

ARKANSAS DEPARTMENT OF TRANSPORTATION  
CONSTRUCTION PLANS FOR STATE HIGHWAY

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. 061621	1	7

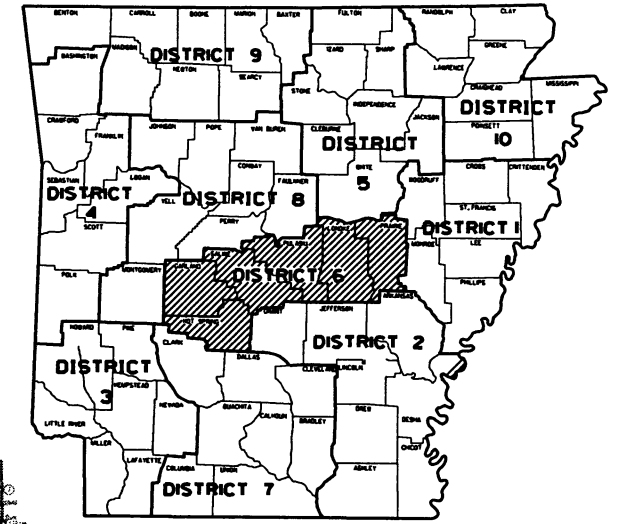
2 DISTRICT 6 APHN RAISED PAVEMENT MARKERS (2019) (S)

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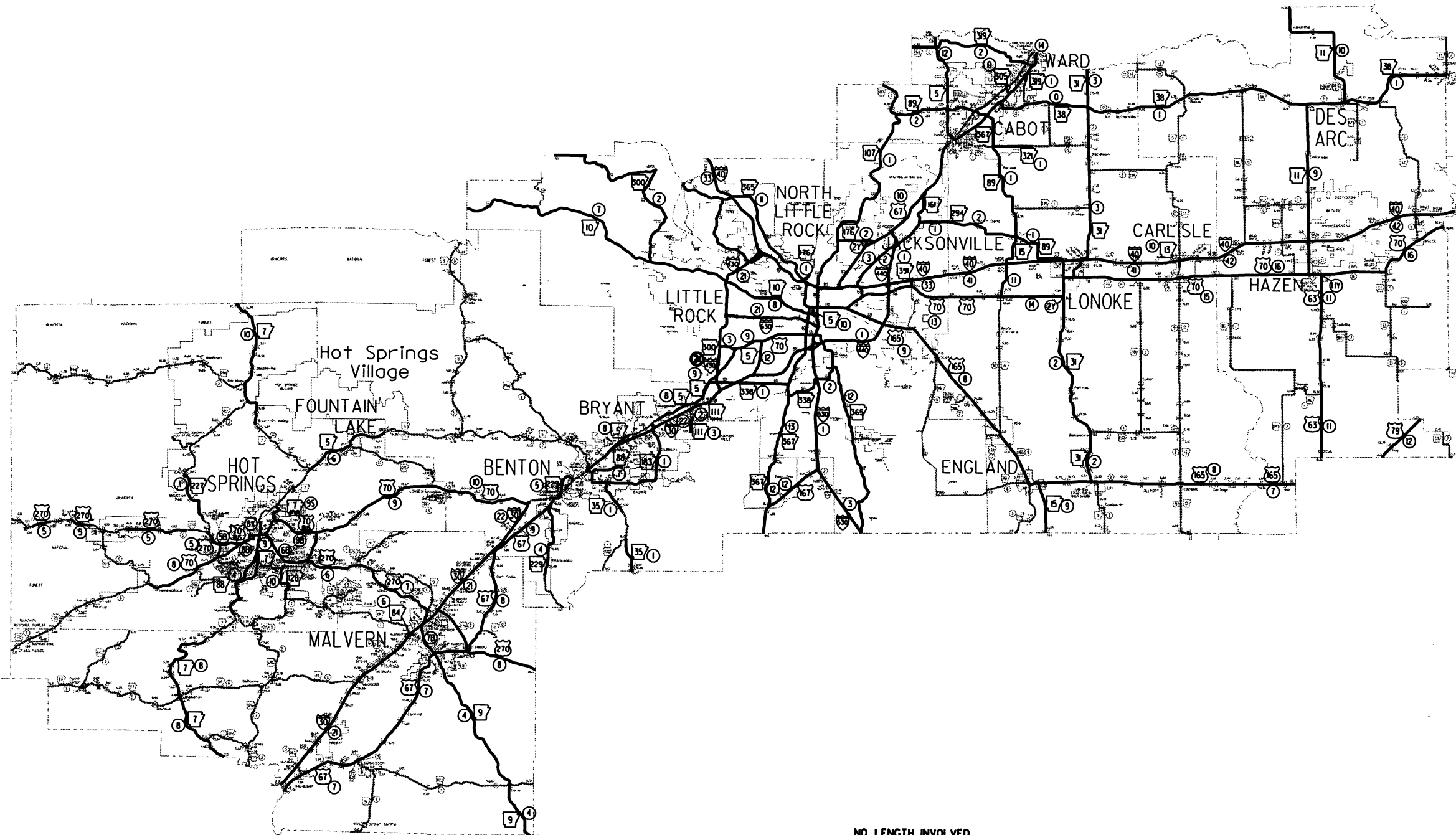
VARIOUS COUNTIES  
FEDERAL AID PROJ. HSIP-0076(I83)

## JOB 061621

NOT TO SCALE



ARK. HWY. DIST. NO. 6



APPROVED



1-3-19

DEPUTY DIRECTOR  
AND CHIEF ENGINEER

NO LENGTH INVOLVED

12/18/2018

R061621.DGN



RAISED PAVEMENT MARKERS (BOX 1 OF 4)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE I (WHITE/RED)	TYPE II (YELLOW/YELLOW)
							EACH	
DISTRICT 6								
GARLAND	2 LANES	5	6	0.00	7.38	7.38		488
GARLAND	2 LANES	7	9	2.29	2.36	0.07		5
GARLAND	5 LANES - TURN LANE/PAINTED MEDIAN	7	9	3.94	10.24	6.30	832	832
GARLAND	3 LANES - PASSING LANE	7	9	10.24	10.71	0.47	32	32
GARLAND	4 LANES - UNDIVIDED	7	9	10.71	11.11	0.40	53	27
GARLAND	4 LANES - DIVIDED	7	9	11.11	11.35	0.24	32	
GARLAND	2 LANES	7	9S	0.00	1.58	1.58		105
GARLAND	3 LANES - TURN LANE	7	10	6.05	7.18	1.13	50	150
GARLAND	3 LANES - PASSING LANE	7	10	7.18	7.93	0.75	50	50
GARLAND	2 LANES	7	10	7.93	17.47	9.54	630	630
GARLAND	5 LANES - TURN LANE/PAINTED MEDIAN	70	8	8.94	15.24	6.31	833	833
GARLAND	5 LANES - TURN LANE/PAINTED MEDIAN	70	8B	0.00	2.10	2.10	278	278
GARLAND	4 LANES - UNDIVIDED	70	8B	2.10	3.04	0.94	125	63
GARLAND	2 LANES - PART OF COUPLER	70	8B	3.04	3.67	0.63	84	
GARLAND	4 LANES - DIVIDED	70	8B	3.67	4.18	0.51	68	
GARLAND	2 LANES - PART OF COUPLER	70	8X	0.00	0.63	0.63	84	
GARLAND	4 LANES - DIVIDED	70	9	0.00	2.56	2.56	338	
GARLAND	5 LANES - TURN LANE/PAINTED MEDIAN	70	9	2.56	14.40	11.84	1563	1563
GARLAND	4 LANES - DIVIDED	70	9B	0.00	1.30	1.30	172	
GARLAND	4 LANES - DIVIDED	70	9B	4.63	5.93	1.30	172	
GARLAND	5 LANES - TURN LANE/PAINTED MEDIAN	88	4	0.00	2.92	2.92	386	386
GARLAND	2 LANES	88	4	2.43	2.92	0.49	33	
GARLAND	2 LANES	128	10	0.00	3.02	3.02	200	
GARLAND	5 LANES - TURN LANE/PAINTED MEDIAN	128	10	3.02	3.51	0.49	65	
GARLAND	2 LANES	128	10	3.51	3.88	0.37	25	
GARLAND	2 LANES	227	1	1.50	6.12	4.62	305	
GARLAND	3 LANES - PASSING LANE	270	5	2.37	3.62	1.25	83	
GARLAND	3 LANES - PASSING LANE	270	5	4.95	6.11	1.16	77	
GARLAND	2 LANES	270	5	6.11	6.91	0.80	53	
GARLAND	3 LANES - PASSING LANE	270	5	10.18	11.11	0.93	62	
GARLAND	2 LANES	270	5	11.11	12.83	1.72	114	
GARLAND	3 LANES - PASSING LANE	270	5	12.83	13.82	0.99	66	
GARLAND	2 LANES	270	5	13.82	15.26	1.44	96	
GARLAND	5 LANES - TURN LANE/PAINTED MEDIAN	270	5	16.90	18.15	1.25	165	
GARLAND	4 LANES - DIVIDED	270	5	18.15	19.12	0.97	129	
GARLAND	5 LANES - TURN LANE/PAINTED MEDIAN	270	5B	0.00	1.89	1.89	224	
GARLAND	4 LANES - DIVIDED	270	6	0.00	4.25	4.25	561	
GARLAND	5 LANES - TURN LANE/PAINTED MEDIAN	270	6	4.25	8.91	4.66	616	
GARLAND	5 LANES - TURN LANE/PAINTED MEDIAN	270	6B	0.00	4.65	4.65	614	
HOT SPRING	2 LANES	7	8	0.00	10.69	10.69	706	
HOT SPRING	3 LANES - PASSING LANE	7	8	10.69	12.39	1.70	113	
HOT SPRING	2 LANES	7	8	12.39	14.32	1.93	128	
HOT SPRING	2 LANES	9	4	0.00	16.85	16.85	1113	
HOT SPRING	3 LANES - TURN LANE	9	4	16.85	17.50	0.65	86	
HOT SPRING	4 LANES - DIVIDED	30	21	80.57	105.46	24.89	3286	
HOT SPRING	ENTRANCE RAMPS (12)	30	21				456	
HOT SPRING	EXIT RAMPS (12)	30	21				1260	
HOT SPRING	2 LANES	67	7	0.00	0.62	0.62	41	
HOT SPRING	4 LANES - UNDIVIDED	67	7	0.62	1.93	1.31	173	
HOT SPRING	2 LANES	67	7	1.93	10.48	8.55	565	
HOT SPRING	4 LANES - UNDIVIDED	67	7	10.48	11.81	1.33	88	
HOT SPRING	2 LANES	67	7	11.81	18.05	6.24	412	
HOT SPRING	3 LANES - TURN LANE	67	8	0.00	11.13	11.13	1470	
HOT SPRING	2 LANES	67	8	2.78	3.60	0.82	55	
HOT SPRING	4 LANES - UNDIVIDED	67	8	3.60	5.75	2.15	142	
HOT SPRING	2 LANES	67	8	5.75	9.64	3.89	257	
HOT SPRING	3 LANES - TURN LANE	67	8	9.64	10.15	0.51	68	
HOT SPRING	2 LANES	67	8	10.15	11.31	1.16	77	
HOT SPRING	2 LANES	84	6	20.94	21.23	0.29	20	
HOT SPRING	5 LANES - TURN LANE/PAINTED MEDIAN	270	7	0.00	8.34	8.34	1101	
HOT SPRING	5 LANES - TURN LANE/PAINTED MEDIAN	270	7B	0.00	2.03	2.03	268	
HOT SPRING	3 LANES - UNDIVIDED	270	7B	2.03	2.63	0.60	40	
HOT SPRING	3 LANES - TURN LANE	270	8	0.00	0.48	0.48	64	
HOT SPRING	2 LANES	270	8	0.48	10.21	9.73	643	
LONOKE	4 LANES - UNDIVIDED	5	12	0.00	0.45	0.45	60	
LONOKE	4 LANES - DIVIDED	5	12	0.45	3.00	2.55	337	
LONOKE	2 LANES	5	12	3.00	9.04	6.04	399	
LONOKE	3 LANES - TURN LANE	13	10	0.91	1.57	0.66	88	
LONOKE	2 LANES	15	9	0.00	4.07	4.07	269	
LONOKE	2 LANES	15	11	0.00	5.41	5.41	358	
LONOKE	2 LANES	31	2	4.48	20.79	16.31	1077	
LONOKE	2 LANES	31	2Y	0.00	0.12	0.12	8	
LONOKE	2 LANES	31	3	0.00	17.32	17.32	1144	
LONOKE	4 LANES - UNDIVIDED	38	0	0.00	0.67	0.67	89	
LONOKE	4 LANES - DIVIDED	38	0	0.67	11.76	11.09	732	
LONOKE	ENTRANCE RAMPS (12)	40	41	163.74	185.47	21.73	2869	
LONOKE	EXIT RAMPS (12)	40	41				456	
LONOKE	4 LANES - DIVIDED	67	11	0.40	10.13	9.73	1260	
LONOKE	ENTRANCE RAMPS (8)	67	11				1285	
LONOKE	EXIT RAMPS (8)	67	11				304	
LONOKE	2 LANES	70	14	0.00	10.74	10.74	840	
LONOKE	3 LANES - TURN LANE	70	14	10.74	11.42	0.68	709	
LONOKE	3 LANES - TURN LANE	70	15	0.00	0.33	0.33	90	
<b>SUBTOTALS (BOX 1 OF 4):</b>							<b>22461</b>	<b>20777</b>

NOTE: SEE PM-2 (ONLY ADDED 19 RAISED PAVEMENT MARKERS FOR THE 1520' LEADING UP TO AN EXIT RAMP. THE OTHER 19 ARE INCLUDED IN MAIN LANE CALCULATIONS).

RAISED PAVEMENT MARKERS PER ENTRANCE RAMP = 38 EACH

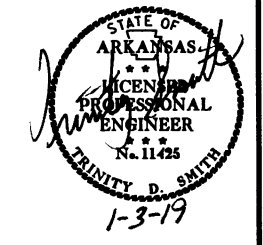
RAISED PAVEMENT MARKERS PER EXIT RAMP = 105 EACH

\*\* QUANTITIES ESTIMATED.

SEE SECTION 104.03 OF THE STD. SPECS.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		3	7
				JOB NO. 061621				

2 QUANTITIES



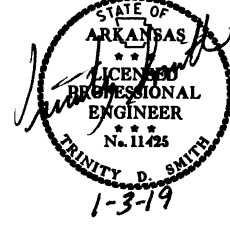
RAISED PAVEMENT MARKERS (BOX 2 OF 4)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II (WHITE/RED)	TYPE II (YELLOW/YELLOW)
							EACH	
DISTRICT 6 (CONTINUED)								
LONOKE	2 LANES	70	15	0.33	8.46	8.13		537
LONOKE	3 LANES - TURN LANE	70	15	8.46	8.97	0.51		68
LONOKE	2 LANES	70	15	8.97	11.54	2.57		170
LONOKE	2 LANES	89	1	0.00	14.58	14.58		963
LONOKE	3 LANES - TURN LANE	89	1	14.58	17.14	2.56		338
LONOKE	4 LANES - UNDIVIDED	89	1	17.14	17.60	0.46	61	31
LONOKE	3 LANES - TURN LANE	89	1	17.60	18.40	0.80		106
LONOKE	4 LANES - UNDIVIDED	89	1	18.40	18.77	0.37	49	25
LONOKE	3 LANES - TURN LANE	89	1	18.77	19.53	0.76		101
LONOKE	2 LANES	89	1	19.53	20.80	1.27		84
LONOKE	2 LANES	165	8	0.00	6.21	6.21		410
LONOKE	3 LANES - TURN LANE	165	8	6.21	6.62	0.41		55
LONOKE	2 LANES	165	8	6.62	17.31	10.69		706
LONOKE	3 LANES - TURN LANE	165	8	17.31	19.70	2.39		316
LONOKE	2 LANES	165	8	19.70	22.90	3.20		212
LONOKE	3 LANES - TURN LANE	165	8	22.90	23.50	0.60		80
LONOKE	2 LANES	165	8	23.50	31.46	7.96		526
LONOKE	2 LANES	294	2	0.00	5.92	5.92		391
LONOKE	2 LANES	305	0	0.00	0.73	0.73		49
LONOKE	2 LANES	319	1	1.78	2.98	1.20		80
LONOKE	2 LANES	319	2	0.00	10.85	10.85		717
LONOKE	2 LANES	321	1	3.24	5.76	2.52		167
LONOKE	3 LANES - TURN LANE	367	14	0.00	9.85	9.85		1301
LONOKE	2 LANES	367	14	0.25	3.09	2.84		188
LONOKE	3 LANES - TURN LANE	367	14	3.09	3.40	0.31		41
LONOKE	2 LANES	367	14	3.40	9.85	6.45		426
PRAIRIE	2 LANES	11	9	0.00	10.94	10.94		723
PRAIRIE	2 LANES	11	10	0.00	11.34	11.34		749
PRAIRIE	2 LANES	38	1	0.01	26.81	26.80		1769
PRAIRIE	4 LANES - DIVIDED	40	42	185.47	205.25	19.78	2611	
PRAIRIE	ENTRANCE RAMP (6)	40	42				228	
PRAIRIE	EXIT RAMP (6)	40	42				630	
PRAIRIE	2 LANES	63	11	0.00	2.69	2.69		178
PRAIRIE	2 LANES	63	11	3.52	5.45	1.93		128
PRAIRIE	3 LANES - PASSING LANE	63	11	5.45	8.35	2.90	192	
PRAIRIE	2 LANES	63	11	8.35	14.86	6.51		430
PRAIRIE	3 LANES - PASSING LANE	63	11	14.86	17.77	2.91	193	
PRAIRIE	2 LANES	63	11	17.77	18.32	0.55		37
PRAIRIE	2 LANES	63	11Y	0.00	0.25	0.25		17
PRAIRIE	2 LANES	70	16	0.00	19.83	19.83		1309
PRAIRIE	2 LANES	79	12	0.00	4.97	4.97		329
PRAIRIE	2 LANES	165	7	0.00	2.86	2.86		189
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	5	9	0.00	1.47	1.47	195	
PULASKI	4 LANES - UNDIVIDED	5	9	1.47	2.39	0.92	122	61
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	5	9	2.39	3.56	1.17	155	
PULASKI	2 LANES	5	9	3.56	5.59	2.03	134	
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	5	9	5.59	8.25	2.66	352	
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	5	10	0.00	0.44	0.44	59	
PULASKI	3 LANES - PASSING LANE	10	7	0.00	2.81	2.81	186	
PULASKI	2 LANES	10	7	2.81	6.80	3.99	264	
PULASKI	3 LANES - PASSING LANE	10	7	6.80	7.70	0.90	60	
PULASKI	2 LANES	10	7	7.70	12.31	4.61	305	
PULASKI	3 LANES - PASSING LANE	10	7	12.31	13.26	0.95	63	
PULASKI	2 LANES	10	7	13.26	16.41	3.15	208	
PULASKI	2 LANES	10	8	0.00	0.51	0.51	34	
PULASKI	3 LANES - TURN LANE	10	8	0.51	1.18	0.67	89	
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	10	8	1.18	10.74	9.56	1262	
PULASKI	4 LANES - UNDIVIDED	10	8	10.74	12.95	2.21	292	146
PULASKI	4 LANES - DIVIDED	10	8	12.95	13.27	0.32	43	
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	10	8	13.27	13.87	0.60	80	
PULASKI	4 LANES - UNDIVIDED	10	8	13.87	14.88	1.01	134	
PULASKI	4 LANES - DIVIDED	10	8	14.88	16.59	1.71	226	
PULASKI	6 LANES - DIVIDED	30	23	126.30	142.86	16.56	4372	
PULASKI	2 LANES - EASTBOUND SERVICE ROAD	30	23	126.30	135.24	8.94	591	
PULASKI	2 LANES - EASTBOUND SERVICE ROAD	30	23	138.41	140.37	1.96	130	
PULASKI	2 LANES - EASTBOUND SERVICE ROAD	30	23	140.91	142.86	1.95	129	
PULASKI	2 LANES - WESTBOUND SERVICE ROAD	30	23	126.30	135.24	8.94	591	
PULASKI	2 LANES - WESTBOUND SERVICE ROAD	30	23	138.41	139.50	1.09	72	
PULASKI	2 LANES - WESTBOUND SERVICE ROAD	30	23	139.75	140.45	0.70	47	
PULASKI	2 LANES - WESTBOUND SERVICE ROAD	30	23	140.90	141.54	0.64	43	
PULASKI	2 LANES - WESTBOUND SERVICE ROAD	30	23	141.82	142.30	0.48	32	
PULASKI	ENTRANCE RAMP (31)	30	23				1178	
PULASKI	EXIT RAMP (33)	30	23				3465	
PULASKI	6 LANES - DIVIDED	40	33	138.25	153.00	14.75	3894	
PULASKI	4 LANES - DIVIDED	40	33	153.00	153.53	0.53	70	
PULASKI	8 LANES - DIVIDED	40	33	153.53	154.60	1.07	424	
PULASKI	6 LANES - DIVIDED	40	33	154.60	156.21	1.61	426	
PULASKI	4 LANES - DIVIDED	40	33	156.21	163.74	7.53	994	
PULASKI	ENTRANCE RAMP (26)	40	33				988	
PULASKI	EXIT RAMP (26)	40	33				2730	
PULASKI	6 LANES - DIVIDED	67	10	0.00	9.35	9.35	2469	
PULASKI	4 LANES - DIVIDED	67	10	9.35	11.20	1.85	245	
<b>SUBTOTALS (BOX 2 OF 4):</b>							<b>30083</b>	<b>18051</b>

NOTE:  
 SEE PM-2 (ONLY ADDED 19 RAISED PAVEMENT MARKERS FOR THE 1520' LEADING UP TO AN EXIT RAMP. THE OTHER 19 ARE INCLUDED IN MAIN LANE CALCULATIONS).  
 RAISED PAVEMENT MARKERS PER ENTRANCE RAMP = 38 EACH  
 RAISED PAVEMENT MARKERS PER EXIT RAMP = 105 EACH  
 \*\* QUANTITIES ESTIMATED.  
 SEE SECTION 104.03 OF THE STD. SPECS.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061621		4	7

2 QUANTITIES



RAISED PAVEMENT MARKERS (BOX 3 OF 4)

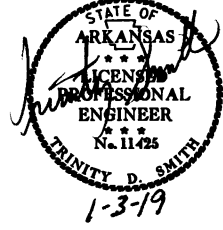
COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS	
							TYPE II (WHIT/RED)	TYPE II (YELLOW/YELLOW)
							EACH	
DISTRICT 6 (CONTINUED)								
PULASKI	2 LANES - NORTHBOUND SERVICE ROAD	67	10	0.95	6.05	5.10	337	
PULASKI	2 LANES - NORTHBOUND SERVICE ROAD	67	10	8.95	11.20	2.25	149	
PULASKI	2 LANES - SOUTHBOUND SERVICE ROAD	67	10	0.90	6.11	5.21	344	
PULASKI	2 LANES - SOUTHBOUND SERVICE ROAD	67	10	9.07	11.20	2.13	141	
PULASKI	ENTRANCE RAMPS (18)	67	10				684	
PULASKI	EXIT RAMPS (19)	67	10				1995	
PULASKI	4 LANES - DIVIDED	70	12	0.00	2.30	2.30	304	
PULASKI	6 LANES - DIVIDED	70	12	2.30	2.95	0.65	172	
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	70	12	2.95	3.67	0.62	82	82
PULASKI	4 LANES - UNDIVIDED	70	12	3.57	7.64	4.07	538	269
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	70	12	7.64	8.92	1.28	169	169
PULASKI	4 LANES - UNDIVIDED	70	13	0.00	0.67	0.67	89	45
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	70	13	0.67	3.03	2.36	312	312
PULASKI	4 LANES - UNDIVIDED	70	13	3.03	4.53	1.50	198	99
PULASKI	2 LANES	70	13	4.53	11.97	7.44	492	492
PULASKI	2 LANES	89	2	0.00	4.32	4.32	286	286
PULASKI	4 LANES - DIVIDED	100	0	0.00	6.58	6.58	869	869
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	100	0	6.58	7.19	0.61	81	81
PULASKI	4 LANES - UNDIVIDED	100	0	7.19	7.65	0.46	61	31
PULASKI	2 LANES	100	0	7.65	8.70	1.05	70	70
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	107	1	0.00	10.26	10.26	1355	1355
PULASKI	2 LANES	107	1	10.26	21.86	11.60	766	766
PULASKI	2 LANES	111	2	0.68	1.24	0.56	37	37
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	161	3	0.00	1.50	1.50	198	198
PULASKI	2 LANES	161	3	1.50	5.52	4.02	266	266
PULASKI	3 LANES - TURN LANE	161	3	5.52	5.91	0.39	52	52
PULASKI	2 LANES	161	3	5.91	9.47	3.56	235	235
PULASKI	4 LANES - UNDIVIDED	161	3	9.47	10.41	0.94	63	63
PULASKI	3 LANES - TURN LANE	161	3	10.41	11.22	0.81	107	107
PULASKI	2 LANES	165	9	0.00	3.29	3.29	218	218
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	176	1	0.00	0.98	0.98	130	130
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	176	2	0.00	3.02	3.02	399	399
PULASKI	3 LANES - TURN LANE	176	2Y	0.00	0.40	0.40	53	53
PULASKI	2 LANES	294	1	0.00	2.30	2.30	152	152
PULASKI	2 LANES	300	2	0.00	20.05	20.05	1324	1324
PULASKI	2 LANES	300	3	0.00	1.05	1.05	70	70
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	300	3	1.05	1.49	0.44	59	59
PULASKI	2 LANES	338	1	0.00	1.86	1.86	123	123
PULASKI	4 LANES - TURN LANE/PAINTED MEDIAN	338	1	1.86	5.75	3.89	514	514
PULASKI	2 LANES	338	1	5.75	6.39	0.64	43	43
PULASKI	2 LANES	338	2	0.00	5.08	5.08	336	336
PULASKI	3 LANES - PASSING LANE	365	11	0.00	1.34	1.34	89	89
PULASKI	2 LANES	365	11	1.34	1.63	0.29	20	20
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	365	11	1.63	3.92	2.29	152	152
PULASKI	2 LANES	365	11	3.92	4.91	0.99	131	131
PULASKI	3 LANES - TURN LANE	365	11	4.91	13.25	8.34	551	551
PULASKI	3 LANES - TURN LANE	365	11	13.25	15.34	2.09	276	276
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	365	11	15.34	16.94	1.60	212	212
PULASKI	2 LANES	365	11	16.94	17.31	0.37	25	25
PULASKI	4 LANES - UNDIVIDED	365	12	0.00	1.15	1.15	152	152
PULASKI	2 LANES	365	12	1.15	1.50	0.35	24	24
PULASKI	4 LANES - UNDIVIDED	365	12	1.50	1.95	0.45	30	30
PULASKI	2 LANES	365	12	1.95	18.91	16.96	1120	1120
PULASKI	3 LANES - TURN LANE	367	13	0.00	1.81	1.81	120	120
PULASKI	2 LANES	367	13	1.81	2.15	0.34	45	45
PULASKI	2 LANES	367	13	2.15	10.66	8.51	562	562
PULASKI	5 LANES - TURN LANE/PAINTED MEDIAN	391	1	3.56	4.24	0.68	90	90
PULASKI	6 LANES - DIVIDED	430	21	0.00	6.00	6.00	1584	1584
PULASKI	8 LANES - DIVIDED	430	21	6.00	7.95	1.95	773	773
PULASKI	6 LANES - DIVIDED	430	21	7.95	13.75	5.80	1532	1532
PULASKI	ENTRANCE RAMPS (22)	430	21				836	836
PULASKI	EXIT RAMPS (20)	430	21				2100	2100
PULASKI	6 LANES - DIVIDED	440	1	0.00	10.12	10.12	2672	2672
PULASKI	ENTRANCE RAMPS (20)	440	1				760	760
PULASKI	EXIT RAMPS (20)	440	1				2100	2100
PULASKI	6 LANES - DIVIDED	440	2	10.12	14.31	4.20	1109	1109
PULASKI	ENTRANCE RAMPS (4)	440	2				152	152
PULASKI	EXIT RAMPS (4)	440	2				420	420
PULASKI	4 LANES - DIVIDED	530	1	0.00	9.20	9.20	1215	1215
PULASKI	ENTRANCE RAMPS (8)	530	1				304	304
PULASKI	EXIT RAMPS (8)	530	1				840	840
PULASKI	4 LANES - DIVIDED	530	3	12.50	16.29	3.80	502	502
PULASKI	ENTRANCE RAMPS (2)	530	3				76	76
PULASKI	EXIT RAMPS (2)	530	3				210	210
PULASKI	8 LANES - DIVIDED	630	21	0.00	0.95	0.95	377	377
PULASKI	6 LANES - DIVIDED	630	21	0.95	4.70	3.75	990	990
PULASKI	8 LANES - DIVIDED	630	21	6.86	7.87	1.01	400	400
PULASKI	ENTRANCE RAMPS (22)	630	21				836	836
PULASKI	EXIT RAMPS (22)	630	21				2310	2310
PULASKI	2 LANES - EASTBOUND SERVICE ROAD	630	21	0.83	1.61	0.78	52	52
PULASKI	2 LANES - EASTBOUND SERVICE ROAD	630	21	1.98	2.20	0.22	15	15
PULASKI	2 LANES - WESTBOUND SERVICE ROAD	630	21	0.85	1.63	0.78	52	52
SALINE	4 LANES - DIVIDED	5	8	0.00	0.47	0.47	63	63
<b>SUBTOTALS (BOX 3 OF 4):</b>							<b>32335</b>	<b>11959</b>

\* NOTE:  
 SEE PM-2 (ONLY ADDED 19 RAISED PAVEMENT MARKERS FOR THE 1520' LEADING UP TO AN EXIT RAMP. THE OTHER 19 ARE INCLUDED IN MAIN LANE CALCULATIONS).  
 RAISED PAVEMENT MARKERS PER ENTRANCE RAMP = 38 EACH  
 RAISED PAVEMENT MARKERS PER EXIT RAMP = 105 EACH

\*\* QUANTITIES ESTIMATED.  
 SEE SECTION 104.03 OF THE STD. SPECS.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		5	7
				JOB NO.	061621			

2 QUANTITIES



RAISED PAVEMENT MARKERS (BOX 4 OF 4)

COUNTY	DESCRIPTION	ROUTE	SECTION	BEGIN LOG MILE	END LOG MILE	LENGTH	**RAISED PAVEMENT MARKERS		
							TYPE II	TYPE II	
							(WHIT/RED)	(YELLOW/YELLOW)	
DISTRICT 6 (CONTINUED)									
SALINE	2 LANES	5	8	0.47	1.68	1.21		80	
SALINE	2 LANES	5	8	4.01	7.44	3.43		227	
SALINE	4 LANES - DIVIDED	30	22	105.46	110.88	5.42	716		
SALINE	6 LANES - DIVIDED	30	22	116.24	126.30	10.06	2656		
*SALINE	ENTRANCE RAMP (17)	30	22				646		
*SALINE	EXIT RAMP (17)	30	22				1785		
SALINE	2 LANES - EASTBOUND SERVICE ROAD	30	22	116.49	126.30	9.81	648		
SALINE	2 LANES - WESTBOUND SERVICE ROAD	30	22	116.24	126.30	10.06	664		
SALINE	4 LANES - UNDIVIDED	35	1	0.00	0.32	0.32	43	22	
SALINE	3 LANES - TURN LANE	35	1	0.32	2.67	2.35		311	
SALINE	2 LANES	35	1	2.67	5.30	2.63		174	
SALINE	2 LANES	35	1	7.74	10.49	2.75		182	
SALINE	2 LANES	67	9	0.00	7.62	7.62		503	
SALINE	5 LANES - TURN LANE/PAINTED MEDIAN	70	10	0.00	7.66	7.66	1012	1012	
SALINE	5 LANES - TURN LANE/PAINTED MEDIAN	88	7	0.00	0.29	0.29	39	39	
SALINE	2 LANES	88	7	0.29	2.62	2.33		154	
SALINE	2 LANES	111	2	0.00	0.68	0.68		45	
SALINE	2 LANES	111	3	0.00	1.06	1.06		70	
SALINE	4 LANES - UNDIVIDED	167	12	0.00	2.42	2.42	320	160	
SALINE	4 LANES - DIVIDED	167	12	2.42	7.43	5.01	662		
SALINE	2 LANES	183	1	0.00	6.38	6.38		422	
SALINE	5 LANES - TURN LANE/PAINTED MEDIAN	183	1	6.38	8.57	2.19	290	290	
SALINE	4 LANES - DIVIDED	183	1	8.57	8.83	0.26	35		
SALINE	2 LANES	229	4	3.21	9.25	6.04		399	
SALINE	2 LANES	229	5	0.00	2.13	2.13		141	
SALINE	2 LANES	367	12	0.00	4.26	4.26		282	
*SALINE	4 LANES - DIVIDED	530	2	9.20	12.50	3.30	436		
*SALINE	ENTRANCE RAMP (4)	530	2				152		
*SALINE	EXIT RAMP (3)	530	2				315		
<b>SUBTOTALS (BOX 4 OF 4):</b>							<b>10419</b>	<b>4513</b>	
<b>SUBTOTALS (BOX 1 OF 4):</b>							<b>22461</b>	<b>20777</b>	
<b>SUBTOTALS (BOX 2 OF 4):</b>							<b>30083</b>	<b>18051</b>	
<b>SUBTOTALS (BOX 3 OF 4):</b>							<b>32335</b>	<b>11959</b>	
<b>TOTALS:</b>							<b>95298</b>	<b>55300</b>	

\* NOTE: SEE PM-2 (ONLY ADDED 19 RAISED PAVEMENT MARKERS FOR THE 1520' LEADING UP TO AN EXIT RAMP. THE OTHER 19 ARE INCLUDED IN MAIN LANE CALCULATIONS).

RAISED PAVEMENT MARKERS PER ENTRANCE RAMP = 38 EACH

RAISED PAVEMENT MARKERS PER EXIT RAMP = 105 EACH

\*\* QUANTITIES ESTIMATED.

SEE SECTION 104.03 OF THE STD. SPECS.

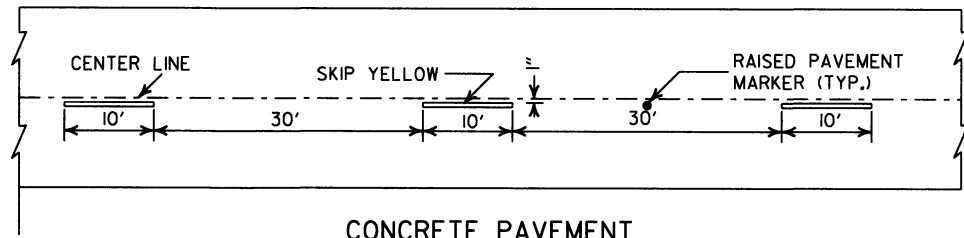
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		6	7
				JOB NO.	061621		6	7

2 QUANTITIES

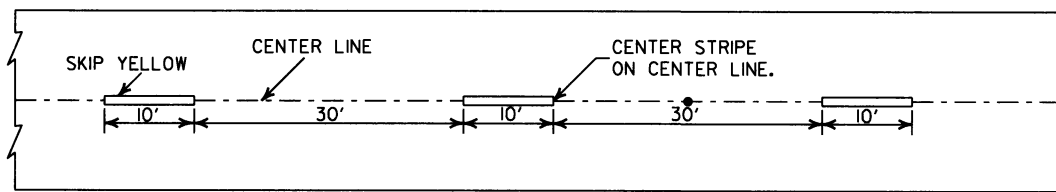






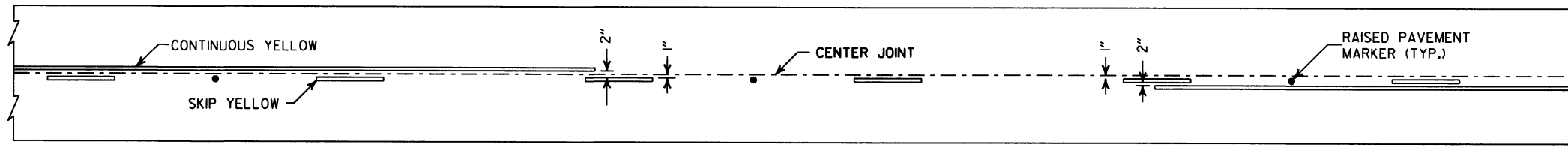


CONCRETE PAVEMENT

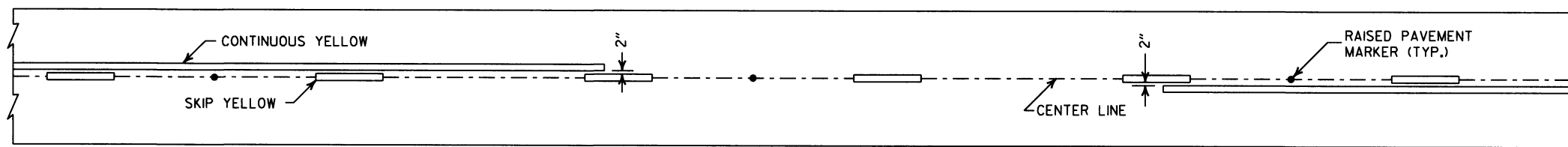


ASPHALT PAVEMENT

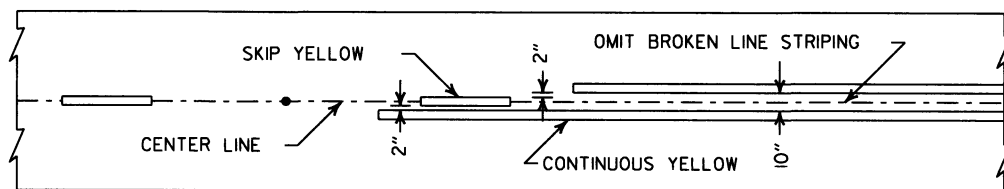
**BROKEN LINE STRIPING**



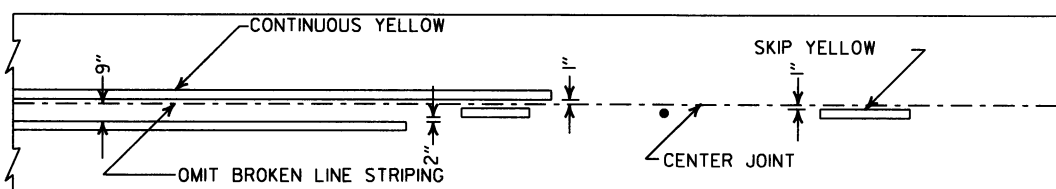
**SOLID LINE STRIPING ON CONCRETE PAVEMENT**



**SOLID LINE STRIPING ON ASPHALT PAVEMENT**

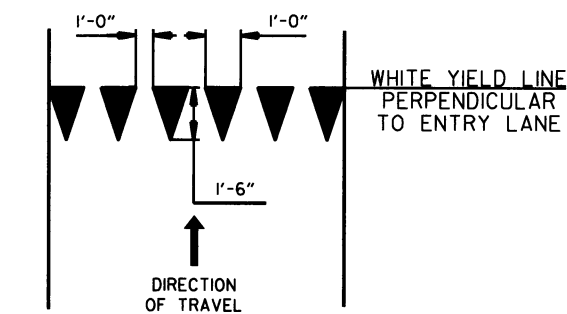


ASPHALT PAVEMENT

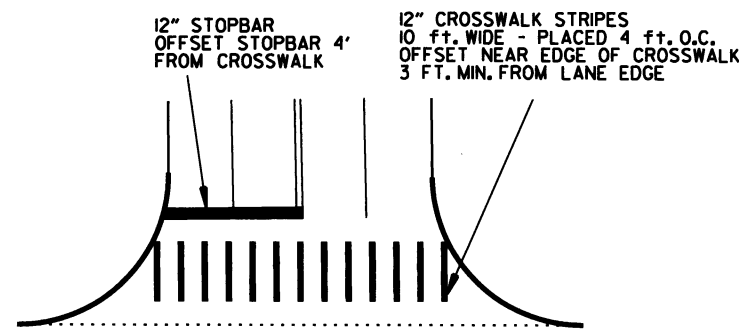


CONCRETE PAVEMENT

**STRIPING AT ADJACENT NO PASSING LANES**



**YIELD LINE DETAIL**

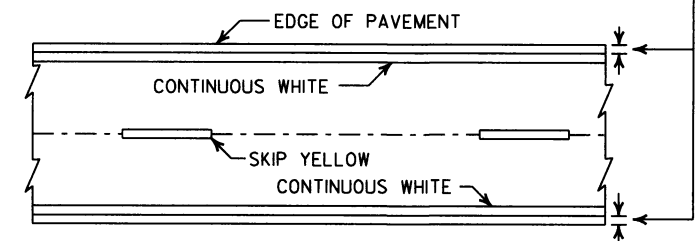


**CROSSWALK AND STOPBAR DETAILS**

NOTES:

1. REFER TO THE STRIPING DETAILS FOR PAVEMENT MARKING LINE WIDTHS.
2. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
3. RAISED PAVEMENT MARKERS SHALL BE PLACED ON AN 80 FEET SPACING UNLESS OTHERWISE SHOWN IN THE PLANS.

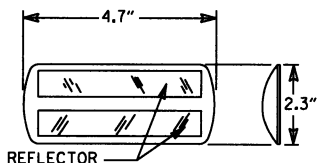
2" FOR ASPHALT OR CONCRETE PAVEMENT  
6" FOR BITUMINOUS SURFACE TREATMENT



**PAVEMENT EDGE LINE MARKING**

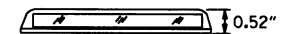
NOTE:  
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.

TYPE II  
RED/CLEAR OR  
YELLOW/YELLOW



PRISMATIC REFLECTOR

NOTE:  
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.



**DETAIL OF STANDARD RAISED PAVEMENT MARKERS**

DATE	REVISION	FILMED
6-1-17	ADDED YIELD LINE DETAIL	
5-12-16	REVISED LINE WIDTHS, SPACING, & NOTES	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REVISED GENERAL NOTES & REMOVED PLOWABLE PAVT MRKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTLS.	
7-02-98	ADDED DETAILS OF STD. RAISED PAVT. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80

ARKANSAS STATE HIGHWAY COMMISSION

**PAVEMENT MARKING DETAILS**

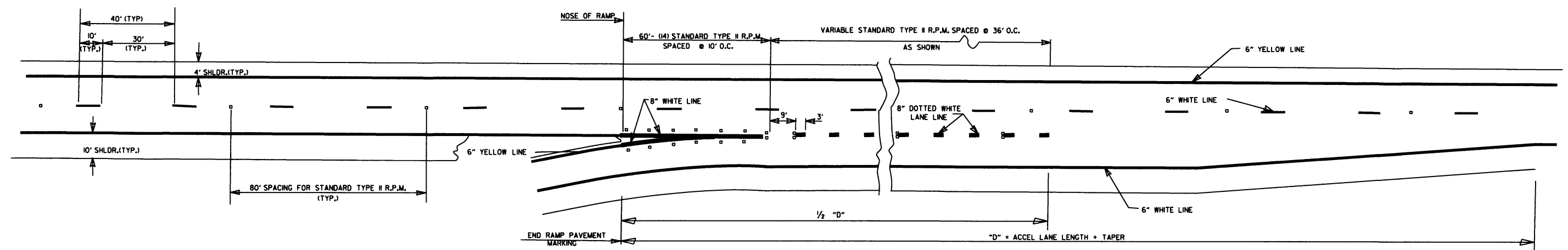
STANDARD DRAWING PM-1



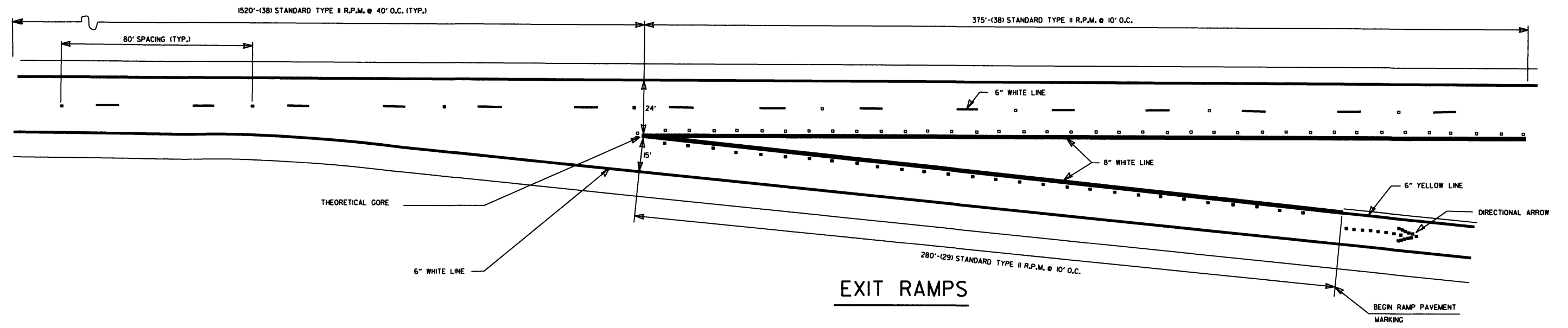
PAVEMENT MARKING QUANTITIES  
(BASED ON 700' ACCEL. LANE + 300' TAPER)

ENTRANCE RAMP  
8" WHITE = 228 LIN. FT.  
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH

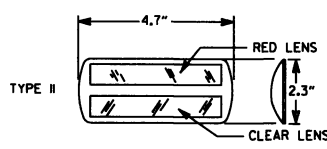
EXIT RAMP  
6" WHITE = 280 LIN. FT.  
8" WHITE = 655 LIN. FT.  
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH  
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 48 EACH  
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH



**ENTRANCE RAMPS**

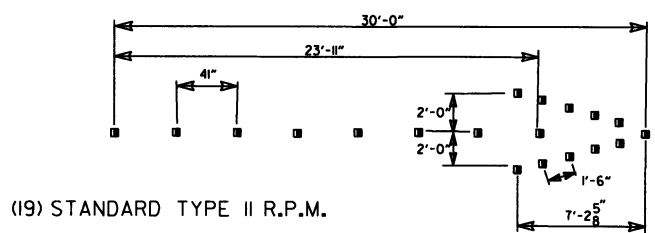


**EXIT RAMPS**



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

NOTE: THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.



(19) STANDARD TYPE II R.P.M. DIRECTIONAL ARROWS


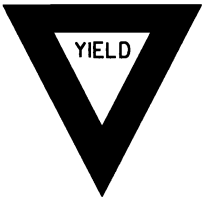







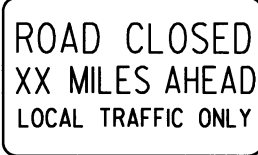
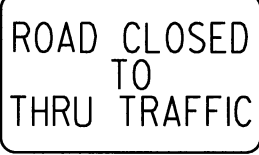








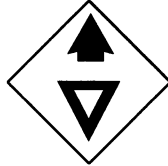
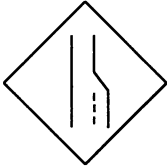













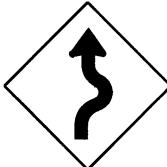



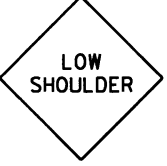

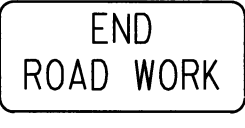
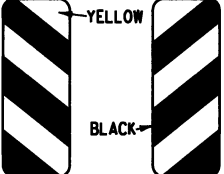


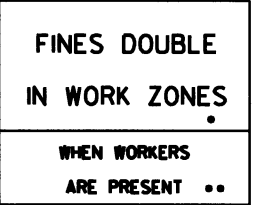
GENERAL NOTES:  
THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE: DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER, REQUESTING APPROVAL FOR SIMILAR PRODUCTS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

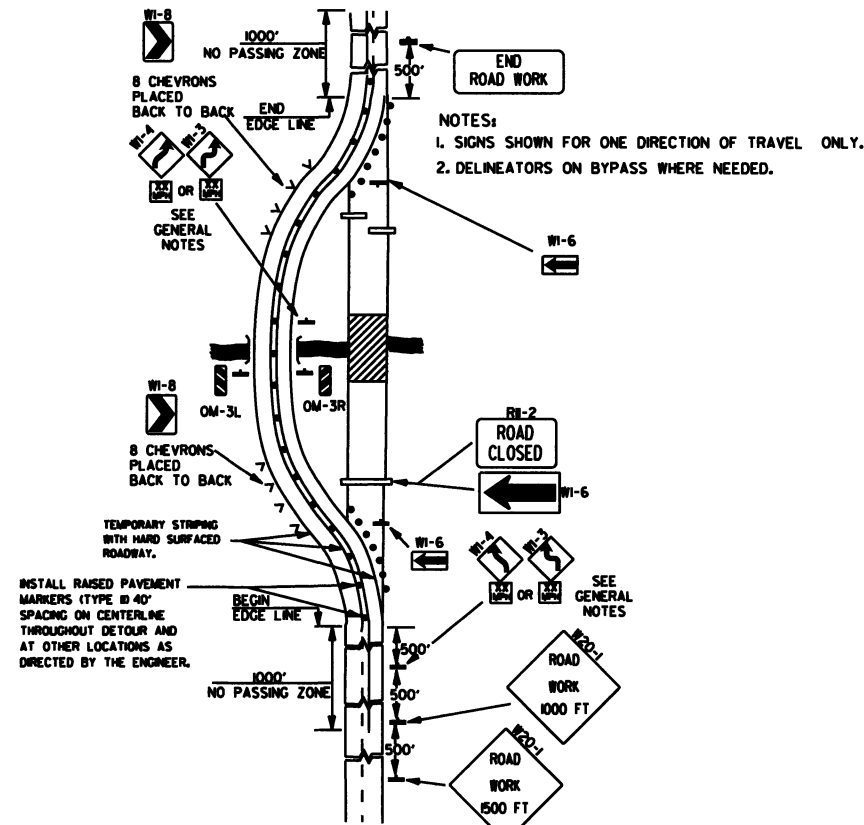
12-8-16	REVISED RAISED PAV'T MARKERS FOR 80' SPACING; REVISED WIDTH OF STRIPING	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
7-26-12	REVISED RPM NOTATION	
12-15-11	REVISED RPMs ACCORDING TO LATEST POLICY	
11-17-10	REMOVED PLOWABLE PAVEMENT MARKERS	
6-3-10	REVISED PER 2009 MUTCD	
11-18-04	REVISED NOTES	
8-22-02	ADDED & REVISED NOTES; REV. ENTRANCE & EXIT RAMPS	
5-18-00	REMOVED HASHMARKS	
7-02-98	CHANGED TYPES TO ROMAN NUMERALS	
4-26-96	ADDED DIMENSIONS & QUANTITIES; REVISED LANE WIDTH ON EXIT RAMP	
2-2-95	PLACED IN USE	2-2-95
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION  
PERMANENT PAVEMENT MARKING ON ACCESS CONTROLLED ROADWAYS  
STANDARD DRAWING PM-2

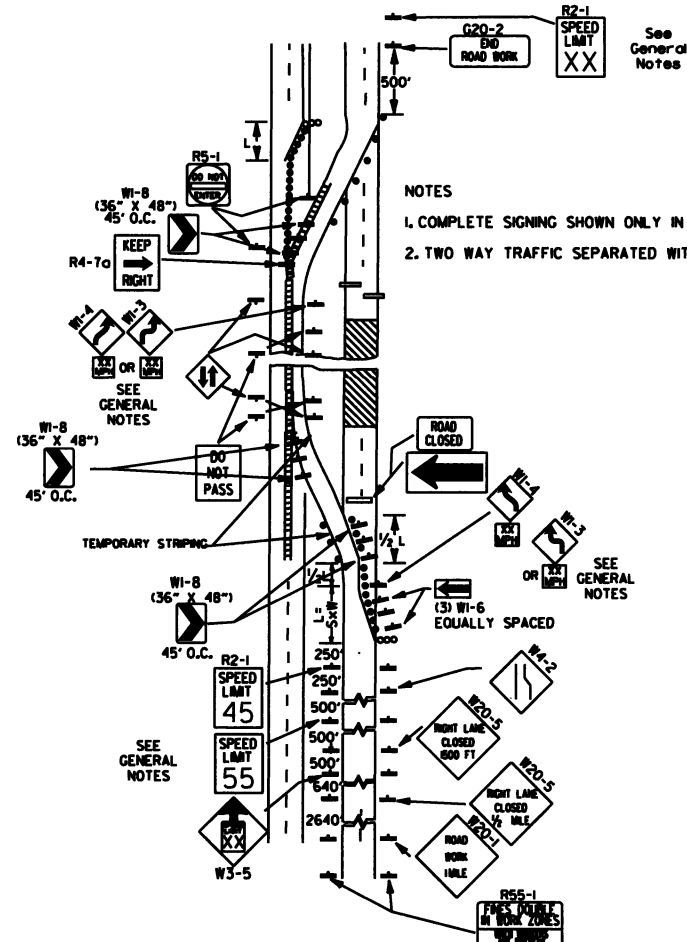
							ADVANCE DISTANCES (XXXX)	
<p>RI-1</p>  <p>STANDARD 30"x30" EXPWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>500 FT 1/2 MILE 1000 FT 3/4 MILE 1500 FT 1 MILE AHEAD</p>	
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>W21-5a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>GENERAL NOTES:</p> <ol style="list-style-type: none"> <li>ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.</li> <li>TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.</li> <li>EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.</li> <li>SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.</li> <li>SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.</li> <li>POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.</li> <li>ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.</li> <li>FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.</li> <li>MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.</li> <li>R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.</li> </ol> <p>* NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 &amp; 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.</p>	
<p>W1-3</p>  <p>STD. 48"x48"</p>	<p>W1-4</p>  <p>STD. 48"x48"</p>	<p>W1-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>W1-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>		
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>		<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>W1-4b</p>  <p>STD. 48"x48"</p>		<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>		<p>R55-1</p>  <p>36"x60"</p> <p>• USE 6" C LETTERS •• USE 4" D LETTERS</p>

4-13-17	DELETED RSP-1 & ADDED W21-5a	
9-2-15	REVISED REDUCED SPEED LIMIT AHEAD SIGNS REVISED ROAD WORK NEXT XX MILES	
12-15-8	REVISED W24-1	
1-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
1-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
1-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
1-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED

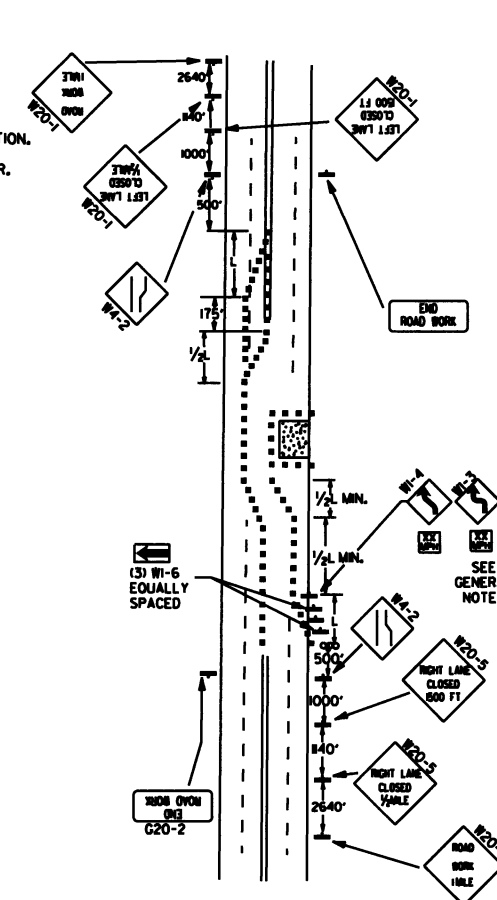
ARKANSAS STATE HIGHWAY COMMISSION  
STANDARD TRAFFIC CONTROLS  
FOR HIGHWAY CONSTRUCTION  
STANDARD DRAWING TC-1



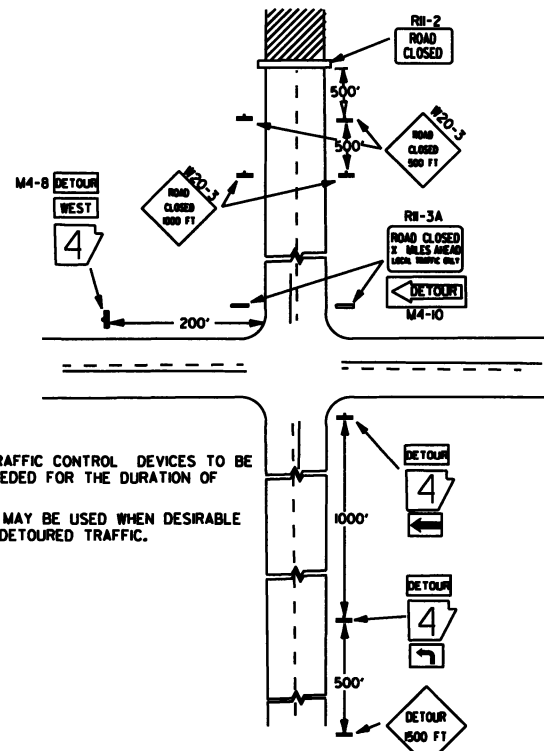
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



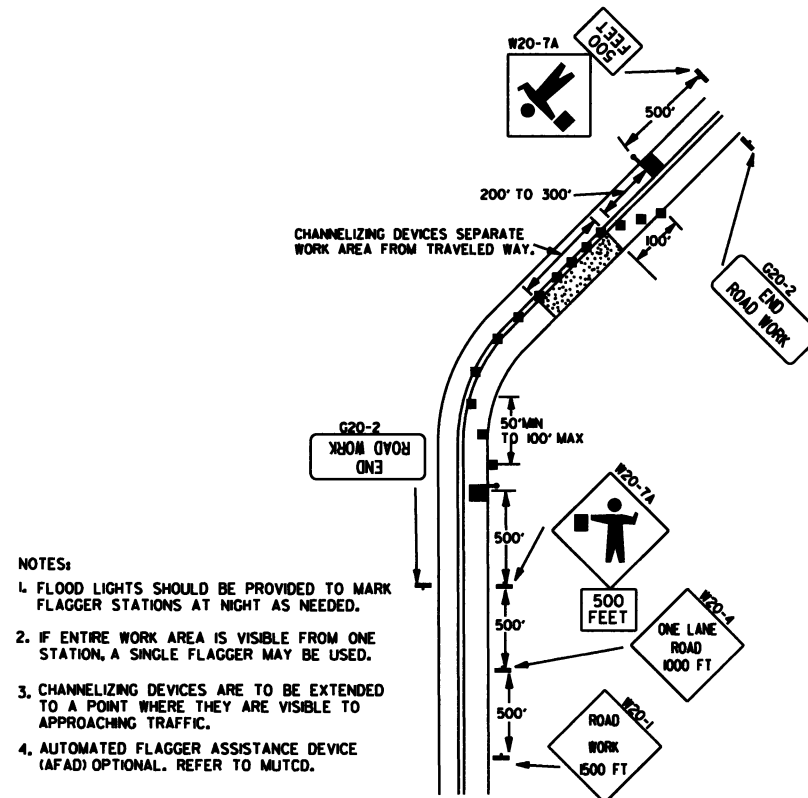
(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



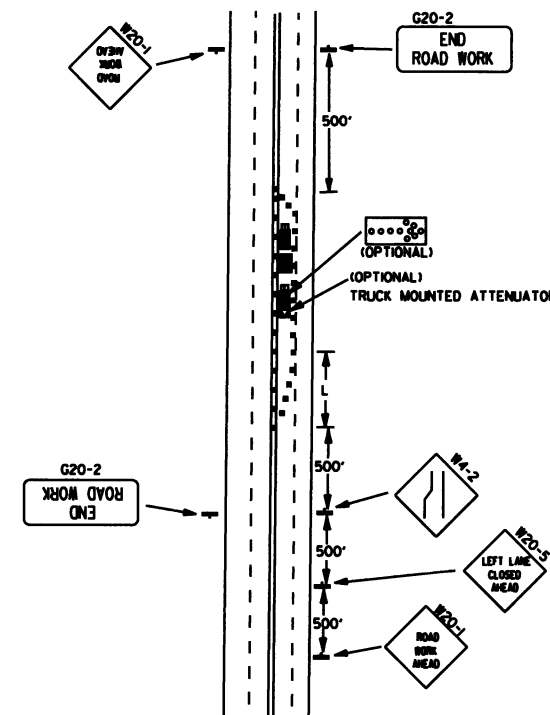
(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.

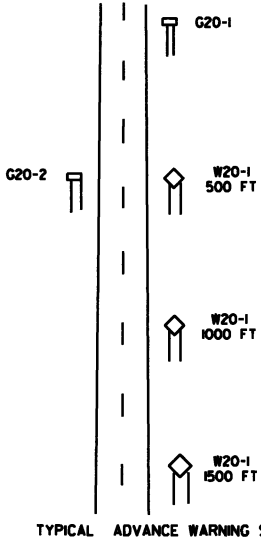
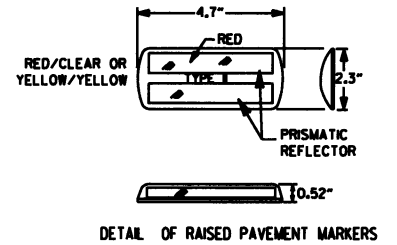


(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

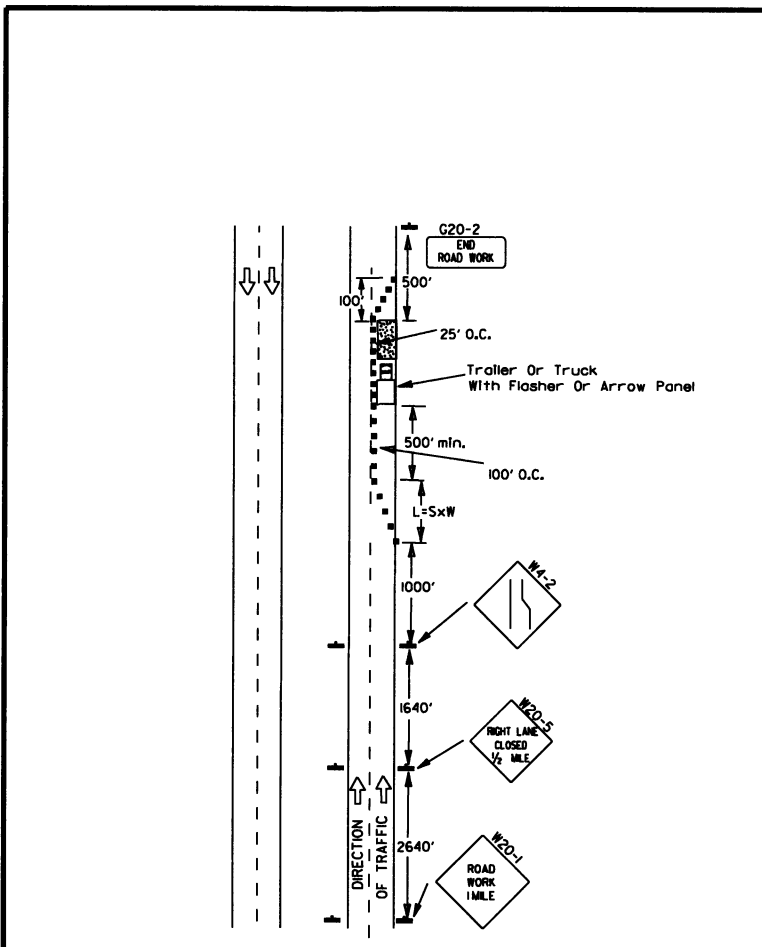
- KEY:
- FLAGGER
  - POSITIVE BARRIER
  - ARROW PANEL (IF REQUIRED)
  - TYPE III BARRICADE
  - CHANNELIZING DEVICE
  - TRAFFIC DRUM
  - RAISED PAVEMENT MARKER



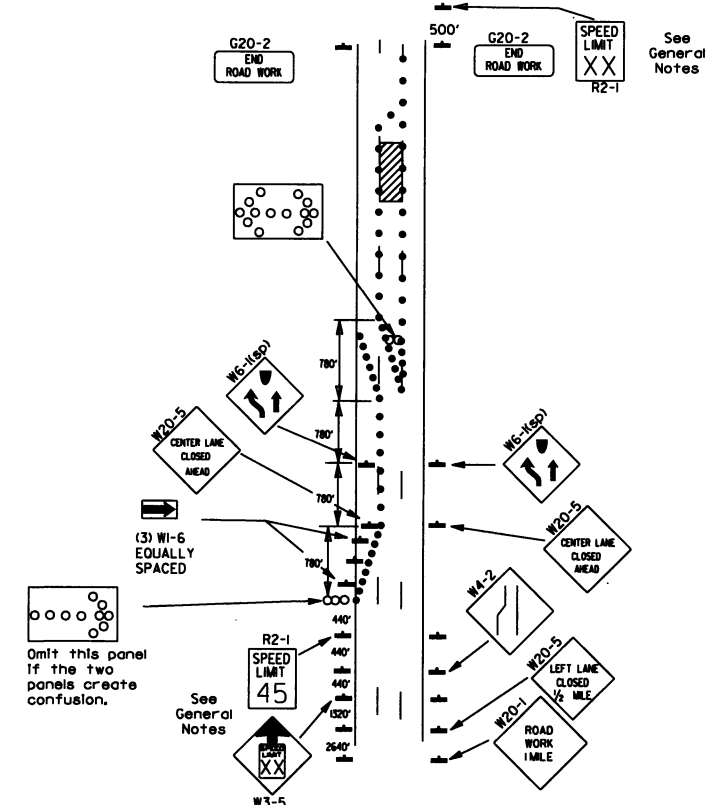
- TAPER FORMULAE:
- $L = SXW$  FOR SPEEDS OF 45MPH OR MORE.
  - $L = \frac{WS^2}{60}$  FOR SPEEDS OF 40MPH OR LESS.
- WHERE:
- L = MINIMUM LENGTH OF TAPER.
  - S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.
  - W = WIDTH OF OFFSET.

- GENERAL NOTES:
- ADVISORY SPEED POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS TO BE DETERMINED AT SITE. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
  - WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-K55 SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-145MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-KXX SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
  - WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-K65 SHALL BE OMITTED. ADDITIONAL R2-155MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-KXX SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
  - THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
  - WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
  - PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
  - TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.
  - DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

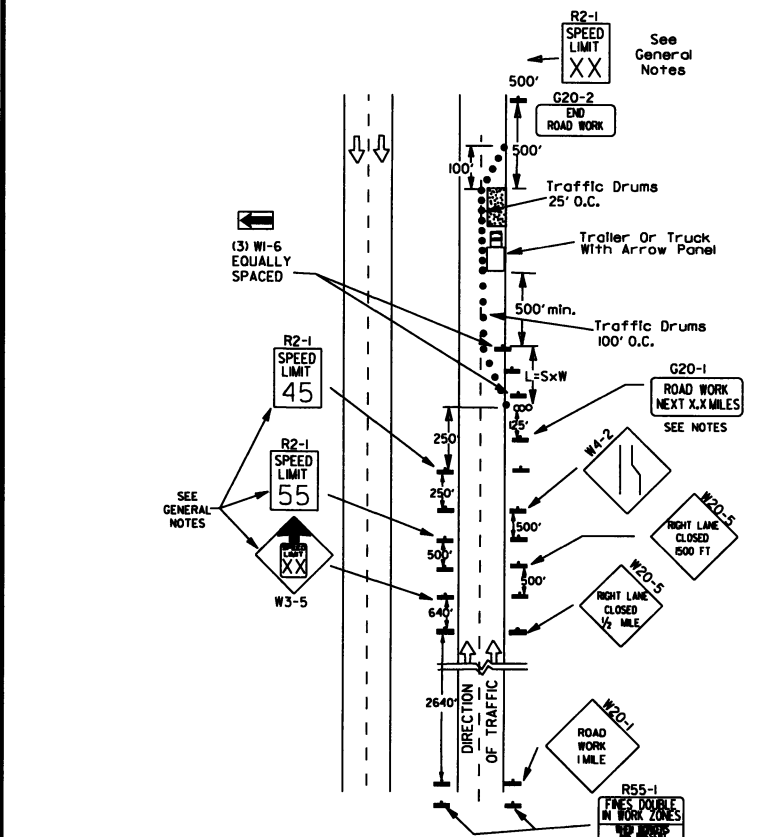
DATE	REVISION	FILED
9-2-95	REVISED NOTE 2, ADDED NOTE 8, REVISED DRAWING (A) & REPLACED R2-5A WITH W3-5	
9-12-93	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-8-80	ADDED (AFAD)	
8-20-08	REVISED SIGN DESIGNATIONS	
8-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (G) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VLMUTCO, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	



(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.



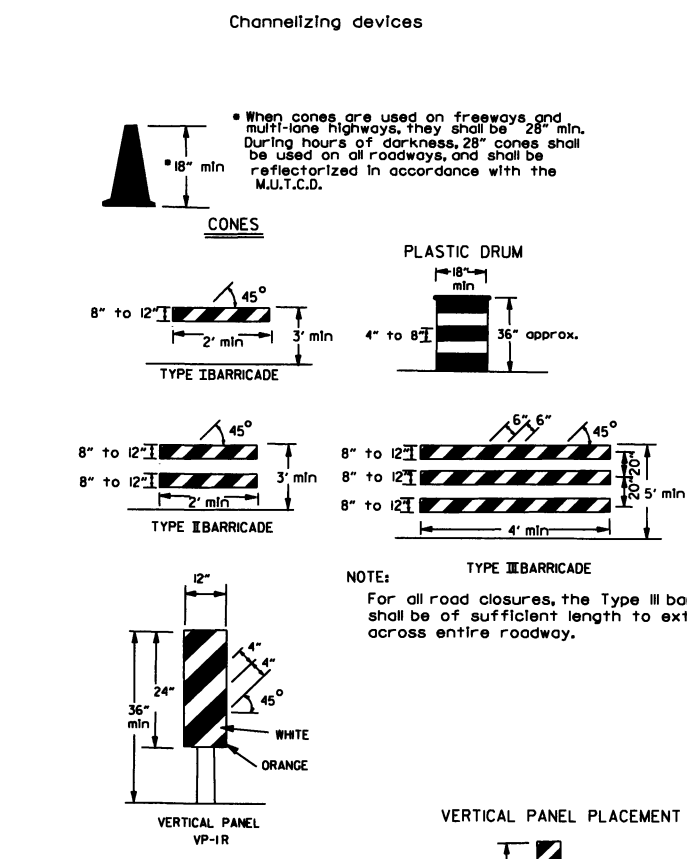
(B) Typical application - 3-lane oneway roadway where center lane is closed.



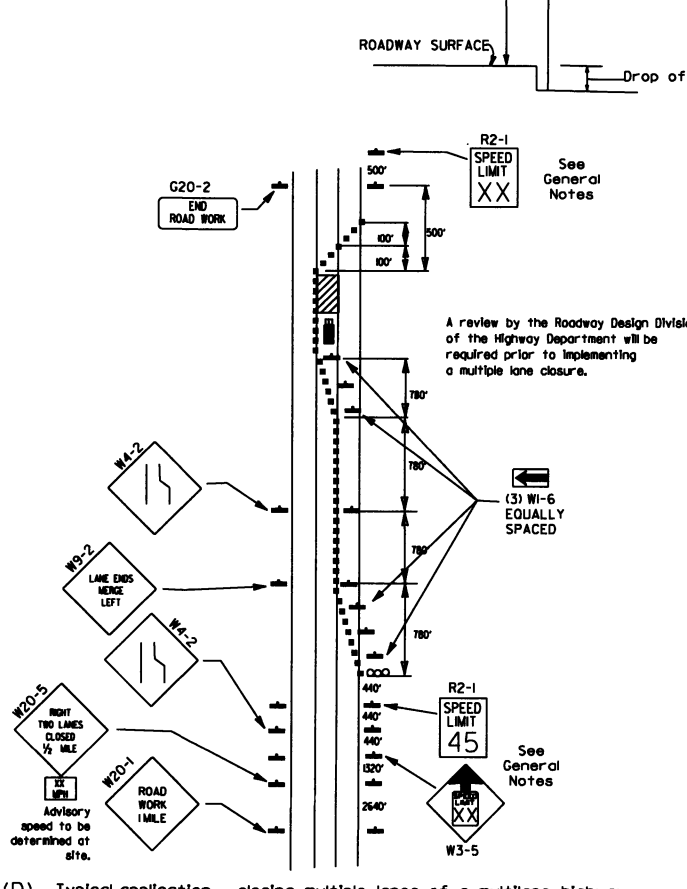
(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.

- KEY:
- Arrow Panel (if Required)
  - Channelizing Device
  - Traffic drum

- GENERAL NOTES:
- A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
  - When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the W3-5 shall be installed at that location. Additional R2-1(45) speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
  - When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(65) shall be omitted. Additional R2-1(55) speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
  - The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
  - Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
  - Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
  - The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 1/2 mile in advance of the job limit. Additional W20-1 (1 MILE) signs are not required in advance of lane closures that begin inside the project limits.
  - Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
  - All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual For Assessing Safety Hardware (MASH).
  - Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

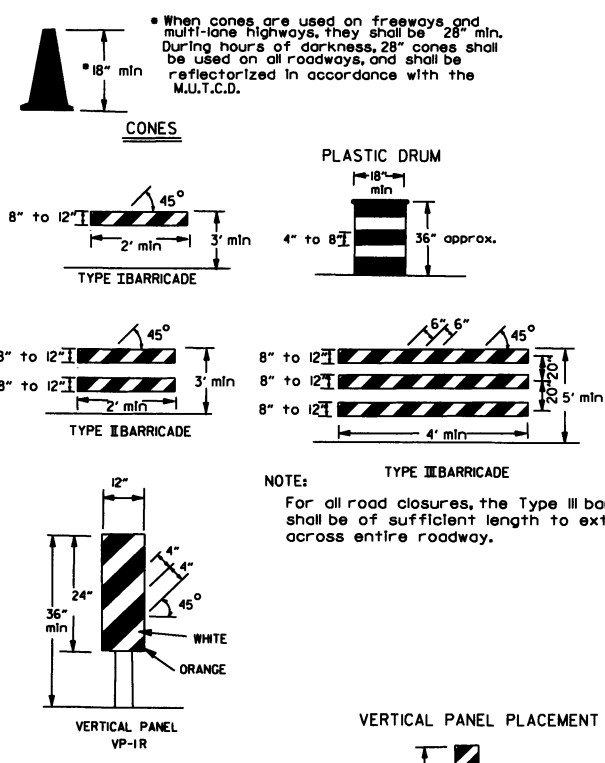


(D) Typical application - closing multiple lanes of a multilane highway.



(D) Typical application - closing multiple lanes of a multilane highway.

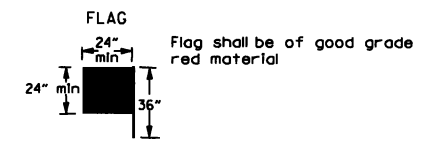
Channelizing devices



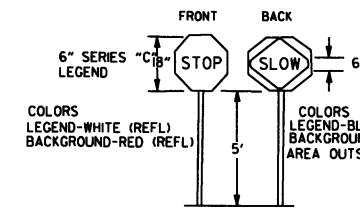
TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	W8-11
1" to 3"	Edge of shoulder	W8-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-1 and vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

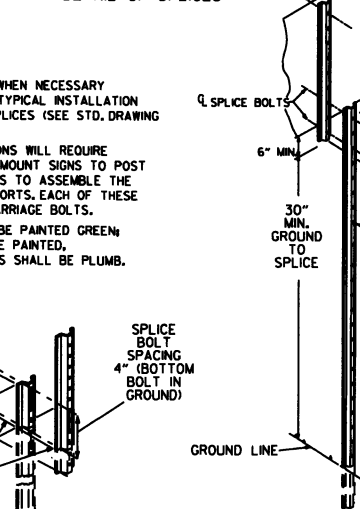
\* When shown on the plans concrete barrier will be used.  
When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



STOP SLOW PADDLE



DETAIL OF SPLICES



- NOTES:
- USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2)
  - NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS.
  - SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.

DATE	REVISION	FILMED
9-2-15	REVISED NOTE 2 & REPLACED R2-5A WITH W3-5	
10-15-09	ADDED REFERENCE TO MASH	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SPI) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	