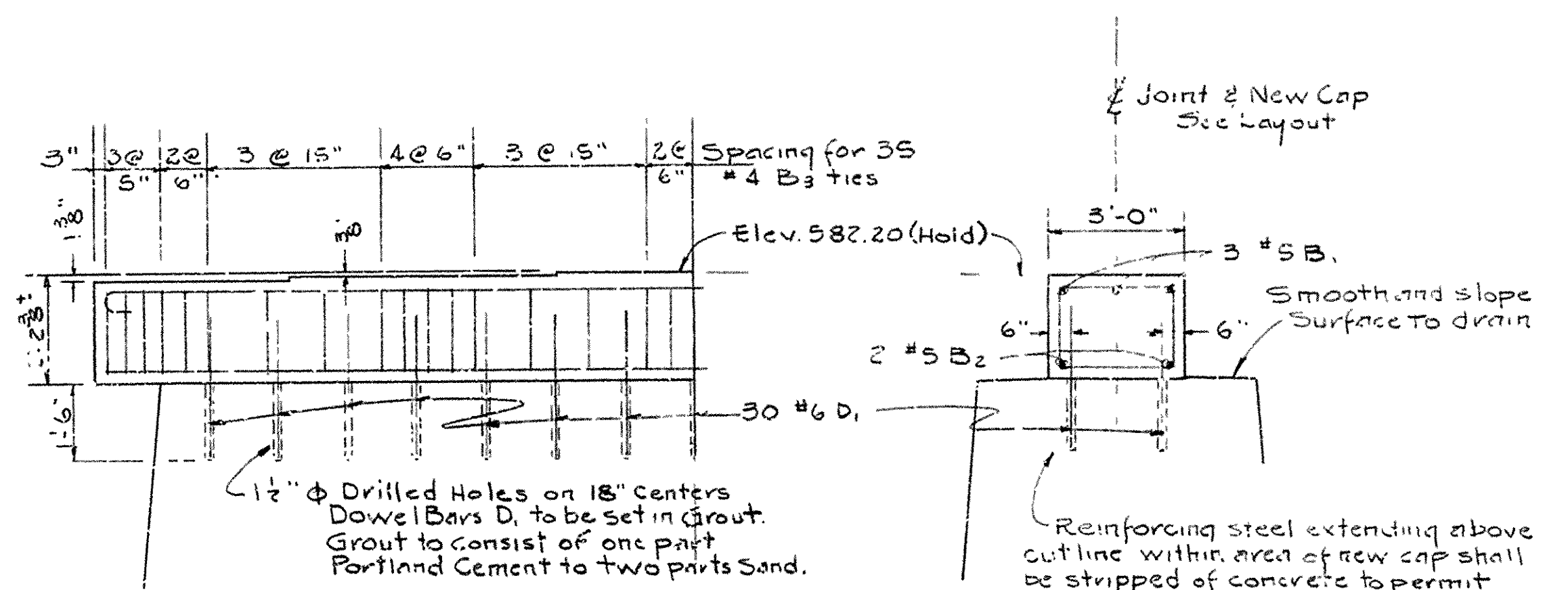
BAR LIST FOR EACH PIER

MARK	SIZE	No.	LENGTH	A	K	"P" Pin. Dia.	BENDING DIAGRAM
B ₁	#5	3	26-9	25-6	5	3/4	
B ₂	#5	2	25-6	-	Str.	-	
B ₃	#4	35	9-4	2-8	4	1 1/2	
D ₁	#6	30	3-0	-	Str.	-	

~ Dimensions are center to center of bars ~



EXISTING PIER
Showing Portion to be Removed



REMODELED PIER
Showing Details of New Cap

GENERAL NOTES

Concrete in remodeled Cap to be Class 5. The concrete to be poured in the fry. All exposed corners shall be chamfered 3/4".

Reinforcing steel to be deformed bars of Intermediate or Hard Grade steel.

For additional notes and details see Drawing Nos. 11261, 5460 and 5462.

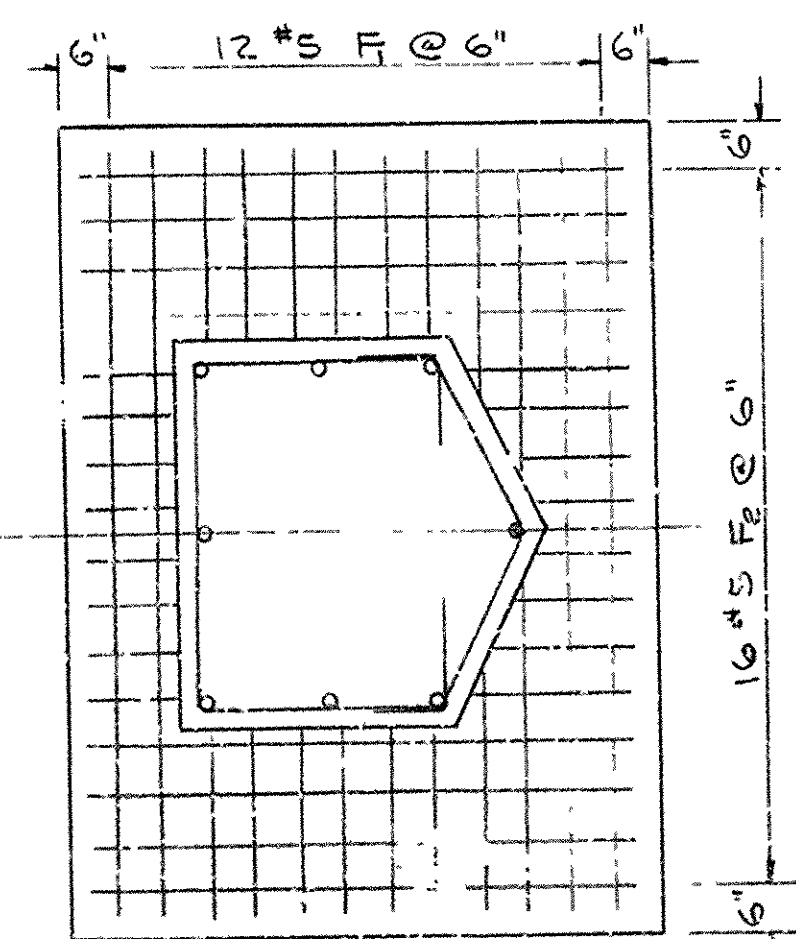
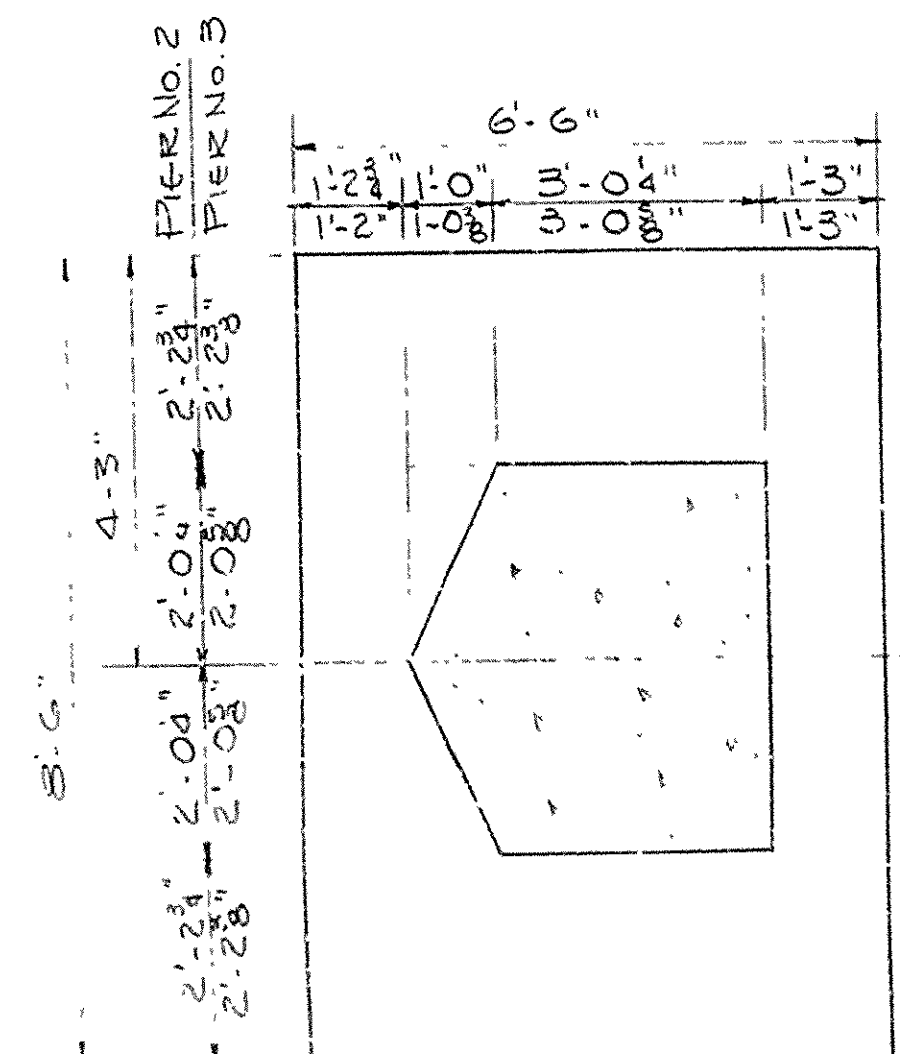
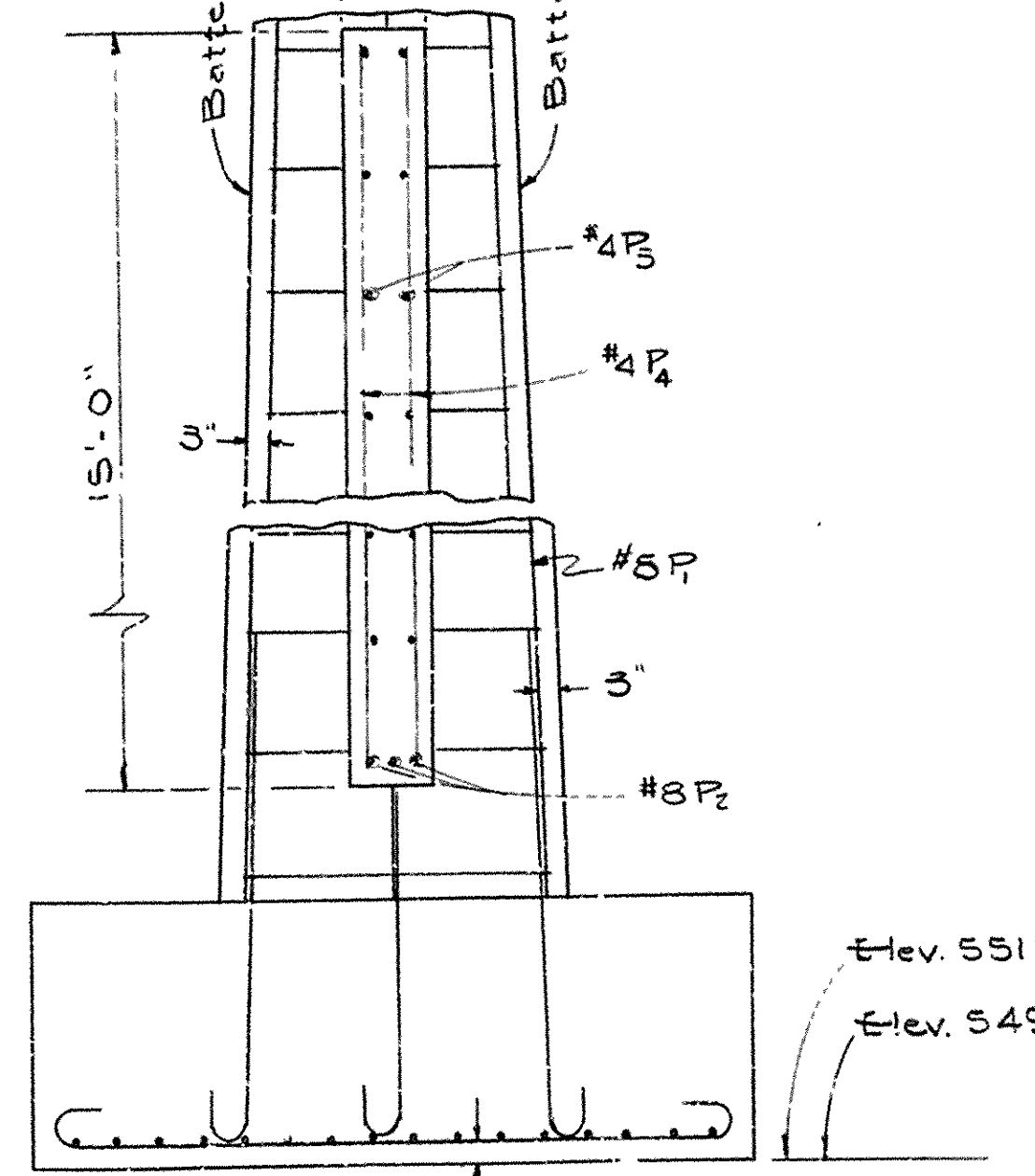
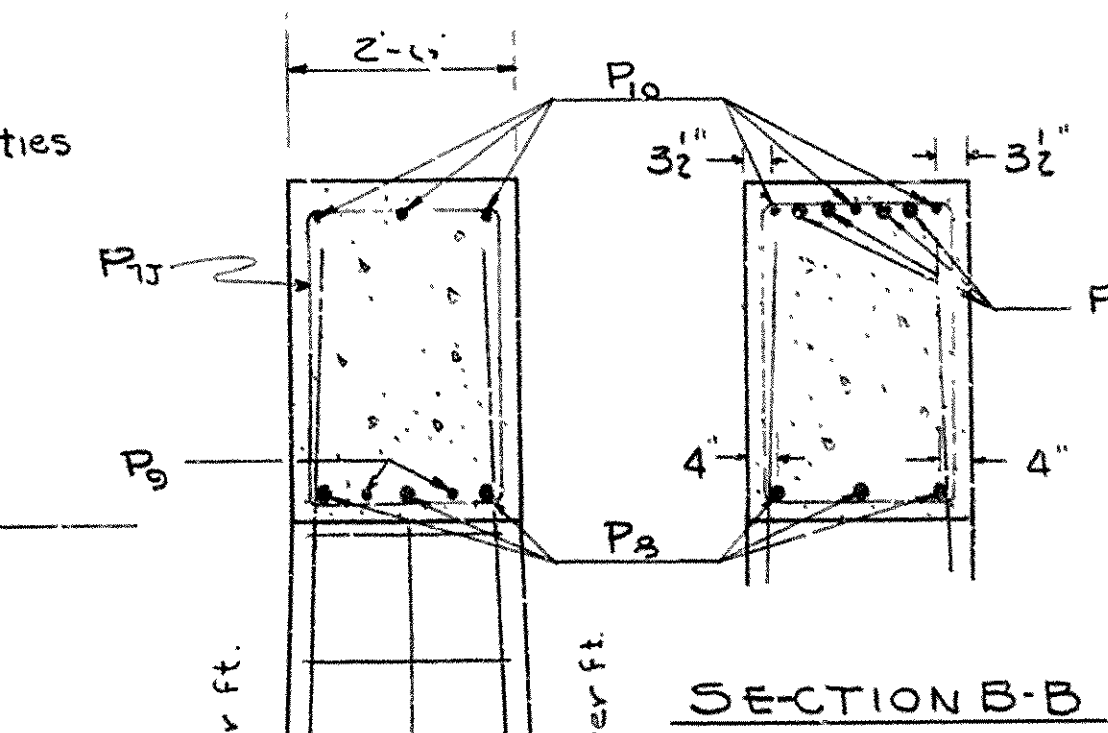
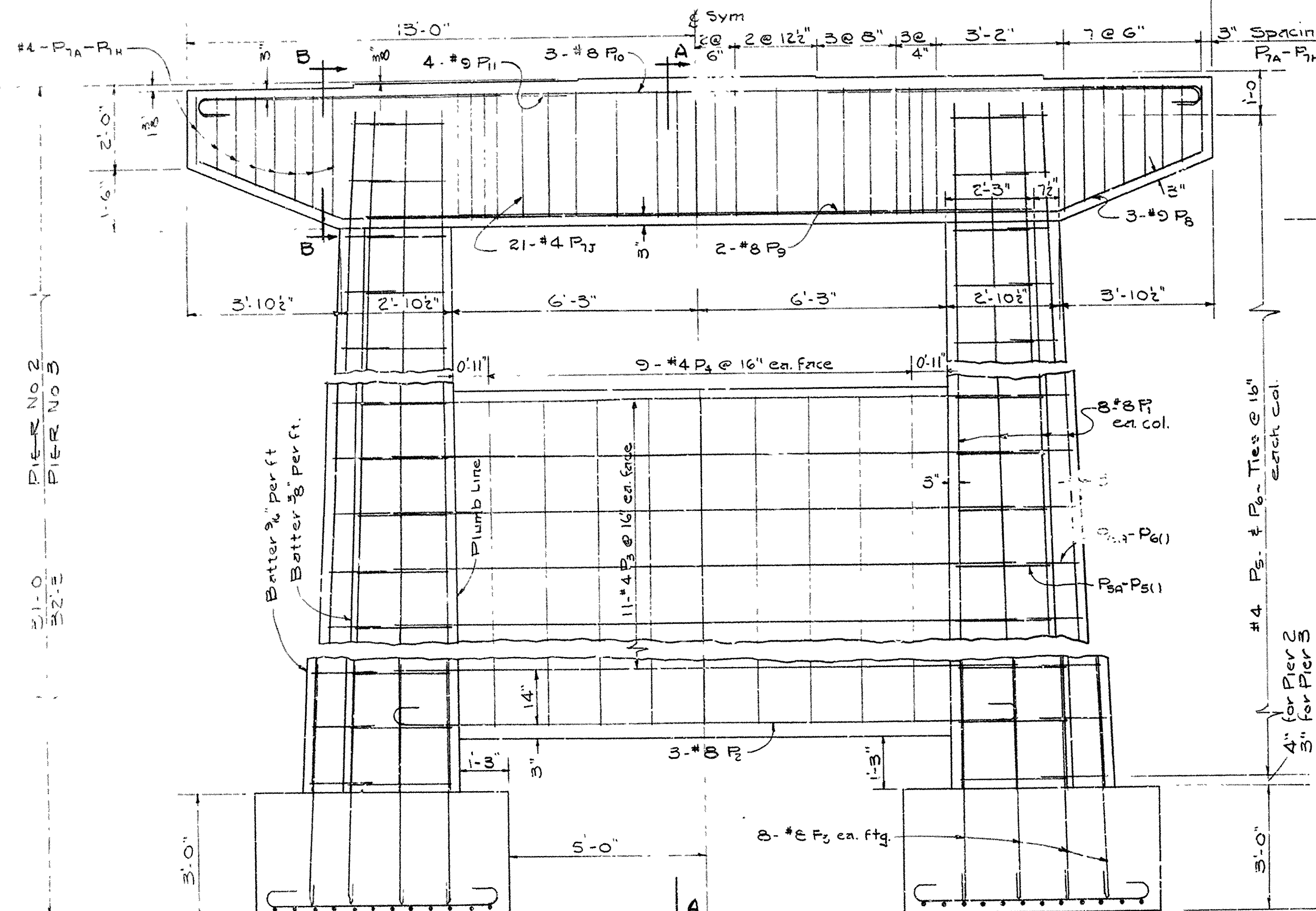
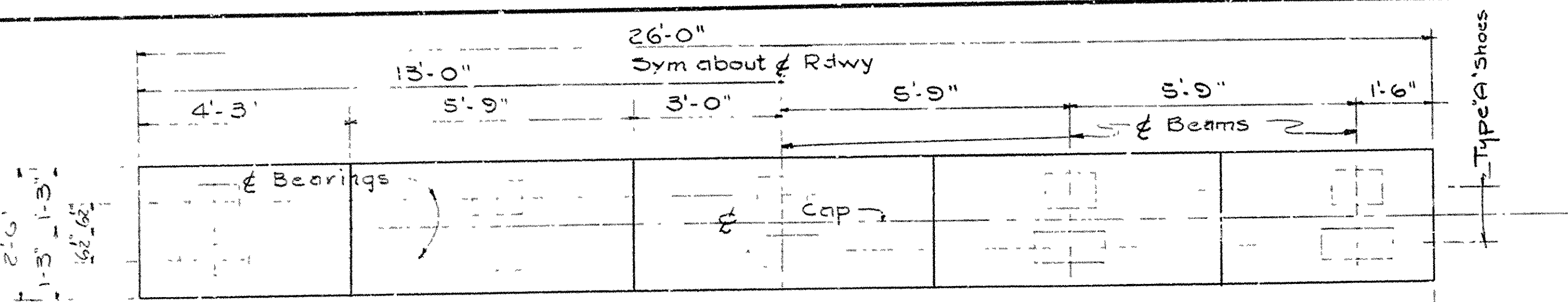
Remodeling of Piers 1 and 4 consisting of the removal of the upper portion of piers, drilling holes and grouting down bars into place, and finishing cut surfaces as shown, shall not be paid for directly, but will be considered subsidiary to the other items of the contract.

DETAILS FOR REMODELING
EXISTING PIERS NO. 1 & 4
BRIDGE OVER
FOURCHE LAFAYE RIVER
SCOTT COUNTY
ROUTE 28 SEC. 2
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: *J.M.* DATE: 12-16-61
TRACED BY: *J.E.M.* DATE: 1-25-61
CHECKED BY: *J.E.M.* DATE: 1-25-61
SCALE: 3/8" = 1'-0"
BRIDGE NO. 3447 DRAWING NO. 11263

J.P. Wilson
BRIDGE ENGINEER

FED. ROAD DIST. NO.	STATE	FED. AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	ARK.	5-146(4)		10	42
JOB No.		4472			



BAR LIST FOR EACH PIER

MARK	SIZE	NO. REIN.	PIER 2	PIER 3	LENGTH	A	B	K	P ₁ Dia.	BENDING DIAGRAM
F ₁	#5	24	24		9-3	8-0		5	3 1/4	A
F ₂	#5	32	32		7-3	6-0		5	3 1/4	P
F ₃	#8	16	16		7-1	5-11 1/2		9	8	F ₁ F ₂ P ₂ P ₁₀
P ₁	#8	16			27-4				Str.	A
P ₂	#8	16			28-7				Str.	P
P ₃	#8	3	3		18-1	15-10		9	8	K P F ₃ P ₁
P ₄	#8	22	22		15-10				Str.	
P ₅	#8	18	18		14-6				Str.	
P _{6A} -P _{6U}	#4	42			8-5 to 11-9	1-11 1/2 to 3-7 1/2	1-10 1/2 to 2-8 1/2		1 1/2	P _{2A} -P _{6U} P _{3A} -P _{5U}
P _{6A} -P _{6V}	#4	44			8-5 to 11-11	1-11 1/2 to 3-8 1/2	1-10 1/2 to 2-9		1 1/2	P _{2A} -P _{6V} P _{3A} -P _{5V}
P _{6A} -P _{6H}	#4	42			5-5 to 7-3 1/2	1-1 to 2-0 1/2			1 1/2	
P _{6A} -P _{6V}	#4	44			5-5 to 7-4 1/2	1-1 to 2-1			1 1/2	
P _{7A} -P _{7H}	#4	16	16		8-0 to 10-8	1-7 1/2 to 2-11 1/2	2-1	4	1 1/2	
P _{7J}	#4	21	21		10-9	3-0	2-1	4	1 1/2	P _{7J} P _{7A} -P _{7H}
P ₈	#9	3	3		26-0	3-8	1-5		9	
P ₉	#8	2	2		17-0				Str.	
P ₁₀	#8	3	3		27-10	25-7		9	8	
P ₁₁	#9	8	8		10-8	9-5		10	9	P ₈

~ Dimensions are center to center of bars ~

GENERAL NOTES

Concrete in footings, columns and web wall to be Class A. Concrete in caps to be Class S. All concrete to be poured in the dry. Exposed corners to be chamfered 3/4" unless otherwise noted.

Reinforcing steel to be of deformed bars of Intermediate or Hard Grade Steel. Shop lists and bending diagrams, including bar supports, shall be submitted and approval secured before fabrication of reinforcing steel is begun.

For additional notes and details see Dwg. Nos. 11261, 5460 & 5462.

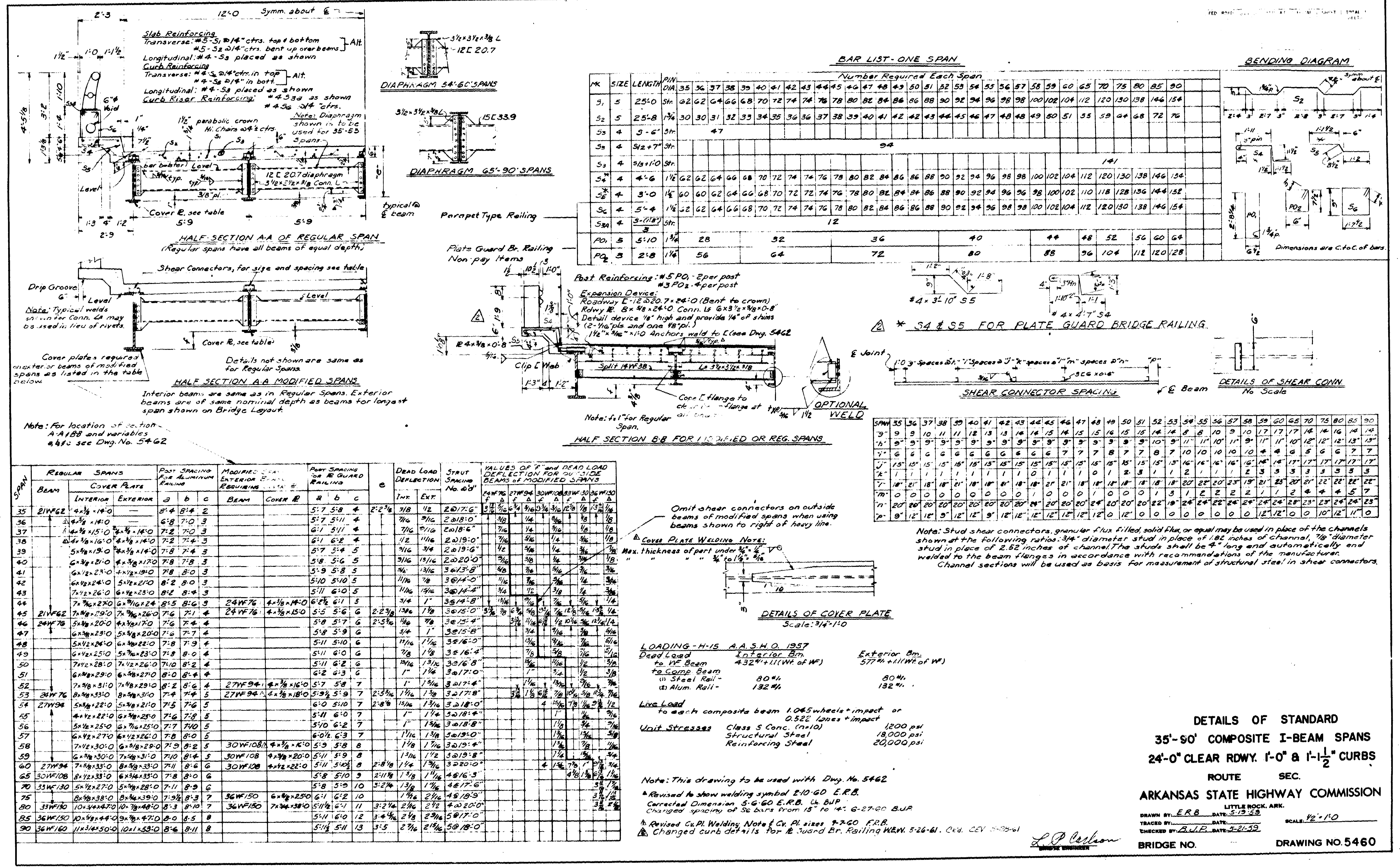
DETAILS OF PIERS NO 2 & 3
BRIDGE OVER
FOURCHE LAFAVER RIVER
SCOTT COUNTY
ROUTE 28 SEC. 2
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: *DM* DATE: 20 Dec 60
TRACED BY: DATE: 1-6-61
CHECKED BY: *J.E.M.* DATE: 1-6-61

BRIDGE NO. 3447 DRAWING NO. 11264

SCALE: 1/2" = 1'-0"

L.P. Carlson
BRIDGE DESIGN ENGINEER

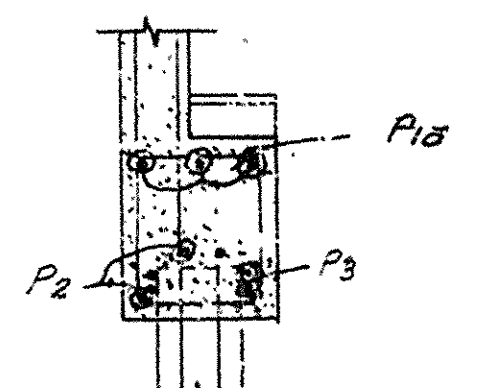


VARIABLE DIMENSIONS FOR END BENTS						
SPAN	END SPAN BEAMS		VARIABLES			
LENGTH	INTERIOR	EXTERIOR	a	b	c	d
35'-45'	21WF62	21WF62	1"	2 1/4"	2'-5 1/8"	—
		24WF76	3 1/16"	5 1/16"		4"
		27WF94	6 3/4"	8"		7"
		30WF108	9 3/4"	11"		10"
		33WF130	12 3/8"	14 1/8"		13"
35'-45'	21WF62	36WF150	15 1/8"	16 13/16"	2'-5 1/8"	16"
46'-53'	24WF76	24WF76	1"	5 1/16"	2'-7 1/16"	—
		27WF94	3 13/16"	8"		4"
		30WF108	6 1 1/2"	11"		7"
		33WF130	10 1/16"	14 1/8"		10"
46'-55'	24WF76	36WF150	12 3/4"	16 1/2"	2'-7 1/4"	13"
54'-60'	27WF94	27WF94	1"	8"	2'-10 3/8"	—
		30WF108	4"	11"		4"
		33WF130	7 1/8"	14 1/8"		7"
54'-60'	27WF94	36WF150	9 3/8"	16 1/2"	2'-10 3/8"	10"
65'	30WF108	30WF108	1"	11"	3'-1 1/8"	—
	30WF108	33WF130	4 1/8"	14 1/8"	3'-1 1/8"	7"
65'	30WF108	36WF150	6 1 1/2"	16 1/2"	3'-1 1/8"	7"
75'	33WF130	33WF130	1"	14 1/8"	3'-5"	—
75'	33WF130	36WF150	3 1/2"	16 1/2"	3'-5"	4"

NOTE

All Concrete to be Class 3; All exposed corners to have 3/4" chamfer unless otherwise noted.

All piling shall be driven to a minimum capacity of 36 tons per pile. Piling shall be either 12DPS, 16" Octagonal Precast Concrete Piles, or Concrete Filled Metal Shell Piles as shown on Layout.

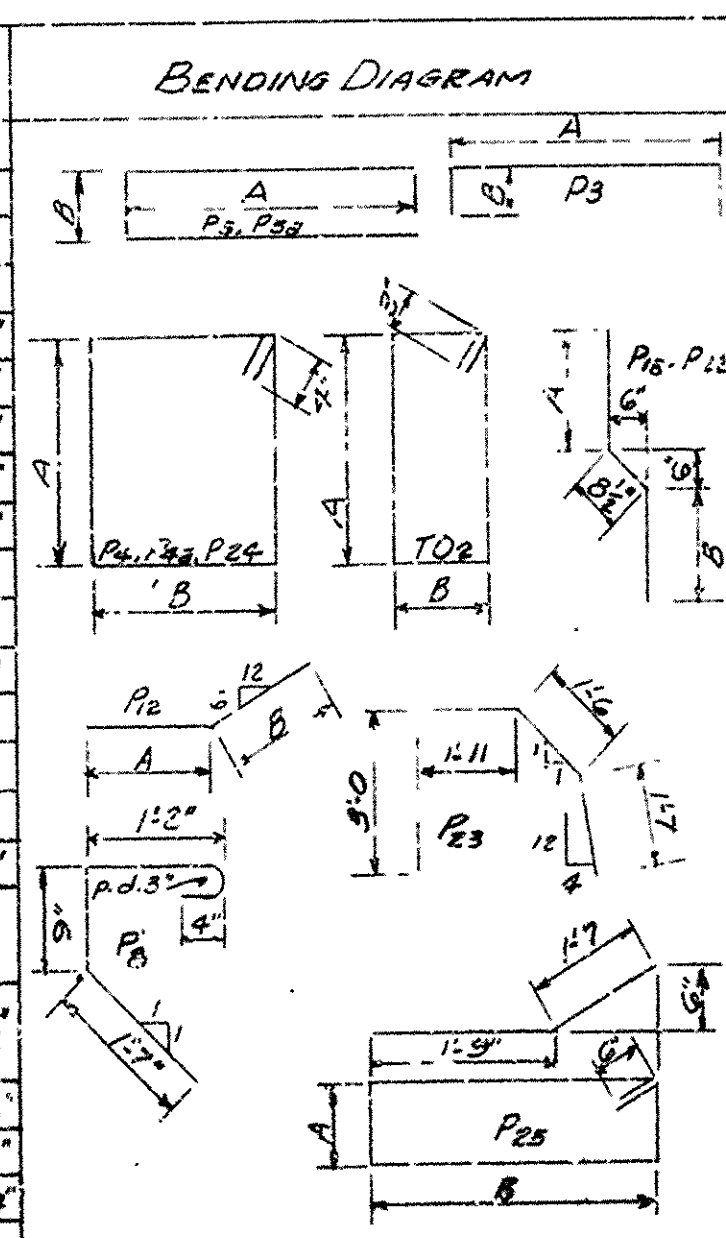


BAR LIST-PER BENT

PK	SIZE (IN)	No. REQ. (1/2" BENT)		CAN X CAP	LENGTH	A	B	PIN DIA
P ₁	6	3	5	—	25'2"	—	—	Str.
P ₁₃	8	—	—	6	7'4"	—	—	Str.
P ₁₄	8	—	—	3	16'11"	—	—	Str.
P ₂	6	4	4	4	25'2"	—	—	Str.
P ₃	6	4	4	4	27'10"	24'9"	11'7"	2 1/4"
P ₄	4	27	47	14	8'9"	2'11/2"	1'11/2"	1 1/2"
P ₄₃	4	—	—	33	8'3 3/4 24'	2'11/2 24'	1'11/2"	1 1/2"
P ₅	6	12	12	6	6'1"	2'11/2"	1'11/2"	2 1/4"
P ₅₃	6	—	—	6	6'11'2 24'	2'11/2 24'	1'11/2"	2 1/4"
P ₆	4	—	8	8	24'10"	—	—	Str.
P ₇	4	—	36	36	4'10"	—	—	Str.
P ₈	4	—	24	24	4'10"	—	—	1 1/2"
P ₉	4	—	6	6	4'5"	—	—	Str.
P ₁₀	—	—	10	10	6'3"	—	—	Str.
P ₁₁	4	—	4	4	3'6"	—	—	Str.
P ₁₂	4	—	4	4	5'10"	1'9"	4'2"	1 1/2"
P _{13 to P₁₇}	4	—	2 ea.	2 ea.	Varies 5'3" to 5'5"	—	—	Str.
P _{18 to P₂₂}	4	—	2 ea.	2 ea.	Var. 3'5" to 5'1"	2'4"	Var. 0'5" to 2'1"	1 1/2"
P ₂₃	6	—	4	4	7'10"	—	—	2 1/4"
P ₂₄	4	—	2	2	4'11"	1'9"	0'5"	1 1/2"
P ₂₅	6	—	4	4	10'10"	1'8"	3'5"	2 1/4"
P ₂₆	4	—	12	12	4'0" to 4'	—	—	Str.
T ₂₁	5	—	12	12	5'2"	—	—	Str.
T ₂₂	3	—	8	8	6'11"	2'9"	0'6"	1 1/4"

BENDING DIAGRAM

For values of "d" see table of variables.
Dimensions are C. to C. of bars.



Intermediate Bents not to be used for spans over 50'

- ④ Revis. cm.
Raised curb height to 12" for State Guard Entrance Railing.
OK'd REV 5-29-61
- ⑤ Changed spacing and added Pile Pz. added
rate of pile batter. ERB 11-61
- ⑥ Added pile splice note. JDB 2-22-62

*** Revised Bar P3 : PWA 1-20-65*

DETAILS OF STANDARD PILE BENTS
FOR 35' TO 75' COMPOSITE I-BEAM SPANS
24' CLEAR ROADWAY 1'-0" OR 1'-1 1/2" CURBS
ROUTE SEC

ARKANSAS STATE HIGHWAY COMMISSION

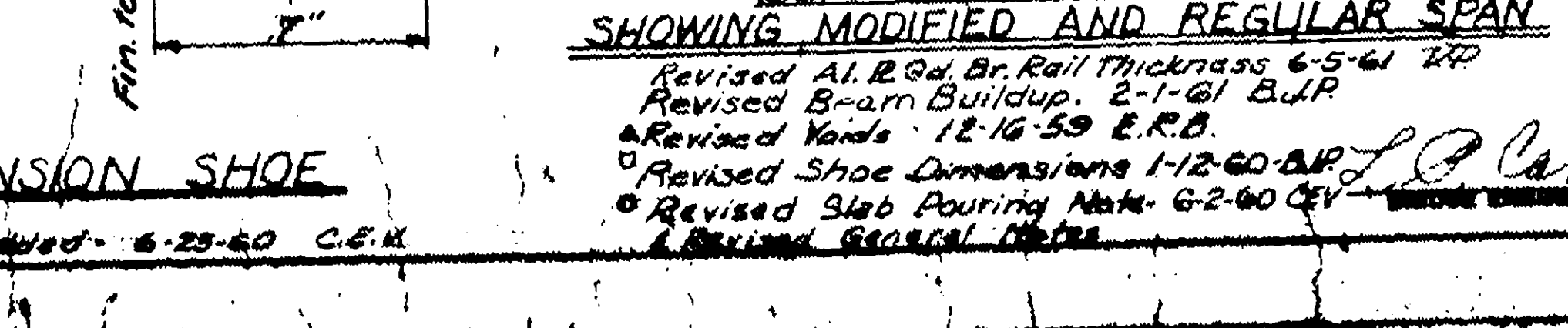
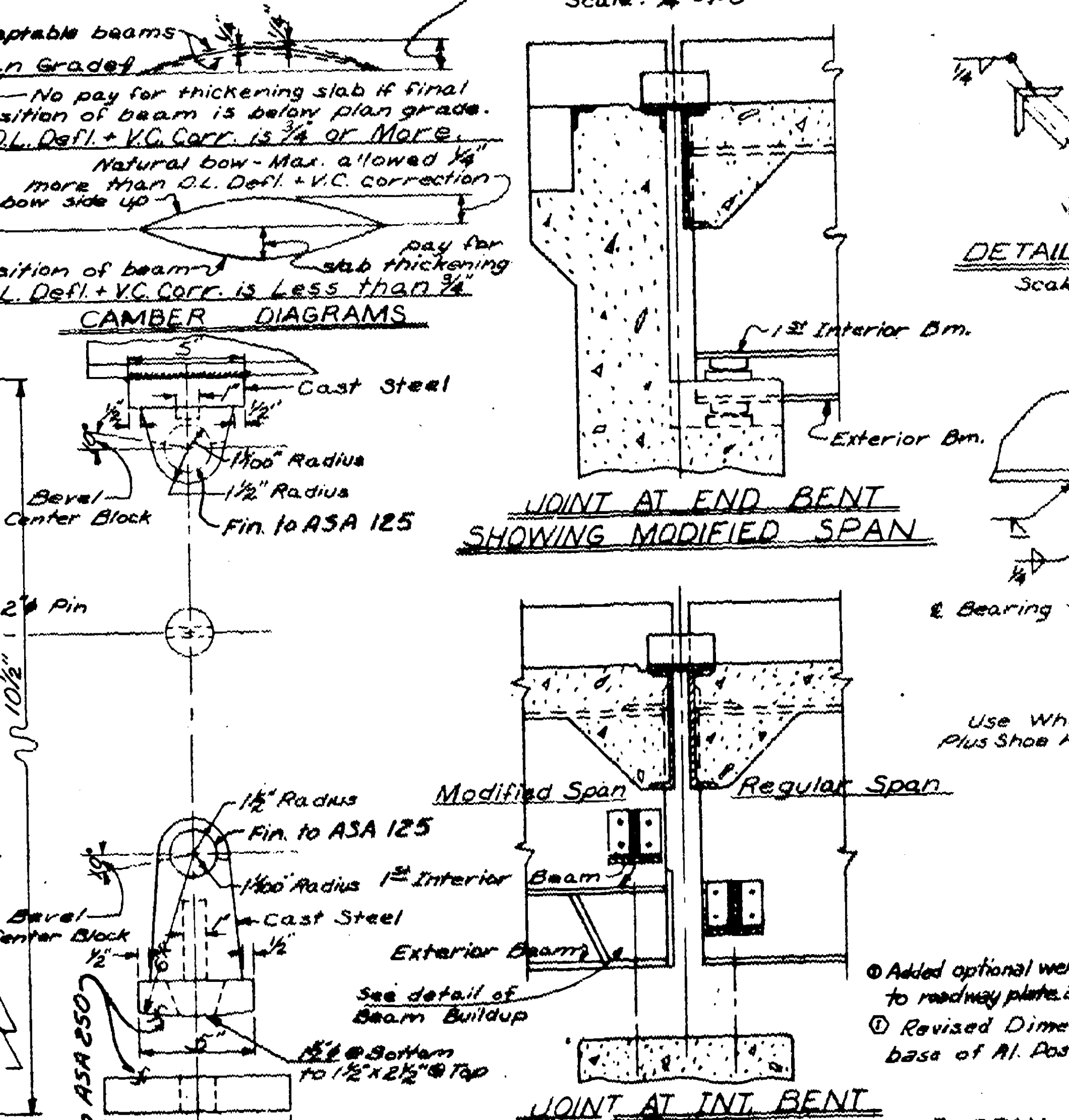
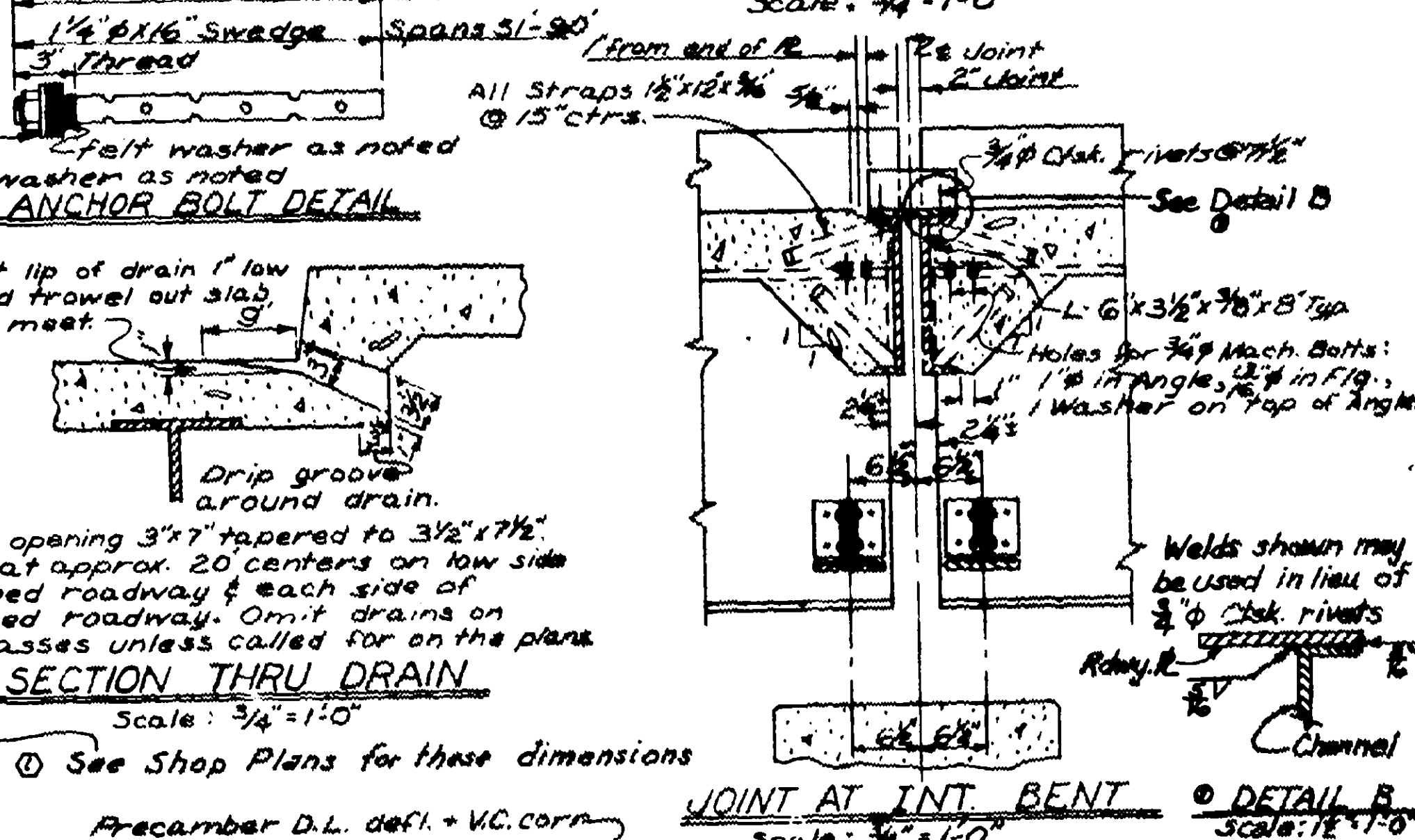
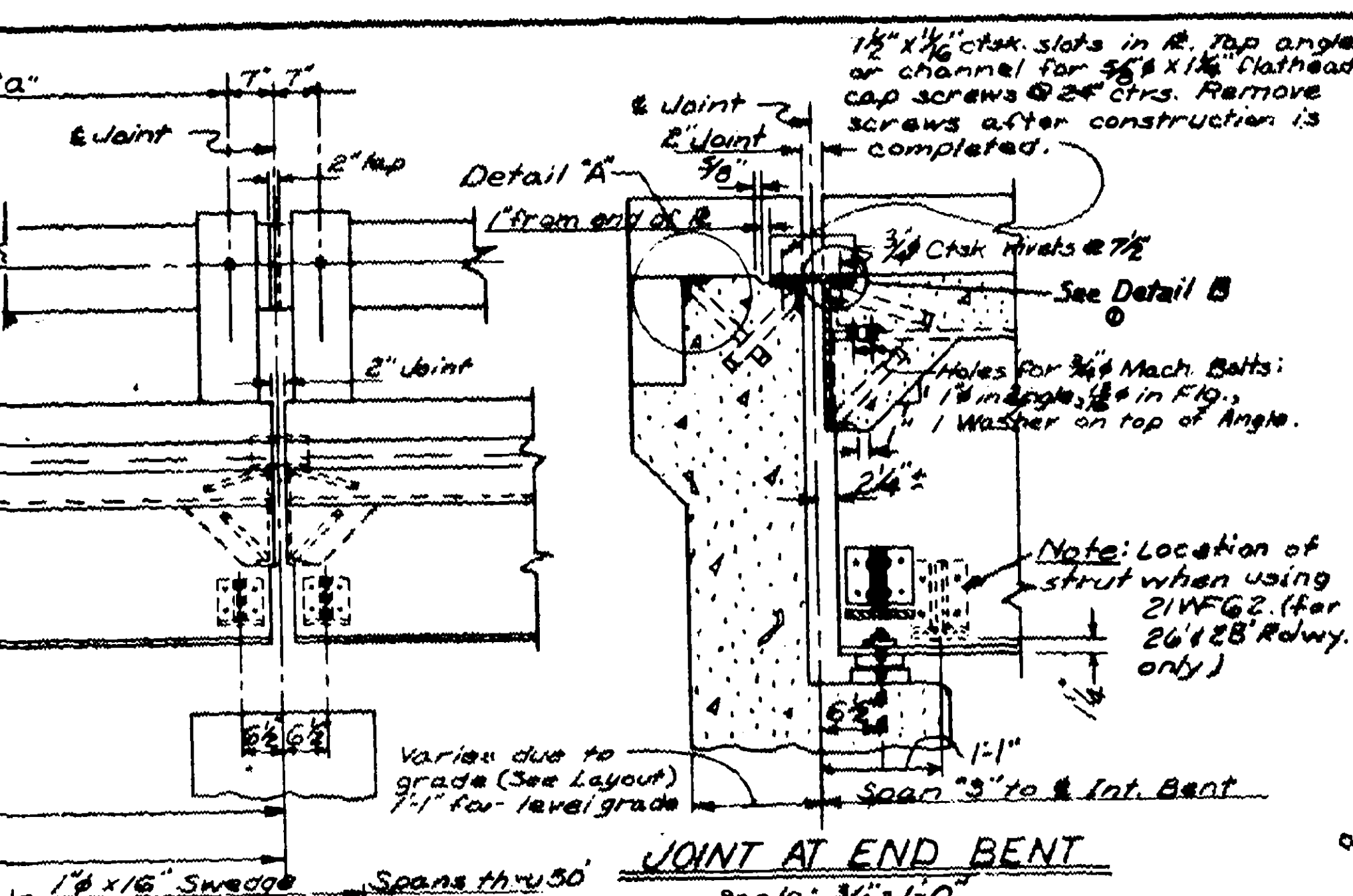
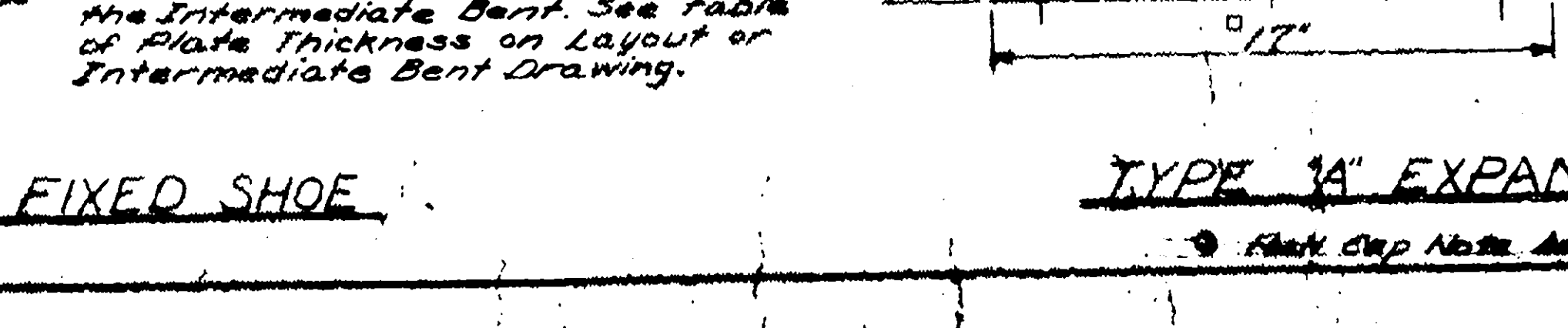
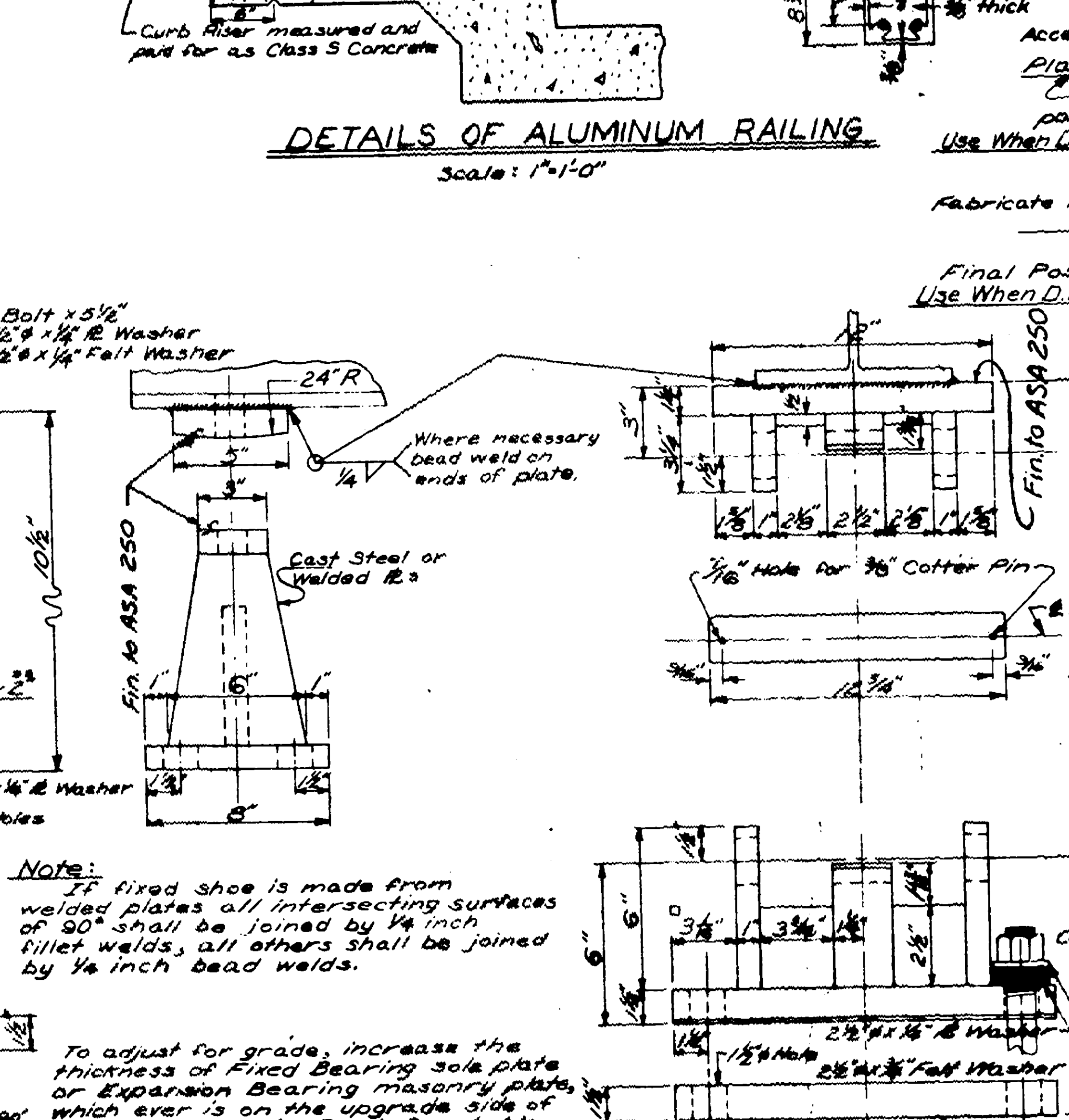
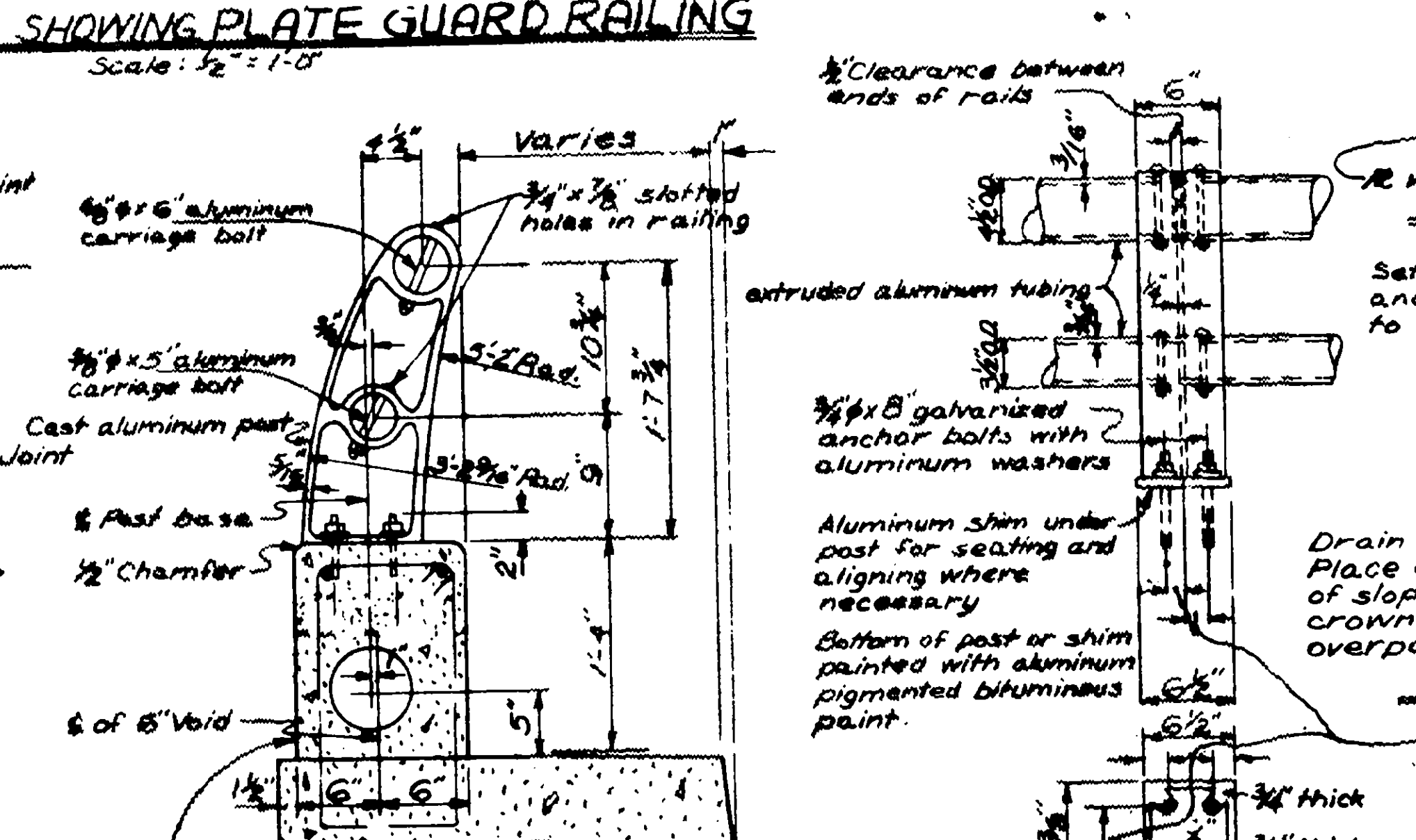
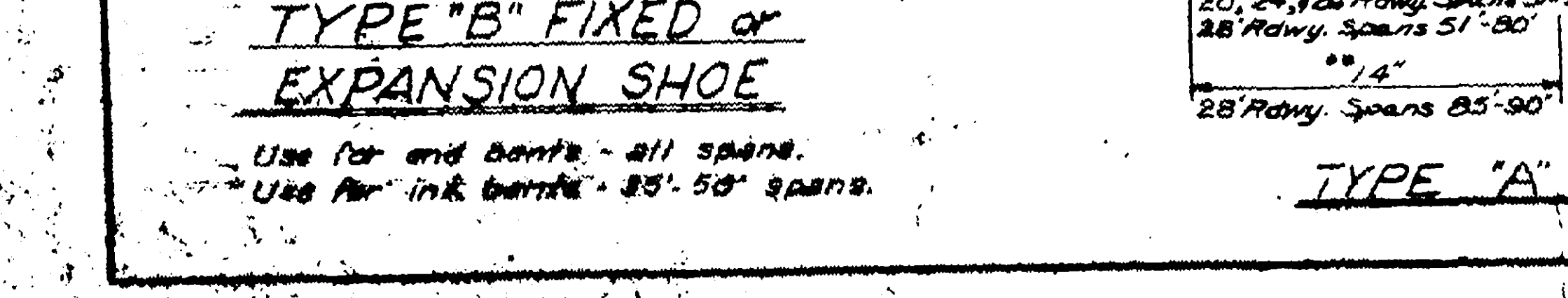
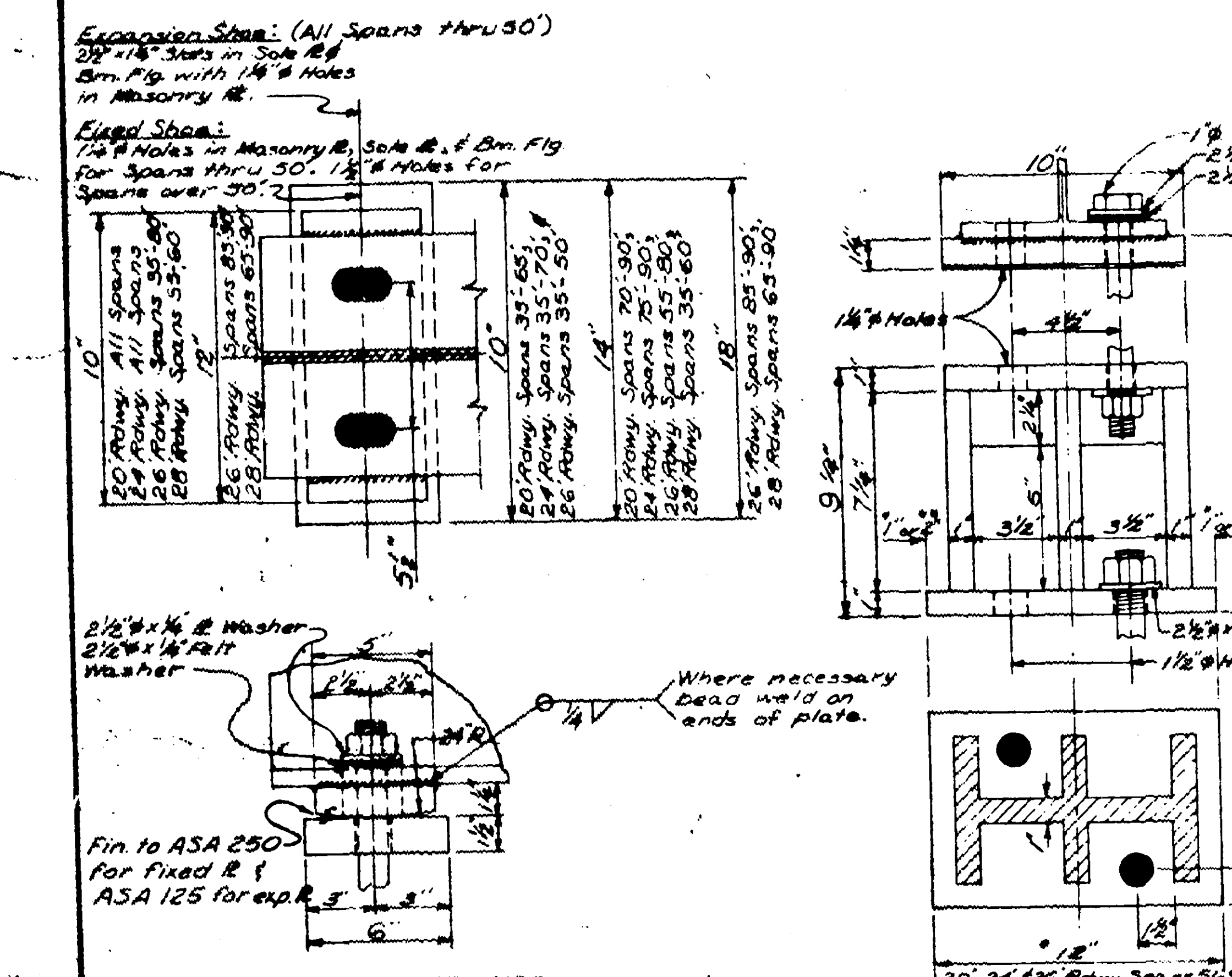
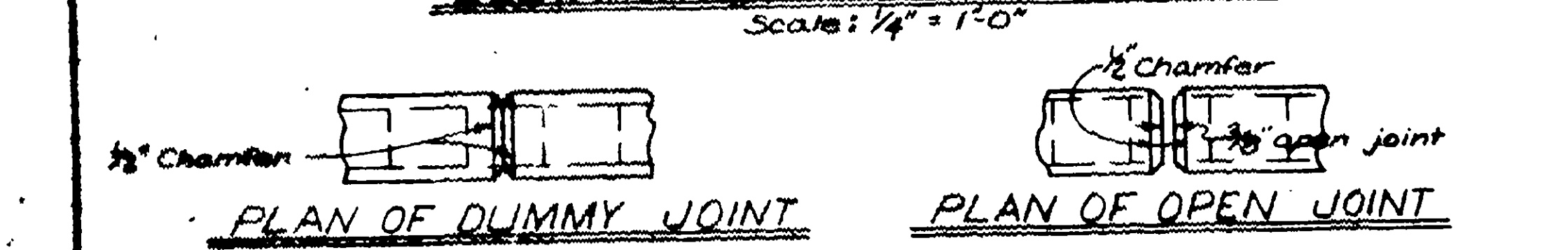
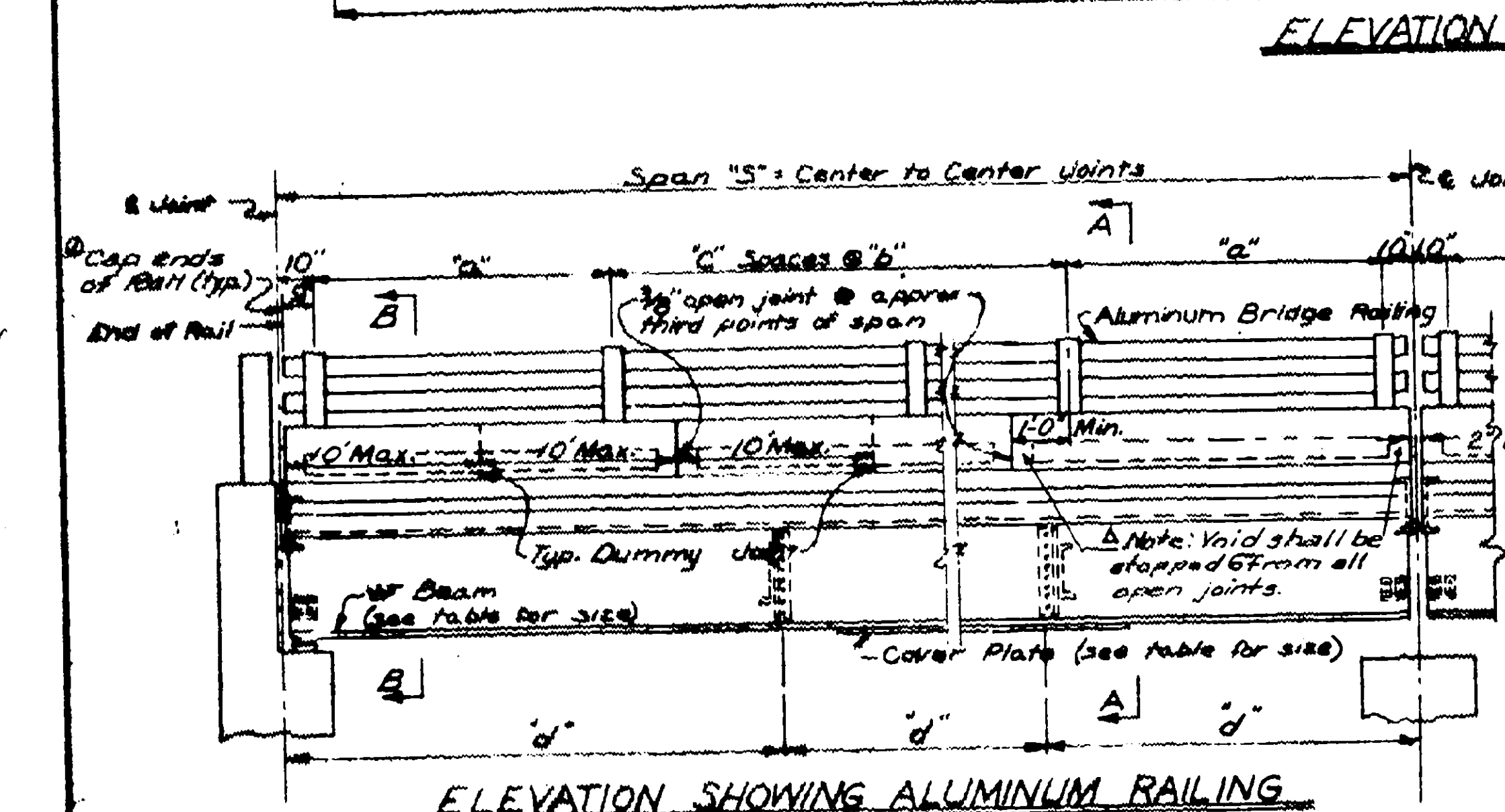
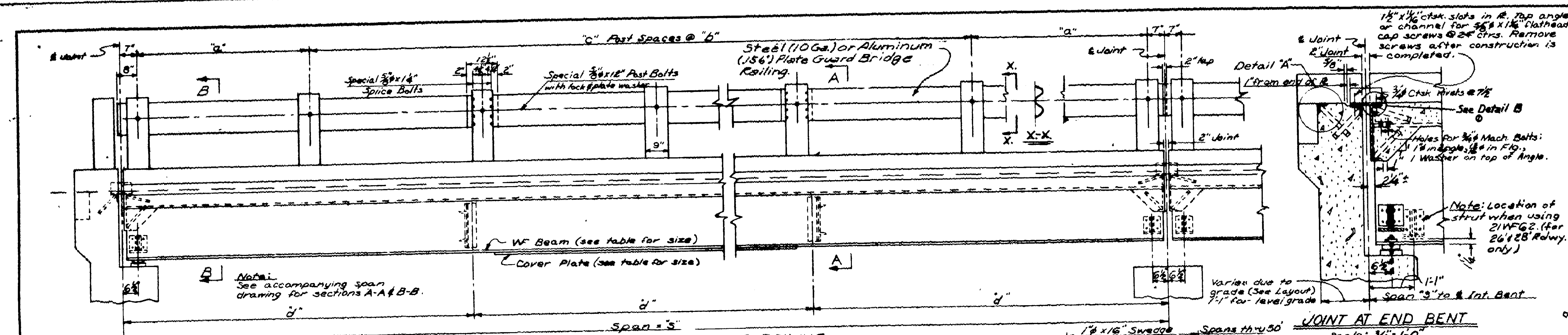
DRAWN BY: E.R.B. DATE: 9/23/59 SCALE: 3/8" = 1'-0" @ 25' 7.0' 10'

TRACED BY: _____ DATE: _____
CHECKED BY: L.F.T. DATE: 9/25/59
DRAWING NO 5460A

BRIDGE NO. DRAWING NO. 3

L. D. Carlson
WELDER ENGINEER

PROJ. NO.	STATE	FED. AID	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	ARK.				
JOB NO.					



GENERAL NOTES

All concrete to be Class S. All exposed corners to be chamfered unless otherwise noted.

Field connections to be riveted or bolted with high strength bolts. Rivets: 3/4" open holes 3/8" except where noted otherwise. Structural shapes of equal or greater strength may be substituted for shapes shown, but payment will be made on the basis of shapes shown or those actually used, whichever is less.

All welded connections to be the filler shop welds except as noted. All welding shall conform to the American Welding Society, Standard Specification for Welded Highway and Railway Bridges, 5th Edition.

Shop Paint: All structural steel except surfaces in contact with concrete shall be given one coat of red lead and black lined oil before shipment.

Field Paint: First coat - red lead tinted with lamp black. Second coat - aluminum paint.

All bearing plates and roadway expansion devices to be paid for as structural steel in beam spans. Bearings shall be finally sealed in a manner set forth in the Specifications. This work and material are to be considered as subsidiary to the item "Structural Steel in Beam Spans" and will not be paid for directly.

This drawing shows general features of design only. Shop drawings shall be made in accordance with the Specifications, submitted and approved secured before fabrication is begun.

Anchor bolts shall be galvanized to conform to ASTM Specification, Designation A 153.

Reinforcing steel to be deformed bars of intermediate or hard grade. The reinforcing steel is to be accurately located in the forms and firmly held in place by steel wire supports sufficient in number and size to prevent displacement during the course of construction. The wire supports will not be paid for directly but will be considered subsidiary to the item of "Reinforcing Steel."

Shop lists and bending diagrams of reinforcing steel, including wire supports, shall be submitted and approved secured before fabrication is begun.

All chamfers on concrete riser for rail are to be 1/4" x 1/4" x 1/4". Shop drawings showing details of railing shall be submitted and approved secured before fabrication is begun.

The aluminum bridge railing, including posts and fasteners, shall be paid for at the unit price bid per linear foot for "Aluminum Bridge Railing."

A rail connection utilizing set screws is an acceptable alternate and may be supplied at the Contractor's option.

Outside surfaces of cast aluminum posts shall be given a No. 220 grit belt finish after which all exposed surfaces of posts shall receive one coat of clear lacquer. The lacquer shall be applied in the form of a spray and shall be applied to the rail including posts and fasteners shall be paid for at the unit price bid per linear foot for "Steel or Aluminum Bridge Railing."

Slab casting notes:

Floor slabs may be poured in one continuous operation with a strikeoff extending over the whole span length, or may be poured in increments with the center one-third to one-half span length poured first. After the center section is poured not less than 10 hours shall elapse before pouring the end sections. The end sections may be poured simultaneously. If not poured simultaneously, 48 hours shall elapse between end section pours.

SPECIFICATIONS:
Arkansas State Highway Commission Standard Specifications for Highway Construction
Edition of 1959.

Notes: Beam builds are required where modified spans are used or adjacent regular spans have different shoe heights. (See accompanying drawings.)

Use When Difference in (e) Shoe Height is 3' or More.

Use When Difference in (e) Shoe Height is Less Than 3'.

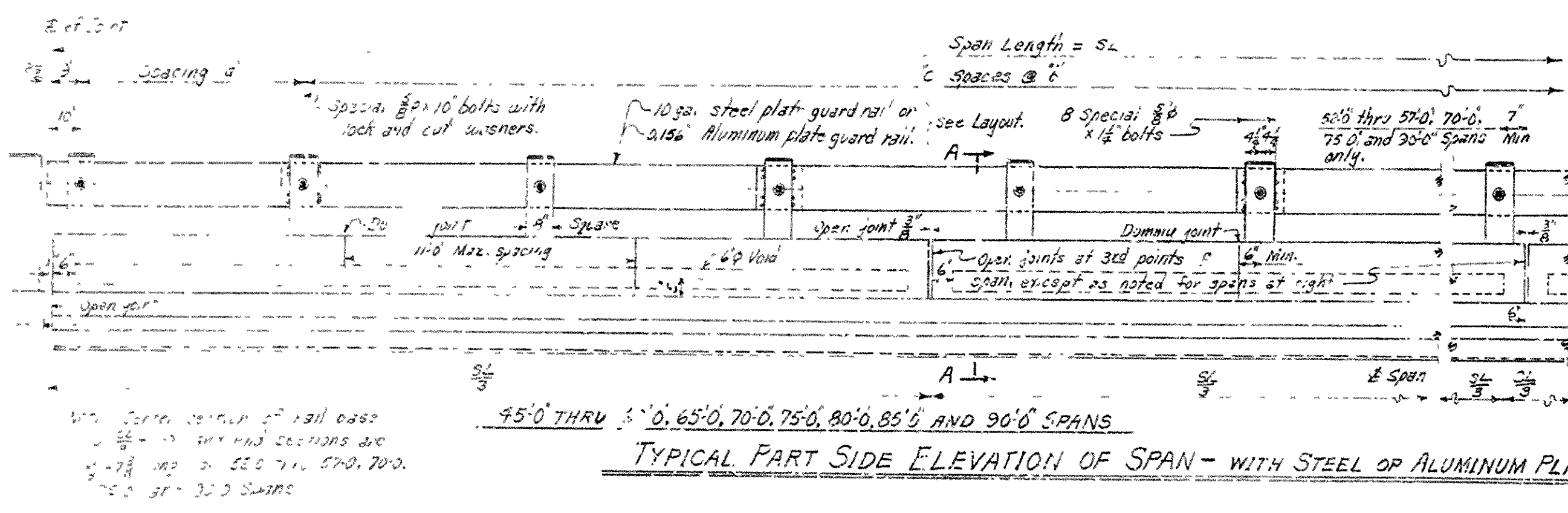
DETAILS COMMON TO STANDARD 35'-0" COMPOSITE I-BEAM SPANS

20', 24', 26', AND 28' ROADWAYS

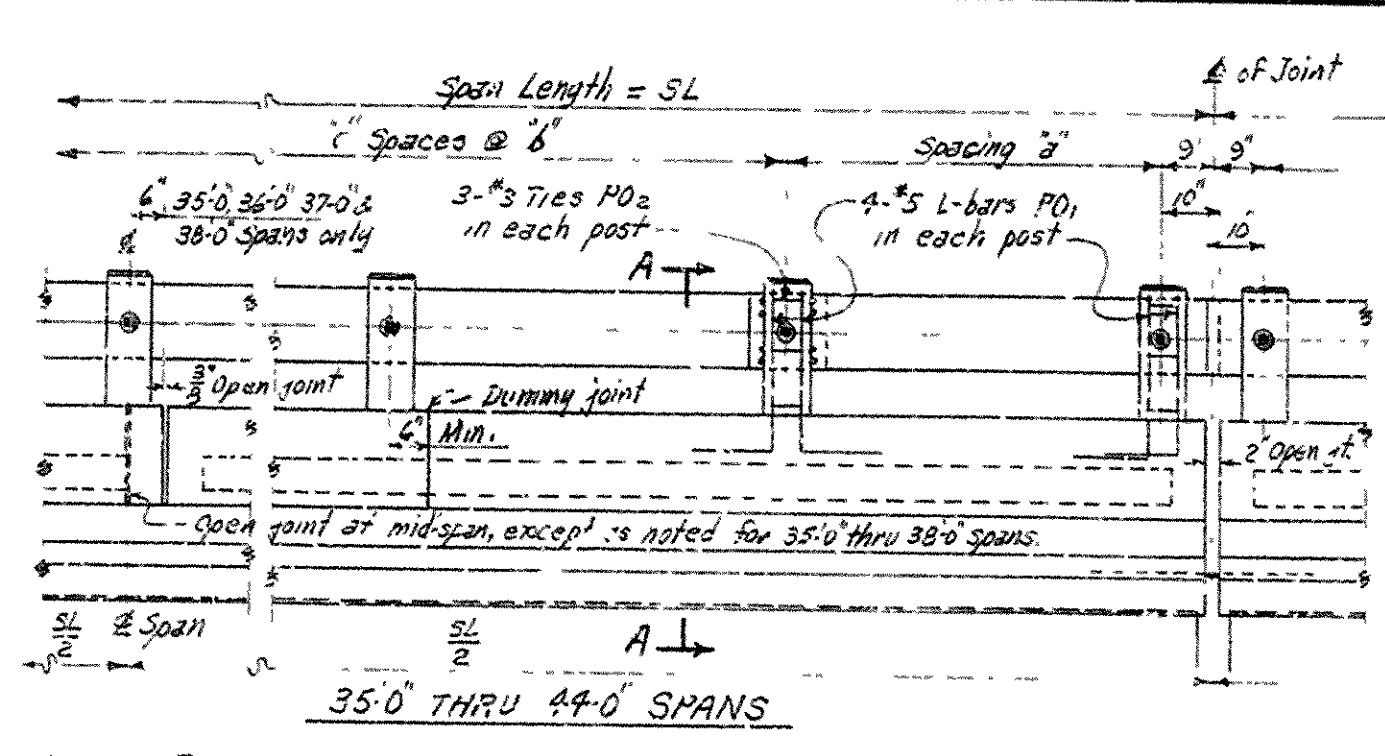
ARKANSAS STATE HIGHWAY COMMISSION

BRIDGE NO. 5462

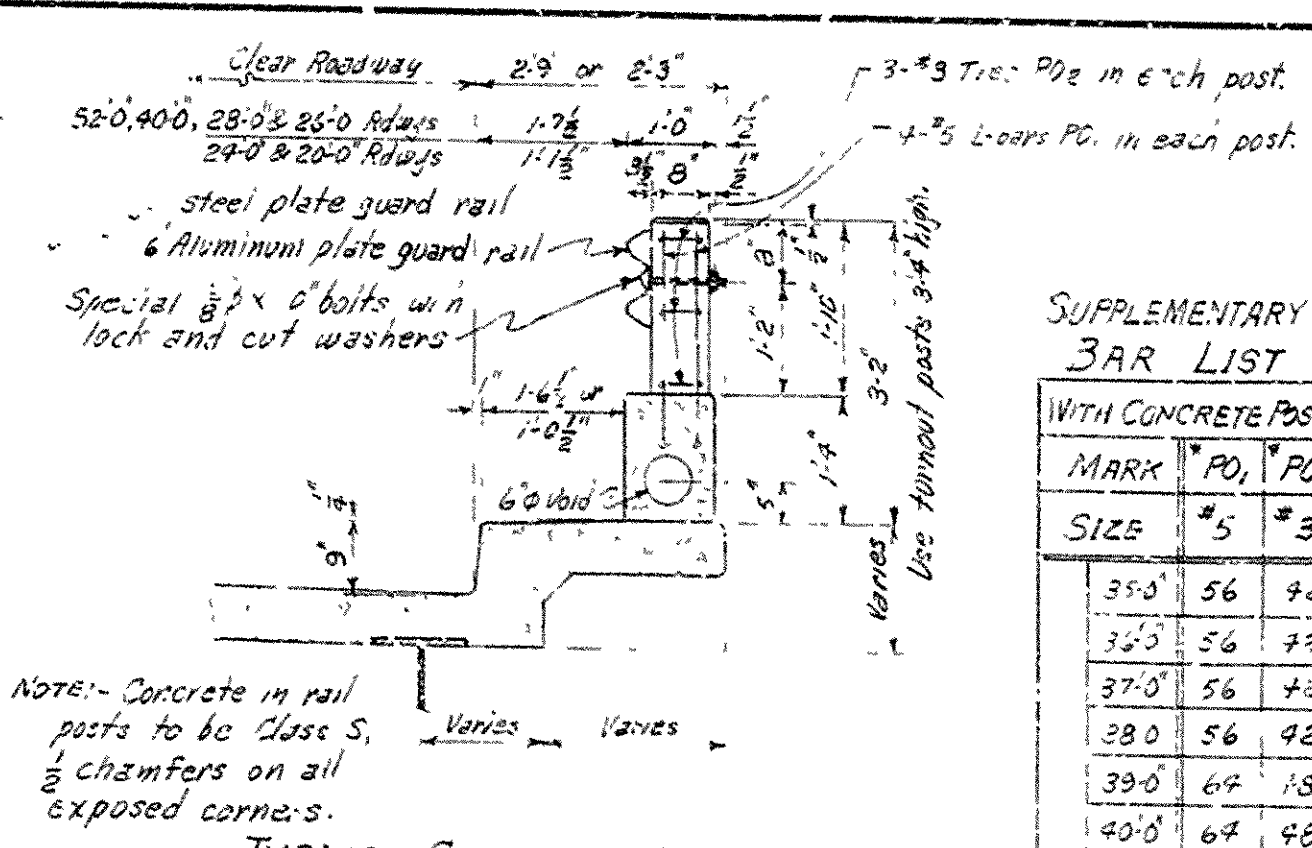
DRAWING NO. 5462



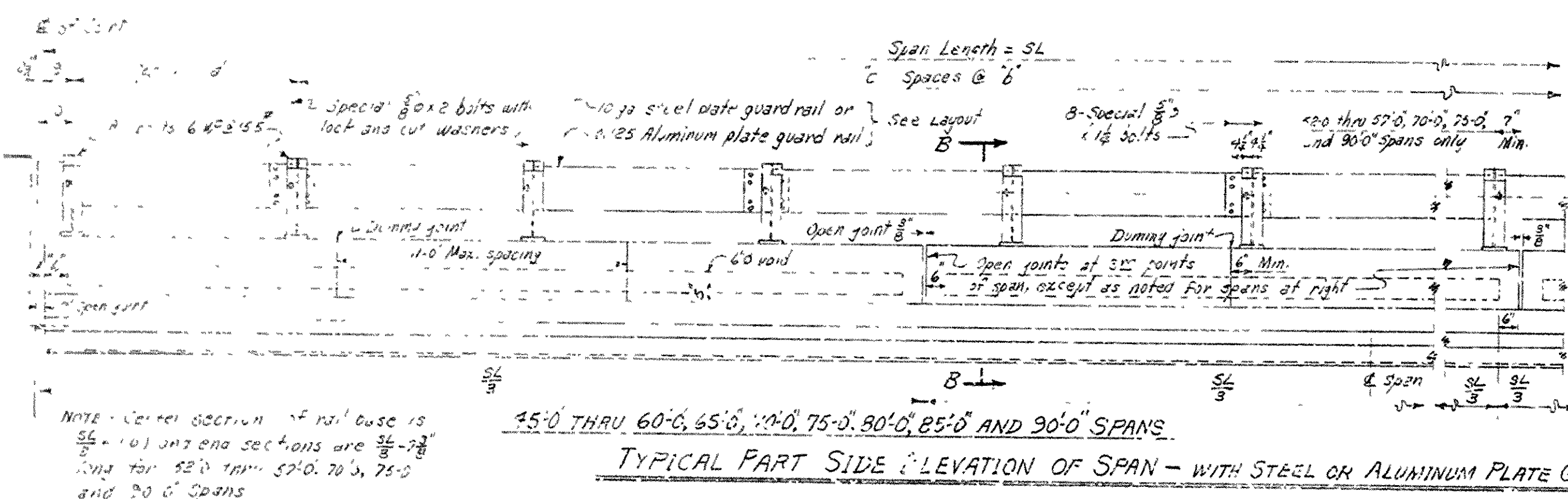
TYPICAL PART SIDE ELEVATION OF SPAN - WITH STEEL OR ALUMINUM PLATE GUARD RAILS AND CONCRETE POSTS



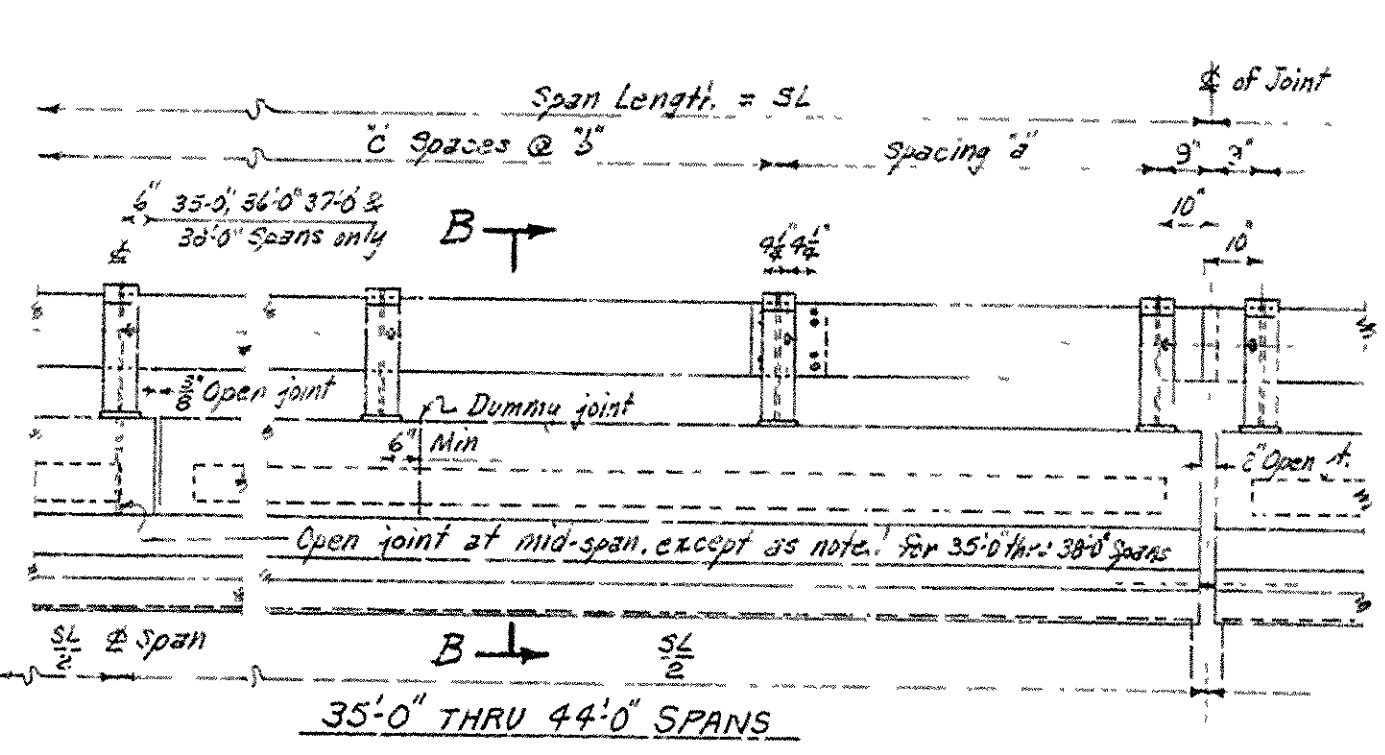
35'0" THRU 44'0" SPANS



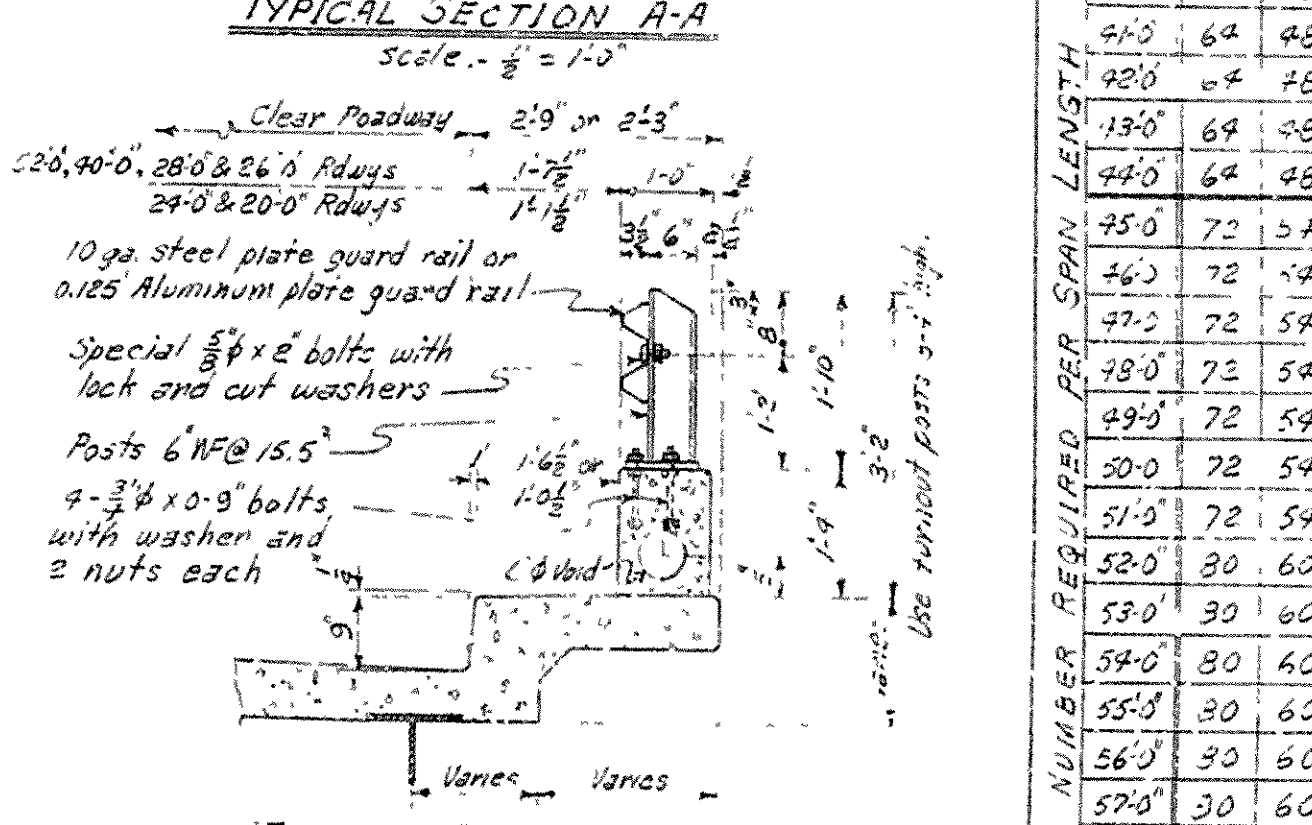
TYPICAL SECTION A-A



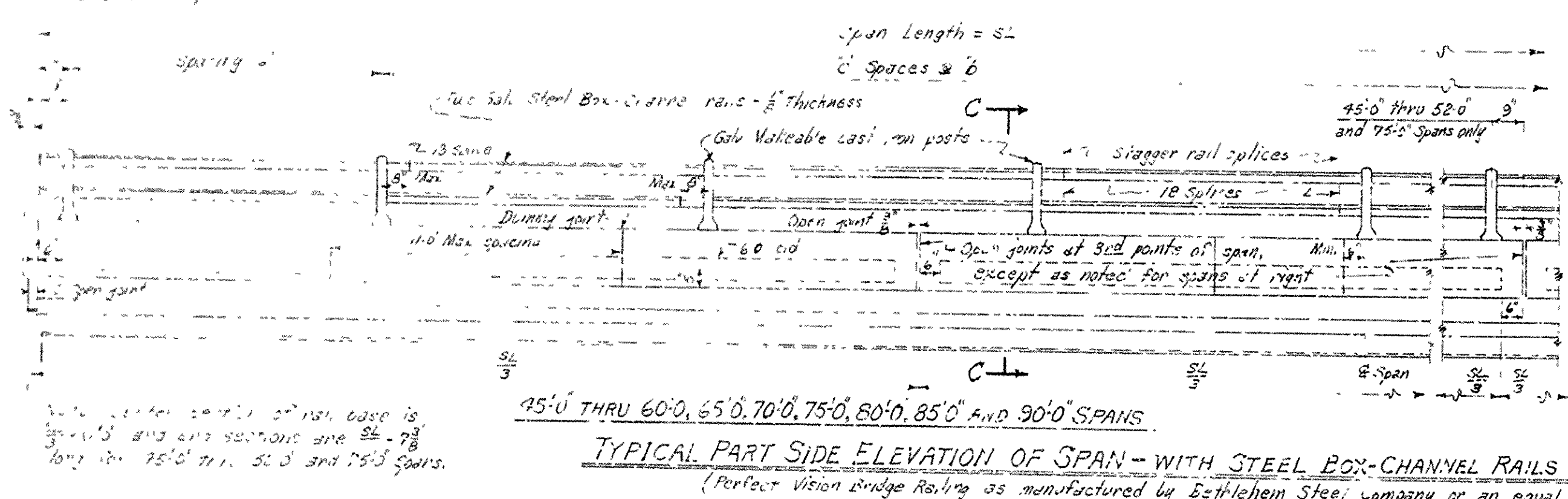
TYPICAL PART SIDE ELEVATION OF SPAN - WITH STEEL OR ALUMINUM PLATE GUARD RAILS AND STEEL POSTS



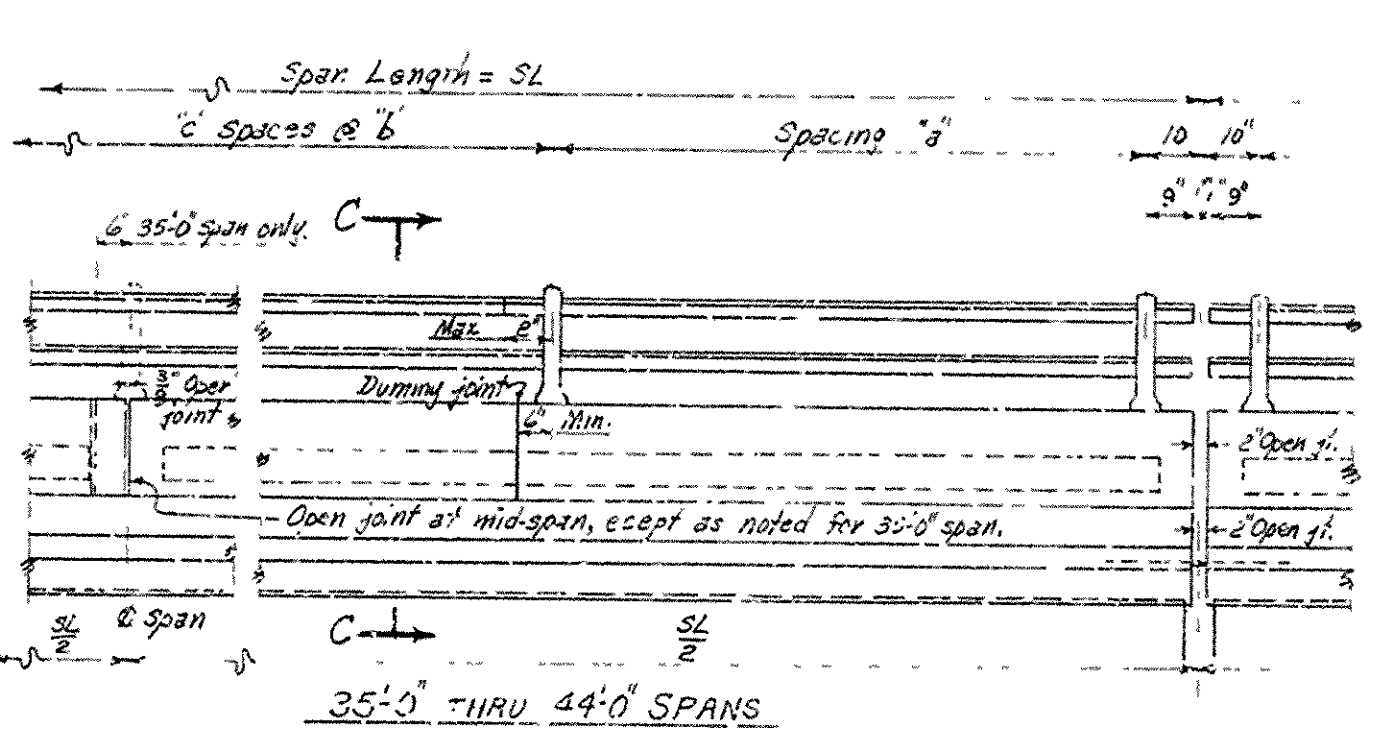
35'0" THRU 44'0" SPANS



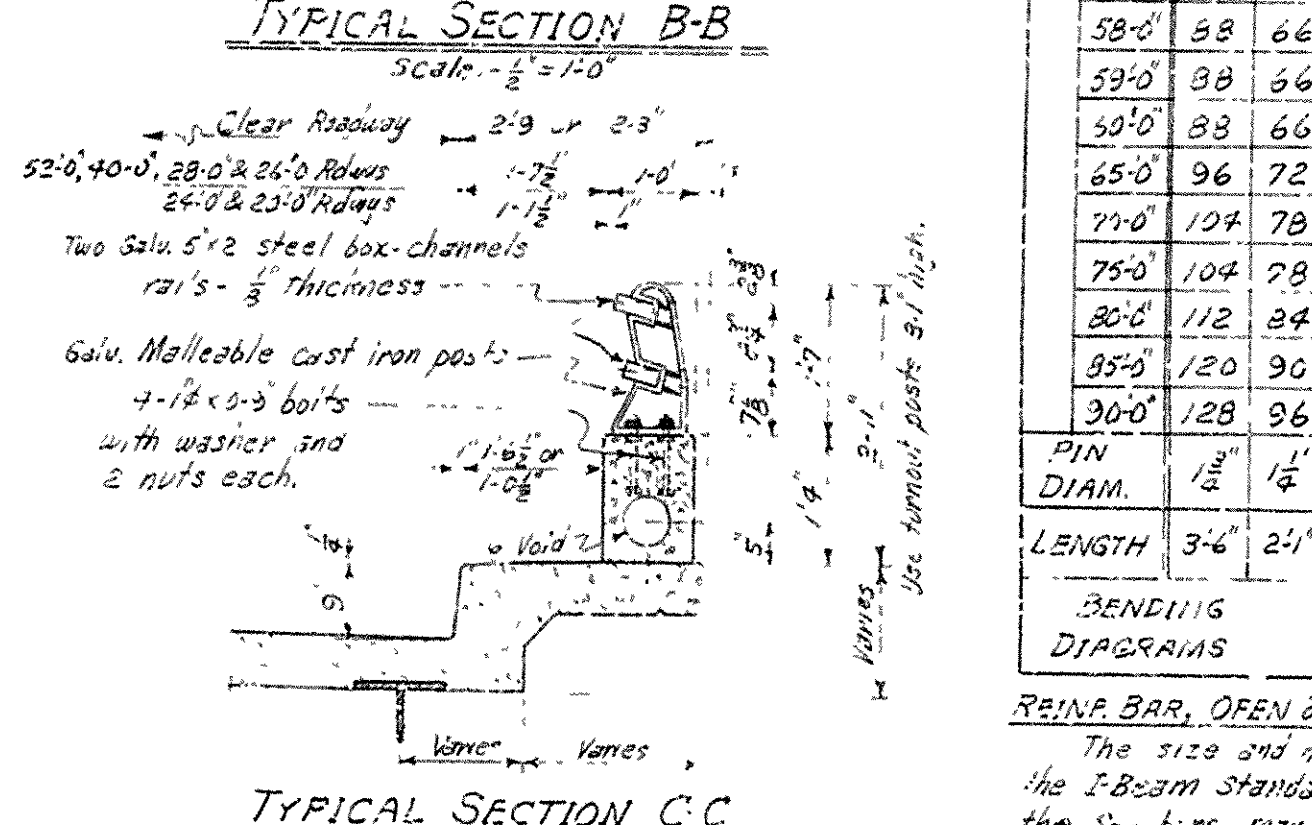
TYPICAL SECTION B-B



TYPICAL PART SIDE ELEVATION OF SPAN - WITH STEEL BOX-CHANNEL RAILS AND M.C.I. POSTS

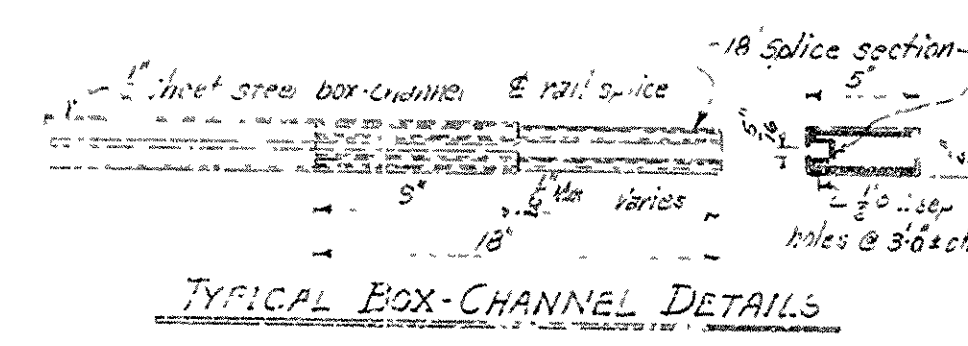


35'0" THRU 44'0" SPANS



TYPICAL SECTION C-C

NOTE: Center section of rail base is 1/2" wide and end sections are 3/4" wide for 45' thru 50', 50', 75' and 90' spans.

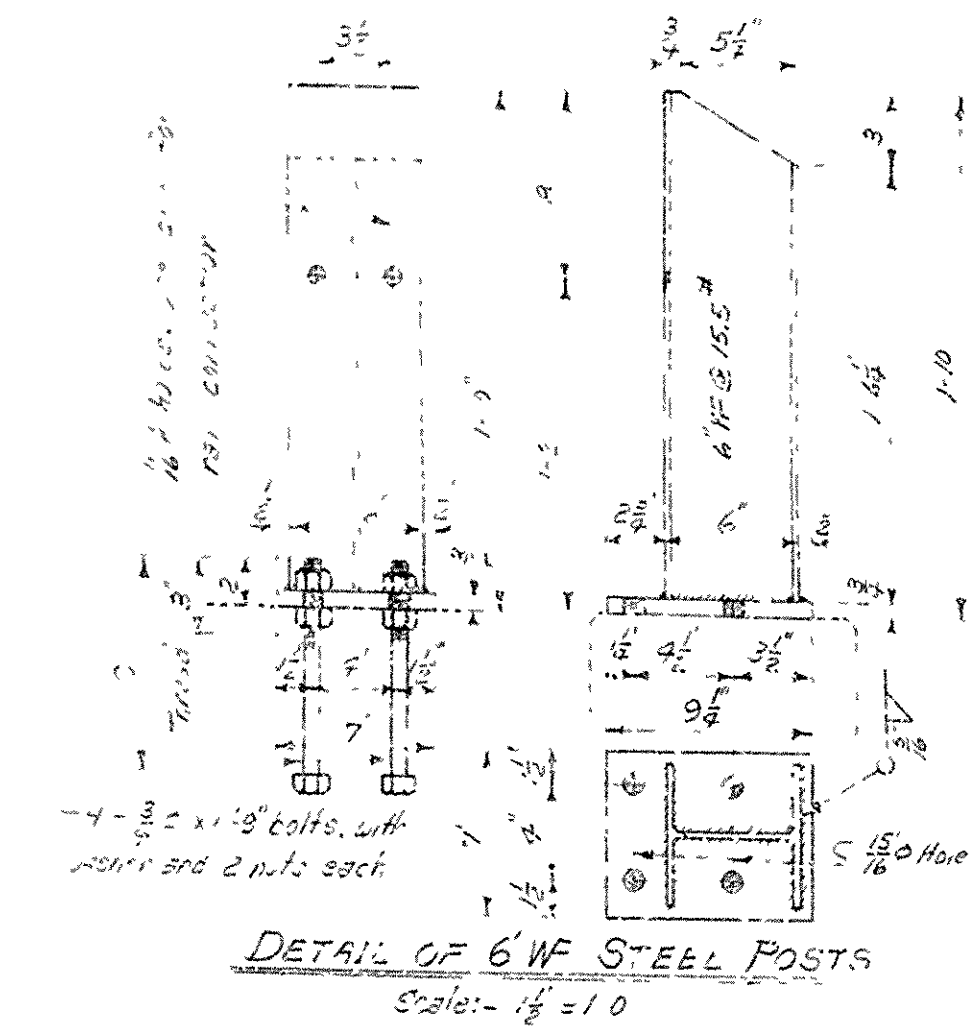


TYPICAL BOX-CHANNEL DETAILS

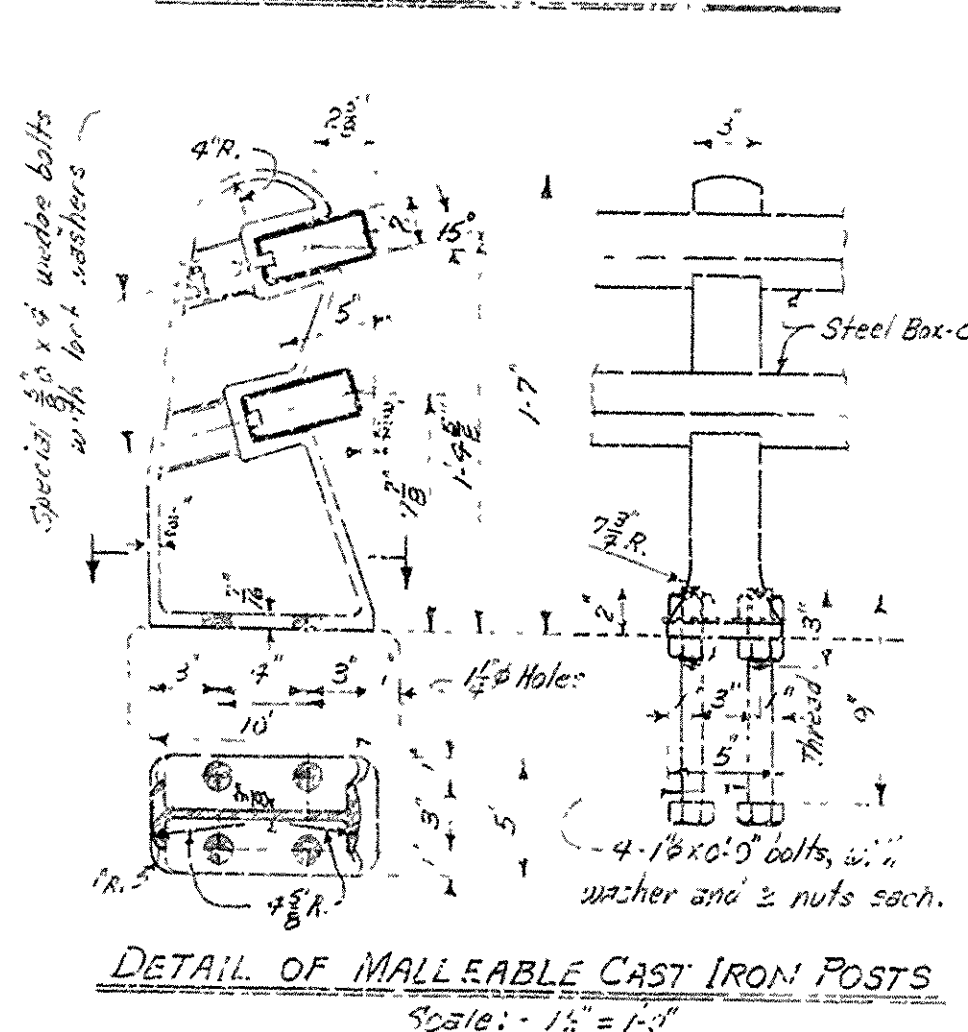
METAL BRIDGE RAILING SPECIFICATIONS:-
Posts- Malleable iron castings, A.S.T.M. A 47, Grade 35018
Galvanized to A.S.T.M. A-123-53.
Rails- 5 1/2" Steel box-channel, A.S.T.M. A-295, Grade L.
Galvanized to A.S.T.M. A-123-53.
Bolts & NUTS- Galvanized to A.S.T.M. A-153.
Washers- Galvanized to A.S.T.M. A-123-53.

GENERAL NOTES:-

PLATE GUARD BRIDGE RAILING- Handrail to be a plate guard rail of the type shown or an equivalent rigid type as approved by the Engineer. The plate guard rails, together with all concrete posts or 6" x 6" steel posts and fastenings, shall be paid for at the unit price bid per linear foot for Steel Plate Guard Bridge Railing or Aluminum Plate Guard Bridge Railing, See Layout for type used.
METAL BRIDGE RAILING- The steel box-channel rails and M.C.I. posts shall be of the type shown or an equivalent type as approved by the Engineer. The box-channel rails, together with all M.C.I. posts and fastenings, shall be paid for at the unit price bid per linear foot for Metal Bridge Railing.



DETAIL OF 6WF STEEL POSTS



DETAIL OF MALLEABLE CAST IRON POSTS

NOTE: All posts to be placed normal to the roadway grade.
Provide miter chimes where necessary to adjust posts to grade.

SUPPLEMENTARY		VARIABLE DIMENSIONS-	
BAR LIST		RAIL POST SPACING	
MARK	SIZE	SPAN LENGTH	POST SPACING WITH PLATE GUARD RAILS
END	INTERIOR	END	INTERIOR
35'0"	56'42"	35'0"	5'5"
36'0"	56'42"	36'0"	5'7"
37'0"	56'42"	37'0"	5'9"
38'0"	56'42"	38'0"	5'11"
39'0"	64'18"	39'0"	5'2"
40'0"	64'48"	40'0"	5'6"
41'0"	64'48"	41'0"	5'7"
42'0"	64'48"	42'0"	5'8"
43'0"	64'48"	43'0"	5'9"
44'0"	64'48"	44'0"	5'10"
45'0"	72'15"	45'0"	5'3"
46'0"	72'15"	46'0"	5'6"
47'0"	72'15"	47'0"	5'6"
48'0"	72'15"	48'0"	5'6"
49'0"	72'15"	49'0"	5'9"
50'0"	72'15"	50'0"	5'9"
51'0"	72'15"	51'0"	6'0"
52'0"	80'60"	52'0"	5'5"
53'0"	80'60"	53'0"	5'7"
54'0"	80'60"	54'0"	5'10"
55'0"	80'60"	55'0"	5'10"
56'0"	80'60"	56'0"	5'8"
57'0"	80'60"	57'0"	5'10"
58'0"	88'66"	58'0"	5'7"
59'0"	88'66"	59'0"	5'5"
60'0"	88'66"	60'0"	5'9"
61'0"	88'66"	61'0"	5'9"
62'0"	96'72"	62'0"	5'5"
63'0"	96'72"	63'0"	5'6"
64'0"	96'72"	64'0"	5'6"
65'0"	96'72"	65'0"	5'9"
66'0"	104'78"	66'0"	5'6"
67'0"	104'78"	67'0"	5'10"
68'0"	104'78"	68'0"	5'10"
69'0"	104'78"	69'0"	5'11"
70'0"	104'78"	70'0"	5'9"
71'0"	104'78"	71'0"	5'9"
72'0"	104'78"	72'0"	5'9"
73'0"	104'78"	73'0"	5'10"
74'0"	104'78"	74'0"	5'10"
75'0"	104'78"	75'0"	5'11"
76'0"	104'78"	76'0"	5'11"
77'0"	104'78"	77'0"	5'11"
78'0"	104'78"	78'0"	5'11"
79'0"	104'78"	79'0"	5'11"
80'0"	104'78"	80'0"	5'11"
81'0"	104'78"	81'0"	5'11"
82'0"	104'78"	82'0"	5'11"
83'0"	104'78"	83'0"	5'11"
84'0"	104'78"	84'0"	5'11"
85'0"	104'78"	85'0"	5'11"
86'0"	104'78"	86'0"	5'11"
87'0"	104'78"	87'0"	5'11"
88'0"	104'78"	88'0"	5'11"
89'0"	104'78"	89'0"	5'11"
90'0"	104'78"	90'0"	5'11"

REIN. BAR, OPEN & DUMMY JOINT NOTES FOR RAIL BASE:-
The size and number of 3/4" bars will be the same as for the I-beam standard used. The number and length of the 3/4" bars may vary from that shown on the I-beam std. due to shift in location of open joints in the rail base.
For spans 44' and under the open joint will occur at the mid-point of span, except where it is shifted to clear post as shown on Elevation. For spans 45' thru 90' the open joints will occur at the third-points of span, except where it is shifted to clear posts, as shown in the Elevation. One or more dummy joints shall be placed in each section of the rail base, (1/4" min. spacing) but in no case closer than 6' from E of pos. S.

NOTE: This drawing to be used as a supplementary drawing for bridge railing to the standard or special drawings as listed on the Layout sheet of each bridge.

SUPPLEMENTARY DETAILS OF BRIDGE RAILING
ON CONCRETE PARAPET WALL FOR
STANDARD COMPOSITE I-BEAM SPANS
35'0" TO 90'0" SPANS INCL.
FOR VARIOUS WIDTHS OF ROADWAYS
SLOPED AND CROWNED ROADWAY
ROUTE SEC.
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: W. C. H. DATE: 11-30-60
TRACED BY: W. C. H. DATE: 12-6-60
CHECKED BY: G. W. DATE: 1-2-61
BRIDGE NO. DRAWING NO. 5462-4.