

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	1	12
HWYS. 82, 270 & 278 SLIDE REPAIRS (S)						

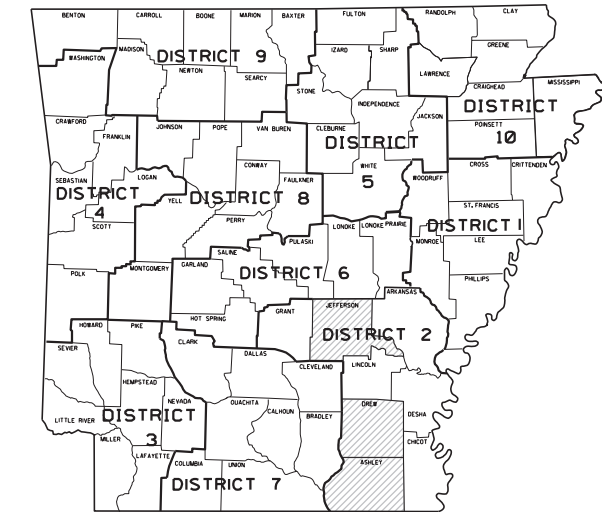
ARKANSAS DEPARTMENT OF TRANSPORTATION
CONSTRUCTION PLANS

HWYS. 82, 270 & 278
SLIDE REPAIRS (S)
ROUTES 82, 270 & 278
SECTIONS 9, 11 & 1S

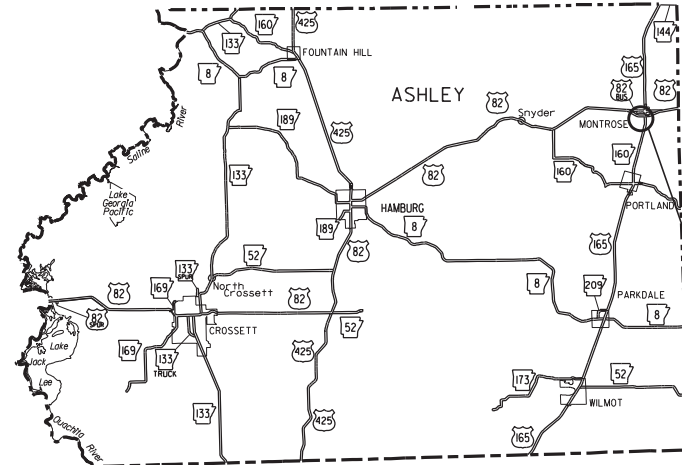
ASHLEY, DREW & JEFFERSON COUNTIES

JOB 020791

FED. AID PROJ. ER-0076(282)



ARKANSAS HIGHWAY DISTRICT 2

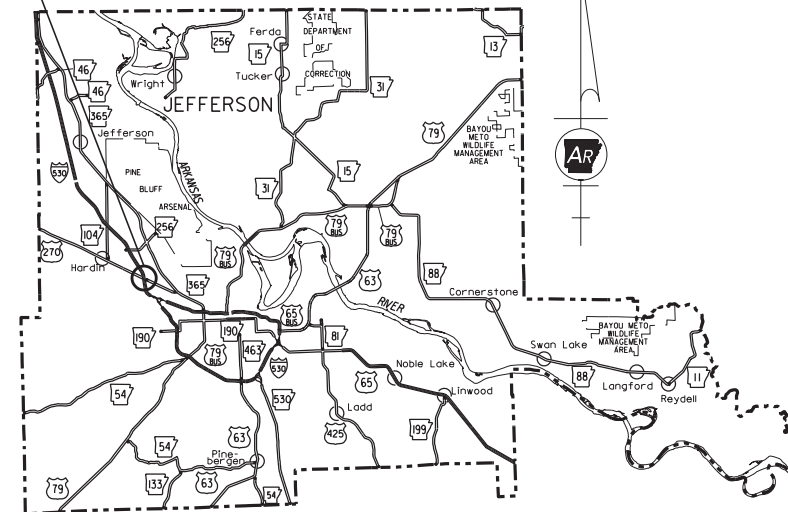


DISTRICT 2 - ASHLEY COUNTY

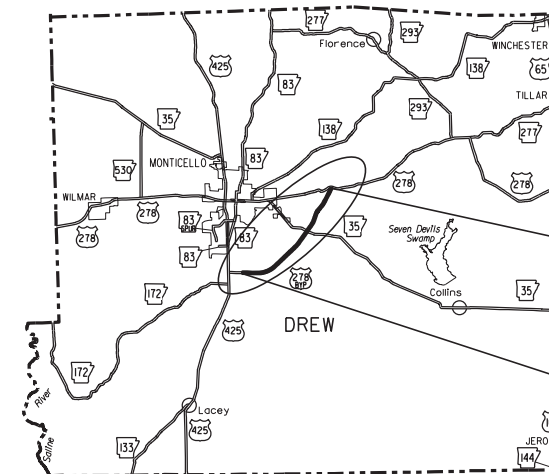


SITE 1 - HWY. 82
LOG MILE 19.460

SITE 2 - HWY. 270
LOG MILE 6.850



DISTRICT 2 - JEFFERSON COUNTY



DISTRICT 2 - DREW COUNTY



HWY. 278 - SECTION IS
END SITE 3
LOG MILE 8.728

HWY. 278 - SECTION IS
BEGIN SITE 3
LOG MILE 0.270

JOB 020791	SITE 1 - MIDPOINT	SITE 2 - MIDPOINT	SITE 3 - MIDPOINT
LATITUDE	N 33°17'40"	N 34°15'41"	N 33°35'16"
LONGITUDE	W 91°29'36"	W 92°06'14"	W 91°43'55"

PROJECT LENGTH COMPUTED ALONG C.L. OF PROJECT HWYS.			
GROSS LENGTH OF PROJECT	44716.20	FEET	8.469 MILES
NET LENGTH OF ROADWAY	12198.00	FEET	2.310 MILES
NET LENGTH OF BRIDGES	000.00	FEET	0.000 MILES
NET LENGTH OF PROJECT	12198.00	FEET	2.310 MILES

- NOTES:
- REFER TO SPECIAL DETAILS FOR INSET ENLARGEMENTS AT SITE LOCATIONS
 - REFER TO SPECIAL DETAILS FOR PROJECT LENGTH DATA



02-03-2023

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	2	12
INDEX OF SHEETS AND STANDARD DRAWINGS						



INDEX OF SHEETS

SHEET NO.	TITLE
1	TITLE SHEET
2	INDEX OF SHEETS AND STANDARD DRAWINGS
3	GOVERNING SPECIFICATIONS AND GENERAL NOTES
4 - 9	SPECIAL DETAILS
10	MAINTENANCE OF TRAFFIC DETAILS
11	QUANTITIES
12	SUMMARY OF QUANTITIES AND REVISIONS

BRIDGE STANDARD DRAWINGS

DRWG.NO.	TITLE	DATE
55001	STANDARD DETAILS FOR DUMPED RIPRAP AND FILTER BLANKET AND COMPUTING EXCAVATION FOR STRUCTURES	02-27-14
55002	STANDARD DETAILS FOR CONCRETE RIPRAP	02-27-14

ROADWAY STANDARD DRAWINGS

DRWG.NO.	TITLE	DATE
TC-1	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	11-07-19
TC-2	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	05-20-21
TC-3	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-12-21
TEC-1	TEMPORARY EROSION CONTROL DEVICES	11-16-17

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	3	12
GOVERNING SPECIFICATIONS & GENERAL NOTES						

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-3	CONTRACTOR'S LICENSE
100-4	DEPARTMENT NAME CHANGE
102-2	ISSUANCE OF PROPOSALS
105-4	MAINTENANCE DURING CONSTRUCTION
107-2	RESTRAINING CONDITIONS
108-1	LIQUIDATED DAMAGES
108-2	WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER
110-1	PROTECTION OF WATER QUALITY AND WETLANDS
210-1	UNCLASSIFIED EXCAVATION
306-1	QUALITY CONTROL AND ACCEPTANCE
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
604-3	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES (MASH)
620-1	MULCH COVER
802-4	CEMENT
JOB 020791	BIDDING REQUIREMENTS AND CONDITIONS
JOB 020791	BUY AMERICA - CONSTRUCTION MATERIALS
JOB 020791	CARGO PREFERENCE ACT REQUIREMENTS
JOB 020791	DISADVANTAGED BUSINESS ENTERPRISE BIDDER'S RESPONSIBILITIES
JOB 020791	ESTABLISHING CONTRACT TIME - WORKING DAY CONTRACT
JOB 020791	GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION
JOB 020791	LIQUIDATED DAMAGES PROCEDURE FOR BID LETTINGS
JOB 020791	MANDATORY ELECTRONIC CONTRACT
JOB 020791	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB 020791	PRICE ADJUSTMENT FOR FUEL
JOB 020791	PROHIBITION OF CERTAIN TELECOMMUNICATIONS AND VIDEO SURVEILLANCE SERVICES OR EQUIPMENT
JOB 020791	STORM WATER POLLUTION PREVENTION PLAN
JOB 020791	UTILITY ADJUSTMENTS

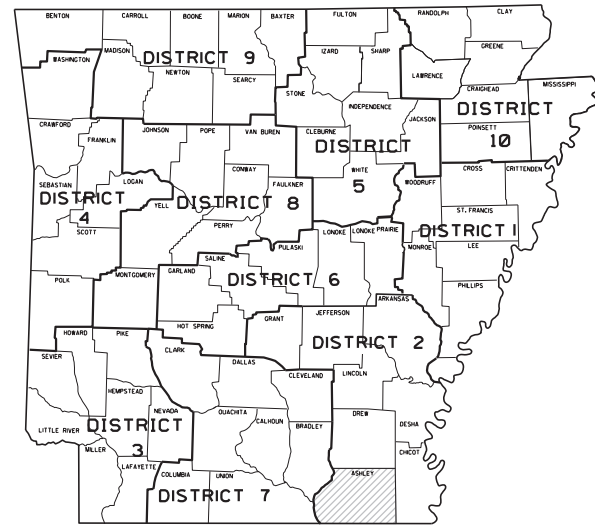
GENERAL NOTES

- ALL PIPE LINES, POWER, TELEPHONE, AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING U. S. MAILBOXES WITHIN THE PROJECT LIMITS IN SUCH A MANNER THAT THE PUBLIC MAY RECEIVE CONTINUED MAIL SERVICE. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO ENSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A FENCE TO CONTROL LIVESTOCK IN AREAS WHERE PASTURES ARE SEVERED. WIRE FENCE MAY BE CONSTRUCTED INITIALLY, OR IN LIEU THEREOF, THE CONTRACTOR AT HIS OWN EXPENSE, MAY ELECT TO PROVIDE TEMPORARY FENCING SUITABLE TO CONTAIN LIVESTOCK.
- ANY DAMAGED SUSTAINED TO THE EXISTING PAVEMENT DURING CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE 2-WAY RADIO COMMUNICATIONS FOR FLAG PERSONS.



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 Jason Toney
 WORKSPACE
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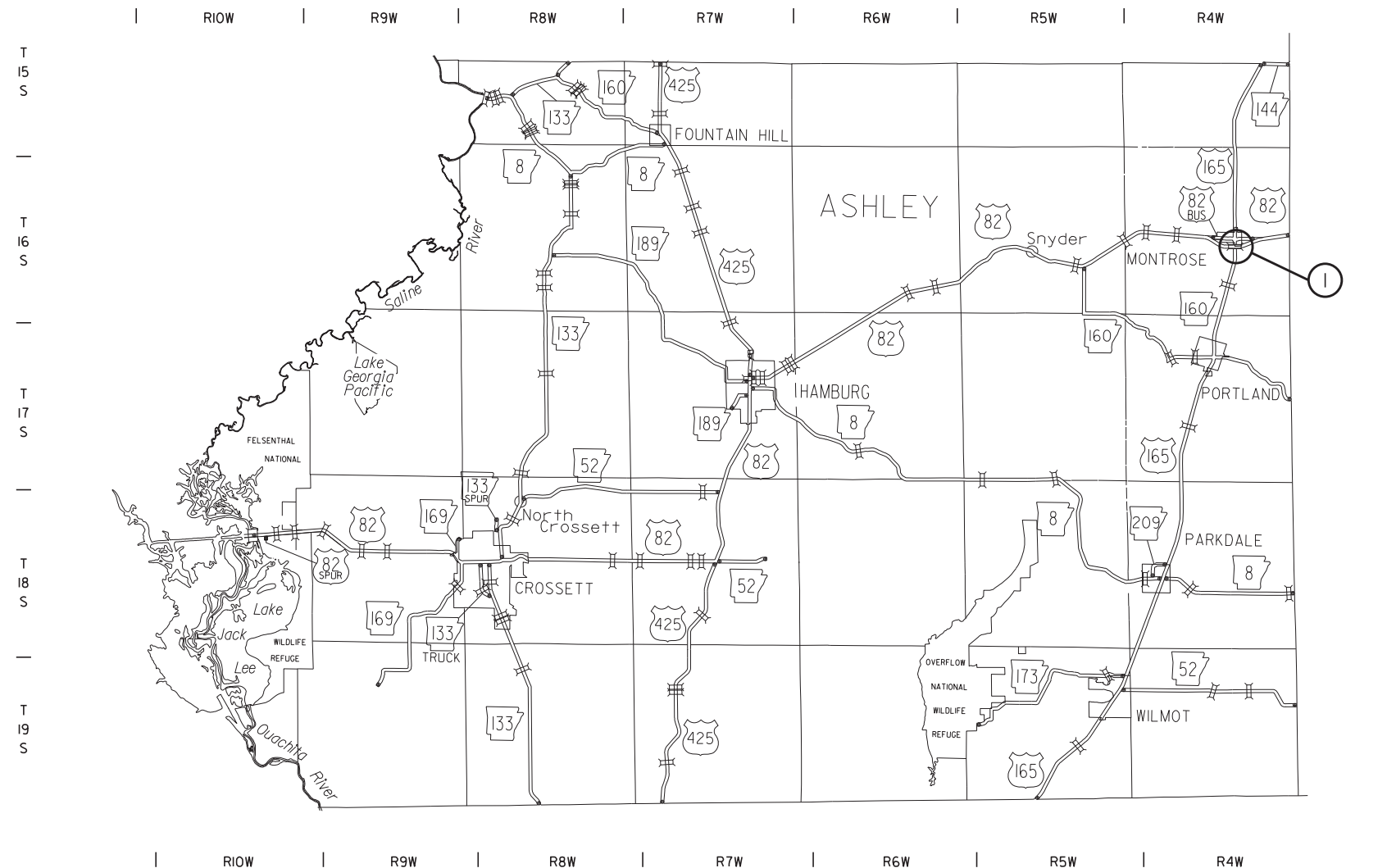
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	4	12
SPECIAL DETAILS						



NOTE: THE PURPOSE OF THIS PROJECT AT SITE 1 IS TO REPAIR APPROXIMATELY 90 LIN. FT. OF FAILED EMBANKMENT AT LOG MILE 19.460 ALONG HWY. 82, SECTION 9 IN ASHLEY COUNTY. THE REPAIR WILL CONSIST OF EXCAVATING AND REMOVING DAMAGED CONCRETE RIPRAP AND ZONE OF FAILED EMBANKMENT, INSTALLING GEOTEXTILE FABRIC AND BACKFILLING WITH DUMPED RIPRAP TO MATCH DESIGN CONFIGURATION OUTLINED IN SITE 1 - HWY. 82 SLIDE REPAIR DETAIL, AND INCORPORATING A KEY TRENCH WITH A TRENCH DRAIN EXTENSION TO DAYLIGHT WATER IN KEY TRENCH AT LOCATION DETERMINED IN THE FIELD BY THE ENGINEER. RESTORE CONCRETE RIPRAP.



ARKANSAS HIGHWAY DISTRICT 2



LEGEND

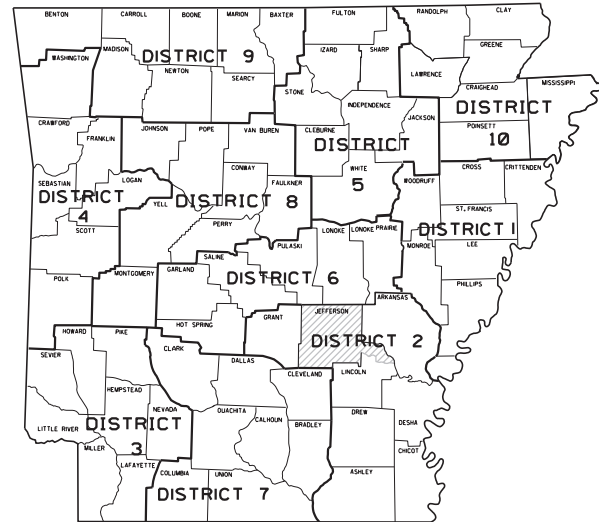
○ - SLIDE REPAIR

① HWY. 82, SEC. 9
LOG MILE 19.460 BR. END
OVER HWY. 165
304' - 0" BRIDGE NO. 07230
40' - 0" CLEAR ROADWAY
(SLIDE REPAIR)

SITE 1 - HWY. 82
ASHLEY COUNTY
INSET - 1
NOT TO SCALE

London Miller 2/27/2023 9:46:48 AM
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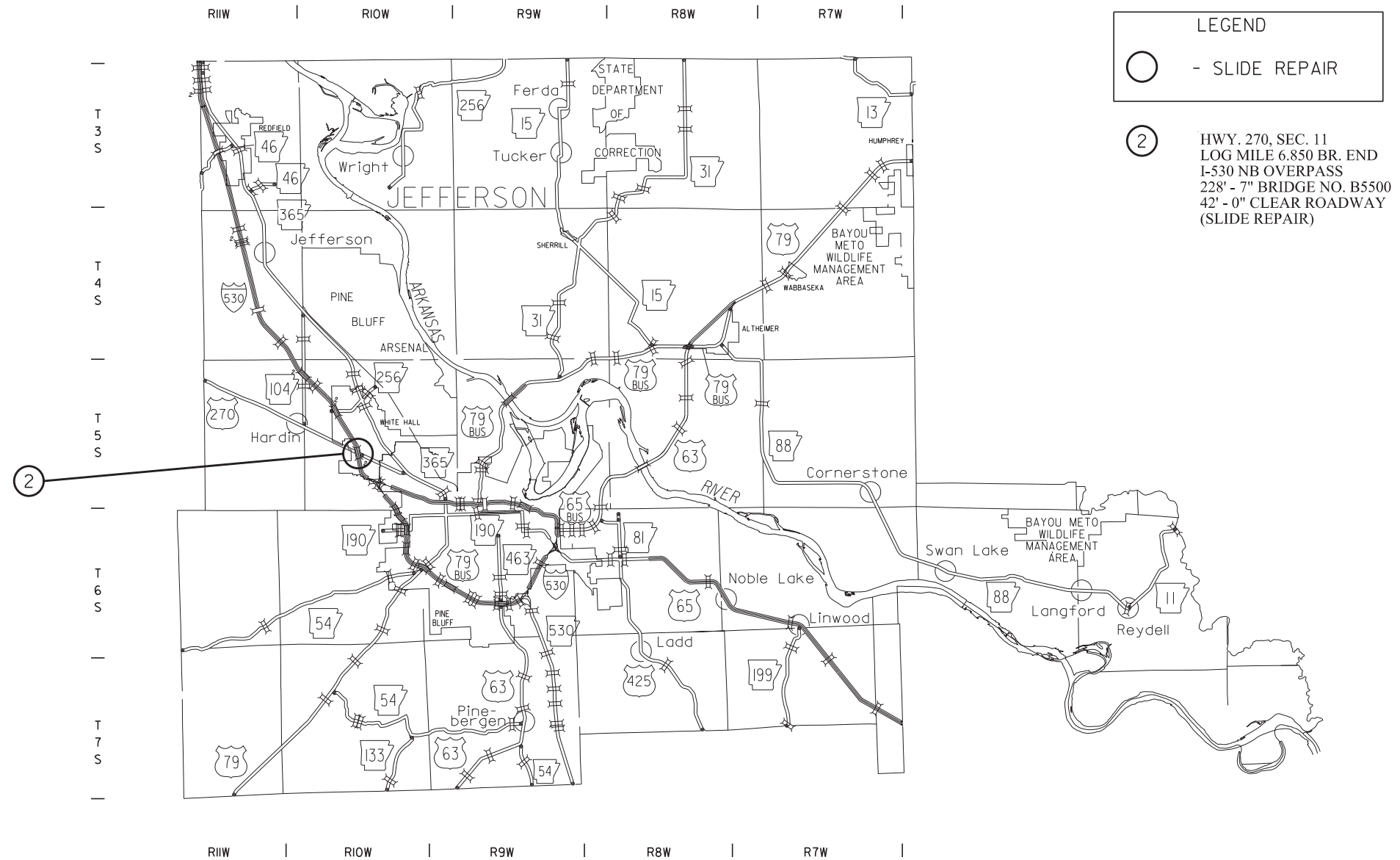
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	5	12
SPECIAL DETAILS						



ARKANSAS HIGHWAY DISTRICT 2



NOTE: THE PURPOSE OF THIS PROJECT AT SITE 2 IS TO REPAIR APPROXIMATELY 63 LIN. FT. OF FAILED EMBANKMENT AT LOG MILE 6.850 ALONG HWY. 270, SECTION 11 IN JEFFERSON COUNTY. THE REPAIR WILL CONSIST OF EXCAVATING AND REMOVING ZONE OF FAILED EMBANKMENT, INSTALLING GEOTEXTILE FABRIC AND BACKFILLING WITH DUMPED RIPRAP TO MATCH DESIGN CONFIGURATION OUTLINED IN SITE 2 - HWY. 270 SLIDE REPAIR DETAIL, AND INCORPORATE A KEY TRENCH WITH A TRENCH DRAIN EXTENSION TO DAYLIGHT WATER IN KEY TRENCH AT LOCATION DETERMINED IN THE FIELD BY THE ENGINEER.



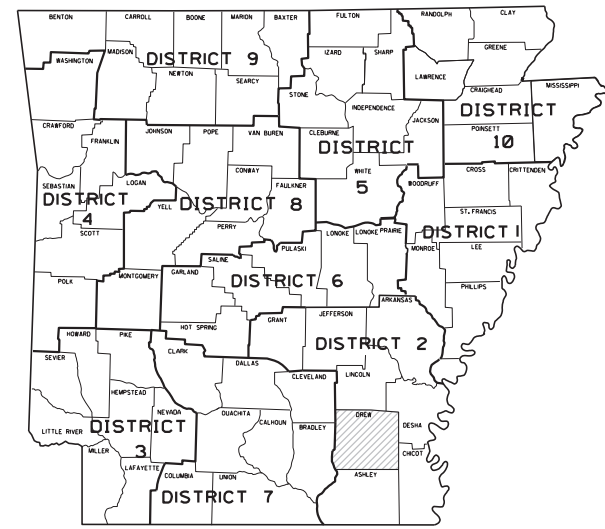
LEGEND

- - SLIDE REPAIR
- ② HWY. 270, SEC. 11
LOG MILE 6.850 BR. END
I-530 NB OVERPASS
228' - 7" BRIDGE NO. B5500
42' - 0" CLEAR ROADWAY
(SLIDE REPAIR)

SITE 2 - HWY. 270
JEFFERSON COUNTY
INSET - 2
NOT TO SCALE

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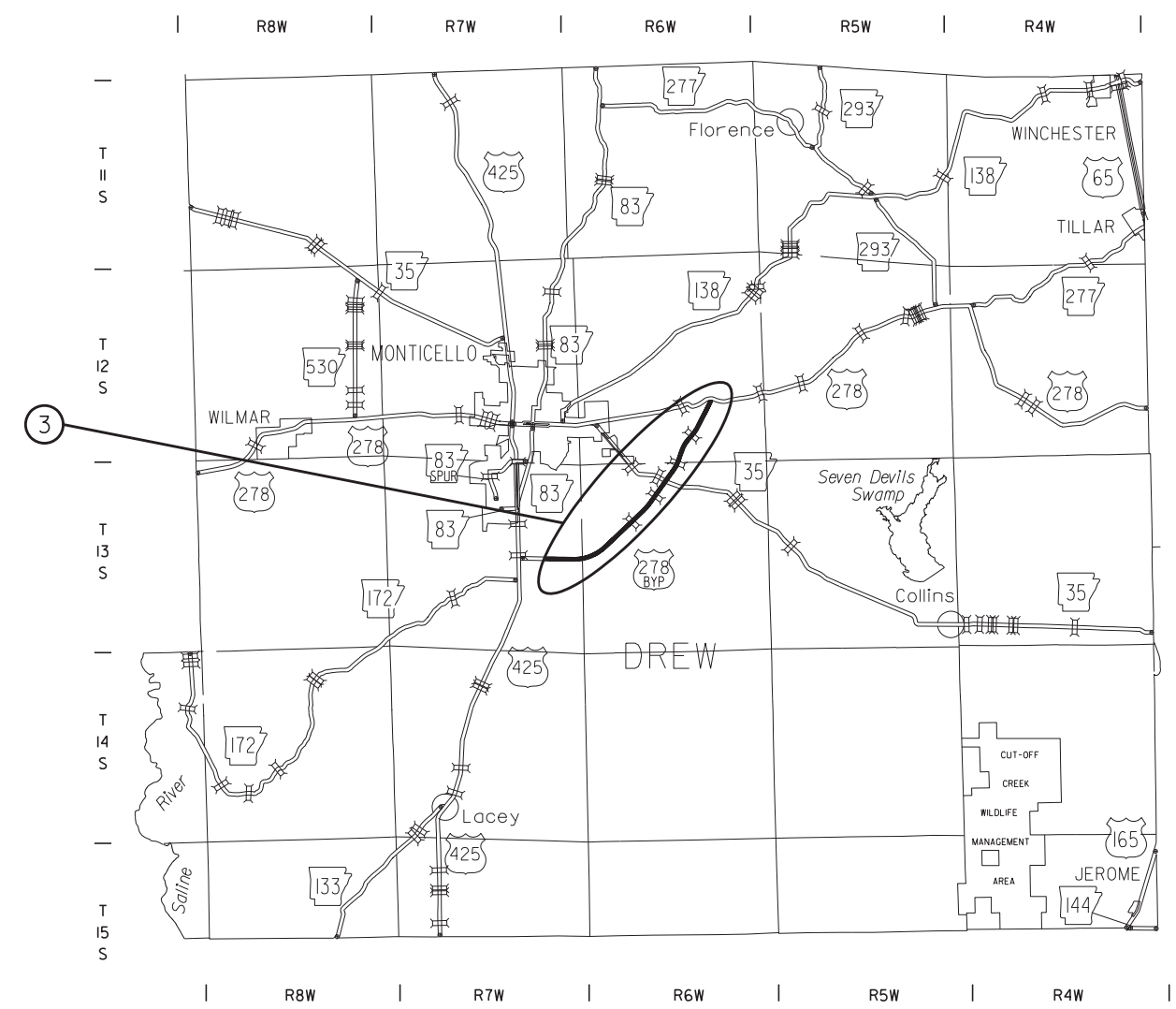
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	6	12
SPECIAL DETAILS						



NOTE: THE PURPOSE OF THIS PROJECT AT SITE 3 IS TO RESHAPE VARIOUS LOCATIONS OF FAILED SLOPE FROM LOG MILE 0.270 TO LOG MILE 8.710 ALONG HWY. 278, SECTION 1S, IN DREW COUNTY. THE WORK WILL CONSIST OF RESHAPING THE SLOPES AND ADDING TOPSOIL AND SEEDING IN ORDER TO CORRECT THE AREAS WITH FAILED SLOPES. THE AREAS OF INTEREST ARE OUTLINED IN THE SITE 3 PROJECT DATA TABLE BELOW.



ARKANSAS HIGHWAY DISTRICT 2



LEGEND

○ - SLIDE REPAIR

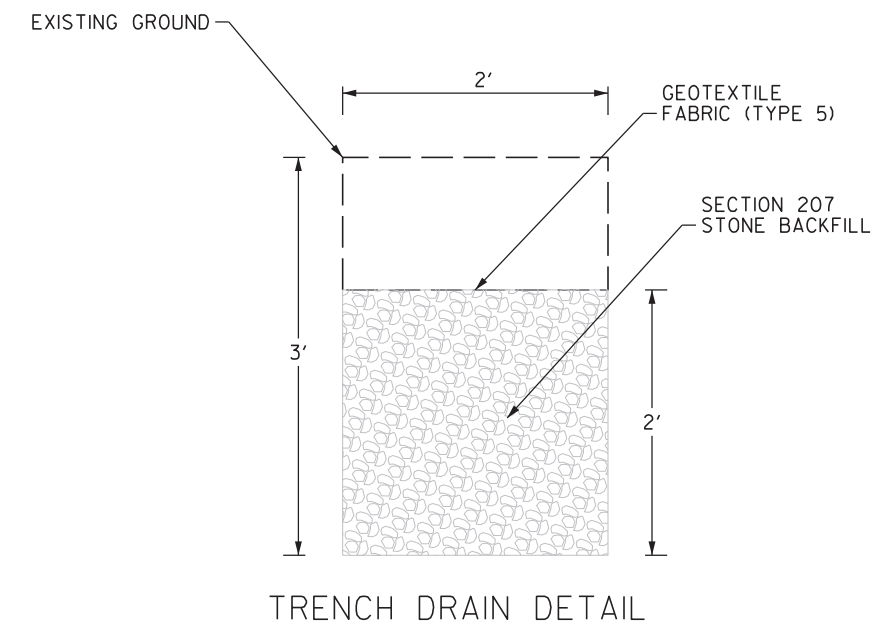
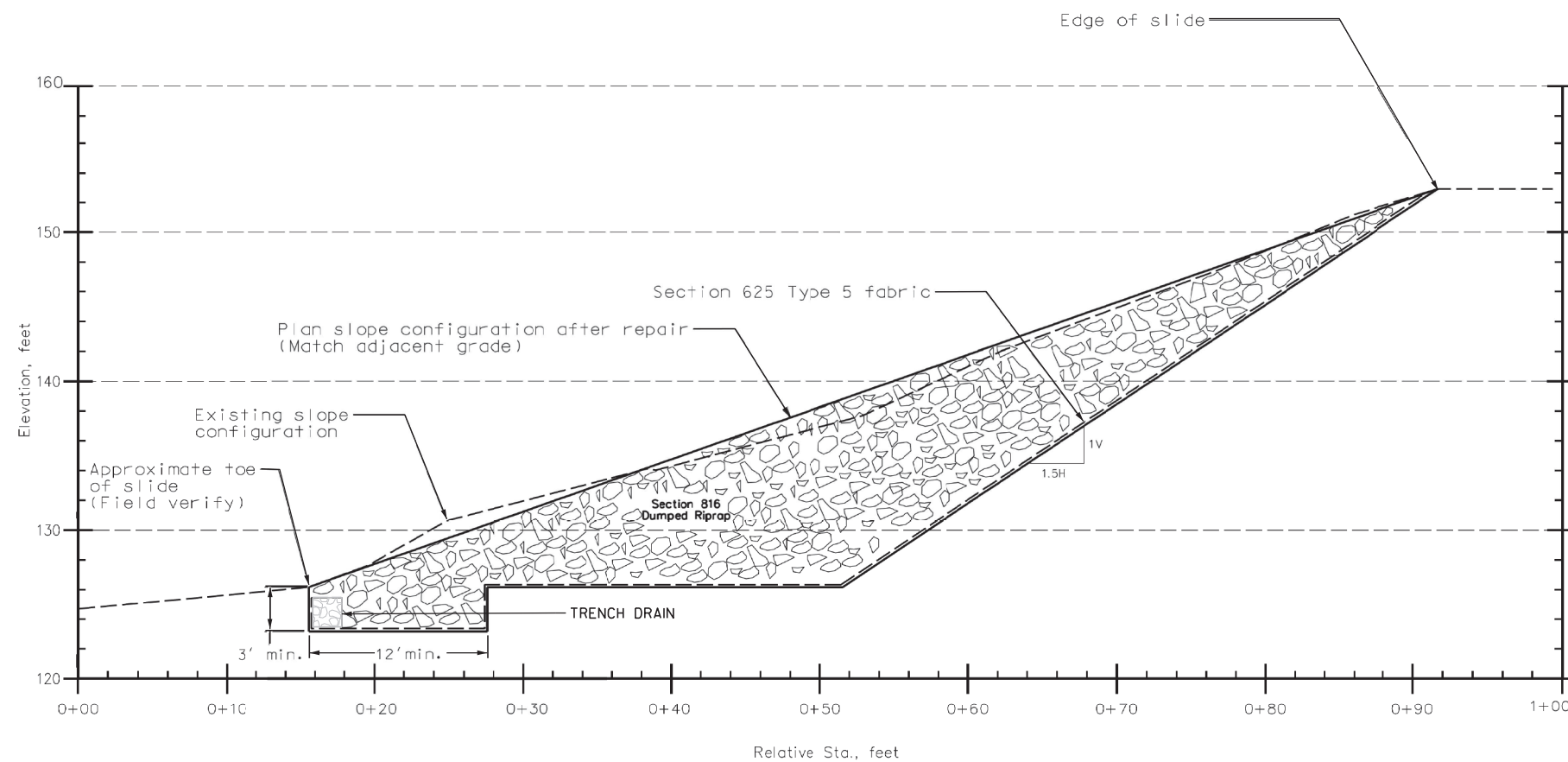
③ HWY. 278, SEC. 1S
 BEGIN LOG MILE 0.270
 END LOG MILE 8.710
 (VARIOUS SLIDE REPAIRS)
 (SEE SITE 3 PROJECT DATA TABLE)

LOG MILE	LOG MILE	SIDE	LENGTH	WIDTH	AREA (SY)	ACRES
0.270	0.315	LT	235	20	522.22	0.11
0.470	0.521	LT	270	20	600.00	0.12
0.580	0.605	RT	130	15	216.67	0.04
0.630	0.656	RT	135	15	225.00	0.05
0.710	0.740	LT	160	20	355.56	0.07
0.760	0.776	LT	85	20	188.89	0.04
0.790	0.806	RT	85	20	188.89	0.04
0.830	0.900	RT	370	20	822.22	0.17
0.860	0.897	LT	195	30	650.00	0.13
0.990	1.079	RT	470	40	2088.89	0.43
1.000	1.056	LT	295	40	1311.11	0.27
1.300	1.342	LT	220	20	488.89	0.10
1.380	1.425	RT	235	20	522.22	0.11
1.400	1.429	LT	155	20	344.44	0.07
1.750	1.776	LT	135	20	300.00	0.06
1.880	2.095	LT	1135	30	3783.33	0.78
1.930	2.145	RT	1135	30	3783.33	0.78
2.200	2.240	RT	210	20	468.67	0.10
2.210	2.402	LT	1015	30	3383.33	0.70
2.280	2.305	RT	130	20	288.89	0.06
4.610	4.631	RT	110	20	244.44	0.05
4.880	4.917	LT	195	25	541.67	0.11
4.880	4.917	RT	195	25	541.67	0.11
5.050	5.068	LT	95	20	211.11	0.04
6.550	6.867	RT	1675	20	3722.22	0.77
6.550	6.569	LT	100	15	166.67	0.03
6.690	6.822	LT	695	20	1544.44	0.32
7.270	7.308	RT	200	15	333.33	0.07
7.380	7.612	RT	1225	20	2722.22	0.56
8.320	8.445	LT	660	15	1100.00	0.23
8.710	8.728	RT	95	15	158.33	0.03

SITE 3 - HWY. 278
 DREW COUNTY
 INSET - 3
 NOT TO SCALE

London Miller 2/27/2023 9:46:50 AM
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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	7	12
SPECIAL DETAILS						



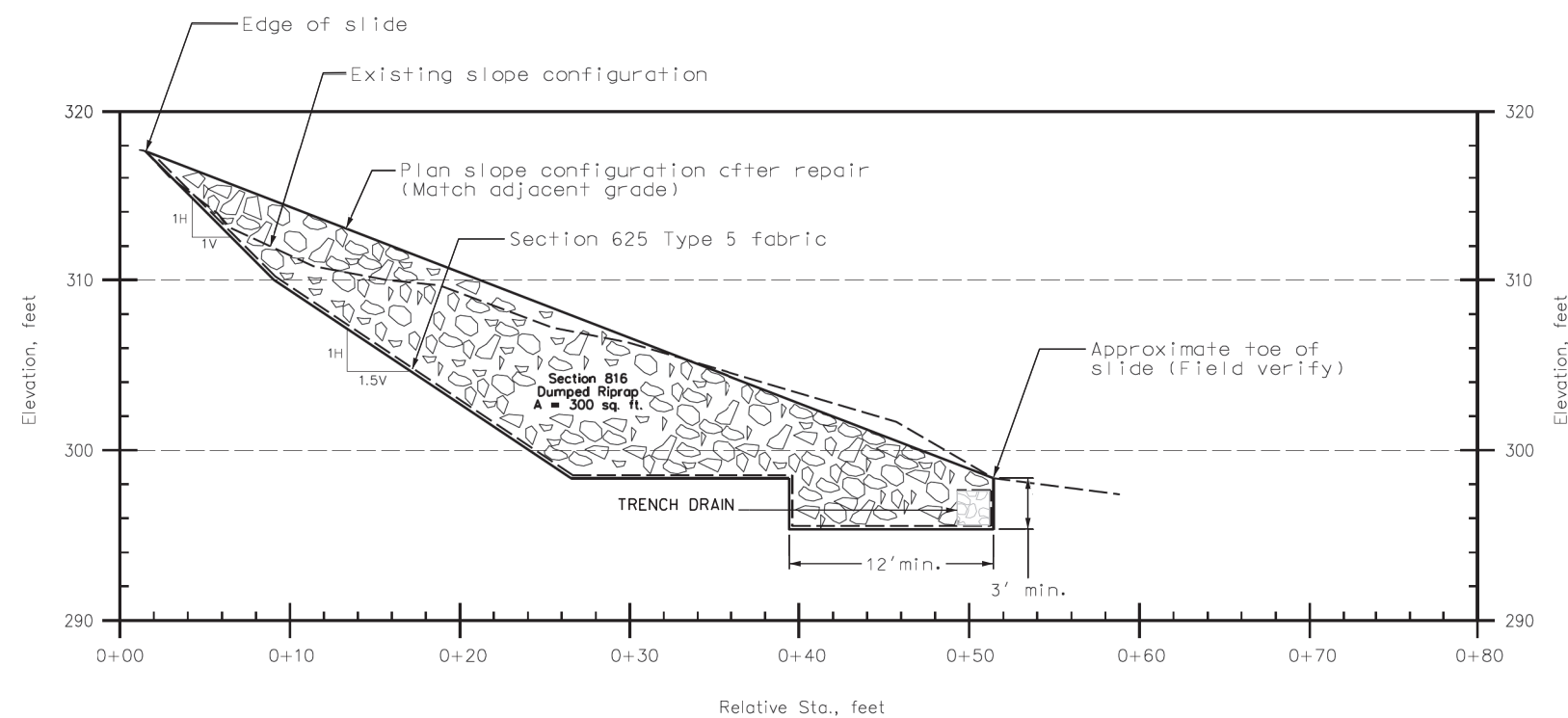
SITE 1 - HWY. 82
SLIDE REPAIR DETAIL

NOTES:

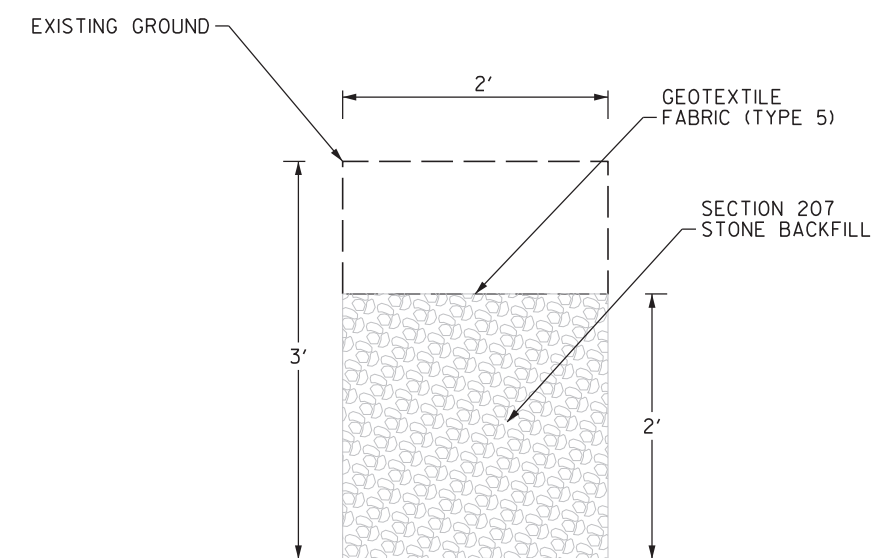
1. REFER TO ARDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION, 2014 EDITION FOR SECTION INFORMATION.
2. THE ANTICIPATED LENGTH PARALLEL TO HWY. 82 C.L. IS 90 FT.
3. THE TOP 18 INCHES OF DUMPED RIPRAP UNDER THE DAMAGED CONCRETE RIPRAP SHALL BE REDUCED IN SIZE AND CAPPED WITH A LAYER OF SECTION 802.02 (C) COARSE AGGREGATE FOR CLASS S CONCRETE (#57 STONE). SEE STANDARD DRAWING 550002 FOR MORE DETAILS.
4. STONE BACKFILL AND #57 STONE SHALL BE PAID FOR AS DUMPED RIPRAP.
5. SECTION 625 TYPE 5 FILTER FABRIC SHALL BE GEOTEXTILE FABRIC (TYPE 5)

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	8	12
SPECIAL DETAILS						



SITE 2 - HWY. 270
SLIDE REPAIR DETAIL



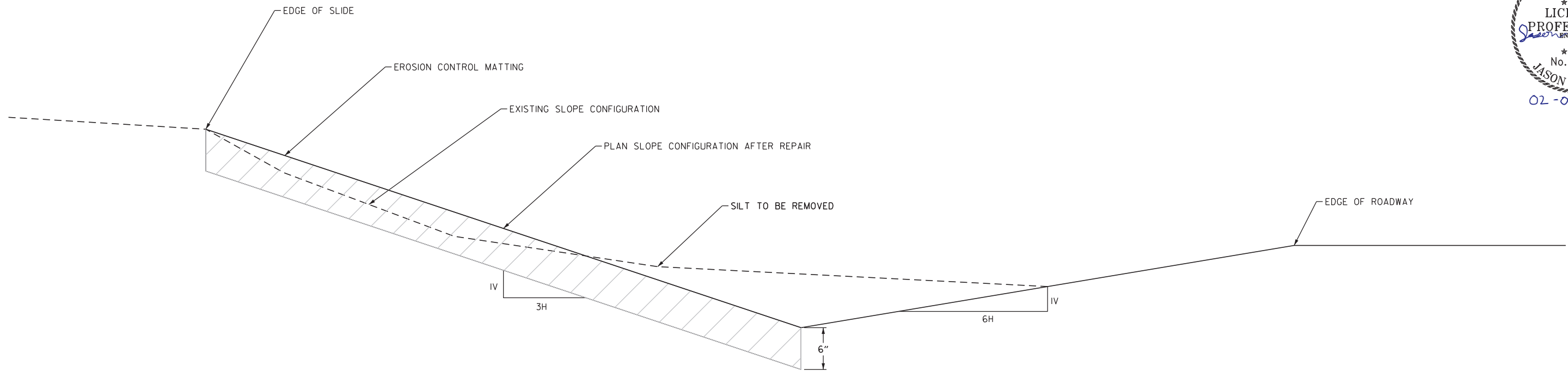
TRENCH DRAIN DETAIL

NOTES:

1. REFER TO ARDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION, 2014 EDITION FOR SECTION INFORMATION.
2. THE ANTICIPATED LENGTH PARALLEL TO HWY. 270 C.L. IS 63 FT.
3. STONE BACKFILL SHALL BE PAID FOR AS DUMPED RIPRAP.
4. SECTION 625 TYPE 5 FILTER FABRIC SHALL BE GEOTEXTILE FABRIC (TYPE 5)

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	9	12
SPECIAL DETAILS						



NOTES:

1. REFER TO ARDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION, 2014 EDITION FOR SECTION INFORMATION.
2. THE ANTICIPATED LENGTH PARALLEL TO HWY. 278 C.L. IS 12045 FT.
3. ALL SILT REMOVED TO BE PAID FOR AS UNCLASSIFIED EXCAVATION AND SHALL CONSIST ON THE EXCAVATION AND DISPOSAL OF ALL MATERIALS ENCOUNTERED IN THE WORK.
4. THE TOP 6" OF THE EXISTING SLOPE CONFIGURATION SHALL BE REMOVED AND REPLACED WITH TOP SOIL. THE SLOPES SHALL BE RESHAPED AND GRADED TO MATCH THE PROPOSED SLOPE CONFIGURATION.
5. EROSION CONTROL MATTING SHALL BE FURNISHED, PLACED, AND MAINTAINED IN ACCORDANCE TO SECTION 626 OF THE ARDOT STANDARD SPECIFICATIONS.

SITE 3 - HWY. 278
SLIDE REPAIR DETAIL




LEGEND

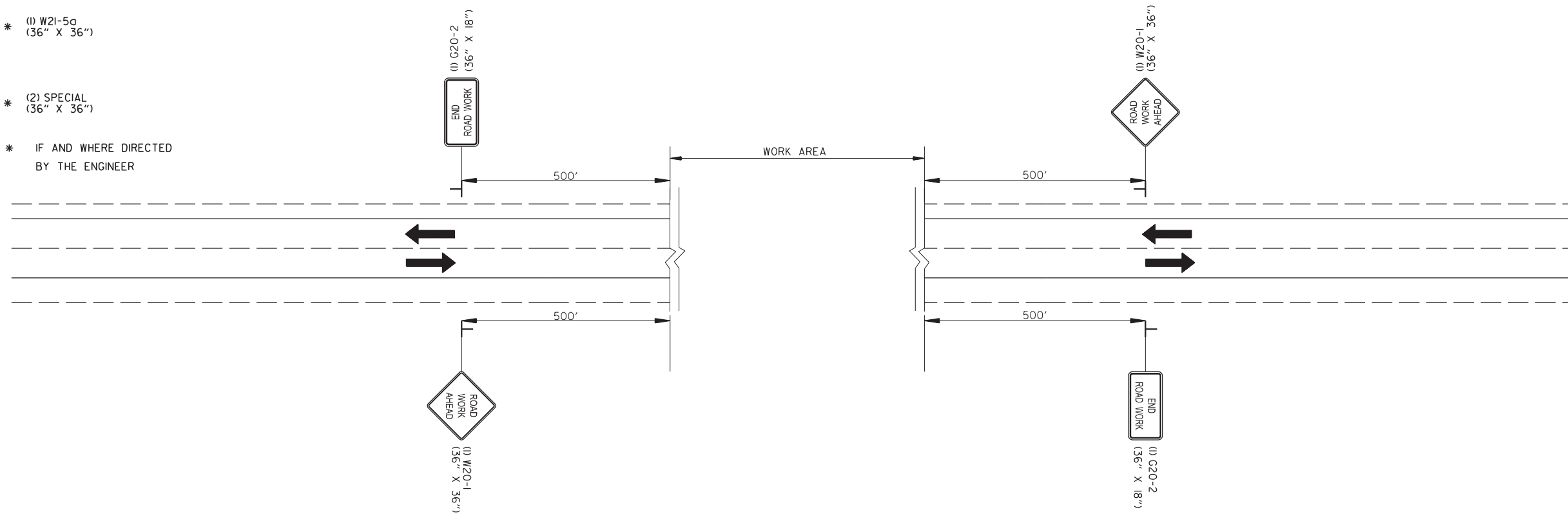


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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	10	12
MAINTENANCE OF TRAFFIC DETAILS						



-  * (2) G20-1 (36" X 18")
-  * (1) W21-5g (36" X 36")
-  * (2) SPECIAL (36" X 36")
- * IF AND WHERE DIRECTED BY THE ENGINEER



ADVANCE WARNING DETAILS
ALL SITES - ALL STAGES

- NOTES:
- ALTERNATING ONE-WAY OPERATION WITH APPROPRIATE TRAFFIC CONTROL WILL BE PERMITTED ON SITES 1&3 IF AND WHERE DIRECTED BY THE ENGINEER. A TRAFFIC CONTROL SUPERVISOR AND TRAINED FLAGGERS WILL BE UTILIZED TO ENSURE PROPER MAINTENANCE OF TRAFFIC TECHNIQUES ARE USED IN A SAFE AND ORDERLY MANNER IN ACCORDANCE TO THE MUTCD PUBLICATION AND SECTION 603 OF THE STANDARD SPECIFICATIONS.
 - THE SHOULDER ADJACENT TO THE SLIDE REPAIR ON SITE 2 MAY BE CLOSED IF AND WHERE DIRECTED BY THE ENGINEER BUT THE EXISTING LANES SHALL REMAIN OPEN TO TRAFFIC.

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	II	12
QUANTITIES						

ADVANCE WARNING SIGNS AND DEVICES

SIGN NUMBER	DESCRIPTION	SIGN SIZE	**			MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED	
			SITE 1	SITE 2	SITE 3		NO.	SQ. FT.
			LIN. FT. - EACH					
W20-1	ROAD WORK AHEAD	36"x36"	2	2	2	6	6	54.0
G20-2	END ROAD WORK	36"x18"	2	2	2	6	6	27.0
G20-1	ROAD WORK NEXT xx MILES	36"x18"			2	2	2	9.0
W21-5a	RIGHT SHOULDER CLOSED	36"x36"	1	1	2	4	4	36.0
W20-7a	FLAGGER	36"x36"	2		2	4	4	36.0
W16-2P	500 FEET (PLAGUE)	18"x24"	2		2	4	4	12.0
W20-4	ONE LANE ROAD 1000 FT	36"x36"	2		2	4	4	36.0
SPECIAL	TRUCKS ENTERING AND EXITING ROADWAY	36"x36"	2	2	2	6	6	54.0
TOTALS:							264.0	

*** NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.
 ** NOTE: THIS IS A LOW TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

* QUANTITY ESTIMATED.
 SEE SECTION 104.03 OF THE STD. SPECS.
 TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.



DUMPED RIPRAP AND FILTER BLANKET

STATION	STATION	LOCATION	DUMPED RIPRAP	CONCRETE RIPRAP	GEOTEXTILE FABRIC (TYPE 5)
			CU. YD.	CU. YD.	SQ. YD.
ENTIRE	PROJECT	SITE 1 - HWY. 82	850	20	650
ENTIRE	PROJECT	SITE 2 - HWY. 270	500		400
TOTALS:			1350	20	1050

* NOTE: QUANTITY ESTIMATED.
 SEE SECTION 104.03 OF THE STANDARD SPECIFICATIONS

NOTE: CONCRETE RIPRAP QUANTITY SHALL BE FIELD MEASURED AND VERIFIED

EROSION CONTROL

STATION	STATION	LOCATION	PERMANENT EROSION CONTROL					TEMPORARY EROSION CONTROL	
			SEEDING	LIME	MULCH COVER	WATER	SECOND SEEDING APPLICATION	WATTLE (20") DITCH CHECKS	SILT FENCE
			ACRE	TON	ACRE	M.GAL.	ACRE	(E-1) LIN. FT.	(E-11) LIN. FT.
ENTIRE	PROJECT	SITE 3 - HWY. 278	6.57	13.14	6.57	670.1	6.57		
ENTIRE	PROJECT	IF AND WHERE DIRECTED BY THE ENGINEER	1.00	2.00	1.00	102.0	1.00	594	12173
TOTALS:			7.57	15.14	7.57	772.1	7.57	594	12173

BASIS OF ESTIMATE:

LIME2 TONS / ACRE OF SEEDING
 WATER102.0 M.G. / ACRE OF SEEDING
 WATTLE DITCH CHECKS9 LIN. FT. / LOCATION

NOTE: THE TEMPORARY EROSION CONTROL DEVICES SHOWN ABOVE AND ON THE PLANS SHALL BE INSTALLED IN SUCH A SEQUENCE AS TO DETER EROSION AND SEDIMENTATION ON U.S. WATERWAYS AS EXPLAINED BY THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMIT.

* QUANTITIES ESTIMATED.
 SEE SECTION 104.03 OF THE STD. SPECS.

TOPSOIL FURNISHED & PLACED

STATION	STATION	LOCATION	TOPSOIL FURNISHED & PLACED
			CU. YD.
ENTIRE	PROJECT	SITE 3 - HWY. 278	5302.78
ENTIRE	PROJECT	IF AND WHERE DIRECTED BY THE ENGINEER	100.00
TOTAL:			5402.78

* NOTE: QUANTITY ESTIMATED.

EARTHWORK

STATION	STATION	LOCATION / DESCRIPTION	UNCLASSIFIED EXCAVATION
			CU. YD.
ENTIRE	PROJECT	SITE 1 - HWY. 82	850
ENTIRE	PROJECT	SITE 2 - HWY. 270	500
ENTIRE	PROJECT	SITE 3 - HWY. 278	13373
TOTALS:			14723

NOTE: EARTHWORK QUANTITIES SHALL BE FIELD MEASURED AND VERIFIED.

* NOTE: QUANTITY ESTIMATED.
 SEE SECTION 104.03 OF THE STANDARD SPECIFICATIONS

EROSION CONTROL MATTING

STATION	STATION	LOCATION	CLASS 3
			SQ. YD.
ENTIRE	PROJECT	SITE 3 - HWY. 278	31816.67
TOTAL:			31816.67

* NOTE: QUANTITY ESTIMATED.
 SEE SECTION 104.03 OF THE STANDARD SPECIFICATIONS

SHAPING ROADWAY SECTION

STATION	STATION	LOCATION	SHAPING ROADWAY SECTION
			STATION
ENTIRE	PROJECT	SITE 3 - HWY. 278	120
TOTAL:			120

* NOTE: QUANTITY ESTIMATED.
 SEE SECTION 104.03 OF THE STANDARD SPECIFICATIONS

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	020791	12	12
SUMMARY OF QUANTITIES AND REVISIONS						

SUMMARY OF QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
SP, SS, & 210	UNCLASSIFIED EXCAVATION	14723	CU. YD.
SP & 213	SHAPING ROADWAY SECTION	120	STATION
601	MOBILIZATION	1.00	LUMP SUM
SS & 603	MAINTENANCE OF TRAFFIC	1.00	LUMP SUM
SS & 604	SIGNS	264	SQ. FT.
620	LIME	15	TON
620	SEEDING	7.57	ACRE
SS & 620	MULCH COVER	7.57	ACRE
620	WATER	772.1	M. GAL.
621	SILT FENCE	12173	LIN. FT.
621	WATTLE (20")	594	LIN. FT.
623	SECOND SEEDING APPLICATION	7.57	ACRE
625	GEOTEXTILE FABRIC (TYPE 5)	1050	SQ. YD.
626	EROSION CONTROL MATTING (CLASS 3)	31817	SQ. YD.
628	TOPSOIL FURNISHED AND PLACED	5403	CU. YD.
635	ROADWAY CONSTRUCTION CONTROL	1.00	LUMP SUM
SS & 816	DUMPED RIPRAP	1350	CU. YD.
SS & 816	CONCRETE RIPRAP	20	CU. YD.

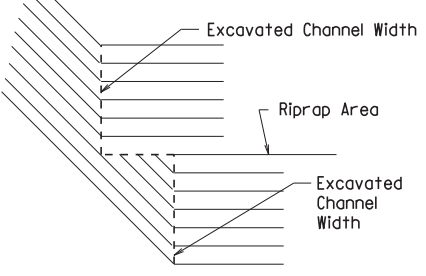
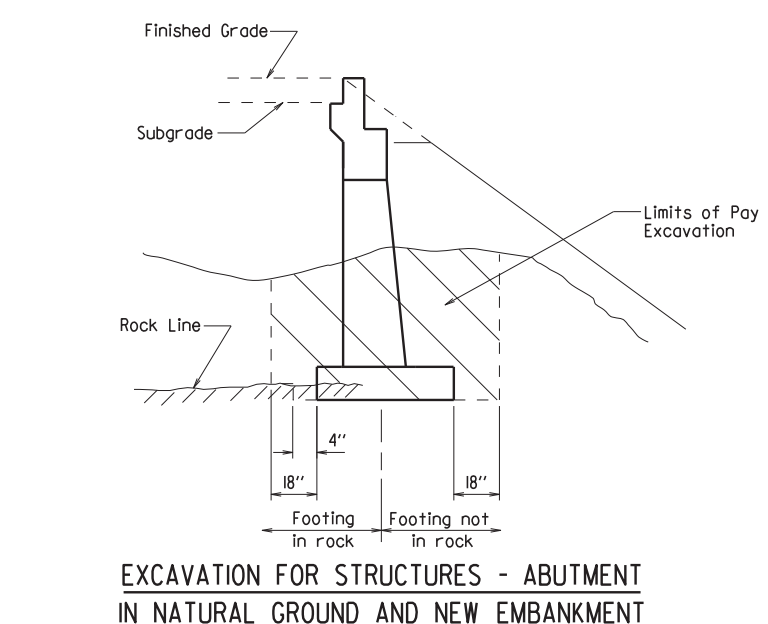
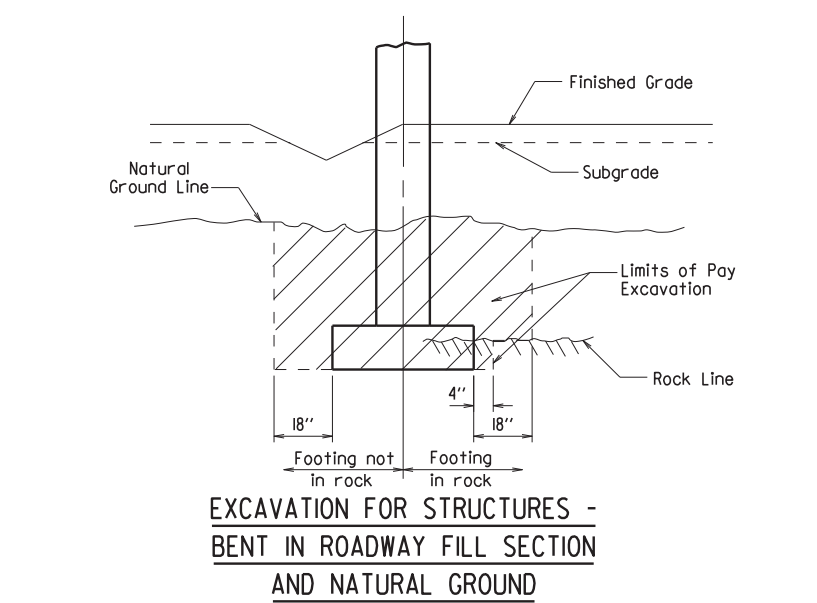
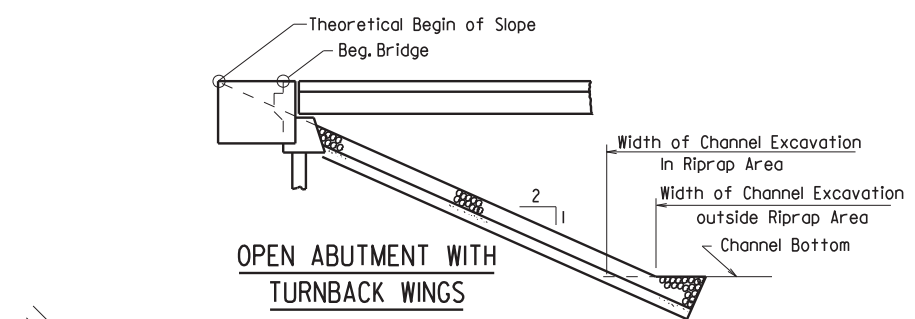
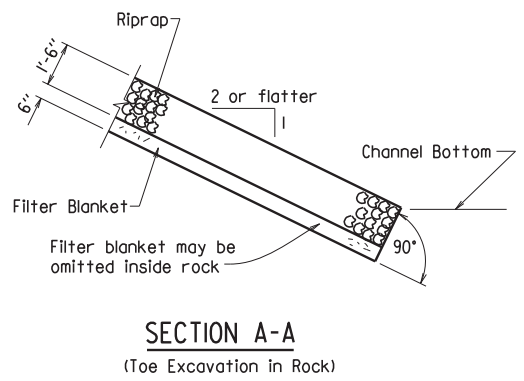
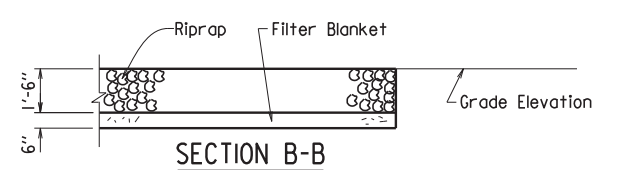
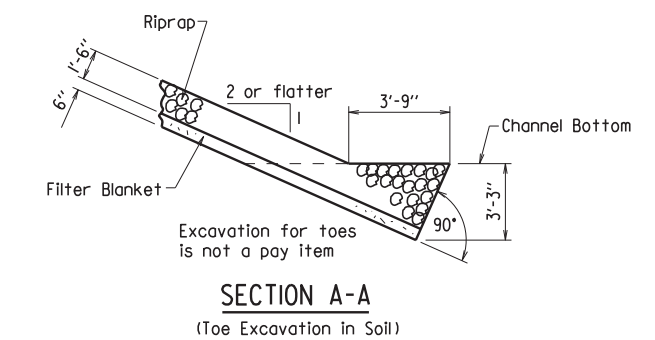
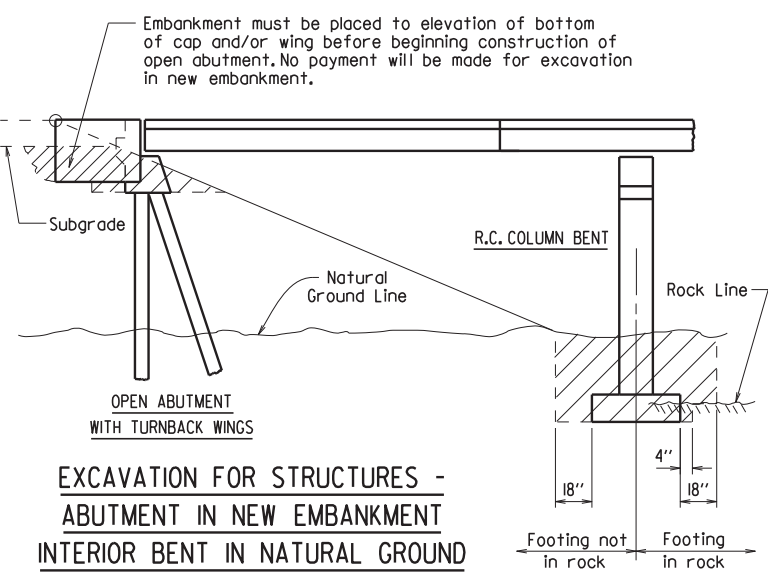
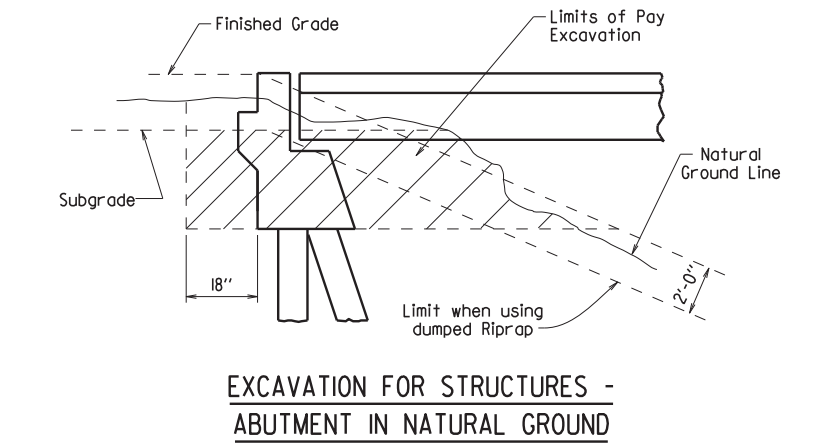
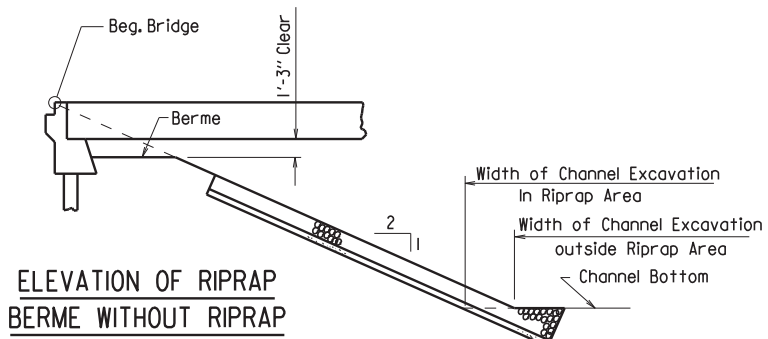
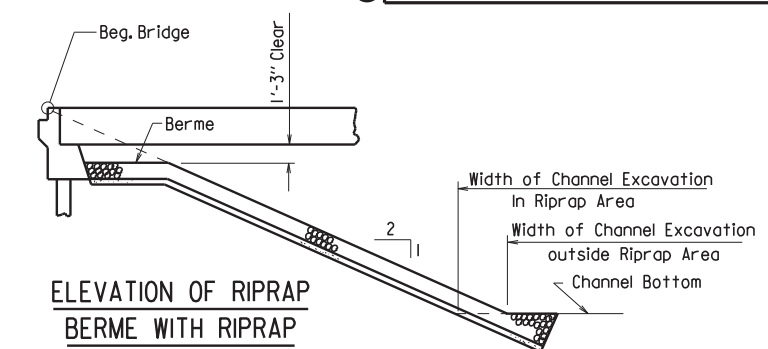
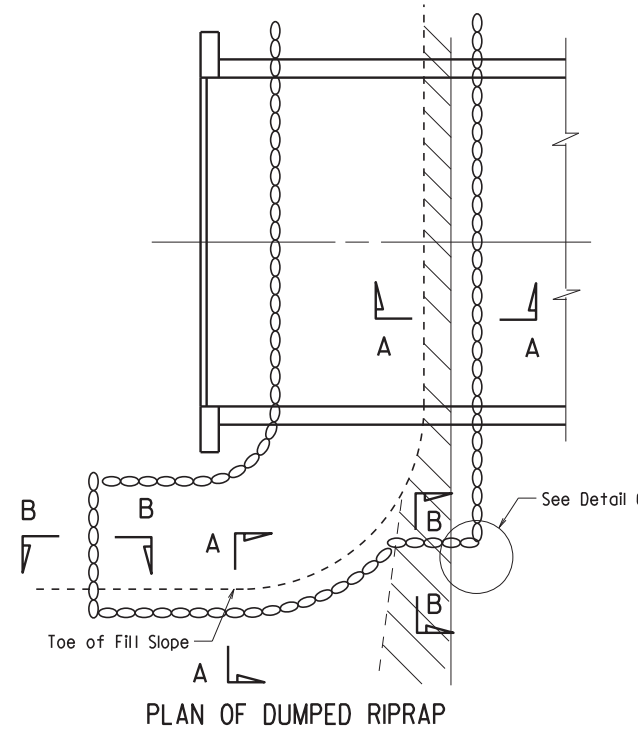
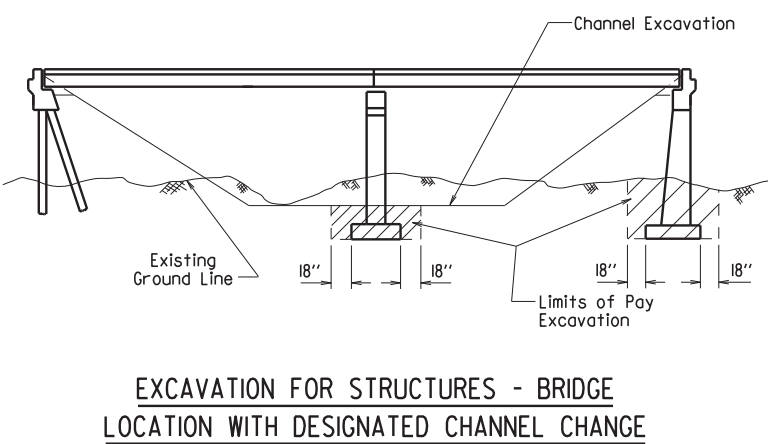
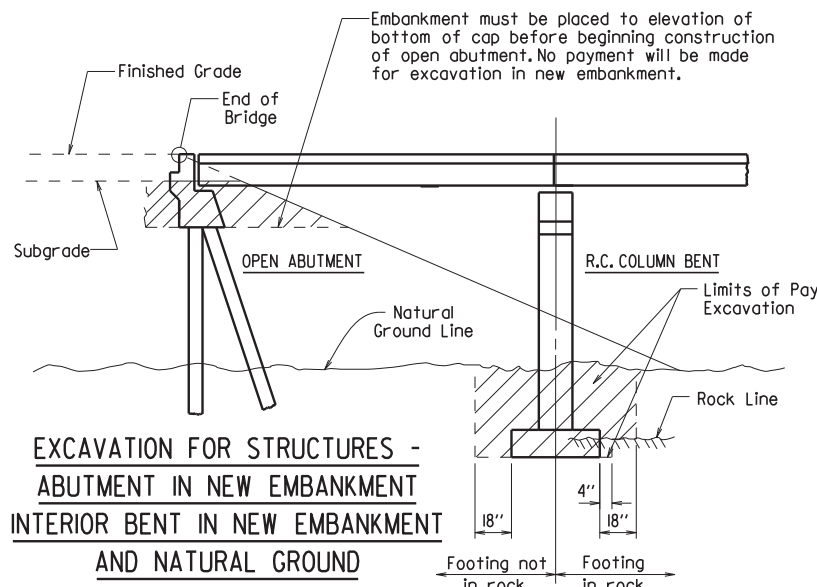
REVISIONS

DATE	REVISION	SHEET NUMBER



L:\cadd\miller\2/23/2023 9:47:24 AM
 WORKSPACE: ARDOT
 C:\Users\Public\mbidw\6\101008583\020791\08B.0TY_001.dgn
 REVISED DATE: **REVIDATE**

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		RIPRAP & EXCAV. 55001		



Note: Use this type of toe when rock is encountered which is in a stable condition.

Note: In lieu of an aggregate filter blanket, a synthetic fiber geotextile fabric complying with the requirements of Subsection 816.02(e) may be used.

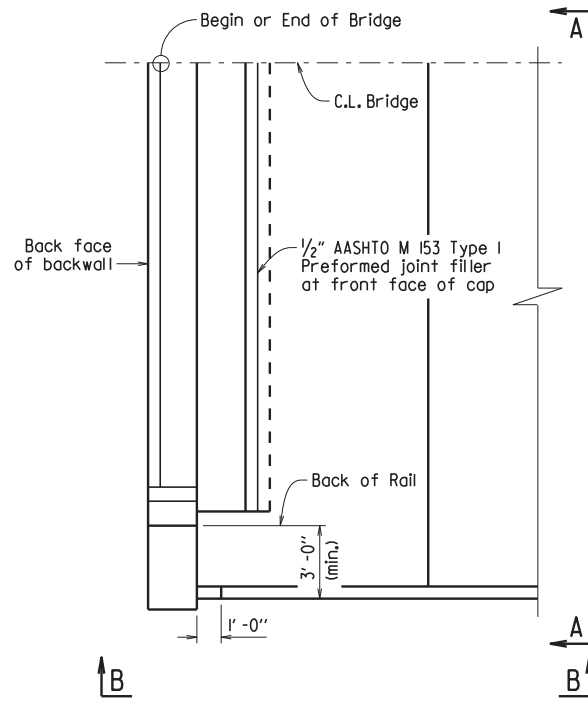
Note: Details for computing excavation for structures are included for information as to how plan quantities were calculated and for use when adjusting quantities when changing footing elevation.

STANDARD DETAILS FOR DUMPED RIPRAP AND FILTER BLANKET AND COMPUTING EXCAVATION FOR STRUCTURES
ARKANSAS STATE HIGHWAY COMMISSION

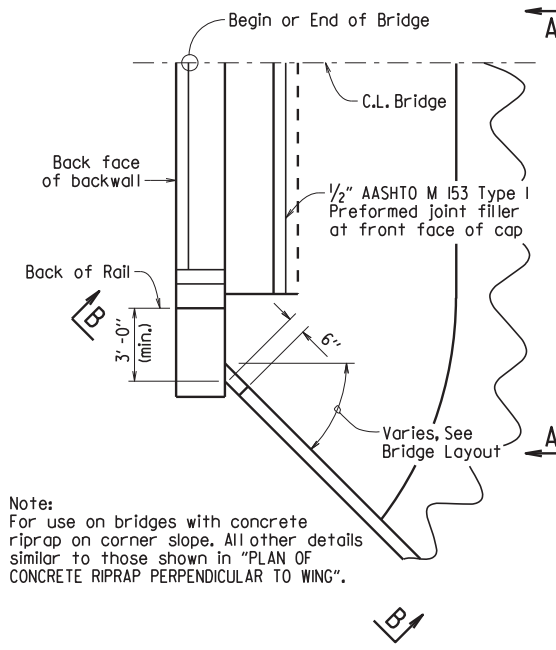
LITTLE ROCK, ARK.
 DRAWN BY: KDH DATE: 2-27-2014 FILENAME: b55001.dgn
 CHECKED BY: BEF DATE: 2-27-2014 SCALE: NO SCALE
 DESIGNED BY: STD. DATE:
 DRAWING NO. 55001

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.							CONCRETE RIPRAP	55002

Note:
Sloped surfaces of concrete riprap to be marked off into blocks (construction joints optional) with an approved grooving tool, spacing the grooved lines about 5' apart.

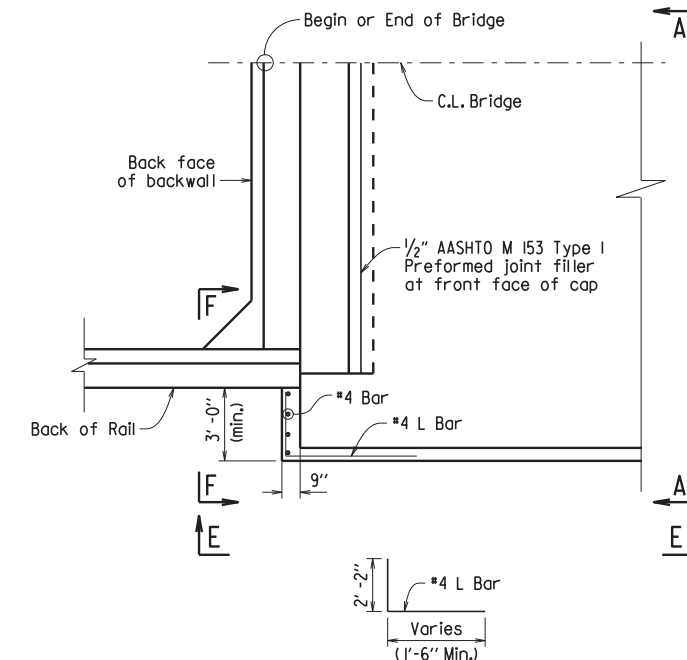


PLAN OF CONCRETE RIPRAP PERPENDICULAR TO WING
1/4" = 1'-0"

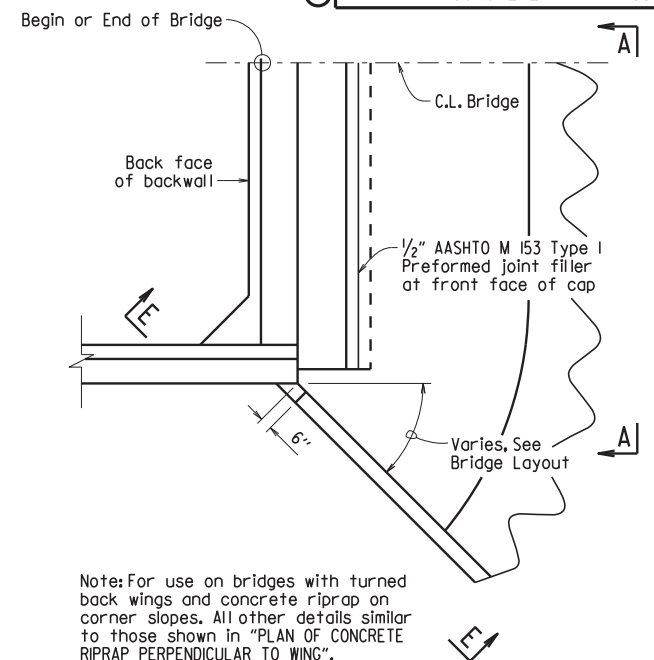


Note:
For use on bridges with concrete riprap on corner slope. All other details similar to those shown in "PLAN OF CONCRETE RIPRAP PERPENDICULAR TO WING".

PLAN OF CONCRETE RIPRAP AT ANGLE TO WING
1/4" = 1'-0"

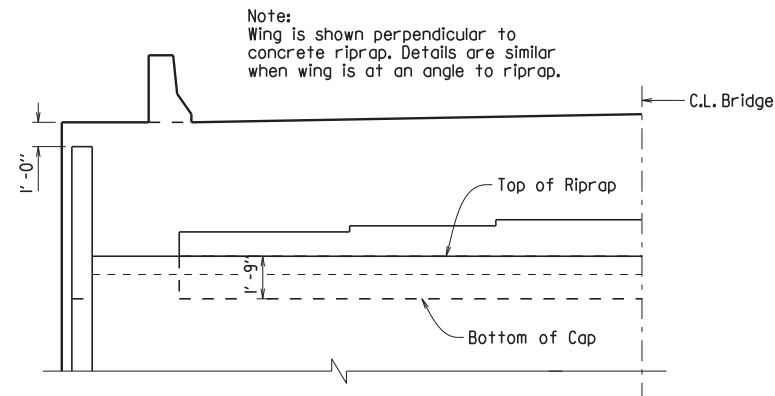


PLAN OF CONCRETE RIPRAP PERPENDICULAR TO TURNED BACK WING
1/4" = 1'-0"

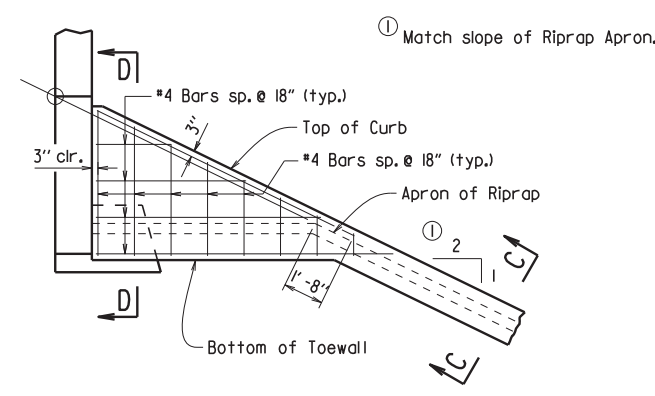


Note: For use on bridges with turned back wings and concrete riprap on corner slopes. All other details similar to those shown in "PLAN OF CONCRETE RIPRAP PERPENDICULAR TO WING".

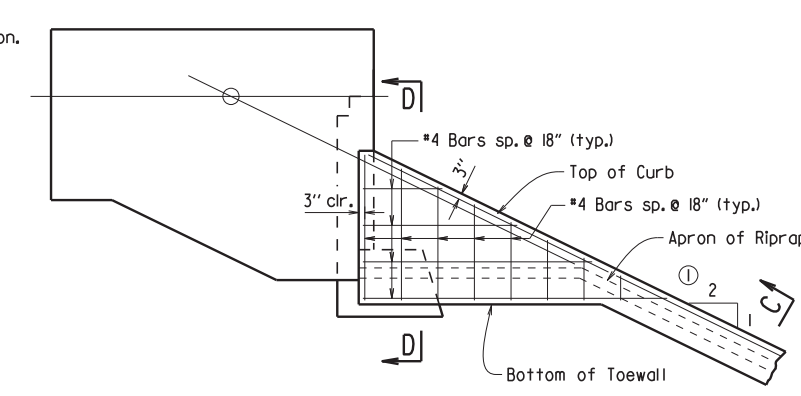
PLAN OF CONCRETE RIPRAP AT ANGLE FROM TURNED BACK WING
1/4" = 1'-0"



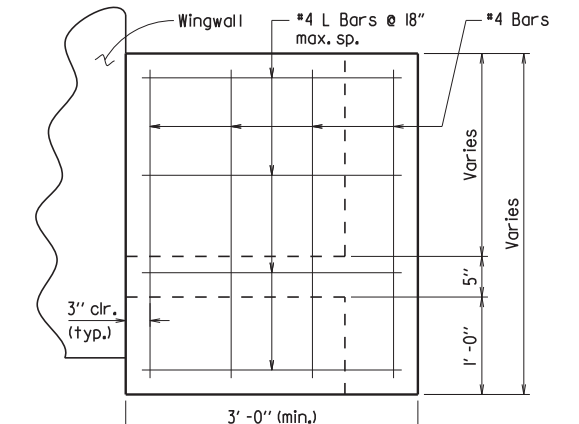
VIEW A-A
1/4" = 1'-0"



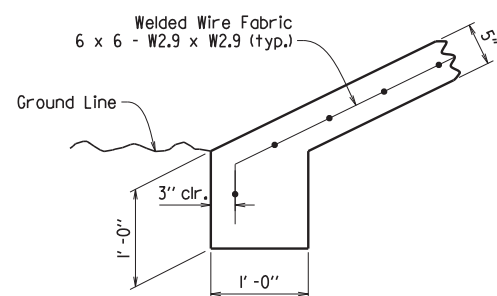
VIEW B-B
1/4" = 1'-0"



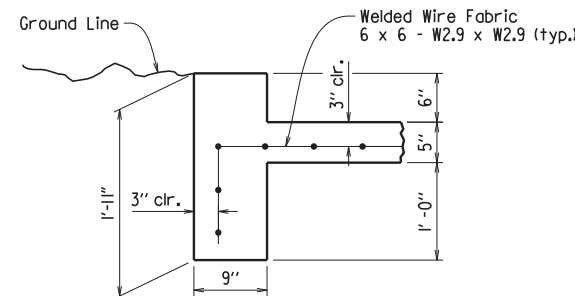
VIEW E-E
1/4" = 1'-0"



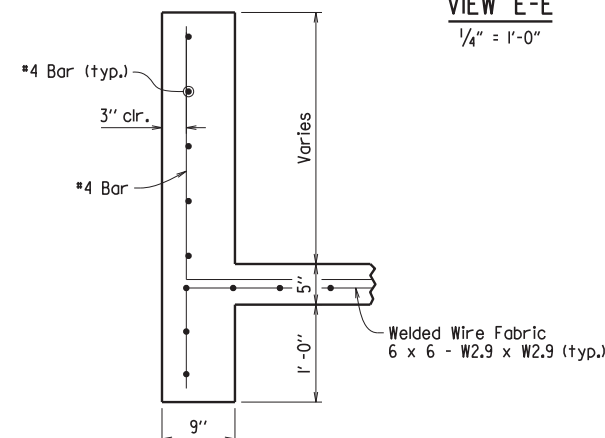
VIEW F-F
1" = 1'-0"



TOE OF CONCRETE RIPRAP
1" = 1'-0"



SECTION C-C
1" = 1'-0"



SECTION D-D
1" = 1'-0"

GENERAL NOTES

All concrete shall be Class A with a minimum compressive strength, f'c = 2,100 psi.

Welded wire fabric shall conform to AASHTO M55 or M221.


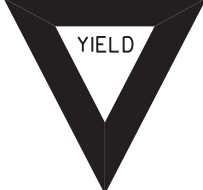

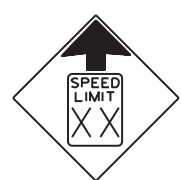





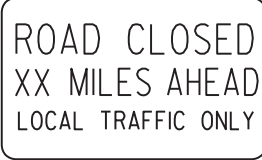


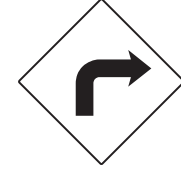







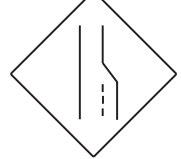



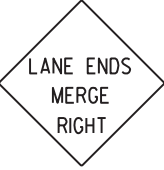


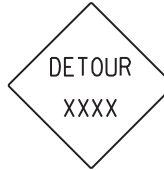










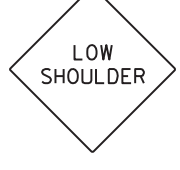

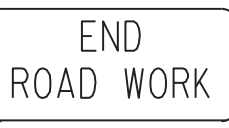
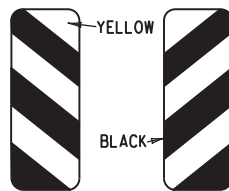


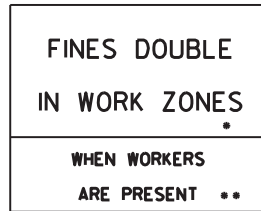
STANDARD DETAILS FOR CONCRETE RIPRAP

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: ACP DATE: 2/27/2014 FILENAME: b55002.dgn
 CHECKED BY: BEF DATE: 2/27/2014 SCALE: AS SHOWN
 DESIGNED BY: Std. DATE: ---

DRAWING NO. 55002

<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>W21-5a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W1-3</p>  <p>STD. 48"x48"</p>	<p>W1-4</p>  <p>STD. 48"x48"</p>	<p>W1-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>W1-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>W1-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60"</p> <p>• USE 6" C LETTERS •• USE 4" D LETTERS</p>

ADVANCE DISTANCES (XXXX)

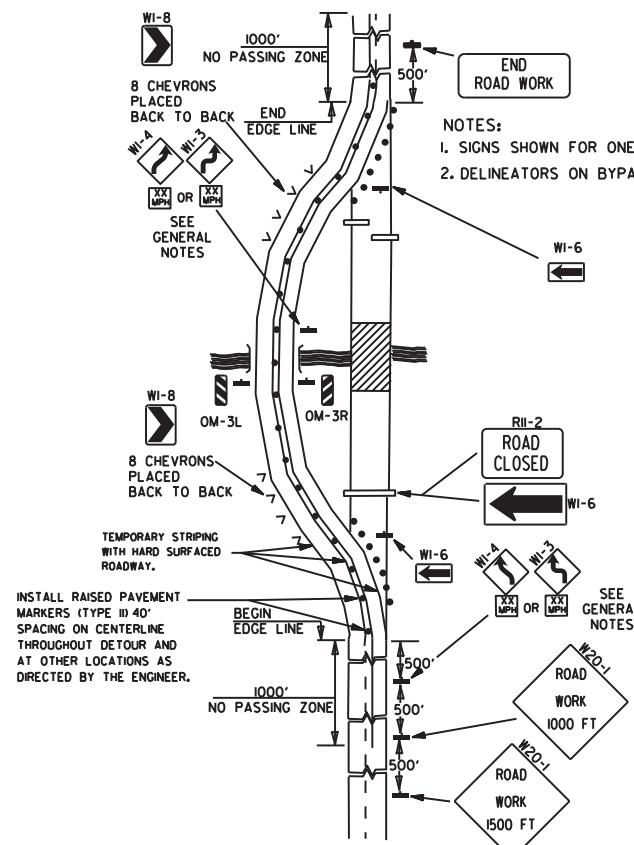
500 FT	1/2 MILE
1000 FT	3/4 MILE
1500 FT	1 MILE AHEAD

GENERAL NOTES:

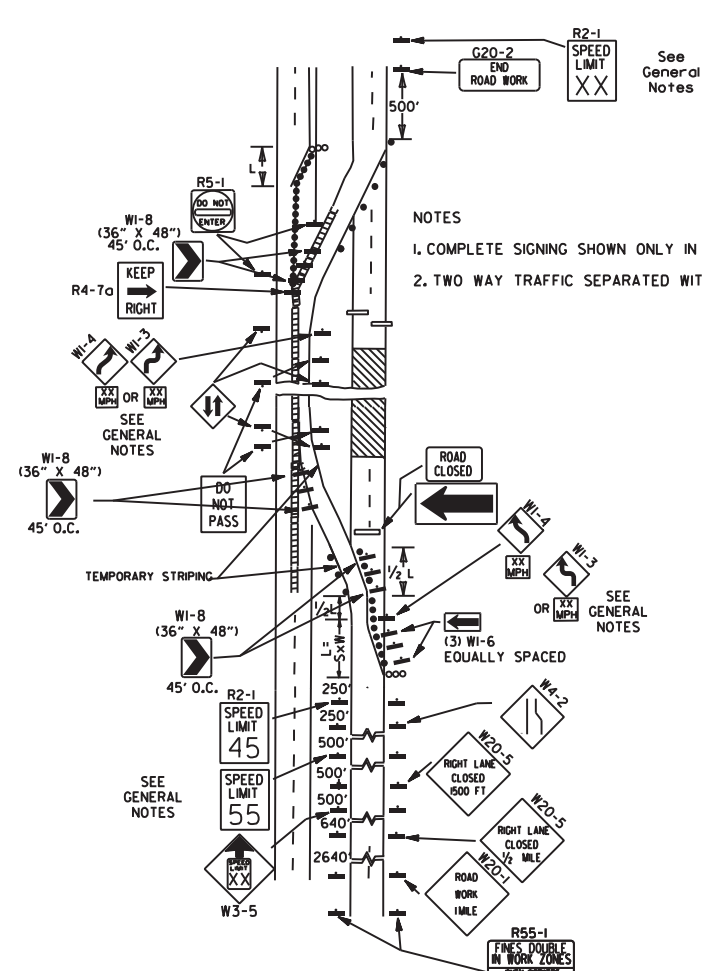
- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SO. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

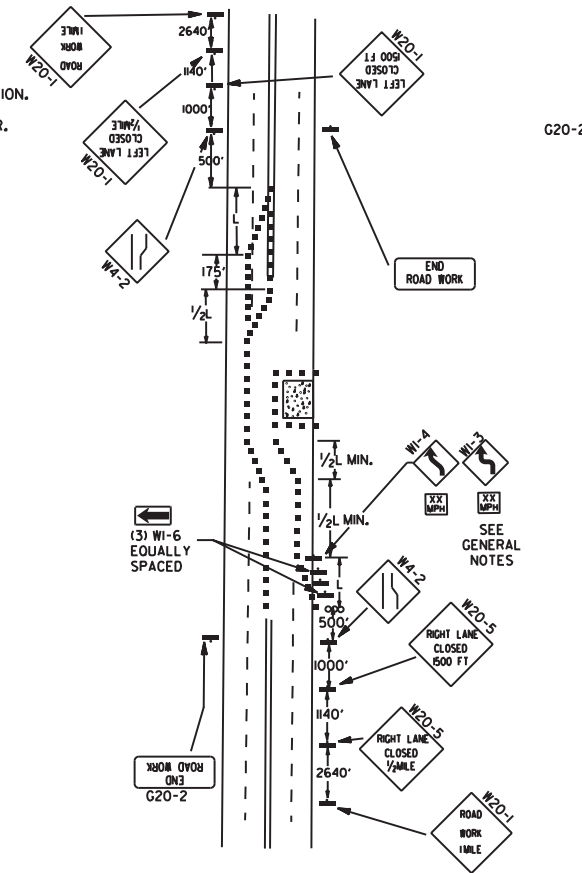
11-07-19	REVISED FOR MASH	
4-13-17	DELETED RSP-1 & ADDED W21-5a	
9-2-15	REVISED REDUCED SPEED LIMIT AHEAD SIGNS REVISED ROAD WORK NEXT XX MILES	
12-15-11	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED



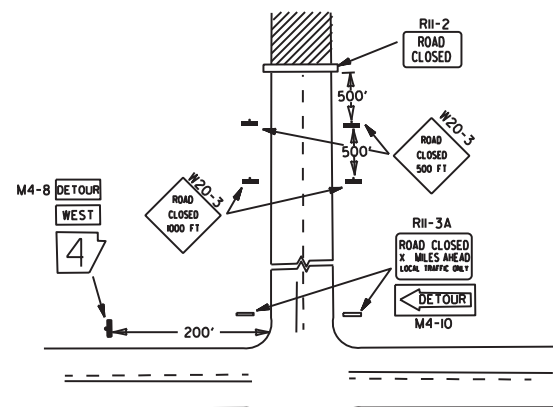
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



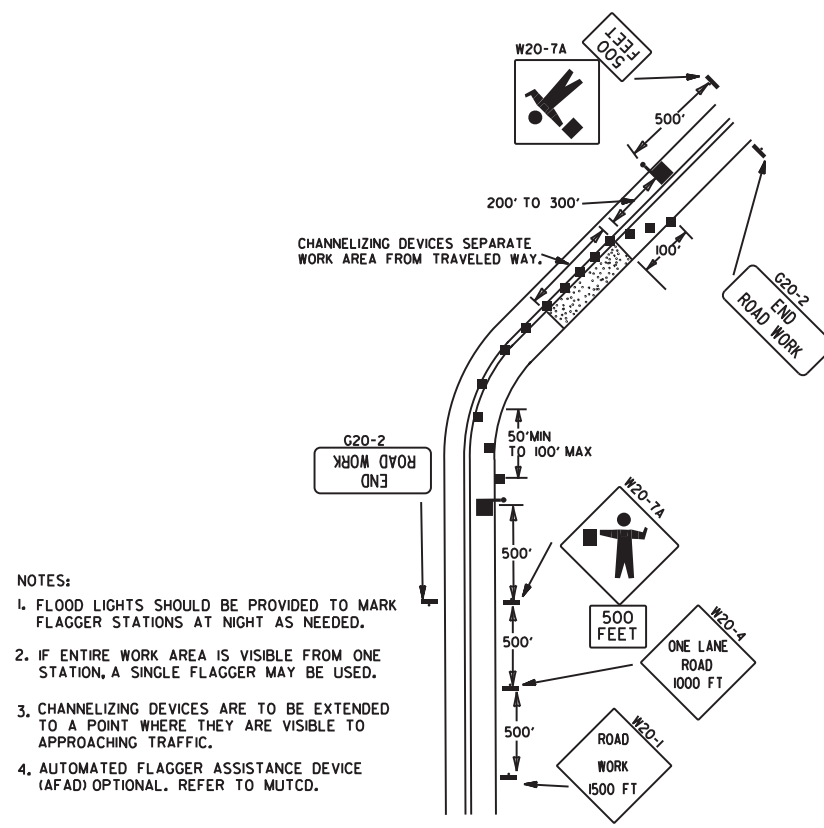
(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



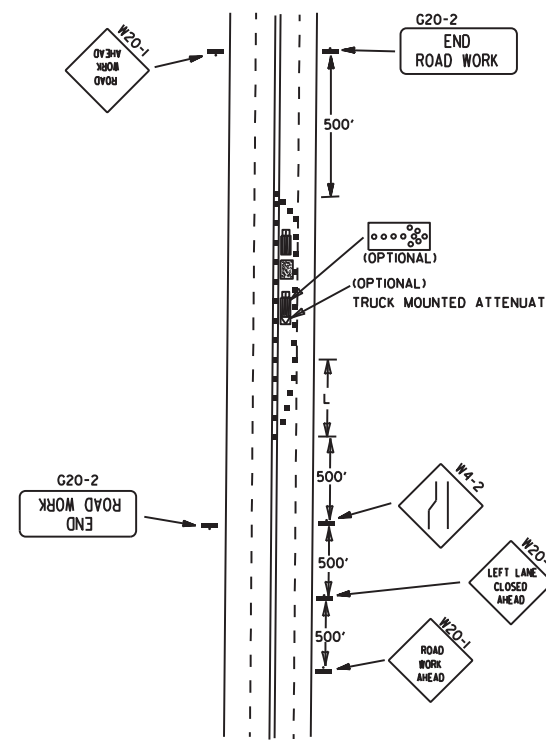
(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.

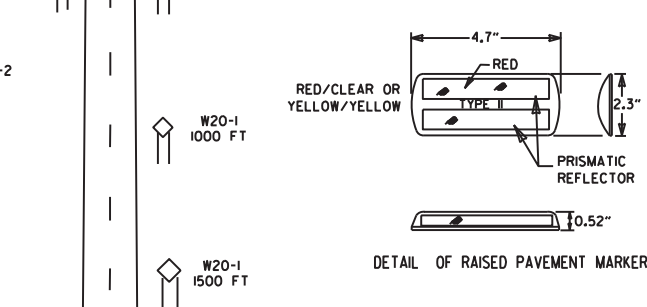


(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

- KEY:
- FLAGGER
 - POSITIVE BARRIER
 - ARROW PANEL (IF REQUIRED)
 - TYPE III BARRICADE
 - CHANNELIZING DEVICE
 - TRAFFIC DRUM
 - RAISED PAVEMENT MARKER



GENERAL NOTES:
1. THE MAINTENANCE DIVISION SHALL CONDUCT A BALL BANK STUDY TO DETERMINE THE ADVISORY SPEED LIMIT PRIOR TO OPENING TO TRAFFIC. THE ADVISORY SPEED WILL BE POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
2. WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(KX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-1(55)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(KX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
7. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
8. DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ADOT QUALIFIED PRODUCTS LIST.
9. ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

DATE	REVISION	FILMED
05-20-21	REVISED NOTE 7	
11-07-19	REVISED NOTE 4, ADDED NOTE 9	
9-2-15	REVISED NOTE 2, ADDED NOTE 8, REVISED DRAWING (A) & REPLACED R2-5A WITH W3-5	
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-11-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

TRAFFIC CONTROL DEVICES

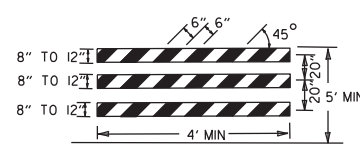
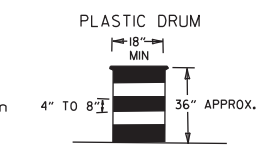
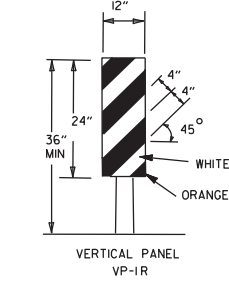
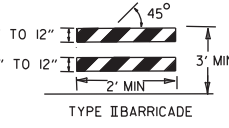
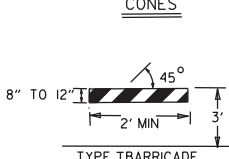
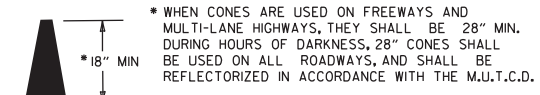
VERTICAL DIFFERENTIAL	LOCATION	TRAFFIC CONTROL	
		≤ 45 MPH	> 45 MPH
≤ 1"	CENTERLINE	W8-11	W8-11
> 1"	CENTERLINE	W8-11 AND CENTERLINE LANE STRIPING	W8-11 AND CENTERLINE LANE STRIPING
≤ 3"	CENTERLINE	STANDARD LANE CLOSURE ⁽⁶⁾	STANDARD LANE CLOSURE ⁽⁶⁾
≤ 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9 AND TRAFFIC DRUMS ⁽¹⁾	W8-9 AND TRAFFIC DRUMS ⁽¹⁾
> 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾
≤ 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	A STABILIZED WEDGE, W8-17, EDGE LINE STRIPING AND TRAFFIC DRUMS ⁽³⁾
> 24"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER ⁽⁴⁾ & EDGE LINES	PRECAST CONCRETE BARRIER ⁽⁴⁾ & EDGE LINES

INTERSTATE		
VERTICAL DIFFERENTIAL	LOCATION	TRAFFIC CONTROL
≤ 3"	CENTERLINE	W8-11 AND LANE STRIPING
≤ 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER & EDGE LINES

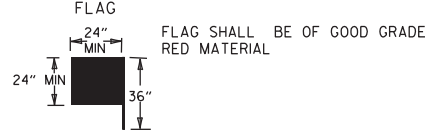
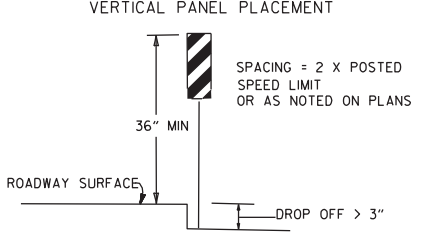
INTERSTATE AND NON-INTERSTATE		
FORESLOPE	HEIGHT	TRAFFIC CONTROL
1:1	> 2 FT	PRECAST CONCRETE BARRIER
2:1	≤ 5 FT	TRAFFIC DRUMS
2:1	> 5 FT	PRECAST CONCRETE BARRIER
Flatter than 2:1	N/A	TRAFFIC DRUMS

- GENERAL NOTES:
- WHEN THE SHOULDER AREA IS USED AS PART OF THE TRAVELED LANE AND THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, THEN VERTICAL PANELS SHOULD BE USED.
 - WHEN THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, A STABILIZED WEDGE SHALL BE USED. PRECAST CONCRETE BARRIER WALL CAN BE USED IN LIEU OF A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS.
 - A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS CAN BE USED IN LIEU OF PRECAST CONCRETE BARRIER WALL, IF AND WHERE DIRECTED BY THE ENGINEER.
 - W21-5, W21-5a, AND/OR W21-5b SIGNS SHALL BE USED WHERE THE ROADWAY IS UNOBSTRUCTED IF AND WHERE DIRECTED BY THE ENGINEER.
 - TIME LIMITATIONS MUST CONFORM TO SECTION 603 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION).

CHANNELIZING DEVICES



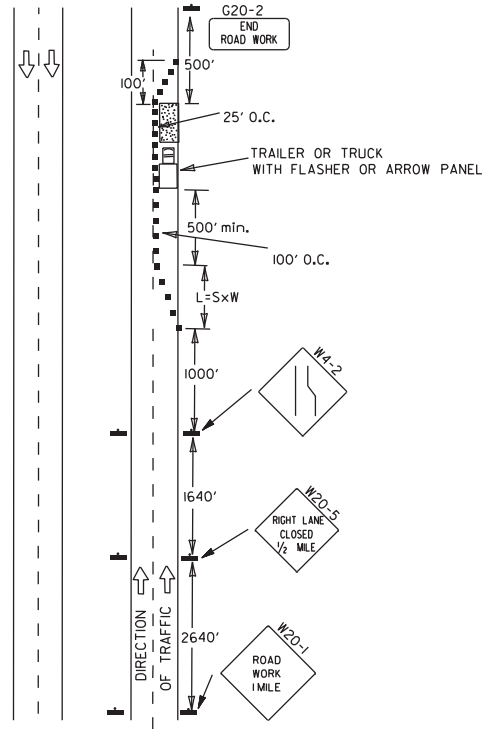
NOTE: FOR ALL ROAD CLOSURES, THE TYPE III BARRICADES SHALL BE OF SUFFICIENT LENGTH TO EXTEND ACROSS ENTIRE ROADWAY.



- KEY:
- ○ ○ ○ ARROW PANEL (IF REQUIRED)
 - CHANNELIZING DEVICE
 - TRAFFIC DRUM

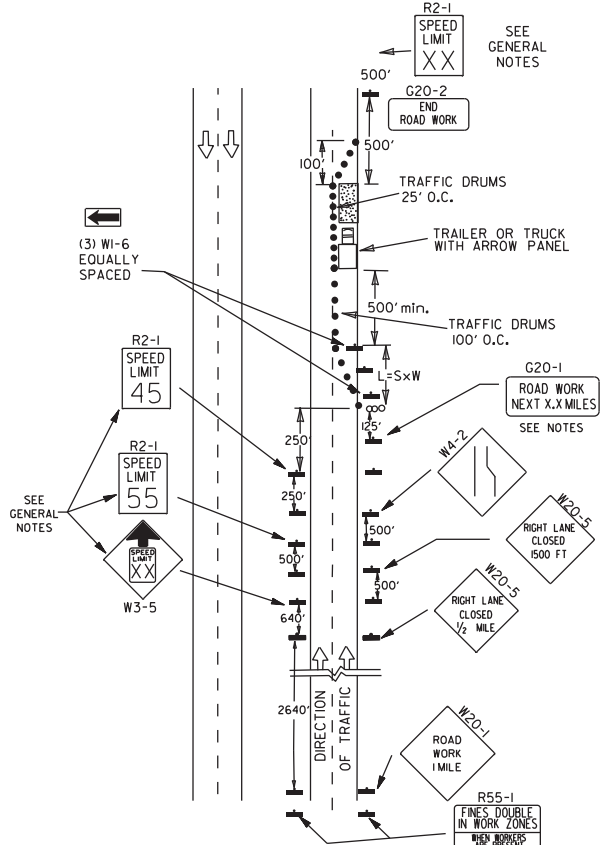
GENERAL NOTES:

- A SPEED LIMIT REDUCTION MAY BE IMPLEMENTED ONLY WHEN DESIGNATED IN THE PLAN OR WHEN RECOMMENDED BY THE ROADWAY DESIGN DIVISION.
- WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
- WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-1(55)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
- THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT OR AS DIRECTED BY THE ENGINEER.
- WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
- PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
- THE G20-1 SIGN WILL BE REQUIRED ON JOBS OF OVER TWO MILES IN LENGTH. WHEN THE LANE CLOSURE IS NOT AT THE BEGINNING OF THE PROJECT, THE G20-1 SIGN SHALL BE ERRECTED 125' IN ADVANCE OF THE JOB LIMIT. ADDITIONAL W20-1(1/4 MILE) SIGNS ARE NOT REQUIRED IN ADVANCE OF LANE CLOSURES THAT BEGIN INSIDE THE PROJECT LIMITS.
- FLAGGERS SHALL USE STOP/SLOW PADDLES FOR CONTROLLING TRAFFIC THROUGH WORK ZONES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- ALL PLASTIC DRUMS AND CONES SHALL MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
- TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
- ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

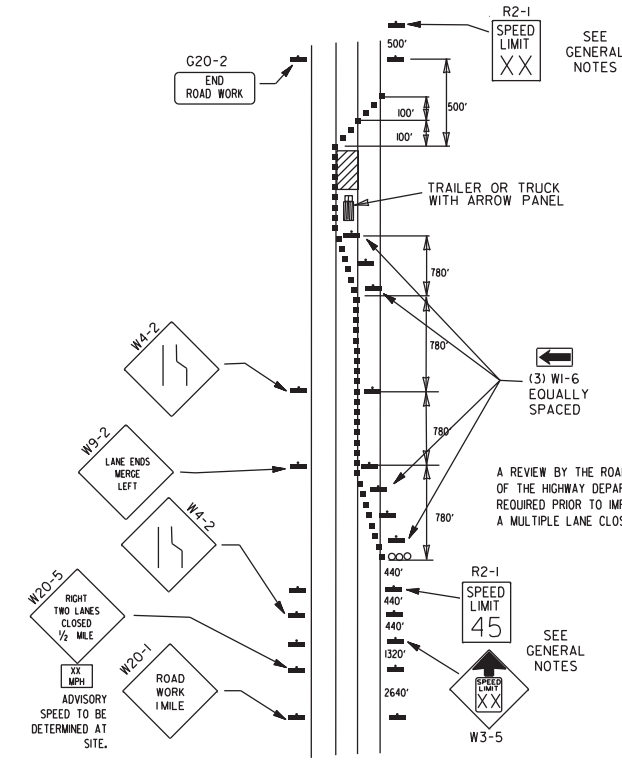


(A) TYPICAL APPLICATION - DAYTIME MAINTENANCE OPERATIONS OF SHORT DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

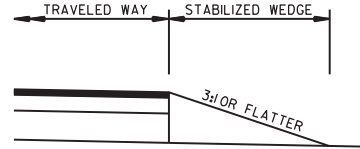
(B) TYPICAL APPLICATION - 3-LANE ONEWAY ROADWAY WHERE CENTER LANE IS CLOSED.



(C) TYPICAL APPLICATION - CONSTRUCTION OPERATIONS OF INTERMEDIATE TO LONG TERM DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

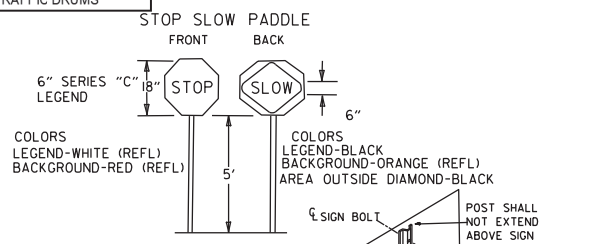


(D) TYPICAL APPLICATION - CLOSING MULTIPLE LANES OF A MULTILANE HIGHWAY.



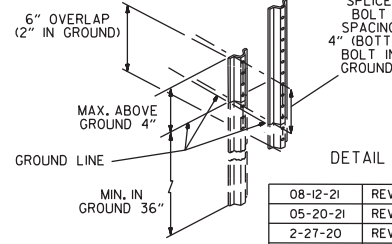
STABILIZED WEDGE

NOTE: MATERIALS FOR THE STABILIZED WEDGE SHALL MEET THE REQUIREMENTS PROVIDED IN SECTION 603.02 OF THE STANDARD SPECIFICATIONS.



NOTES:

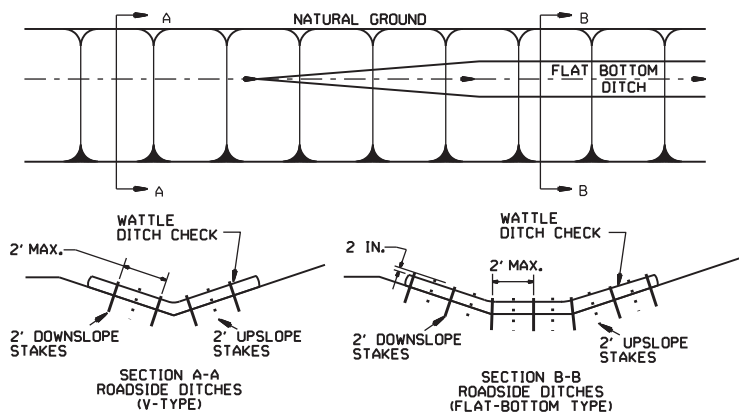
- USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2)
- NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS.
- SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED.
- ALL SIGN POSTS SHALL BE PLUMB.



DATE	REVISION	REVISION	FILMED
08-12-21	REVISED TRAFFIC CONTROL DEVICES AND NOTES		
05-20-21	REVISED NOTE 10		
2-27-20	REVISED TRAFFIC CONTROL DEVICES DETAILS		
11-07-19	REVISED NOTE 9, ADDED NOTE II		
7-25-19	REVISED TRAFFIC CONTROL DEVICES DETAILS		
9-2-15	REVISED NOTE 2 & REPLACED R2-5A WITH W3-5		
10-15-09	ADDED REFERENCE TO MASH		
11-20-08	REVISED SIGN DESIGNATIONS		
11-18-04	ADDED NOTE		
10-1-98	ADDED NOTE		
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE		
10-18-96	ADDED R55-1		
10-12-95	MOVED UPPER SPLICE		
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95	
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993		
8-15-91	DRAWN AND PLACED IN USE		

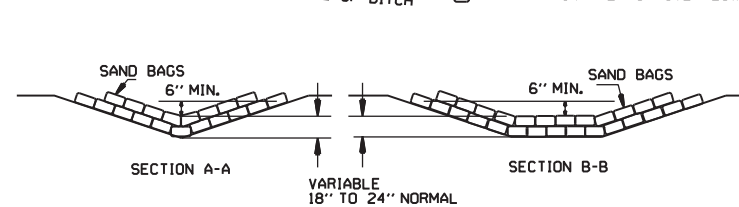
GENERAL NOTES

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

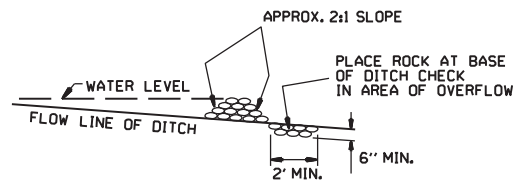


WATTLE DITCH CHECK (E-1)

NUMBER OF SAND BAGS AND ARRANGEMENT VARIABLE WITH ON-SITE CONDITIONS. PLACE SAND BAGS AT BASE OF DITCH CHECK IN AREA OF OVERFLOW.

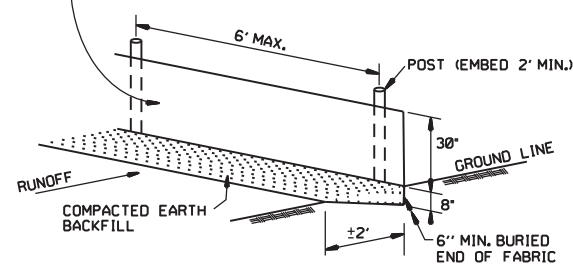


SAND BAG DITCH CHECK (E-5)

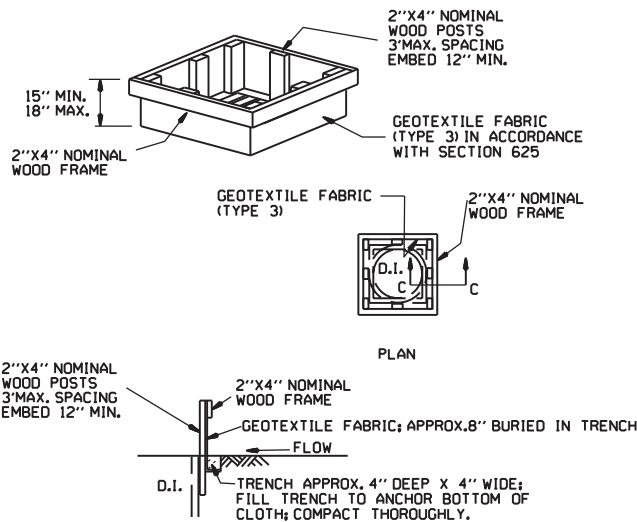


ROCK DITCH CHECK (E-6)

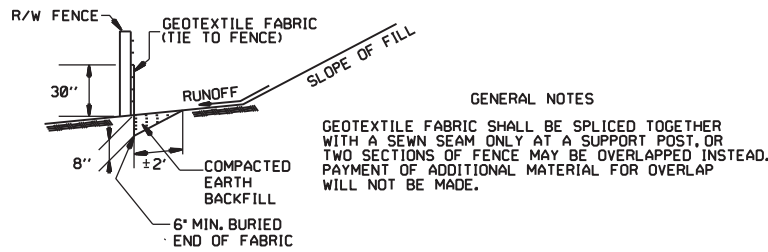
GENERAL NOTES
 GEOTEXTILE FABRIC (TYPE 4) IN ACCORDANCE WITH SECTION 625
 GEOTEXTILE FABRIC SHALL BE SPLICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST OR TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.



SILT FENCE (E-11)

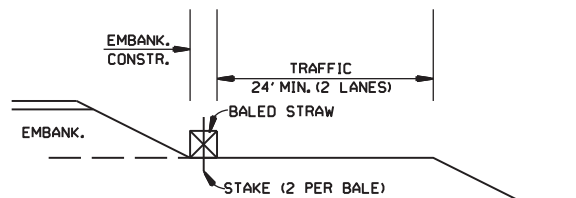


DROP INLET SILT FENCE (E-7)

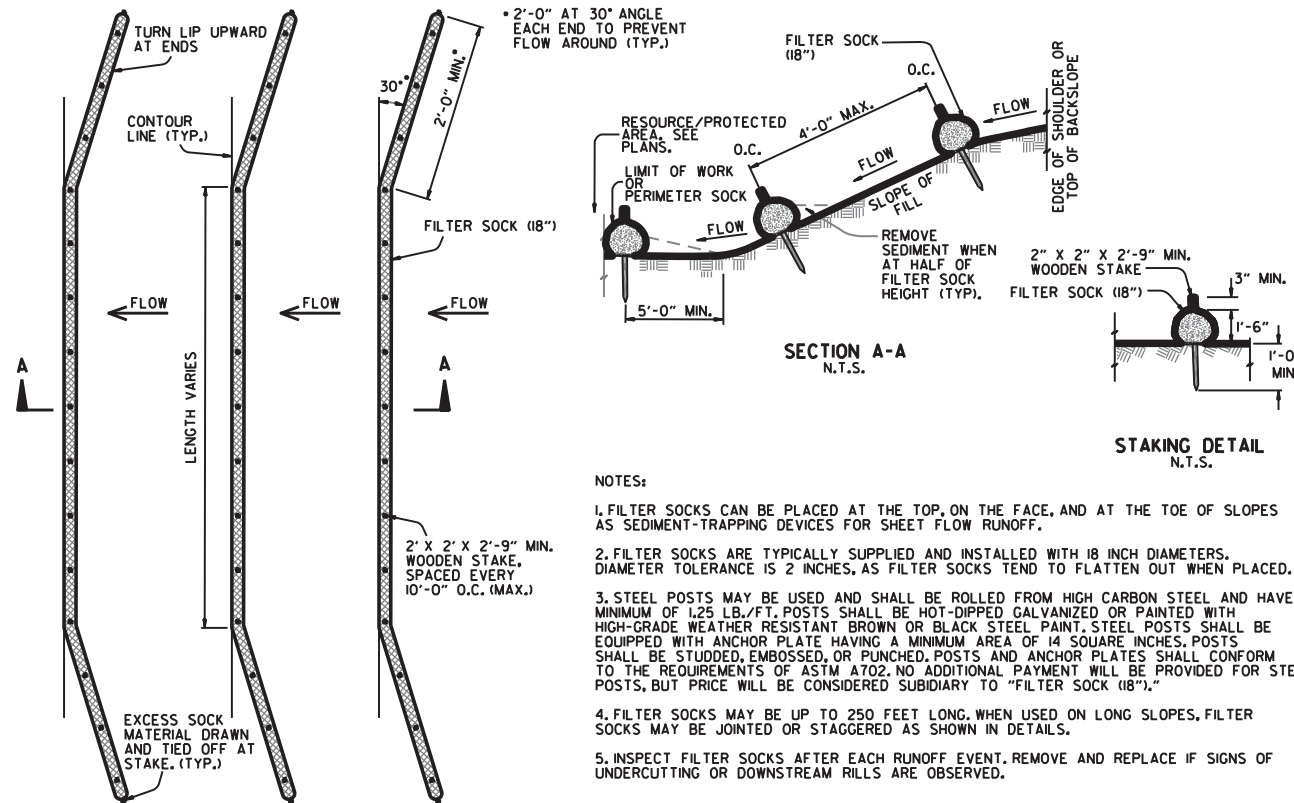


SILT FENCE ON R/W FENCE (E-4)

GENERAL NOTES
 1. STRAW BALES SHALL BE INSTALLED SO THAT THE BINDINGS ARE ORIENTED AROUND THE SIDES RATHER THAN ALONG THE TOPS AND BOTTOMS OF THE BALES. THE BALES SHALL BE A MINIMUM OF 30 INCHES IN LENGTH.
 2. NO GAPS SHALL BE LEFT BETWEEN BALES.
 3. BALED STRAW FILTER BARRIERS COMPLETED AND ACCEPTED WILL BE MEASURED BY THE BALE IN PLACE AS AUTHORIZED BY THE ENGINEER AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID PER BALE FOR BALED STRAW DITCH CHECKS.

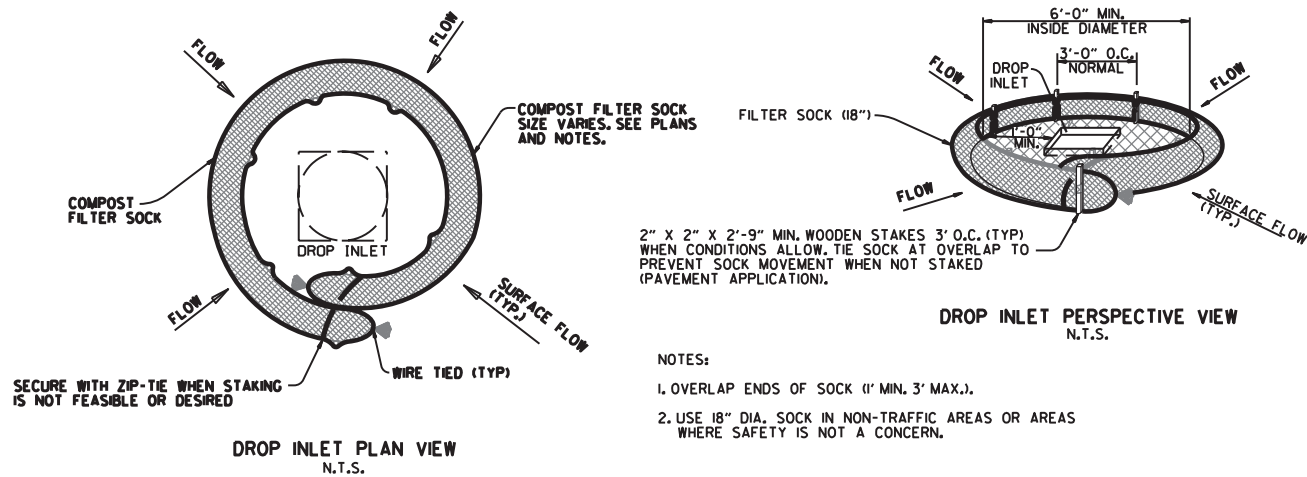


BALED STRAW FILTER BARRIER (E-2)



FILTER SOCK ALONG SLOPE (E-3)

- NOTES:**
1. FILTER SOCKS CAN BE PLACED AT THE TOP, ON THE FACE, AND AT THE TOE OF SLOPES AS SEDIMENT-TRAPPING DEVICES FOR SHEET FLOW RUNOFF.
 2. FILTER SOCKS ARE TYPICALLY SUPPLIED AND INSTALLED WITH 18 INCH DIAMETERS. DIAMETER TOLERANCE IS 2 INCHES, AS FILTER SOCKS TEND TO FLATTEN OUT WHEN PLACED.
 3. STEEL POSTS MAY BE USED AND SHALL BE ROLLED FROM HIGH CARBON STEEL AND HAVE A MINIMUM OF 125 LB./FT. POSTS SHALL BE HOT-DIPPED GALVANIZED OR PAINTED WITH HIGH-GRADE WEATHER RESISTANT BROWN OR BLACK STEEL PAINT. STEEL POSTS SHALL BE EQUIPPED WITH ANCHOR PLATE HAVING A MINIMUM AREA OF 14 SQUARE INCHES. POSTS SHALL BE STUDDED, EMBOSSED, OR PUNCHED. POSTS AND ANCHOR PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A702. NO ADDITIONAL PAYMENT WILL BE PROVIDED FOR STEEL POSTS, BUT PRICE WILL BE CONSIDERED SUBDIARY TO "FILTER SOCK (18\"/>



COMPOST FILTER SOCK DROP INLET PROTECTION (E-13)

- NOTES:**
1. OVERLAP ENDS OF SOCK (1' MIN, 3' MAX.).
 2. USE 18" DIA. SOCK IN NON-TRAFFIC AREAS OR AREAS WHERE SAFETY IS NOT A CONCERN.

11-16-17	ADDED FILTER SOCK E-3 AND E-13	
12-15-11	DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK	
11-18-98	ADDED NOTES	
07-02-98	ADDED BALED STRAW FILTER BARRIER (E-2)	
07-20-95	REVISED SILT FENCE E-4 AND E-11	7-20-95
07-15-94	REV. E-4 & E-11 MIN. 13" BURIED END OF FABRIC	
06-02-94	REVISED E-1,4,7 & 11; DELETED E-2 & 3	6-2-94
04-01-93	REDRAWN	
10-01-92	REDRAWN	
08-02-76	ISSUED R.D.M.	298-7-28-76
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION
 TEMPORARY EROSION CONTROL DEVICES
 STANDARD DRAWING TEC-1