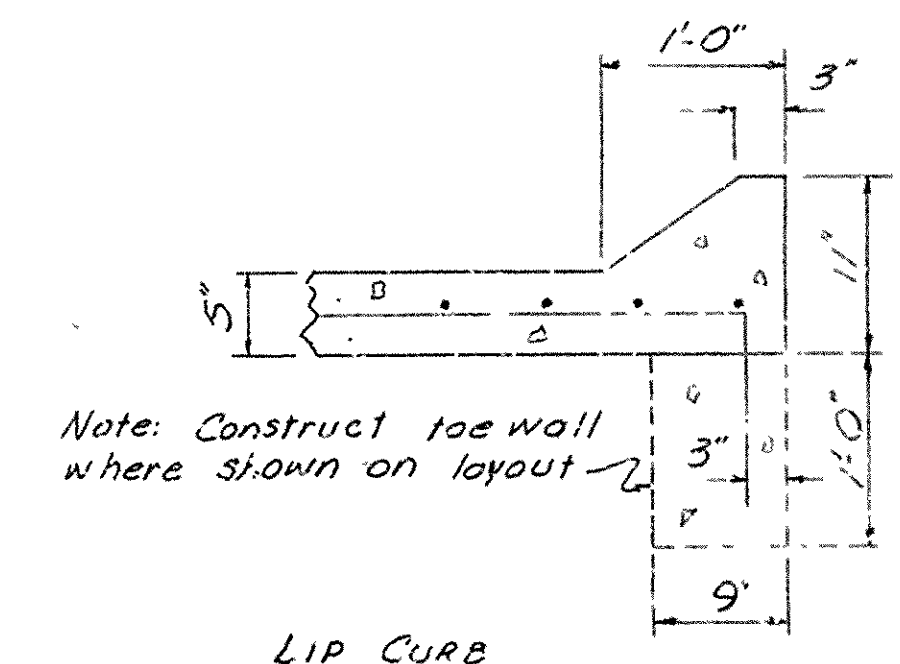


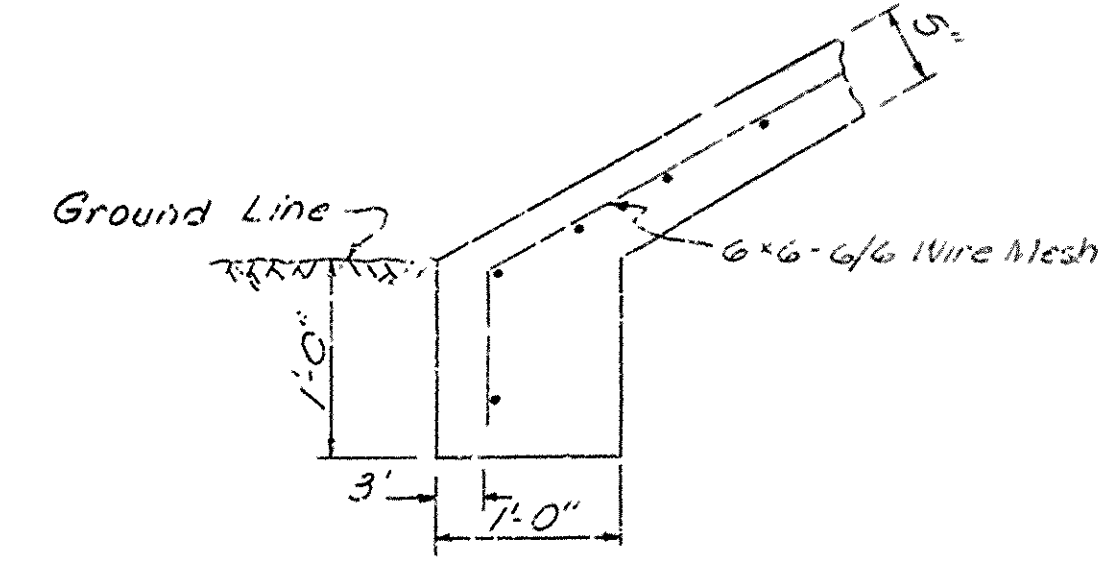
| | | | | | |
|---------------|-------|------------------------|-------------|-----------|--------------|
| FED. ROAD NO. | STATE | FISC. AND PROJECT YEAR | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| 6 | Ark. | 30-2 (27)80 | | 12 | 234 |
| JOB NO. | | 6808 | | | |

SUMMARY OF BRIDGE QUANTITIES - JOB 6808

| Code No. | Bridge No. | Name Plate Title | UNIT OF BRIDGE | Item No. | SP # 601 | SP # 602 | SP # 602 | SP # 602 | SP # 602 | SP # 603 | 804 | 804 | SP # 805 (Alt. #1) SP # 805 (Alt. #2) | SP # 806 | SP # 812 | 817 | SP # 817 | | | |
|------------------------------------|------------|------------------------|------------------------------------|----------|--|------------------------------------|------------------|--|---|-------------------|---------------------------------|---------------------------------|--|--------------------------------|-----------------------------|--------|-----------------|---------|---------|---------|
| | | | | Item | Unclassified Excavation for Structures | Class A Concrete | Class S Concrete | 46'-0" Prestressed Concrete Beams (Type II.) | 65'-0" Prestressed Concrete Beams (Type III.) | Reinforcing Steel | Steel Bearing Piling (12 BP 53) | Steel Bearing Piling (10 BP 42) | Metal (Aluminum) Bridge Railing (Alt. #1) Metal/Steel Bridge Railing (Alt. #2) | Structural Steel in Beam Spans | Bridge Name Plates (Type C) | Riprap | Concrete Riprap | | | |
| | | | | | | | | | | | | | | | | | | Cu. Yd. | Cu. Yd. | Cu. Yd. |
| X771 | 3896 | Friendship Interchange | Span 1 | | | | 32.56 | | | 7,053 | | | 84 | 24,091 | | | | | | |
| | | | Span 2 & 3 | | | | 102.45 | | | 22,604 | | | 268 | 103,145 | | | | | | |
| | | | Span 4 | | | | 42.36 | | | 9,303 | | | 110 | 34,046 | | | | | | |
| | | | End Bent 1 | 60 | 19.61 | | | | 1,160 | | | | 679 | 1 | | 10.00 | | | | |
| | | | Bent 2 | 19 | 14.95 | 11.09 | | | 3,606 | | | | | | | | | | | |
| | | | Bent 3 | 17 | 16.81 | 11.09 | | | 3,872 | | | | | | | | | | | |
| | | | Bent 4 | 17 | 16.55 | | | | 3,632 | | | | | | | | | | | |
| | | | End Bent 5 | 60 | 5.48 | | | | 2,850 | | | | 679 | | | 17.00 | | | | |
| | | | Totals for Bridge No. 3896 | 173 | 77.40 | | | | 54,280 | | | 462 | 162,440 | 1 | | 27.00 | | | | |
| | | | X071 | 3897A | West Fork Delisle Creek | Spans 1-3 | | | | 137.59 | | | 32,393 | | | 276 | 114,454 | | | |
| End Bents 1 & 4 | 94 | | | | | | 34.23 | | | 4,838 | 432 | | | 2,076 | 1 | 235 | | | | |
| Bents 2 & 3 | 153 | 46.10 | | | | | 29.82 | | | 10,859 | | | | | | | | | | |
| Totals for Bridge No. 3897A | 247 | 46.10 | | | | | 201.70 | | | 48,090 | 432 | | 276 | 114,530 | 1 | 235 | | | | |
| Spans 1-3 | | | | | | | 137.61 | | | 32,391 | | | 276 | 114,454 | | | | | | |
| End Bents 1 & 4 | 94 | | | | | | 34.55 | | | 4,838 | 416 | | | 2,076 | 1 | 186 | | | | |
| Bents 2 & 3 | 124 | 45.70 | | | | | 29.94 | | | 10,821 | | | | | | | | | | |
| Totals for Bridge No. 3897B | 218 | 45.70 | | | | | 202.10 | | | 48,050 | 416 | | 276 | 114,530 | 1 | 186 | | | | |
| Spans 1-3 | | | | | | | 192.42 | | | 45,842 | | | 390 | 222,202 | | | | | | |
| End Bents 1 & 4 | 104 | | | | | | 40.98 | | | 5,512 | 240 | | | 2,318 | 1 | 229 | | | | |
| Bents 2 & 3 | 44 | 44.20 | | 35.00 | | | 10,226 | | | | | | | | | | | | | |
| Totals for Bridge No. 3898A | 148 | 44.20 | | 268.40 | | | 61,580 | 240 | | 390 | 224,520 | 1 | 229 | | | | | | | |
| X071 | 3898B | Delisle Creek | Spans 1-3 | | | | 192.62 | | | 45,842 | | | 390 | 222,202 | | | | | | |
| | | | End Bents 1 & 4 | 104 | | | 40.98 | | | 5,512 | 240 | | | 2,318 | 1 | 208 | | | | |
| | | | Bents 2 & 3 | 44 | 44.20 | | 35.00 | | | 10,226 | | | | | | | | | | |
| | | | Totals for Bridge No. 3898B | 148 | 44.20 | | 268.60 | | | 61,580 | 240 | | 390 | 224,520 | 1 | 208 | | | | |
| | | | Totals for Job No. 6808 | * 934 | 257.60 | | 1,164.70 | | | 273,580 | 1388 | 1794 | 844,740 | 5 | 858 | 27.00 | | | | |
| | | | X081 | 3897A | West Fork Delisle Creek | 3-46' Prestressed Conc. Beam Spans | | | | 157.80 | 21 | | 34,639 | | | 276 | 5,565 | | | |
| | | | | | | End Bents 1 & 4 | 94 | | | 41.52 | | | 5,548 | 486 | | | 2,005 | 1 | 235 | |
| | | | | | | Bents 2 & 3 | 148 | 51.30 | | 33.28 | | | 11,593 | | | | | | | |
| | | | | | | Totals for Bridge No. 3897A | 242 | 51.30 | | 231.60 | 21 | | 51,840 | 486 | | 276 | 7,570 | 1 | 235 | |
| | | | | | | 3-46' Prestressed Conc. Beam Spans | | | | 157.80 | 21 | | 34,639 | | | 276 | 5,565 | | | |
| End Bents 1 & 4 | 94 | | | | | | 41.52 | | | 5,548 | 468 | | | 2,005 | 1 | 186 | | | | |
| Bents 2 & 3 | 131 | 50.80 | | | | | 31.83 | | | 11,340 | | | | | | | | | | |
| Totals for Bridge No. 3897B | 225 | 50.80 | | | | | 231.20 | 21 | | 51,700 | 468 | | 276 | 7,570 | 1 | 186 | | | | |
| 3-65' Prestressed Conc. Beam Spans | | | | | | | 223.32 | 21 | | 38,324 | | | 390 | 6,322 | | | | | | |
| End Bents 1 & 4 | 104 | | | | | | 47.98 | | | 6,342 | 270 | | | 2,238 | 1 | 229 | | | | |
| Bents 2 & 3 | 54 | 50.00 | | 36.30 | | | 12,524 | | | | | | | | | | | | | |
| Totals for Bridge No. 3898A | 158 | 50.00 | | 308.20 | 21 | | 68,170 | 270 | | 390 | 8,560 | 1 | 229 | | | | | | | |
| X081 | 3898B | Delisle Creek | 3-65' Prestressed Conc. Beam Spans | | | | 223.32 | 21 | | 38,324 | | | 390 | 6,322 | | | | | | |
| | | | End Bents 1 & 4 | 104 | | | 47.98 | | | 6,342 | 270 | | | 2,238 | 1 | 208 | | | | |
| | | | Bents 2 & 3 | 54 | 50.00 | | 36.30 | | | 12,524 | | | | | | | | | | |
| | | | Totals for Bridge No. 3898B | 158 | 50.00 | | 308.20 | 21 | | 68,170 | 270 | | 390 | 8,560 | 1 | 208 | | | | |
| | | | Totals for Job No. 6806 | **956 | 279.50 | | 1,302.90 | 42 | 42 | 734,740 | 494 | 1794 | 194,900 | 5 | 858 | 27.00 | | | | |



LIP CURB



TOE WALL

DETAILS OF CONCRETE RIPRAP

Scale: 1"=1'-0"

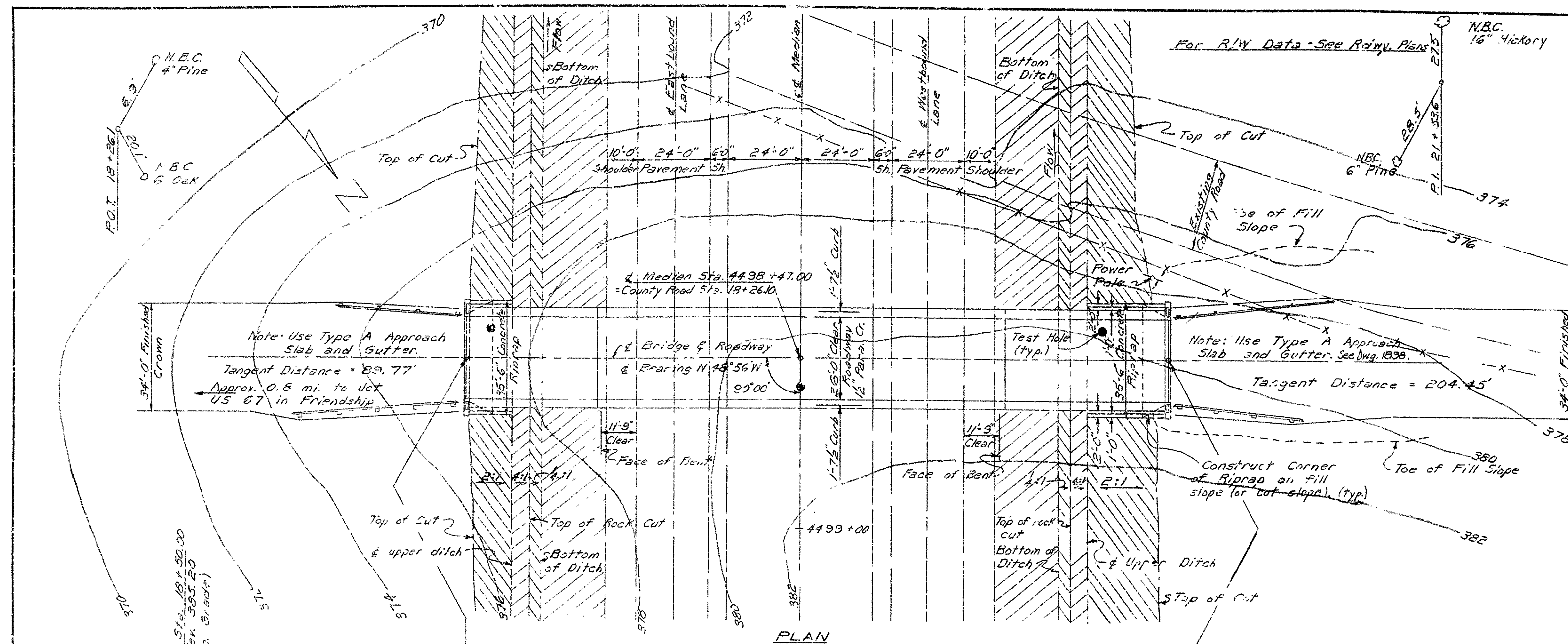
Note: Sloped surfaces of concrete riprap to be marked off into blocks (construction joints optional) with an approved grooving tool, spacing the grooved lines about 5' apart

SUMMARY OF BRIDGE QUANTITIES
FRIENDSHIP-MIDWAY
HOT SPRING COUNTY
INT. ROUTE 30 SEC. 2
ARKANSAS STATE HIGHWAY COMMISSION

DRAWN BY: R. W. MEAN DATE: 6-22-64
CHECKED BY: C. K. DATE: 8-25-64
BRIDGE NO. 3896, 3897A, 3898A, 3898B DRAWING NO. 12925

* Includes approximately 197 cu. yds. of rock.
** Includes approximately 234 cu. yds. of rock.

| FEED ROAD NO. | STATE | FED. AID PROJECT | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------|-------|------------------|-------------|-----------|--------------|
| 6 | ARK. | I-30- 2(678) | | | |
| JOB No. | | | 6608 | 31 | 234 |



GENERAL NOTES

Bench Mark - Nail in Side 12" Pine 141' Right Sta. 4429+15, centerline median,
Elevation 379.08.

All footings shall be set a minimum of 1'0" into rock. Rock excavations shall be made to neat lines of concrete footing. Care shall be exercised to avoid shattering of rock faces by excessive blasting. Concrete in footings shall be poured in the dry directly against excavated surfaces of rock.

For Details of End Bents see Diag. No. 12931

For Details of Intermediate Bents see Dwg. No. 12952

For Details of Superstructure see Dwg. No. 15020 Rev. and 14990A.

SPECIFICATIONS: Arkansas State Highway Commission Standard Specifications for Highway Construction, Edition of 1959.

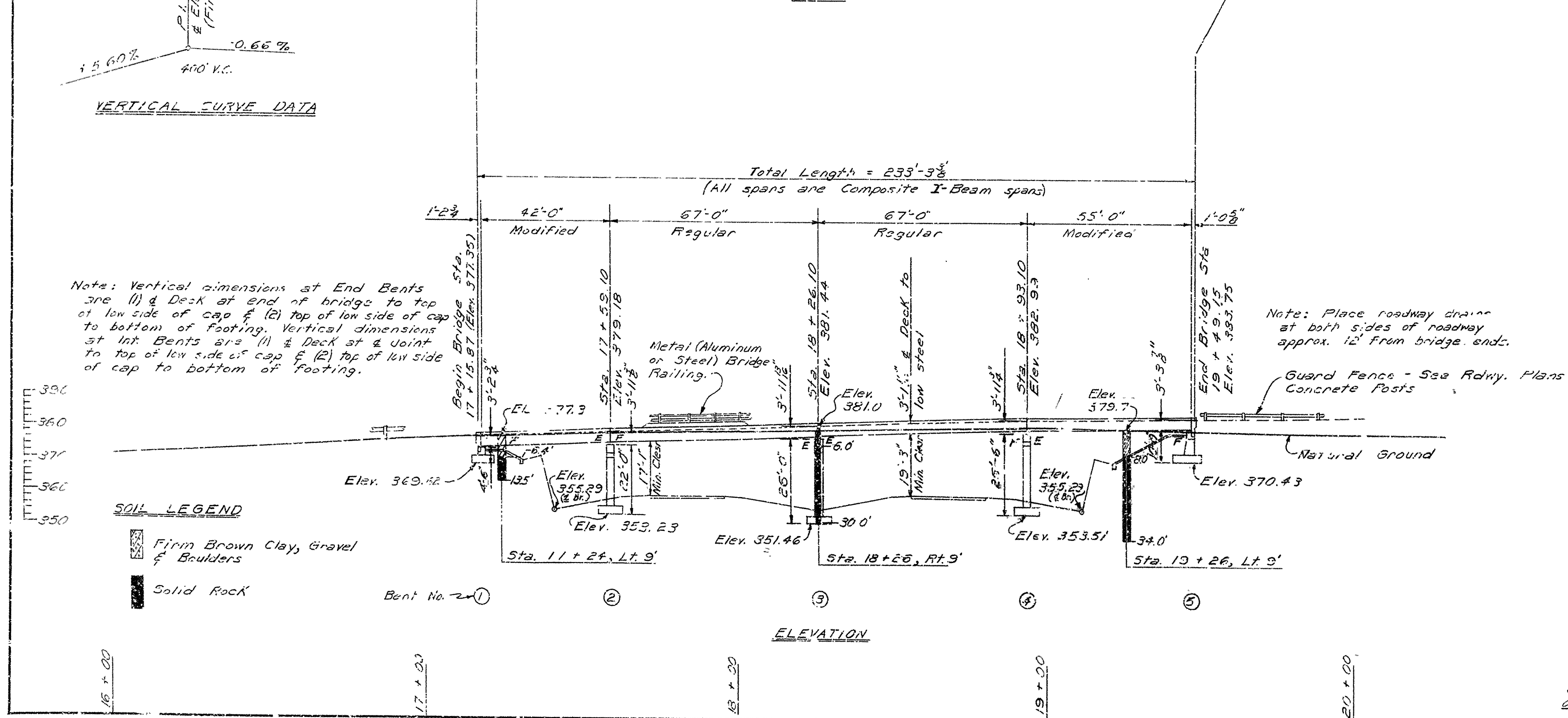
DESIGN SPECIFICATIONS

AASNO 1961

Live Loading: H 20

| | | |
|----------------|-------------------------|------------|
| Unit Stresses: | Class A Concrete (n=15) | 940 psi |
| | Class S Concrete (n=10) | 1,200 psi |
| | Reinforcing Steel | 20,000 psi |
| | Structural Steel (A 36) | 20,000 psi |

Foundation Pressure - Group III 10,000 psf Max.

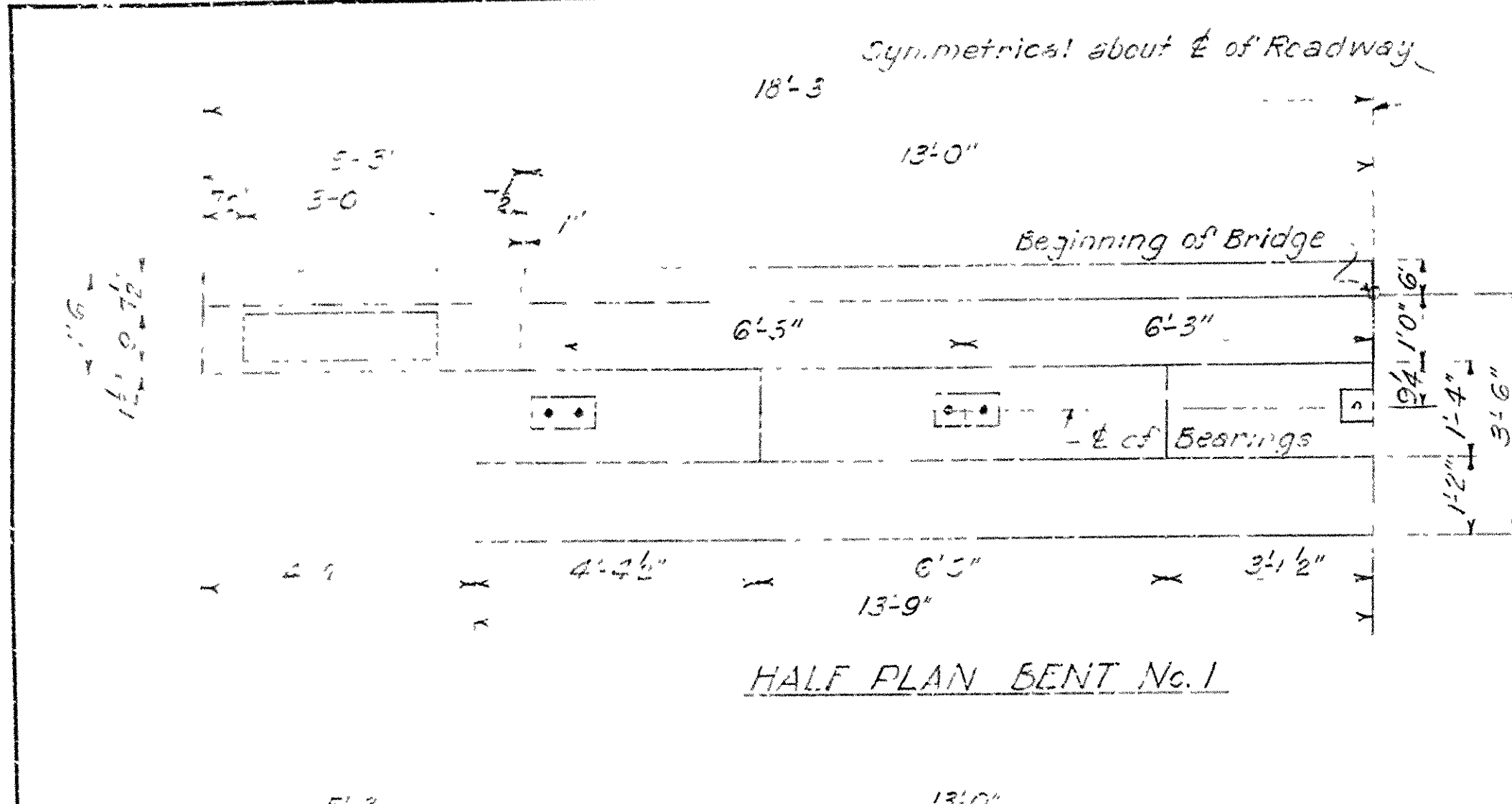


LAYOUT OF FRIENDSHIP INTERCHANGE
FRIENDSHIP -- MIDWAY
HOT SPRING COUNTY
INT. ROUTE 30 SEC. 2
ARKANSAS STATE HIGHWAY COMMISSION

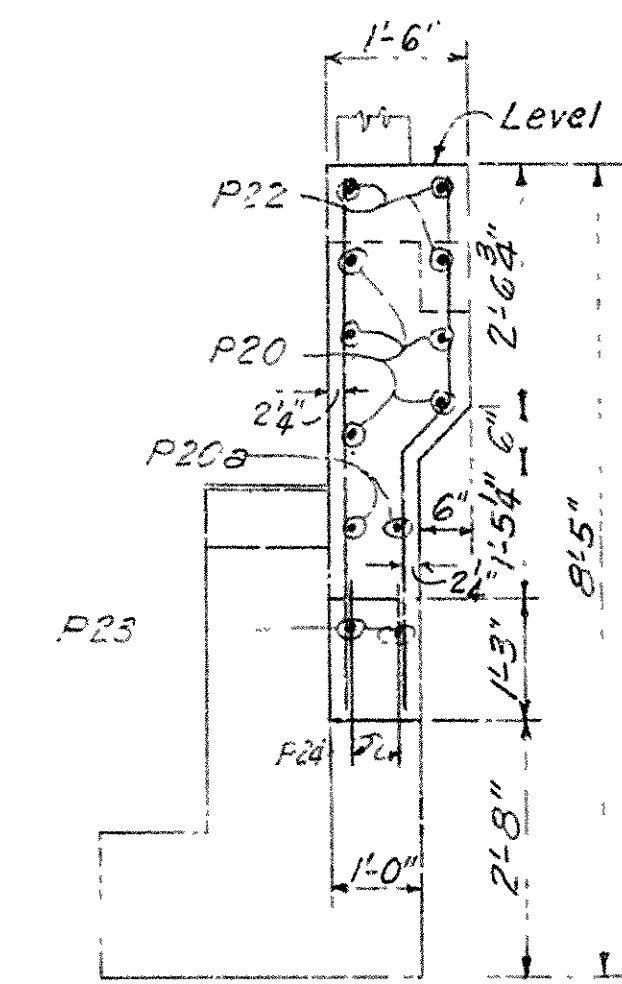
DRAWN BY: IAS DATE: 6-3-64
 TRACED BY: _____ DATE: _____ SCALE: 1" = 50'
 CHECKED BY: DFL DATE: 6-4-64

BRIDGE NO. 3896 **DRAWING NO. 12926**

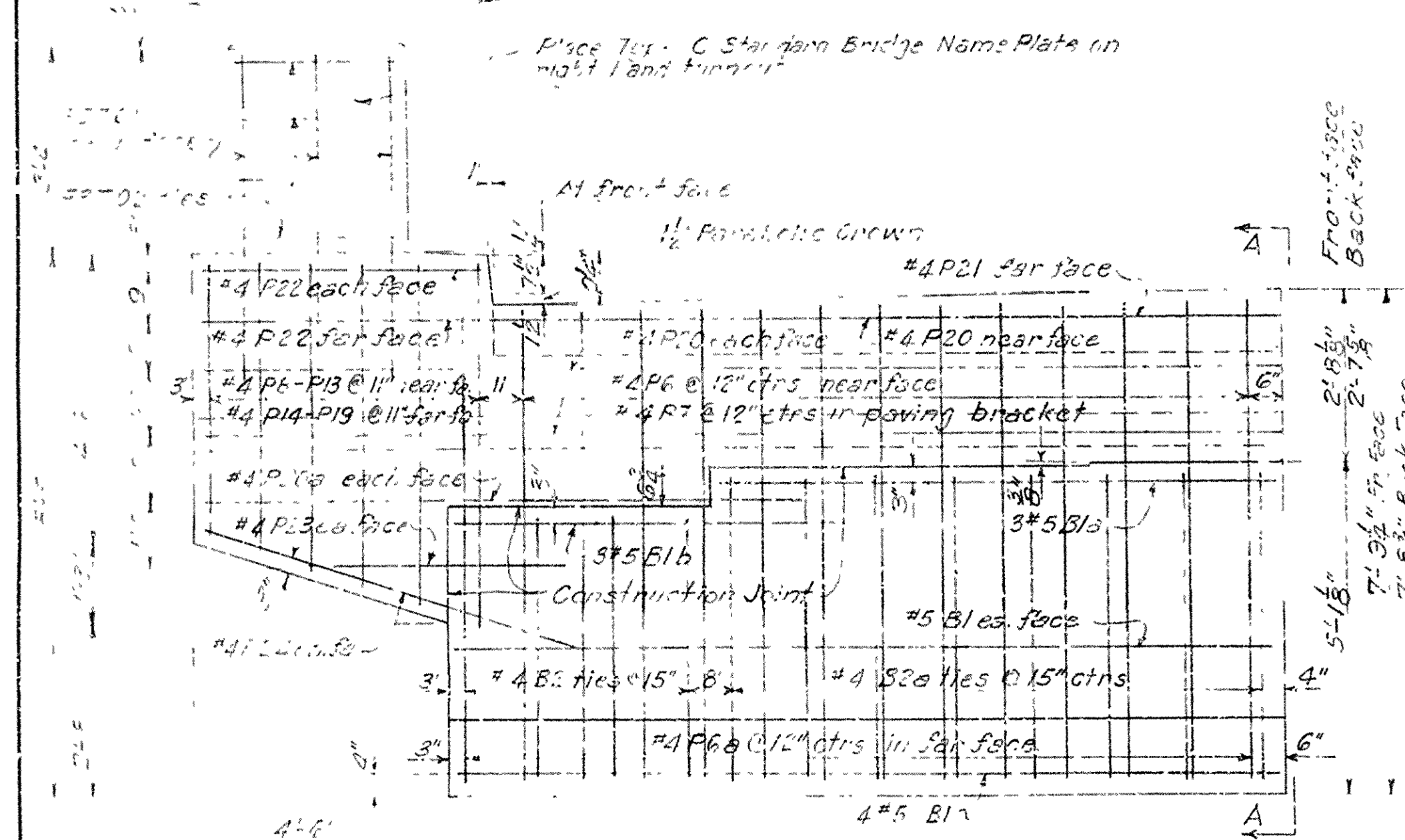
| FED. ROAD DIST. NO. | STATE | FED. AID PROJECT NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|----------------------|-------------|-----------|--------------|
| 6 | ARK. | 7-30-50 | 6808 | 36 | 234 |
| JOB NO. | 6808 | | | | |



HALF PLAN BENT No. 1



END ELEVATION

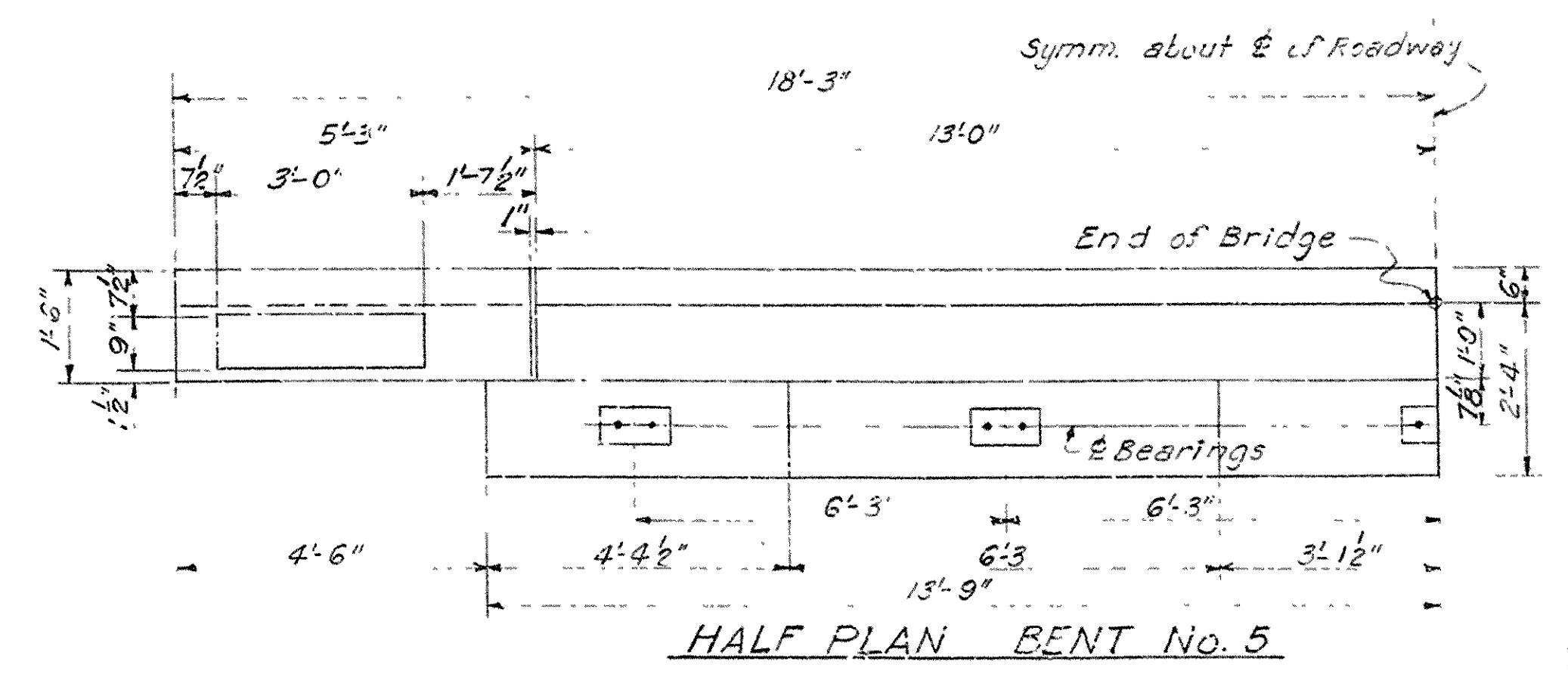


HALF ELEVATION BENT No. 1

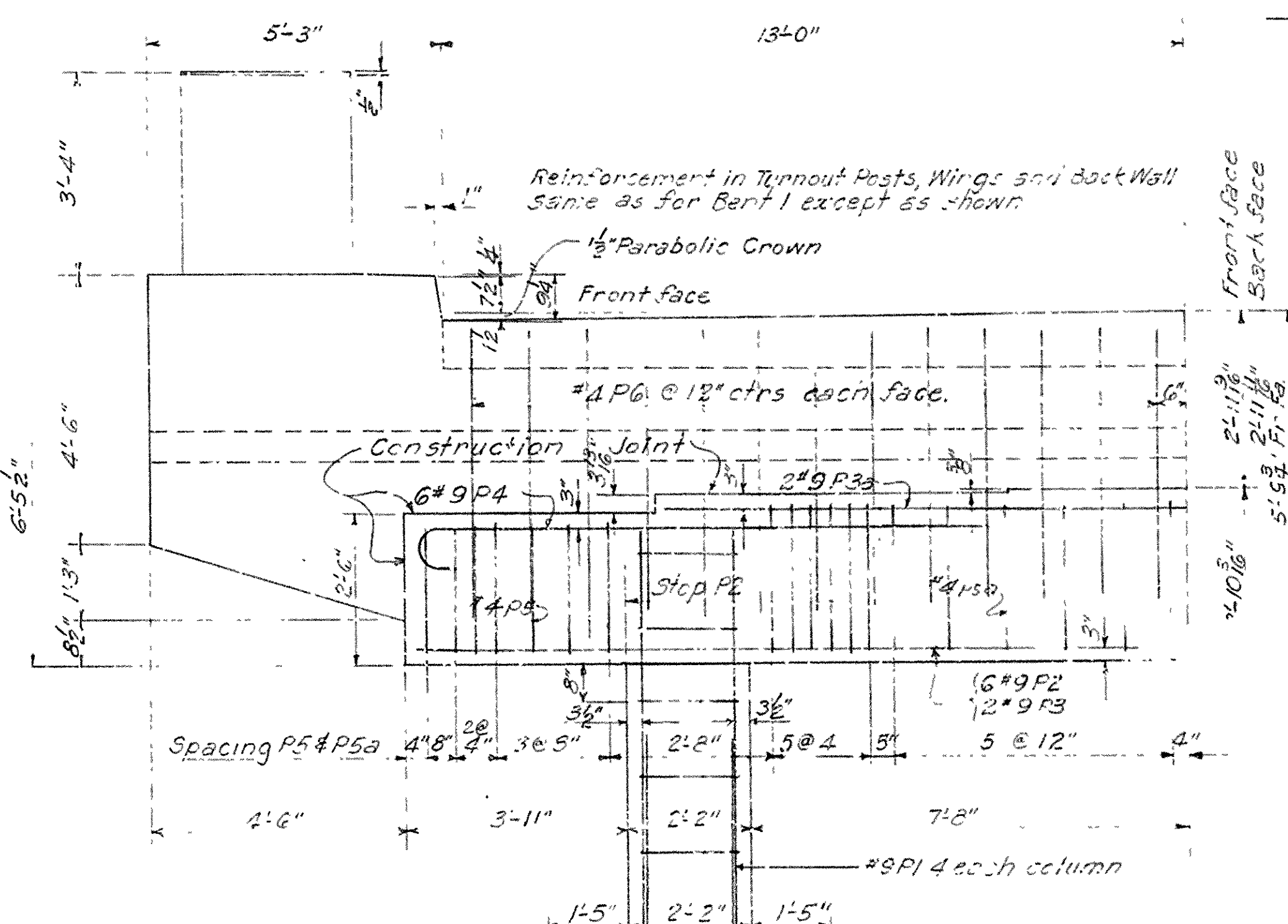
| MARK | SIZE | NO. | LENGTH | A | B | FIN |
|------|------|-----|--------|---|---|------|
| P1 | #5 | 6 | 17'-2" | | | Str. |
| P2 | #5 | 6 | 17'-2" | | | Str. |
| P3 | #5 | 6 | 17'-2" | | | Str. |
| P4 | #5 | 6 | 17'-2" | | | Str. |
| P5 | #5 | 6 | 17'-2" | | | Str. |
| P6 | #5 | 6 | 17'-2" | | | Str. |
| P7 | #5 | 6 | 17'-2" | | | Str. |
| P8 | #5 | 6 | 17'-2" | | | Str. |
| P9 | #5 | 6 | 17'-2" | | | Str. |
| P10 | #5 | 6 | 17'-2" | | | Str. |
| P11 | #5 | 6 | 17'-2" | | | Str. |
| P12 | #5 | 6 | 17'-2" | | | Str. |
| P13 | #5 | 6 | 17'-2" | | | Str. |
| P14 | #5 | 6 | 17'-2" | | | Str. |
| P15 | #5 | 6 | 17'-2" | | | Str. |
| P16 | #5 | 6 | 17'-2" | | | Str. |
| P17 | #5 | 6 | 17'-2" | | | Str. |
| P18 | #5 | 6 | 17'-2" | | | Str. |
| P19 | #5 | 6 | 17'-2" | | | Str. |
| P20 | #5 | 6 | 17'-2" | | | Str. |
| P21 | #5 | 6 | 17'-2" | | | Str. |
| P22 | #5 | 6 | 17'-2" | | | Str. |
| P23 | #5 | 6 | 17'-2" | | | Str. |
| P24 | #5 | 6 | 17'-2" | | | Str. |
| P25 | #5 | 6 | 17'-2" | | | Str. |
| P26 | #5 | 6 | 17'-2" | | | Str. |
| P27 | #5 | 6 | 17'-2" | | | Str. |
| P28 | #5 | 6 | 17'-2" | | | Str. |
| P29 | #5 | 6 | 17'-2" | | | Str. |
| P30 | #5 | 6 | 17'-2" | | | Str. |
| P31 | #5 | 6 | 17'-2" | | | Str. |
| P32 | #5 | 6 | 17'-2" | | | Str. |
| P33 | #5 | 6 | 17'-2" | | | Str. |
| P34 | #5 | 6 | 17'-2" | | | Str. |
| P35 | #5 | 6 | 17'-2" | | | Str. |
| P36 | #5 | 6 | 17'-2" | | | Str. |
| P37 | #5 | 6 | 17'-2" | | | Str. |
| P38 | #5 | 6 | 17'-2" | | | Str. |
| P39 | #5 | 6 | 17'-2" | | | Str. |
| P40 | #5 | 6 | 17'-2" | | | Str. |
| P41 | #5 | 6 | 17'-2" | | | Str. |
| P42 | #5 | 6 | 17'-2" | | | Str. |
| P43 | #5 | 6 | 17'-2" | | | Str. |
| P44 | #5 | 6 | 17'-2" | | | Str. |
| P45 | #5 | 6 | 17'-2" | | | Str. |
| P46 | #5 | 6 | 17'-2" | | | Str. |
| P47 | #5 | 6 | 17'-2" | | | Str. |
| P48 | #5 | 6 | 17'-2" | | | Str. |
| P49 | #5 | 6 | 17'-2" | | | Str. |
| P50 | #5 | 6 | 17'-2" | | | Str. |
| P51 | #5 | 6 | 17'-2" | | | Str. |
| P52 | #5 | 6 | 17'-2" | | | Str. |
| P53 | #5 | 6 | 17'-2" | | | Str. |
| P54 | #5 | 6 | 17'-2" | | | Str. |
| P55 | #5 | 6 | 17'-2" | | | Str. |
| P56 | #5 | 6 | 17'-2" | | | Str. |
| P57 | #5 | 6 | 17'-2" | | | Str. |
| P58 | #5 | 6 | 17'-2" | | | Str. |
| P59 | #5 | 6 | 17'-2" | | | Str. |
| P60 | #5 | 6 | 17'-2" | | | Str. |
| P61 | #5 | 6 | 17'-2" | | | Str. |
| P62 | #5 | 6 | 17'-2" | | | Str. |
| P63 | #5 | 6 | 17'-2" | | | Str. |
| P64 | #5 | 6 | 17'-2" | | | Str. |
| P65 | #5 | 6 | 17'-2" | | | Str. |
| P66 | #5 | 6 | 17'-2" | | | Str. |
| P67 | #5 | 6 | 17'-2" | | | Str. |
| P68 | #5 | 6 | 17'-2" | | | Str. |
| P69 | #5 | 6 | 17'-2" | | | Str. |
| P70 | #5 | 6 | 17'-2" | | | Str. |
| P71 | #5 | 6 | 17'-2" | | | Str. |
| P72 | #5 | 6 | 17'-2" | | | Str. |
| P73 | #5 | 6 | 17'-2" | | | Str. |
| P74 | #5 | 6 | 17'-2" | | | Str. |
| P75 | #5 | 6 | 17'-2" | | | Str. |
| P76 | #5 | 6 | 17'-2" | | | Str. |
| P77 | #5 | 6 | 17'-2" | | | Str. |
| P78 | #5 | 6 | 17'-2" | | | Str. |
| P79 | #5 | 6 | 17'-2" | | | Str. |
| P80 | #5 | 6 | 17'-2" | | | Str. |
| P81 | #5 | 6 | 17'-2" | | | Str. |
| P82 | #5 | 6 | 17'-2" | | | Str. |
| P83 | #5 | 6 | 17'-2" | | | Str. |
| P84 | #5 | 6 | 17'-2" | | | Str. |
| P85 | #5 | 6 | 17'-2" | | | Str. |
| P86 | #5 | 6 | 17'-2" | | | Str. |
| P87 | #5 | 6 | 17'-2" | | | Str. |
| P88 | #5 | 6 | 17'-2" | | | Str. |
| P89 | #5 | 6 | 17'-2" | | | Str. |
| P90 | #5 | 6 | 17'-2" | | | Str. |
| P91 | #5 | 6 | 17'-2" | | | Str. |
| P92 | #5 | 6 | 17'-2" | | | Str. |
| P93 | #5 | 6 | 17'-2" | | | Str. |
| P94 | #5 | 6 | 17'-2" | | | Str. |
| P95 | #5 | 6 | 17'-2" | | | Str. |
| P96 | #5 | 6 | 17'-2" | | | Str. |
| P97 | #5 | 6 | 17'-2" | | | Str. |
| P98 | #5 | 6 | 17'-2" | | | Str. |
| P99 | #5 | 6 | 17'-2" | | | Str. |
| P100 | #5 | 6 | 17'-2" | | | Str. |

| MARK | SIZE | NO. | LENGTH | A | B | FIN |
|------|------|-----|--------|--------|--------|-------|
| F1 | #5 | 18 | 8'-9" | 7'-6" | 5' | 3'-2" |
| F2 | #5 | 30 | 5'-9" | 4'-6" | 5' | 3'-2" |
| F3 | #5 | 8 | 7'-0" | 5'-9" | 10' | 9" |
| F4 | #5 | 8 | 7'-2" | | | Str. |
| F5 | #5 | 2 | 6'-11" | 1'-5" | 10' | 1'-5" |
| F6 | #5 | 2 | 9'-0" | 2'-5" | | Str. |
| F7 | #5 | 6 | 19'-8" | | | Str. |
| F8 | #5 | 2 | 27'-2" | | | Str. |
| F9 | #5 | 2 | 18'-5" | | | Str. |
| F10 | #5 | 12 | 11'-3" | 10'-0" | 10' | 9" |
| F11 | #5 | 1 | 8'-5" | 2'-11" | 1'-11" | 1'-2" |
| F12 | #5 | 24 | 9'-5" | 5'-3" | 1'-11" | 1'-2" |
| F13 | #5 | 32 | 4'-9" | | | Str. |
| F14 | #5 | 2 | 4'-3" | | | Str. |
| F15 | #5 | 2 | 4'-3" | | | Str. |
| F16 | #5 | 2 | 4'-3" | | | Str. |
| F17 | #5 | 2 | 4'-3" | | | Str. |
| F18 | #5 | 2 | 4'-3" | | | Str. |
| F19 | #5 | 2 | 4'-3" | | | Str. |
| F20 | #5 | 2 | 4'-3" | | | Str. |
| F21 | #5 | 2 | 4'-3" | | | Str. |
| F22 | #5 | 2 | 4'-3" | | | Str. |
| F23 | #5 | 2 | 4'-3" | | | Str. |
| F24 | #5 | 2 | 4'-3" | | | Str. |
| F25 | #5 | 2 | 4'-3" | | | Str. |
| F26 | #5 | 2 | 4'-3" | | | Str. |
| F27 | #5 | 2 | 4'-3" | | | Str. |
| F28 | #5 | 2 | 4'-3" | | | Str. |
| F29 | #5 | 2 | 4'-3" | | | Str. |
| F30 | #5 | 2 | 4'-3" | | | Str. |
| F31 | #5 | 2 | 4'-3" | | | Str. |
| F32 | #5 | 2 | 4'-3" | | | Str. |
| F33 | #5 | 2 | 4'-3" | | | Str. |
| F34 | #5 | 2 | 4'-3" | | | Str. |
| F35 | #5 | 2 | 4'-3" | | | Str. |
| F36 | #5 | 2 | 4'-3" | | | Str. |
| F37 | #5 | 2 | 4'-3" | | | Str. |
| F38 | #5 | 2 | 4'-3" | | | Str. |
| F39 | #5 | 2 | 4'-3" | | | Str. |
| F40 | #5 | 2 | 4'-3" | | | Str. |
| F41 | #5 | 2 | 4'-3" | | | Str. |
| F42 | #5 | 2 | 4'-3" | | | Str. |
| F43 | #5 | 2 | 4'-3" | | | Str. |
| F44 | #5 | 2 | 4'-3" | | | Str. |
| F45 | #5 | 2 | 4'-3" | | | Str. |
| F46 | #5 | 2 | 4'-3" | | | Str. |
| F47 | #5 | 2 | 4'-3" | | | Str. |
| F48 | #5 | 2 | 4'-3" | | | Str. |
| F49 | #5 | 2 | 4'-3" | | | Str. |
| F50 | #5 | 2 | 4'-3" | | | Str. |
| F51 | #5 | 2 | 4'-3" | | | Str. |
| F52 | #5 | 2 | 4'-3" | | | Str. |
| F53 | #5 | 2 | 4'-3" | | | Str. |
| F54 | #5 | 2 | 4'-3" | | | Str. |
| F55 | #5 | 2 | 4'-3" | | | Str. |
| F56 | #5 | 2 | 4'-3" | | | Str. |
| F57 | #5 | 2 | 4'-3" | | | Str. |
| F58 | #5 | 2 | 4'-3" | | | Str. |
| F59 | #5 | 2 | 4'-3" | | | Str. |
| F60 | #5 | 2 | 4'-3" | | | Str. |
| F61 | #5 | 2 | 4'-3" | | | Str. |
| F62 | #5 | 2 | 4'-3" | | | Str. |
| F63 | #5 | 2 | 4'-3" | | | Str. |
| F64 | #5 | 2 | 4'-3" | | | Str. |
| F65 | #5 | 2 | 4'-3" | | | Str. |
| F66 | #5 | 2 | 4'-3" | | | Str. |
| F67 | #5 | 2 | 4'-3" | | | Str. |
| F68 | #5 | 2 | 4'-3" | | | Str. |
| F69 | #5 | 2 | 4'-3" | | | Str. |
| F70 | #5 | 2 | 4'-3" | | | Str. |
| F71 | #5 | 2 | 4'-3" | | | Str. |
| F72 | #5 | 2 | 4'-3" | | | Str. |
| F73 | #5 | 2 | 4'-3" | | | Str. |
| F74 | #5 | 2 | 4'-3" | | | Str. |
| F75 | #5 | 2 | 4'-3" | | | Str. |
| F76 | #5 | 2 | 4'-3" | | | Str. |
| F77 | #5 | 2 | 4'-3" | | | Str. |
| F78 | #5 | 2 | 4'-3" | | | Str. |
| F79 | #5 | 2 | 4'-3" | | | Str. |
| F80 | #5 | 2 | 4'-3" | | | Str. |
| F81 | #5 | 2 | 4'-3" | | | Str. |
| F82 | #5 | 2 | 4'-3" | | | Str. |
| F83 | #5 | 2 | 4'-3" | | | Str. |
| F84 | #5 | 2 | 4'-3" | | | Str. |
| F85 | #5 | 2 | 4'-3" | | | Str. |
| F86 | #5 | 2 | 4'-3" | | | Str. |
| F87 | #5 | 2 | 4'-3" | | | Str. |
| F88 | #5 | 2 | 4'-3" | | | Str. |
| F89 | #5 | 2 | 4'-3" | | | Str. |
| F90 | #5 | 2 | 4'-3" | | | Str. |
| F91 | #5 | 2 | 4'-3" | | | Str. |
| F92 | #5 | 2 | 4'-3" | | | Str. |
| F93 | #5 | 2 | 4'-3" | | | Str. |
| F94 | #5 | 2 | 4'-3" | | | Str. |
| F95 | #5 | 2 | 4'-3" | | | Str. |
| F96 | #5 | 2 | 4'-3" | | | Str. |
| F97 | #5 | 2 | 4'-3" | | | Str. |
| F98 | #5 | 2 | 4'-3" | | | Str. |
| F99 | #5 | 2 | 4'-3" | | | Str. |
| F100 | #5 | 2 | 4'-3" | | | Str. |

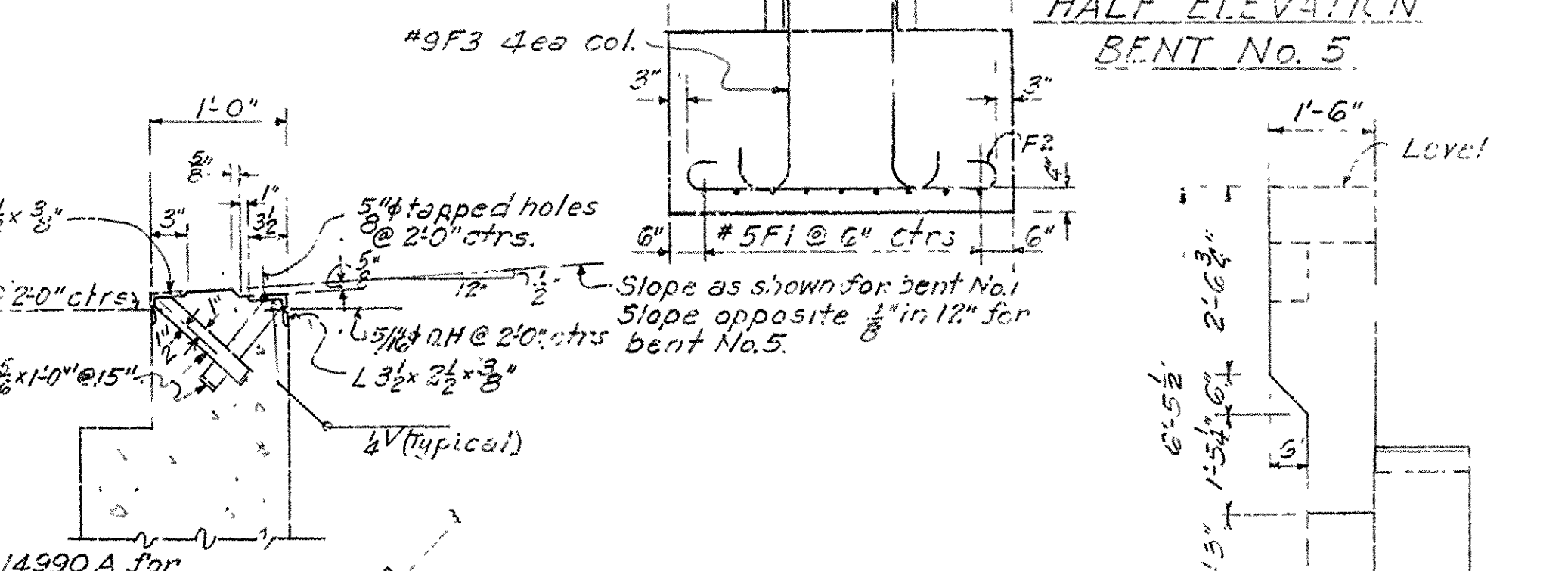
Dimensions are ctn to ctr of lars



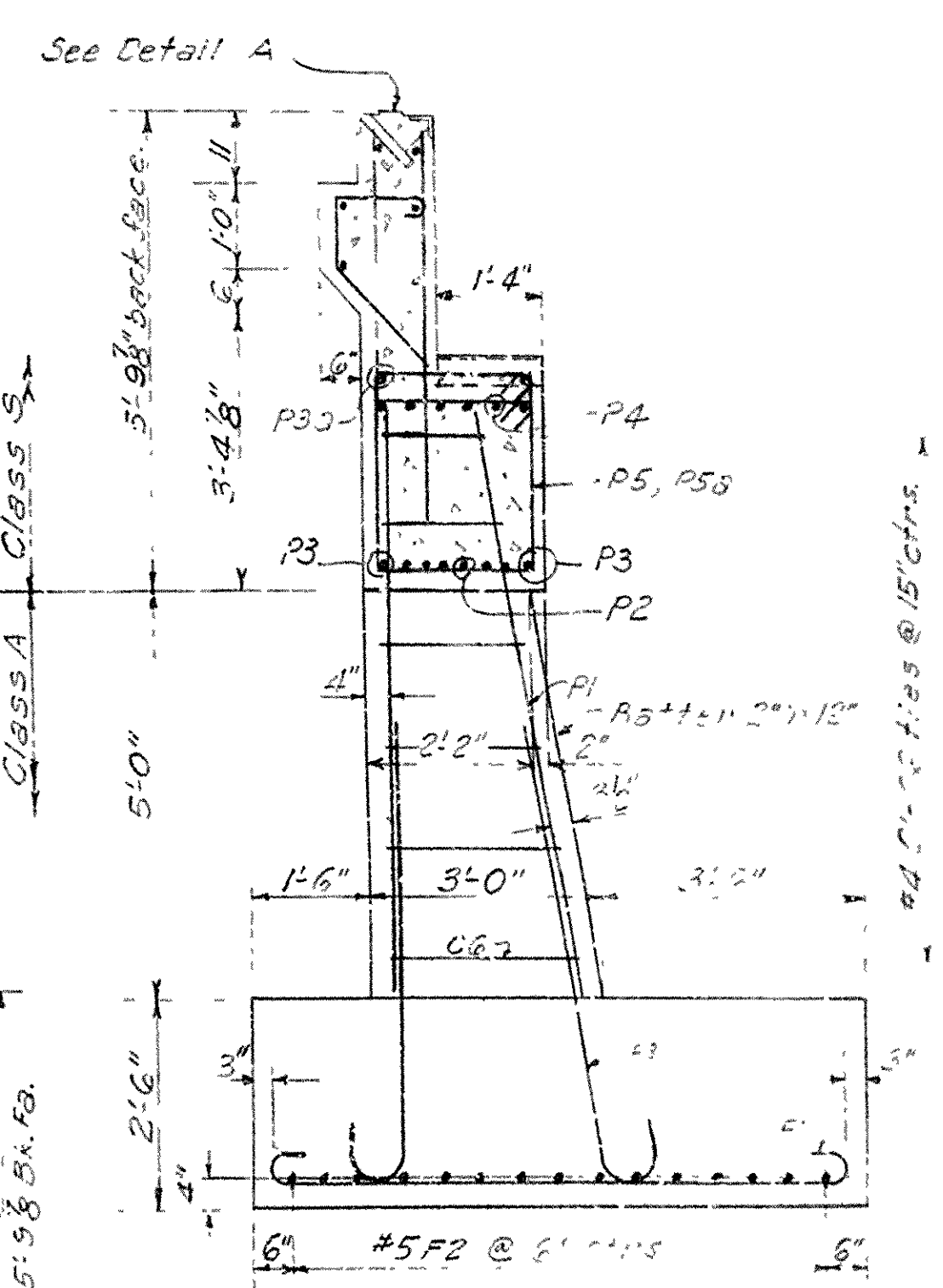
HALF PLAN BENT No. 5



HALF ELEVATION BENT No. 5



DETAIL A Scale 1"=1'-0"



GENERAL RULES

Reinforcing steel to be conform to bars of intermediate or hard grade. The reinforcing steel is to be accurately located in the forms and firmly held in place by steel wire supports, furnished in number and size to prevent displacement during the course of construction. The wire supports will be paid for directly but will be considered subsidiary to the item of "Reinforcing Steel."

Shop lists and bending diagrams of reinforcing steel, including wire supports, shall be submitted and approved, secured before fabrication of railing is begun. Shop drawings showing details of railing shall be submitted and approved, secured before fabrication is begun.

The aluminum bridge railing, including posts and fastenings, shall be paid for at the unit price bid per linear foot for Metal Aluminum Bridge Railing.

A rail connection utilizing set screws is an acceptable alternative and may be suggested at the Contractor's option.

Outside surfaces of flanges of cast steel rail supports shall be given a No. 100 grit sand blast finish, all surfaces to be painted with one coat of zinc lacquer.

If Shop or Engineer finds Guard Bridge railing to have a sharp or an equivalent ridge type as approved by the Engineer, the rail including posts and fastenings shall be the unit price bid per linear foot for Metal Aluminum Bridge Railing.

Painting Metal Bridge Railing.

Floor slabs may be painted in one continuous operation, a strikeoff extending over the entire surface shall be poured in increments of 100 square feet, the surface shall be kept in place until the concrete has gained sufficient strength to support the desired finish, pouring the next end sections may be poured simultaneously, if not more than one section, 48 hours shall elapse between end sections.

SPECIFICATIONS:
Arkansas State Highway Commission Standard Specifications for Highway Construction Edition of 1954.

DETAILS COMMON TO STANDARD 35'-90'

ALL ROADWAY WIDTHS

RECEIVED BY EE DATE 9-27-63
LITTLE ROCK, ARK.

DRAWING NO. 14990AA
 SCALE: AS SHOWN
 DESIGNED BY: ENH
 DATE: 9-30-63
 CHECKED BY: _____
 DATE: _____
 APPROVED BY: _____
 DATE: _____

Do not use 10/35/6

