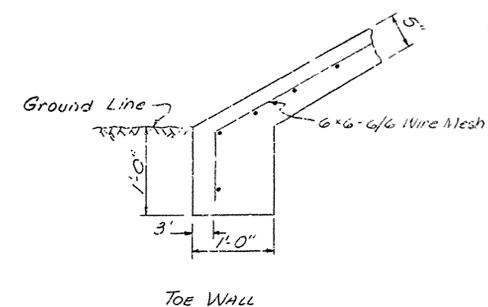
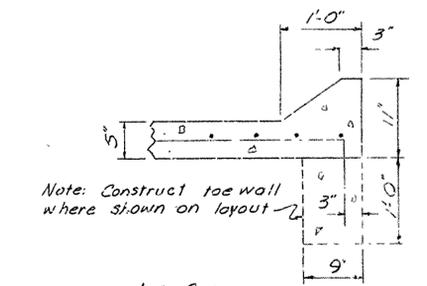


FED. ROAD DIST. NO.	STATE	FED. AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	ARK.	30-2 (7)80		12	234
JOB NO.		6808			

SUMMARY OF BRIDGE QUANTITIES - JOB 6808

Code No.	Bridge No.	Name of Bridge	UNIT OF BRIDGE	Item No.	SP # 601	SP # 602	SP # 602	SP # 602	SP # 602	SP # 603	804	804	SP # 805 (Alt. #1) SP # 805 (Alt. #2)	SP # 806	SP # 812	817	SP # 817								
					Item	Unclassified Excavation for Structures	Class A Concrete	Class S Concrete	46'-0" Prestressed Concrete Beams (Type II)	65'-0" Prestressed Concrete Beams (Type III)	Reinforcing Steel	Steel Bearing Piling (12 BP 53)	Steel Bearing Piling (10 BP 42)	Metal (Aluminum) Bridge Railing (Alt. #1) Metal (Steel) Bridge Railing (Alt. #2)	Structural Steel in Beam Spans	Bridge Name Plates (Type C)	Riprap	Concrete Riprap							
				Unit	Cu. Yd.	Cu. Yd.	Cu. Yd.	Unit	Unit	Lb.	Lin. Ft.	Lin. Ft.	Lin. Ft.	Lb.	Plate	Cu. Yd.	Cu. Yd.								
X771	3896	Friendship Interc. Bridge	Span 1				32.56			7,053			84	24,091											
				Span 2 & 3			102.65			22,604		268	103,145												
				Span 4			42.36			9,303		110	34,046												
				End Bent 1	60	19.61				1,160			679	1				10.00							
				Bent 2	19	14.95				3,506															
				Bent 3	17	16.81				3,872															
				Bent 4	17	16.55				3,832															
				End Bent 5	60	5.88				2,850			679						17.00						
				Totals for Bridge No. 3896	173	77.40				54,280			462	162,440	1					27.00					
				X071	3897A	West Fork Delisle Creek	Spans 1-3				137.59			32,391			276	114,454							
End Bents 1 & 4	34							34.23		4,838		432		2,076	1	235									
Bents 2 & 3	153	46.10						29.82		10,859															
Totals for Bridge No. 3897A	247	46.10						201.70		48,090		432	276	114,530	1		235								
X071	3897B	West Fork Delisle Creek	Spans 1-3								137.61			32,391			276	114,454							
								End Bents 1 & 4	94			34.55		4,838		416		2,076		186					
								Bents 2 & 3	124	45.70		29.94		10,821											
								Totals for Bridge No. 3897B	218	45.70		202.10		48,050		416	276	114,530	1		186				
								X071	3898A	Delisle Creek	Spans 1-3				192.42			45,842			390	222,202			
												End Bents 1 & 4	104			40.98		5,512		240		2,918	1	229	
				Bents 2 & 3	44	44.20						35.00		10,226											
				Totals for Bridge No. 3898A	148	44.20						268.40		61,580		240	390	224,520	1		229				
				X071	3898B	Delisle Creek	Spans 1-3								192.62			45,842			390	222,202			
												End Bents 1 & 4	104			40.98		5,512		240		2,918	1	208	
Bents 2 & 3	44	44.20										35.00		10,226											
Totals for Bridge No. 3898B	148	44.20										268.60		61,580		240	390	224,520	1		208				
Totals for Job No. 6808												* 934	257.60	1,164.70			273,580		1388	1794	844,740	5	858	27.00	
X081	3897A	West Fork Delisle Creek	3-46' Prestressed Conc. Beam Spans												157.80	21		34,639			276	5,565			
								End Bents 1 & 4	34			41.52		5,548		486		2,005	1	235					
								Bents 2 & 3	148	51.30		33.28		11,593											
								Totals for Bridge No. 3897A	242	51.30		231.60	21	51,840		486	276	7,570	1		235				
								X081	3897B	West Fork Delisle Creek	3-46' Prestressed Conc. Beam Spans				157.80	21		34,639			276	5,565			
				End Bents 1 & 4	94							41.52		5,548		468		2,005	1	186					
				Bents 2 & 3	131	50.80						31.83		11,340											
				Totals for Bridge No. 3897B	225	50.80						231.20	21	51,700		468	276	7,570	1		186				
				X081	3898A	Delisle Creek	3-65' Prestressed Conc. Beam Spans								223.32	21		38,324			390	6,322			
												End Bents 1 & 4	104			47.98		6,342		270		2,238	1	229	
Bents 2 & 3	54	50.00										36.30		12,524											
Totals for Bridge No. 3898A	158	50.00										308.20	21	68,170		270	390	8,560	1		229				
X081	3898B	Delisle Creek	3-65' Prestressed Conc. Beam Spans												223.32	21		38,324			390	6,322			
												End Bents 1 & 4	104			47.98		6,342		270		2,238	1	208	
								Bents 2 & 3	54	50.00		36.30		12,524											
								Totals for Bridge No. 3898B	158	50.00		308.20	21	68,170		270	390	8,560	1		208				
								Totals for Job No. 6808					** 956	279.80	1,302.90	42	42	234,740		1794	194,900	5	858	27.00	

\* Includes approximately 197 cu. yds. of rock.  
 \*\* Includes approximately 236 cu. yds. of rock.



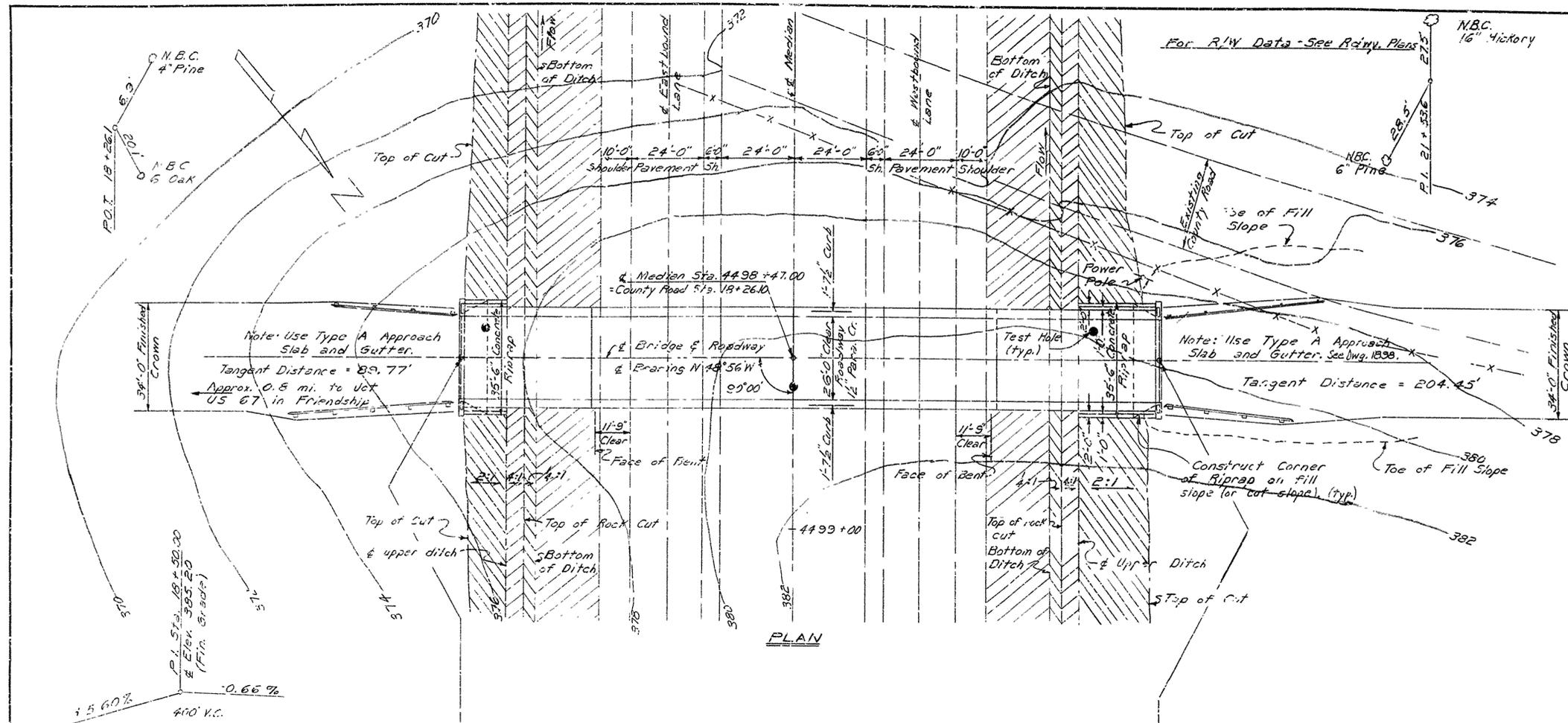
DETAILS OF CONCRETE RIPRAP  
 Scale: 1" = 1'-0"

Note: Sloped surfaces of concrete riprap to be marked off into blocks (construction joints optional) with an approved grooving tool, spacing the grooved lines about 5' apart.

SUMMARY OF BRIDGE QUANTITIES  
 FRIENDSHIP-MIDWAY  
 HOT SPRING COUNTY  
 INT. ROUTE 30 SEC. 2  
 ARKANSAS STATE HIGHWAY COMMISSION

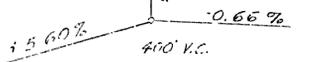
LITTLE ROCK, ARK.  
 6-22-64  
 DRAWN BY: R. W. MEAN  
 CHECKED BY: C. K. [Signature]  
 BRIDGE NO. 3896, 3897A, 3898A, 3898B  
 DRAWING NO. 12925

STATE	FED. AID PROJECT	FISCAL YEAR	SHEET No.	TOTAL SHEETS
6 ARK.	2-30-216289		31	234
JOB No. 6608				



PLAN

VERTICAL CURVE DATA



**GENERAL NOTES**

Bench Mark - Nail in Side 12" Pine 141' Right Sta. 4429+15, centerline mudlar, Elevation 379.08.

All footings shall be set a minimum of 1'0" into rock. Rock excavations shall be made to neat lines of concrete footing. Care shall be exercised to avoid shattering of rock faces by excessive blasting. Concrete in footings shall be poured in the dry directly against excavated surfaces of rock.

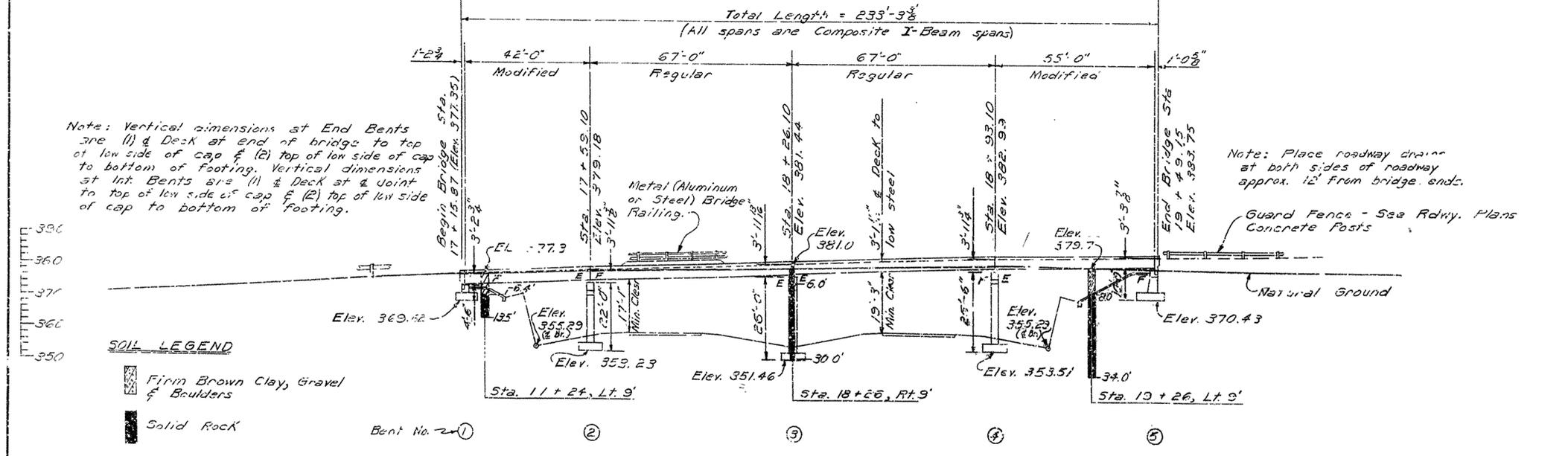
For Details of End Bents see Dwg. No. 12931  
 For Details of Intermediate Bents see Dwg. No. 12952  
 For Details of Superstructure see Dwg. No. 15070 Rev. and 14990A.

**SPECIFICATIONS:** Arkansas State Highway Commission Standard Specifications for Highway Construction, Edition of 1959.

**DESIGN SPECIFICATIONS** AASHTO 1961

Live Loading: H 20  
 Unit Stresses: Class A Concrete (n=15) 340 psi  
 Class S Concrete (n=10) 1,200 psi  
 Reinforcing Steel 20,000 psi  
 Structural Steel (A 36) 20,000 psi

Foundation Pressure - Group III 12,000 psf Max.



ELEVATION

Note: Vertical dimensions at End Bents are (1) Deck at end of bridge to top of low side of cap & (2) top of low side of cap to bottom of footing. Vertical dimensions at Int. Bents are (1) Deck at Joint to top of low side of cap & (2) top of low side of cap to bottom of footing.

Note: Place roadway drain at both sides of roadway approx. 12' from bridge ends.

Guard Fence - See Rdwy. Plans Concrete Posts

**SOIL LEGEND**

Firm Brown Clay, Gravel & Boulders

Solid Rock

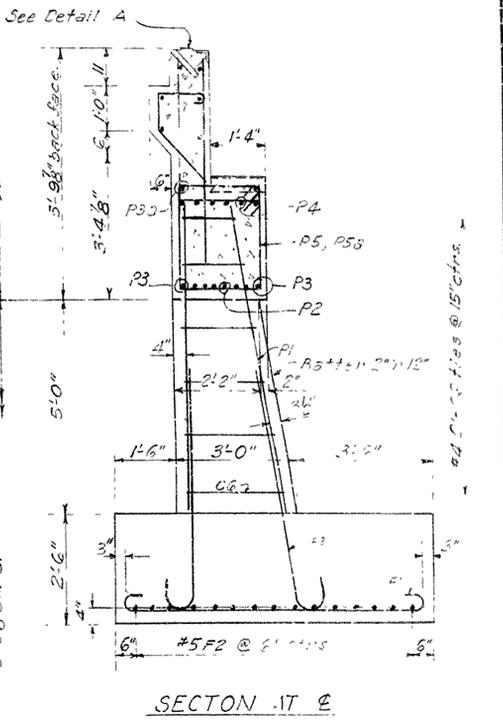
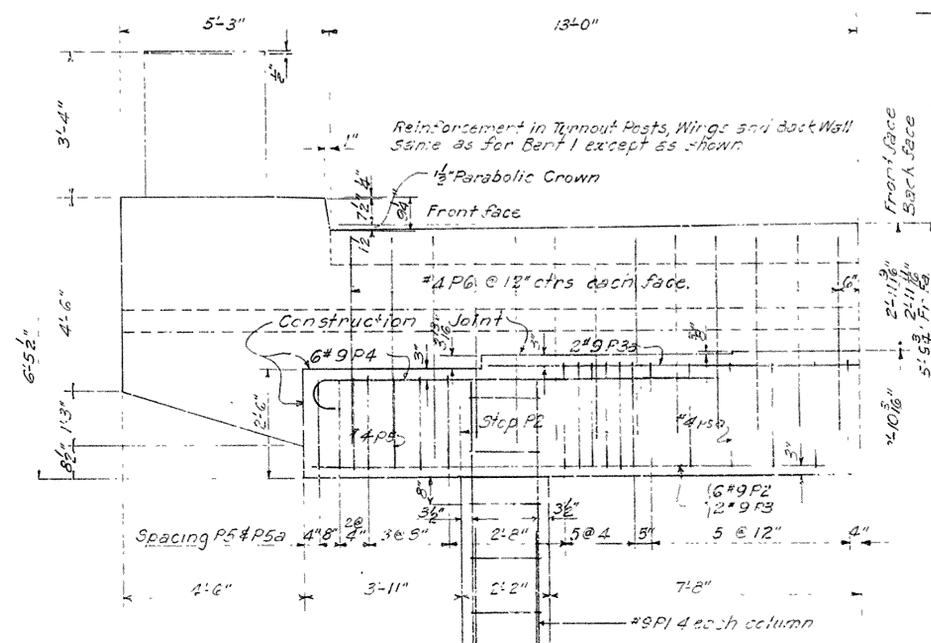
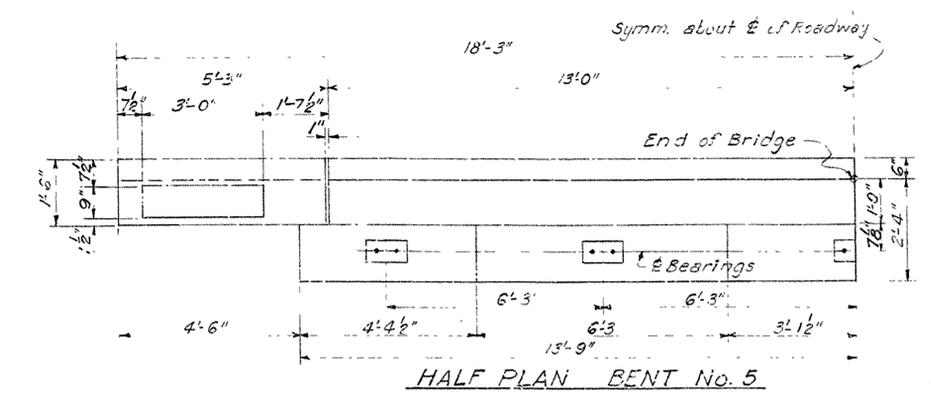
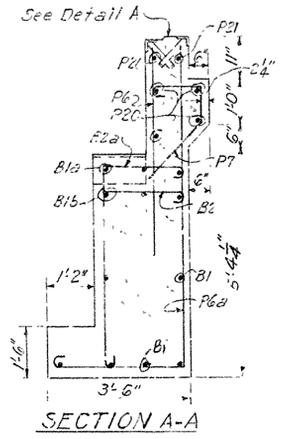
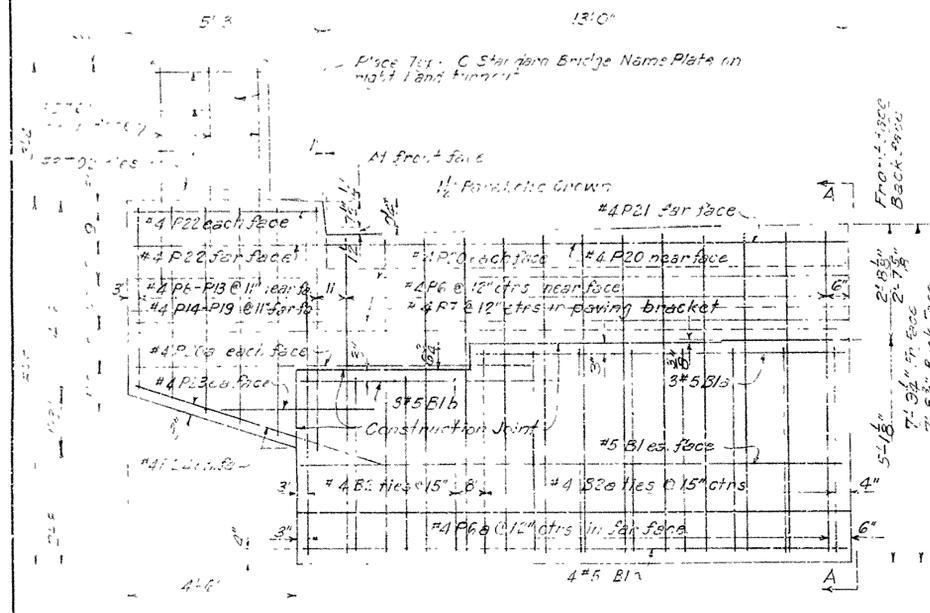
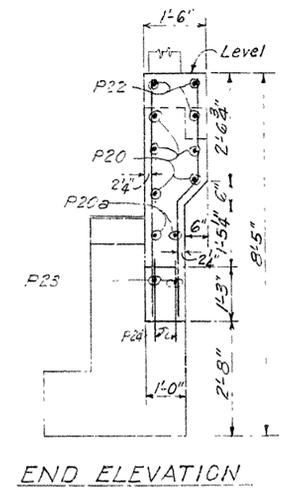
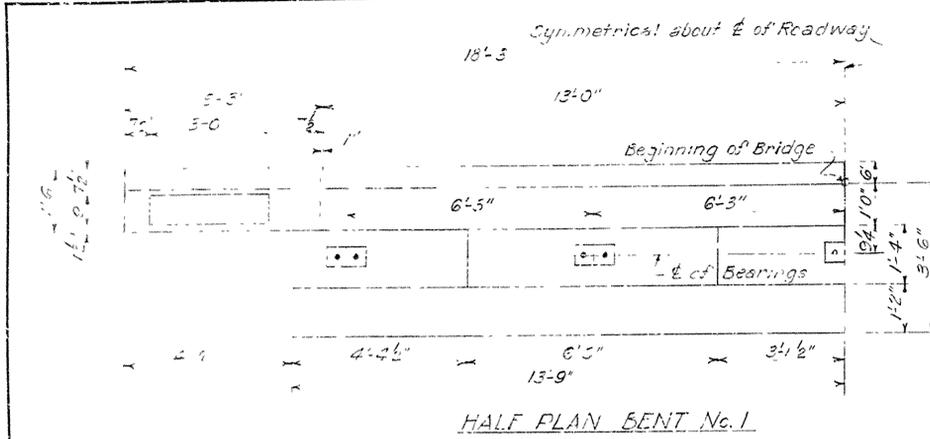
**LAYOUT OF FRIENDSHIP INTERCHANGE**  
**FRIENDSHIP - MIDWAY**  
**HOT SPRING COUNTY**  
**INT. ROUTE 30 SEC. 2**  
**ARKANSAS STATE HIGHWAY COMMISSION**  
 LITTLE ROCK, ARK.

DRAWN BY: JAS DATE: 6-2-64  
 CHECKED BY: DFL DATE: 6-5-64  
 SCALE: 1" = 50'

**BRIDGE NO. 3896** **DRAWING NO. 12926**

*J.P. Clouston*  
 BRIDGE ENGINEER

FED. ROAD No.	STATE	FED. AID PROJECT No.	FISCAL YEAR	SHEET No.	TOTAL SHEETS
6	ARK.	7-30-50		36	234
JOB No.		6808	36	234	



MARK	SIZE	NO.	LENGTH	A	B	FIN
P1	#5	6	17'-2"			Str.
P1a	#5	2	16'-5"			Str.
E11	#5	6	6'-6"			Str.
F2	#4	2	7'-6"	1-11 1/2"	4'-0 1/2"	1/2"
F2a	#4	2	7'-6"	1-11 1/2"	4'-7"	1/2"
F2b	#4	2	4'-1"			Str.
F2c	#4	2	10'-9"	7'-2"	3'-1 1/2"	1 1/2"
F2d	#4	2	3'-11"	See Diagram		1 1/2"
F2e	#4	2	4'-3 to 4'-5 1/2			1/2"
F2f	#4	2	5'-6"			1/2"
F2g	#4	2	3'-2"			Str.
F2h	#4	2	10'-0"			Str.
F2i	#4	2	5'-6"			Str.
F2j	#4	2	4'-10"			Str.
F2k	#4	2	4'-0"			Str.
F2l	#4	2	4'-5"			Str.
F2m	#4	2	4'-5"			Str.
F2n	#4	2	4'-5"			Str.
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F2r	#4	2	4'-5"			Str.
F2s	#4	2	4'-5"			Str.
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F2gn	#4	2	4'-5"			Str.
F2go	#4	2	4'-5"			Str.
F2gp	#4	2	4'-5"			Str.
F2gq	#4	2	4'-5"			Str.
F2gr	#4	2	4'-5"			Str.
F2gs	#4	2	4'-5"			Str.
F2gt	#4	2	4'-5"			Str.
F2gu	#4	2	4'-5"			Str.
F2gv	#4	2	4'-5"			Str.
F2gw	#4	2	4'-5"			Str.
F2gx	#4	2	4'-5"			Str.
F2gy	#4	2	4'-5"			Str.
F2gz	#4	2	4'-5"			Str.
F2ha	#4	2	4'-5"			Str.
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F2hf	#4	2	4'-5"			Str.
F2hg	#4	2	4'-5"			Str.
F2hh	#4	2	4'-5"			Str.
F2hi	#4	2	4'-5"			Str.
F2hj	#4	2	4'-5"			Str.
F2hk	#4	2	4'-5"			Str.
F2hl	#4	2	4'-5"			Str.
F2hm	#4	2	4'-5"			Str.
F2hn	#4	2	4'-5"			Str.
F2ho	#4	2	4'-5"			Str.
F2hp	#4	2	4'-5"			Str.
F2hq	#4	2	4'-5"			Str.
F2hr	#4	2	4'-5"			Str.
F2hs	#4	2	4'-5"			Str.
F2ht	#4	2	4'-5"			Str.
F2hu	#4	2	4'-5"			Str.
F2hv	#4	2	4'-5"			Str.
F2hw	#4	2	4'-5"			Str.
F2hx	#4	2	4'-5"			Str.
F2hy	#4	2	4'-5"			Str.
F2hz	#4	2	4'-5"			Str.
F2ia	#4	2	4'-5"			Str.
F2ib</						

