

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	080444	I 52
						② HAW CREEK STR. & APPRS. (S)		

ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT
CONSTRUCTION PLANS FOR STATE HIGHWAY

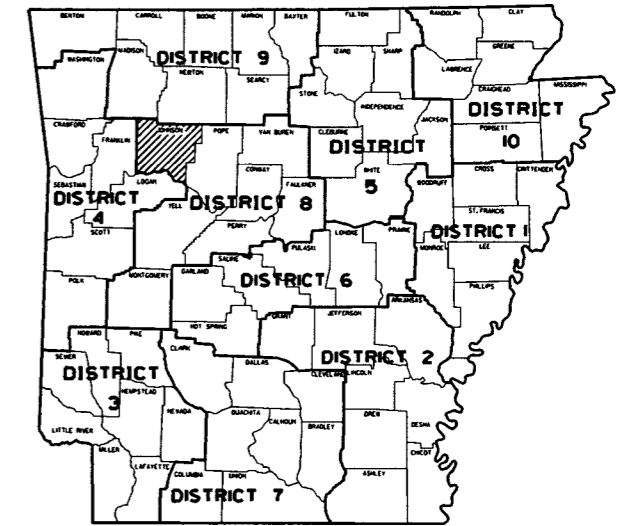
HAW CREEK STR. & APPRS. (S)

JOHNSON COUNTY

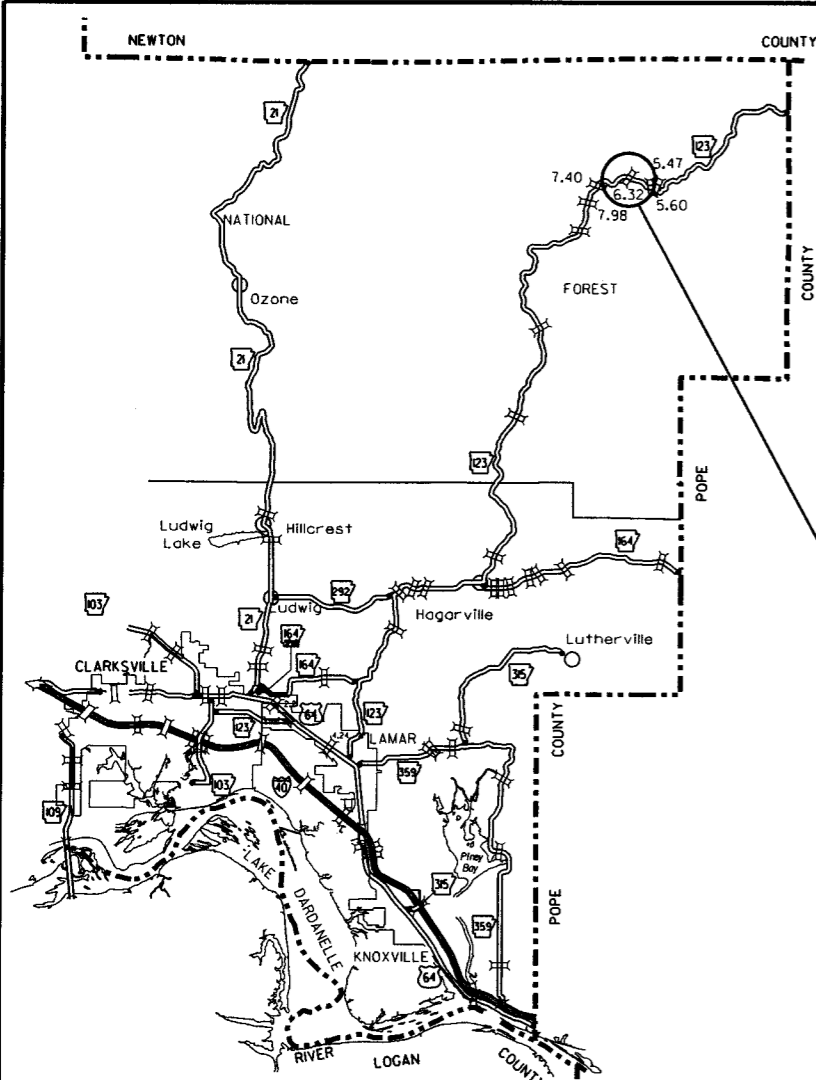
ROUTE 123 SECTION 3

JOB 080444

FED. AID PROJ. NHPP-0036(17)

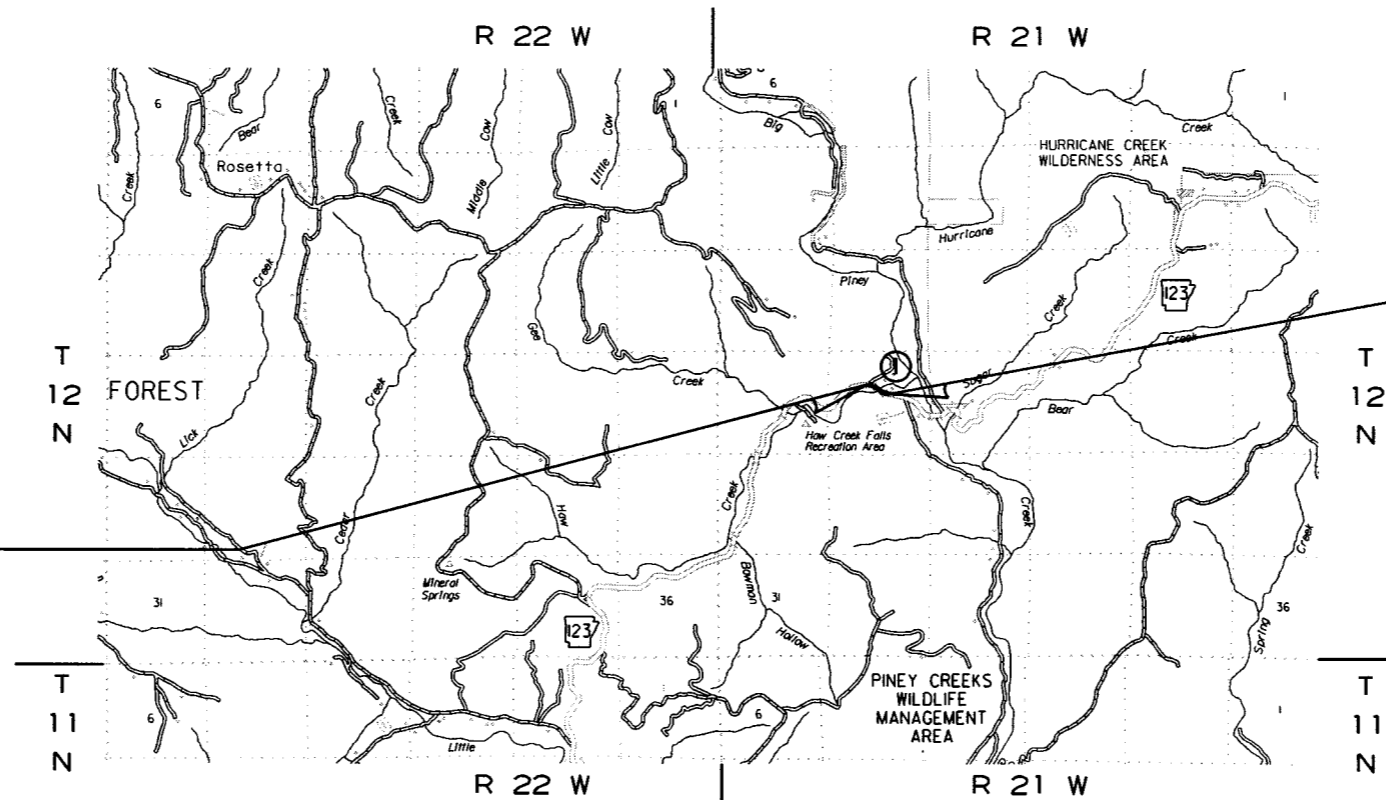


ARK. HWY. DIST. NO. 8



VICINITY MAP

NOT TO SCALE



STA. 121+71.29
END JOB 080444
LOG MILE 6.20



APPROVED



8-3-16
DEPUTY DIRECTOR
AND CHIEF ENGINEER

STRUCTURE OVER 20' - 0" SPAN

- ① STA. 115+55.00 CONSTRUCT TRI. 28' X 12' X 83' THREE-SIDED PRECAST CULVERT ON 35° LT. FWD SKEW W/WINGS ON 3:1 LT. & RT. TOTAL SPAN = 110' - 7 1/2" Q25 = 7000 CFS DA = 15.3 SQ. MI.

STA. 112+10.00
BEGIN JOB 080444
LOG MILE 6.38

BEGINNING:	
LAT: N 35° 40' 55"	LONG: W 93° 14' 49"
MID POINT:	
LAT: N 35° 40' 53"	LONG: W 93° 14' 44"
ENDING:	
LAT: N 35° 40' 50"	LONG: W 93° 14' 38"

	GROSS LENGTH OF PROJECT	961.29	FEET OR	0.182	MILES
NET	" " ROADWAY	850.67	" "	0.161	"
NET	" " BRIDGES	10.62	" "	0.021	"
NET	" " PROJECT	961.29	" "	0.182	"

P.E. 080444

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						080444	2	52

② INDEX, GOVERN. SPECS., AND GEN. NOTES

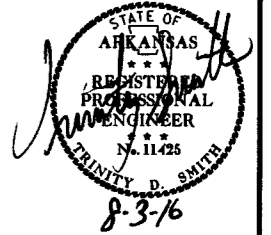
INDEX OF SHEETS

SHEET NO.	TITLE	BRIDGE NO.	DRWG. NO.	DATE
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2	INDEX OF SHEETS, GOVERNING SPECIFICATIONS, AND GENERAL NOTES			
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15	PERMANENT PAVEMENT MARKING DETAILS			
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22 - 23	SURVEY CONTROL DETAILS			
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GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-3	CONTRACTOR'S LICENSE
108-1	LIQUIDATED DAMAGES
108-2	WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER
303-1	AGGREGATE BASE COURSE
400-1	TACK COATS
410-1	CONSTRUCTION REQUIREMENTS AND ACCEPTANCE OF ASPHALT CONCRETE PLANT MIX COURSES
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
620-1	MULCH COVER
JOB 080444	BIDDING REQUIREMENTS AND CONDITIONS
JOB 080444	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB 080444	CARGO PREFERENCE ACT REQUIREMENTS
JOB 080444	CAVE DISCOVERY
JOB 080444	DELAY IN RIGHT OF WAY OCCUPANCY
JOB 080444	DOCUMENTATION OF PAYMENTS MADE TO DISADVANTAGED BUSINESS ENTERPRISES
JOB 080444	DUMPED RIPRAP USE AND STORAGE
JOB 080444	FOREST SERVICE REQUIREMENTS
JOB 080444	ISSUANCE OF PROPOSALS
JOB 080444	MANDATORY ELECTRONIC CONTRACT
JOB 080444	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB 080444	OFF-SITE RESTRAINING CONDITIONS FOR INDIANA AND NORTHERN LONG-EARED BATS
JOB 080444	PARTNERING REQUIREMENTS
JOB 080444	REMOVAL AND DISPOSAL OF GUARDRAIL
JOB 080444	SECTION 404 NATIONWIDE 14 PERMIT REQUIREMENTS
JOB 080444	SHORING FOR CULVERTS
JOB 080444	SOIL STABILIZATION
JOB 080444	SPECIAL CLEARING REQUIREMENTS
JOB 080444	SPECIAL SEEDING REQUIREMENTS
JOB 080444	STORM WATER POLLUTION PREVENTION PLAN
JOB 080444	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB 080444	THREE-SIDED PRECAST CULVERTS
JOB 080444	TOPSOIL FURNISHED AND PLACED REQUIREMENTS
JOB 080444	UTILITY ADJUSTMENTS
JOB 080444	VALUE ENGINEERING
JOB 080444	VEGETATED BUFFER
JOB 080444	WARM MIX ASPHALT
JOB 080444	WATER POLLUTION CONTROL & RESTRAINING CONDITION

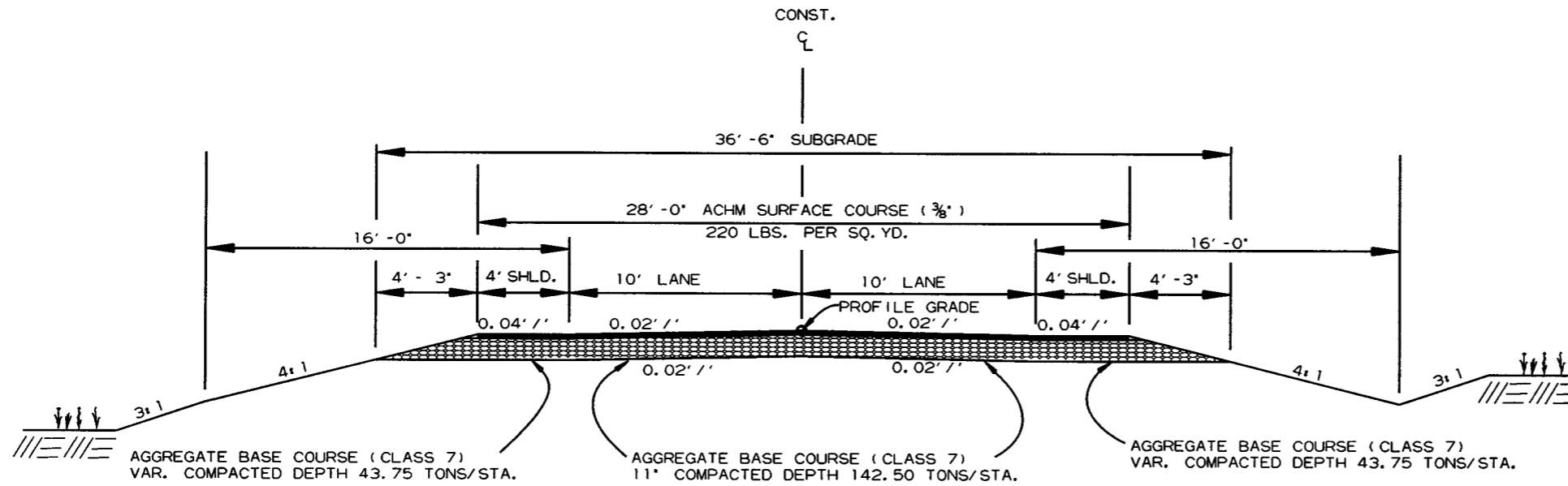
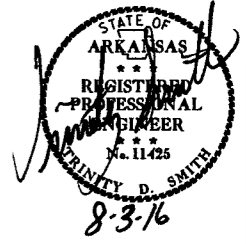


GENERAL NOTES

- GRADE LINE DENOTES FINISHED GRADE WHERE SHOWN ON PLANS.
- ALL PIPE LINES, POWER, TELEPHONE, AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING U. S. MAILBOXES WITHIN THE PROJECT LIMITS IN SUCH A MANNER THAT THE PUBLIC MAY RECEIVE CONTINUED MAIL SERVICE. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO INSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A FENCE TO CONTROL LIVESTOCK IN AREAS WHERE PASTURES ARE SEVERED. WIRE FENCE MAY BE CONSTRUCTED INITIALLY, OR IN LIEU THEREOF, THE CONTRACTOR AT HIS OWN EXPENSE, MAY ELECT TO PROVIDE TEMPORARY FENCING SUITABLE TO CONTAIN LIVESTOCK.
- ALL FLEXIBLE BASE AND ASPHALTIC PAVEMENTS REMOVED SHALL BE PAID FOR UNDER THE ITEM NO. 210 - UNCLASSIFIED EXCAVATION.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE OF THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

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						JOB NO. 080444	3	52

2 TYPICAL SECTIONS OF IMPROVEMENT



TYPICAL SECTIONS OF IMPROVEMENT
STA. 112+40.00 TO STA. 121+00.00

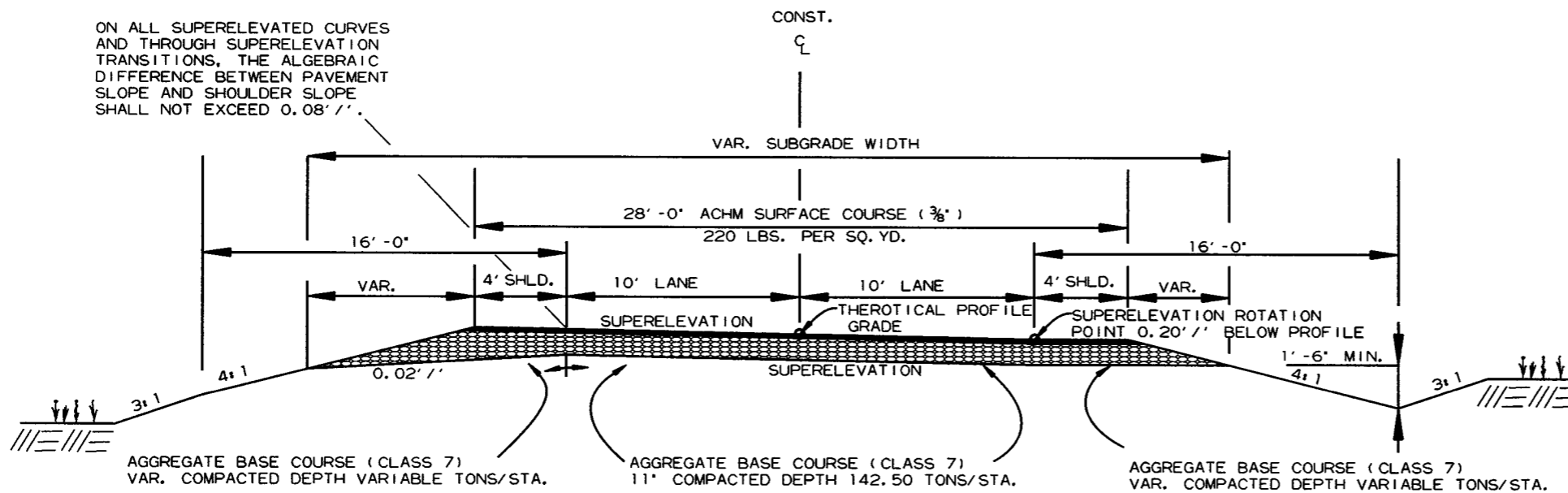
NOTES:
REFER TO CROSS SECTIONS FOR DEVIATION FROM THE NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.

THE THICKNESS OF AGGREGATE BASE COURSE SHALL BE WITHIN PLUS OR MINUS ONE INCH OF THE PLAN THICKNESS SHOWN. THE CONTRACTOR WILL CORRECT ANY DEFICIENT THICKNESS THAT DOES NOT MEET TOLERANCE INDICATED. PAYMENT WILL NOT BE MADE FOR MATERIAL PLACED IN EXCESS OF THE TOLERANCE INDICATED.

THE FINAL 2" OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT LANE LINES.

AFTER PLACING FINAL 2" OF SURFACE COURSE, THE EXISTING SLOPE SHALL BE REDRESSED AS DIRECTED BY THE ENGINEER PRIOR TO SEEDING IN ORDER TO MAINTAIN A UNIFORM SLOPE. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS CONTRACT ITEMS.

ON ALL SUPERELEVATED CURVES AND THROUGH SUPERELEVATION TRANSITIONS, THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT SLOPE AND SHOULDER SLOPE SHALL NOT EXCEED 0.08'/'.

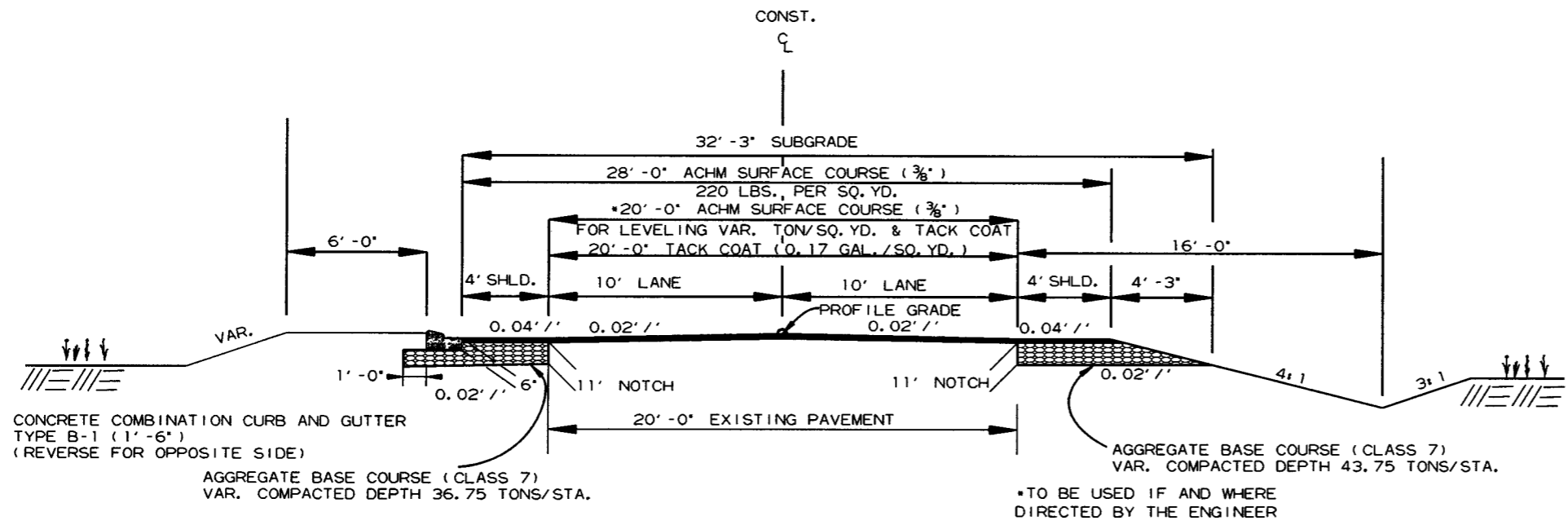
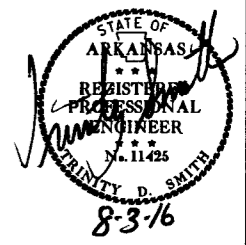


TYPICAL SECTIONS OF IMPROVEMENT

TYPICAL SECTIONS OF IMPROVEMENT

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				6	ARK.			
JOB NO. 080444							4	52

② TYPICAL SECTIONS OF IMPROVEMENT



TYPICAL SECTIONS OF IMPROVEMENT

STA. 112+10.00 TO STA. 112+36.00
 STA. 121+00.00 TO STA. 121+71.29 (REVERSE CURB FOR RT. SIDE)

NOTES:
 REFER TO CROSS SECTIONS FOR DEVIATION FROM THE NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.

THE THICKNESS OF AGGREGATE BASE COURSE SHALL BE WITHIN PLUS OR MINUS ONE INCH OF THE PLAN THICKNESS SHOWN. THE CONTRACTOR WILL CORRECT ANY DEFICIENT THICKNESS THAT DOES NOT MEET TOLERANCE INDICATED. PAYMENT WILL NOT BE MADE FOR MATERIAL PLACED IN EXCESS OF THE TOLERANCE INDICATED.

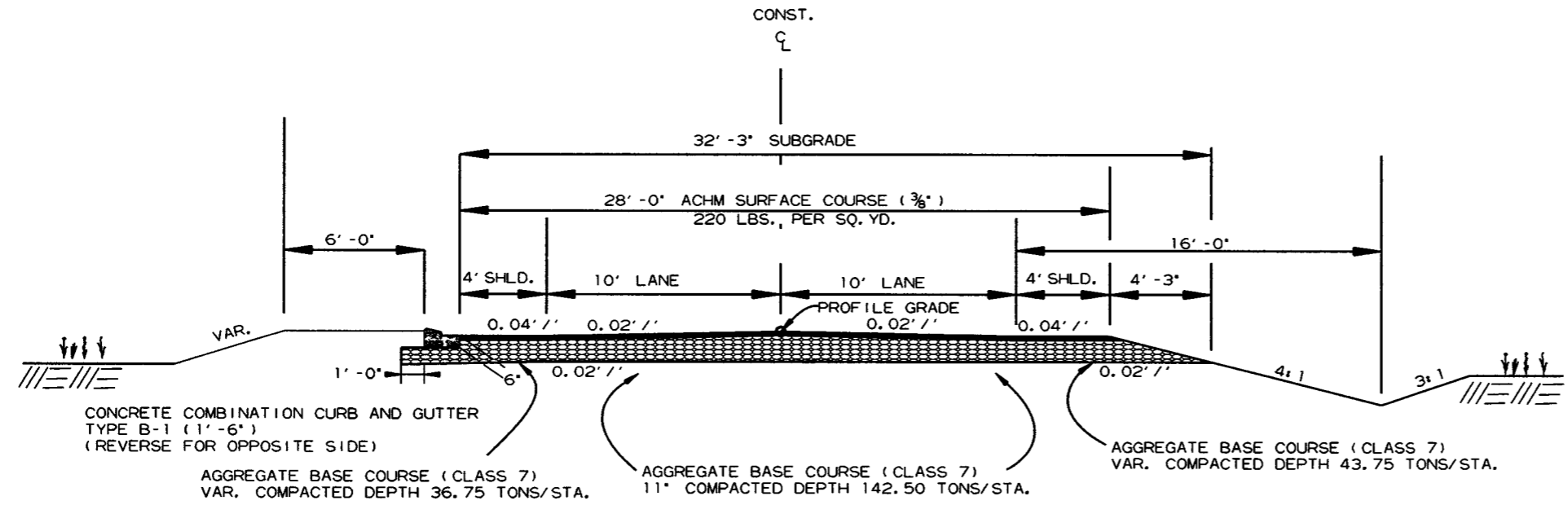
ASPHALT FOR LEVELING OF EXISTING PAVEMENT SHALL BE PLACED ONLY IF AND WHERE DIRECTED BY THE ENGINEER. CALCULATIONS FOR THE AMOUNT OF LEVELING AND/OR LEVELING OPERATIONS SHALL BE PERFORMED BEFORE CONSTRUCTING NOTCH AND WIDENING. CALCULATIONS WILL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED TO BE INCLUDED IN THE VARIOUS PAY ITEMS.

THE FINAL 2" OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT LANE LINES.

AFTER PLACING FINAL 2" OF SURFACE COURSE, THE EXISTING SLOPE SHALL BE REDRESSED AS DIRECTED BY THE ENGINEER PRIOR TO SEEDING IN ORDER TO MAINTAIN A UNIFORM SLOPE. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS CONTRACT ITEMS.

THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE OF THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

PRIOR TO AND DURING PLACEMENT OF PAVEMENT IN FRONT OF THE CURB AND GUTTER, THE CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE AT ALL TIMES. THE METHOD(S) USED SHALL BE APPROVED BY THE ENGINEER. PAYMENT FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS.



TYPICAL SECTIONS OF IMPROVEMENT

STA. 112+36.00 TO STA. 112+40.00

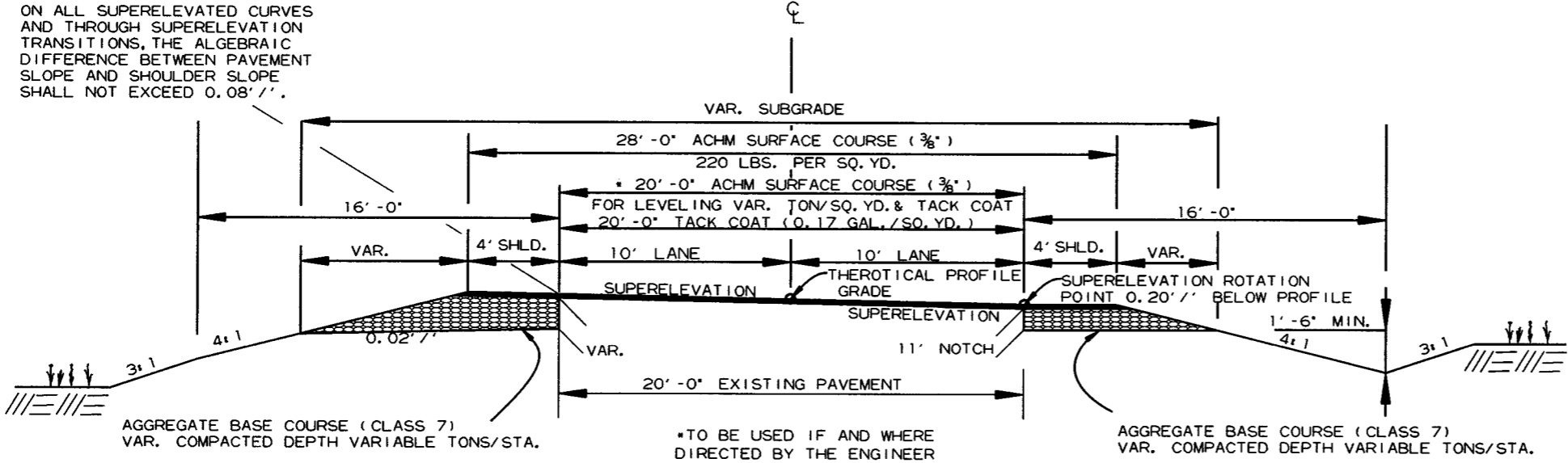
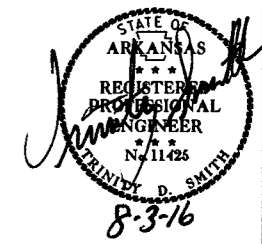
TYPICAL SECTIONS OF IMPROVEMENT

8/2/2016

R080444.DGN

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						JOB NO. 080444	5	52

② TYPICAL SECTIONS OF IMPROVEMENT



TYPICAL SECTIONS OF IMPROVEMENT - SUPERELEVATION

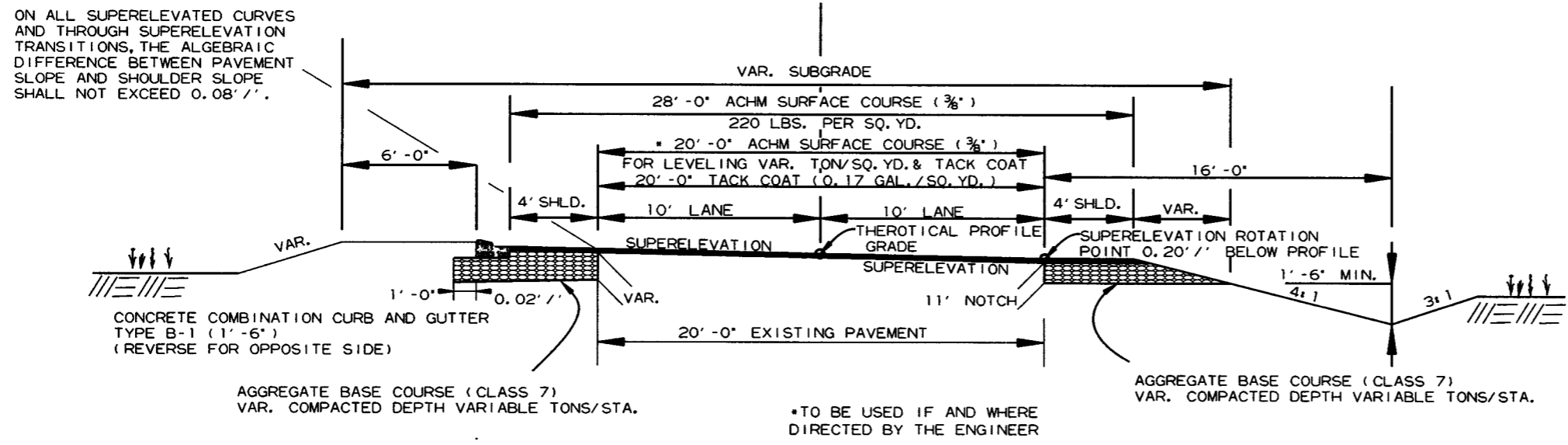
NOTES:
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THE THICKNESS OF AGGREGATE BASE COURSE SHALL BE WITHIN PLUS OR MINUS ONE INCH OF THE PLAN THICKNESS SHOWN. THE CONTRACTOR WILL CORRECT ANY DEFICIENT THICKNESS THAT DOES NOT MEET TOLERANCE INDICATED. PAYMENT WILL NOT BE MADE FOR MATERIAL PLACED IN EXCESS OF THE TOLERANCE INDICATED.

ASPHALT FOR LEVELING OF EXISTING PAVEMENT SHALL BE PLACED ONLY IF AND WHERE DIRECTED BY THE ENGINEER. CALCULATIONS FOR THE AMOUNT OF LEVELING AND/OR LEVELING OPERATIONS SHALL BE PERFORMED BEFORE CONSTRUCTING NOTCH AND WIDENING. CALCULATIONS WILL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED TO BE INCLUDED IN THE VARIOUS PAY ITEMS.

THE FINAL 2" OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT LANE LINES.

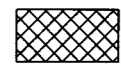
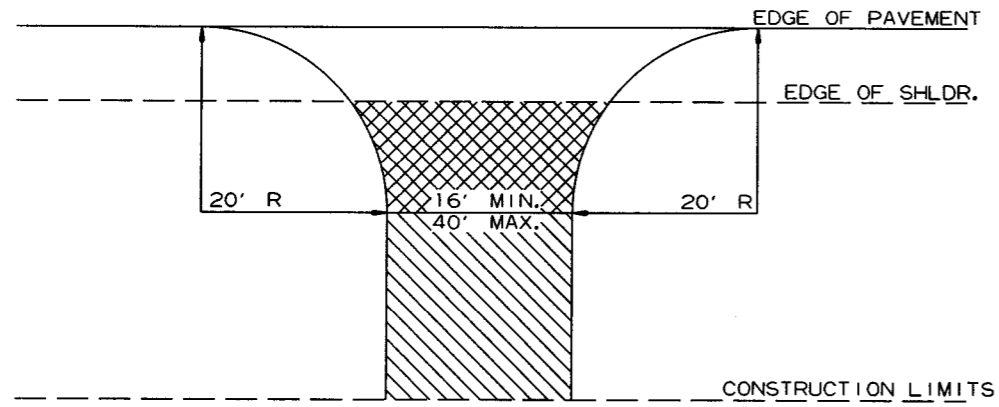
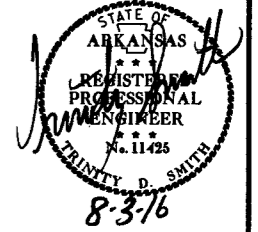
AFTER PLACING FINAL 2" OF SURFACE COURSE, THE EXISTING SLOPE SHALL BE REDRESSED AS DIRECTED BY THE ENGINEER PRIOR TO SEEDING IN ORDER TO MAINTAIN A UNIFORM SLOPE. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS CONTRACT ITEMS.



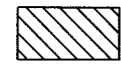
TYPICAL SECTIONS OF IMPROVEMENT - SUPERELEVATION

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2 SPECIAL DETAILS

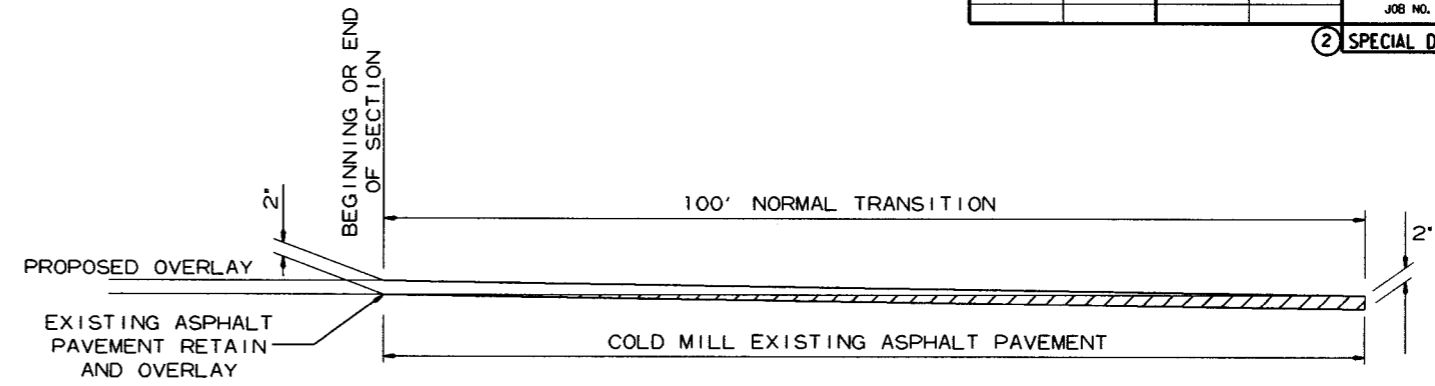


ASPHALT CONCRETE HOT MIX SURFACE COURSE (220 LBS. PER SQ. YD.)
 AGGREGATE BASE COURSE (CLASS 7)
 7" COMP. DEPTH IF ASPHALT DRIVE EXIST OR
 6" CONCRETE IF CONCRETE DRIVE EXIST.



AGGREGATE BASE COURSE (CLASS 7)
 9" COMP. DEPTH OR CONFORM
 TO EXISTING DRIVEWAY

DETAIL FOR DRIVEWAY TURNOUTS



DETAIL FOR TRANSITIONS

7/25/2016

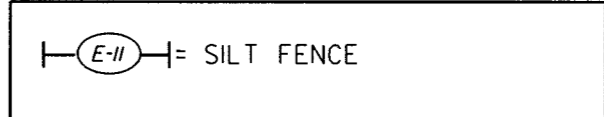
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SPECIAL DETAILS

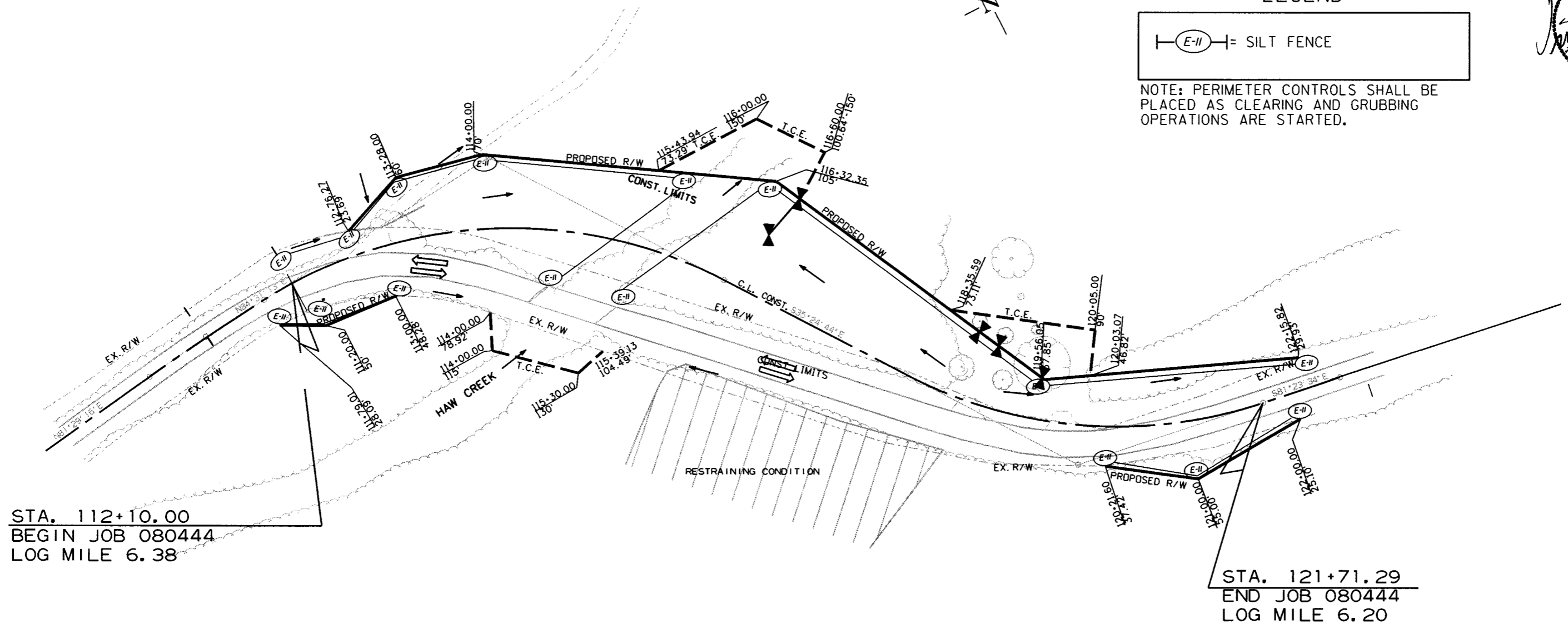
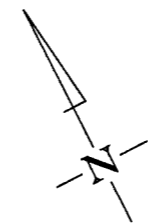
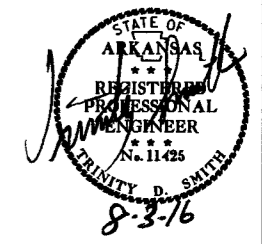
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② TEMPORARY EROSION CONTROL DETAILS

LEGEND



NOTE: PERIMETER CONTROLS SHALL BE PLACED AS CLEARING AND GRUBBING OPERATIONS ARE STARTED.



STA. 112+10.00
BEGIN JOB 080444
LOG MILE 6.38

STA. 121+71.29
END JOB 080444
LOG MILE 6.20

REVISIONS

DATE	REVISION

• MAINTAIN ALL EROSION CONTROL DEVICES UNTIL THE END OF THE JOB UNLESS OTHERWISE SPECIFIED.

SILT FENCE (E-II)			
STA. 111+79	TO	STA. 113+00	115' RT.
STA. 112+10	TO	STA. 115+95	440' LT.
STA. 114+65	TO	STA. 115+95	195' LT. & RT.
STA. 115+40	TO	STA. 116+32	190' LT. & RT.
STA. 116+32	TO	STA. 122+16	555' LT.
STA. 120+20	TO	STA. 122+00	200' RT.

EROSION CONTROL GENERAL NOTES

THE QUANTITIES AND LOCATIONS OF THE EROSION CONTROL DEVICES SHOWN IN THE PLANS ARE ESTIMATED AND MAY BE ALTERED IF AND WHERE DIRECTED BY THE ENGINEER TO MAXIMIZE THEIR EFFECTIVENESS. THE DEVICES ARE TO BE INSTALLED IN AN AREA ONLY WHEN THE SOIL DISTURBING ACTIVITY IN THAT AREA BEGINS.

REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.

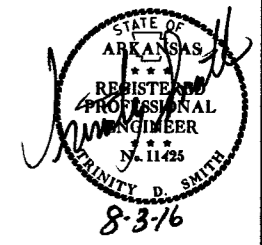
CLEARING AND GRUBBING STAGE
TEMPORARY EROSION CONTROL DETAILS

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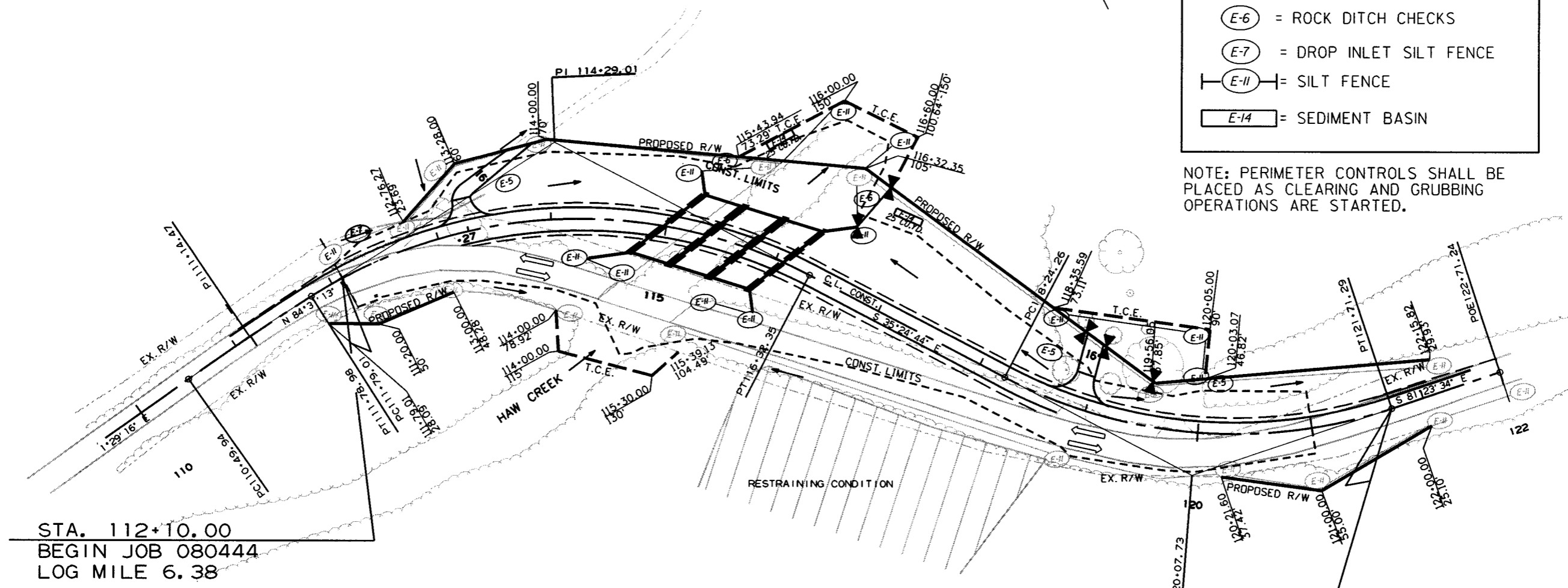
② TEMPORARY EROSION CONTROL DETAILS

LEGEND

- (E-5) = SAND BAG DITCH CHECKS
- (E-6) = ROCK DITCH CHECKS
- (E-7) = DROP INLET SILT FENCE
- (E-II) = SILT FENCE
- (E-14) = SEDIMENT BASIN



NOTE: PERIMETER CONTROLS SHALL BE PLACED AS CLEARING AND GRUBBING OPERATIONS ARE STARTED.



STA. 112+10.00
BEGIN JOB 080444
LOG MILE 6.38

STA. 121+71.29
END JOB 080444
LOG MILE 6.20

SILT FENCE (E-II)			
STA. 111+79	TO STA. 113+00	115' RT.	RETAIN
STA. 112+10	TO STA. 115+95	440' LT.	RETAIN
STA. 114+25	TO STA. 11+65	35' RT.	
STA. 114+65	TO STA. 115+95	195' LT. & RT.	RETAIN
STA. 115+40	TO STA. 116+32	190' LT. & RT.	RETAIN
STA. 115+44	TO STA. 116+00	80' LT.	
STA. 116+32	TO STA. 122+16	555' LT.	RETAIN
STA. 116+50	TO STA. 116+60	105' LT.	
STA. 118+35	TO STA. 120+05	180' LT.	
STA. 120+20	TO STA. 122+00	200' RT.	RETAIN

SAND BAG DITCH CHECKSS (E-5)	
STA. 113+50	22 BAGS LT.
STA. 118+70	22 BAGS LT.
STA. 119+95	22 BAGS LT.

ROCK DITCH CHECKSS (E-6)	
STA. 115+35	3 CU.YD. LT.
STA. 116+40	3 CU.YD. LT.

DROP INLET SILT FENCE (E-7)	
STA. 112+36	20 LIN.FT. LT.

SEDIMENT BASIN (E-14)	
STA. 115+70	25 CU.YD. LT.
STA. 117+00	25 CU.YD. LT.

EROSION CONTROL GENERAL NOTES

THE QUANTITIES AND LOCATIONS OF THE EROSION CONTROL DEVICES SHOWN IN THE PLANS ARE ESTIMATED AND MAY BE ALTERED IF AND WHERE DIRECTED BY THE ENGINEER TO MAXIMIZE THEIR EFFECTIVENESS. THE DEVICES ARE TO BE INSTALLED IN AN AREA ONLY WHEN THE SOIL DISTURBING ACTIVITY IN THAT AREA BEGINS.

REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.

REVISIONS

DATE	REVISION

* MAINTAIN ALL EROSION CONTROL DEVICES UNTIL THE END OF THE JOB UNLESS OTHERWISE SPECIFIED.

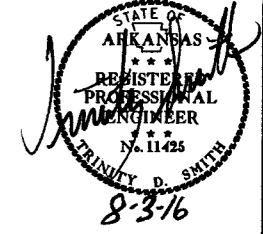
STAGE 1
TEMPORARY EROSION CONTROL DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		9	52
				JOB NO.	080444			

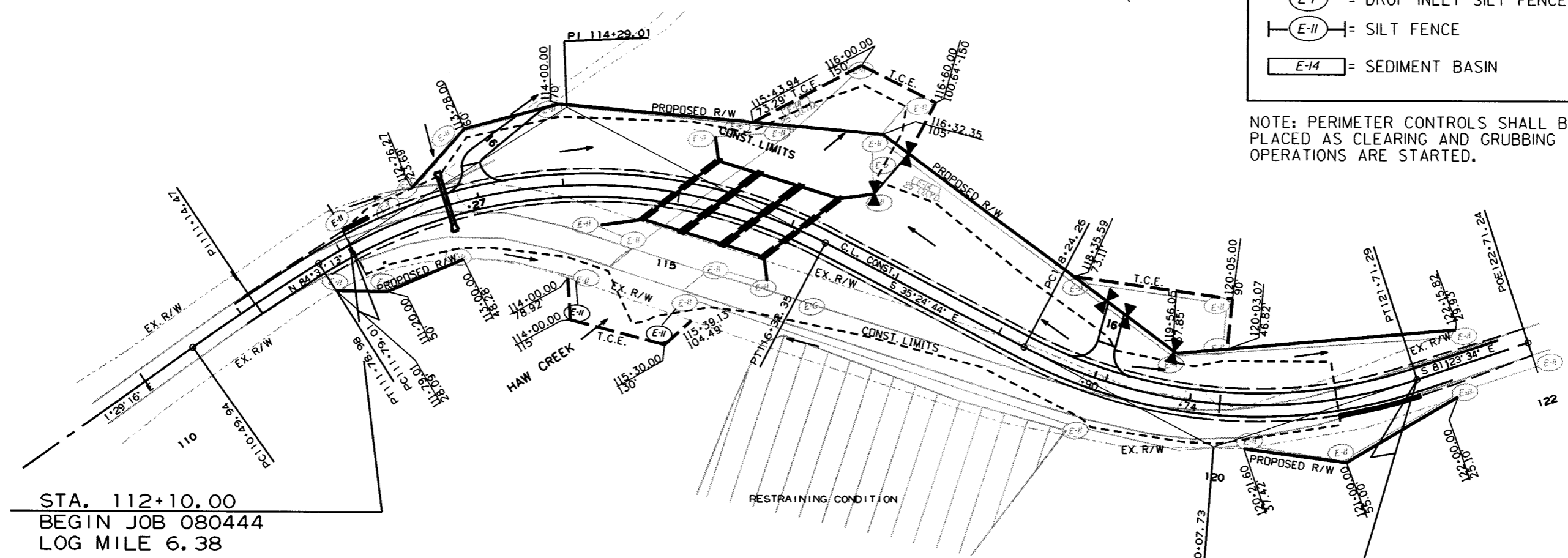
② TEMPORARY EROSION CONTROL DETAILS

LEGEND

- (E-5) = SAND BAG DITCH CHECKS
- (E-6) = ROCK DITCH CHECKS
- (E-7) = DROP INLET SILT FENCE
- (E-II) = SILT FENCE
- E-14 = SEDIMENT BASIN



NOTE: PERIMETER CONTROLS SHALL BE PLACED AS CLEARING AND GRUBBING OPERATIONS ARE STARTED.



STA. 112+10.00
BEGIN JOB 080444
LOG MILE 6.38

STA. 121+71.29
END JOB 080444
LOG MILE 6.20

SILT FENCE (E-II)			
STA. 111+79	TO STA. 113+00	115' RT.	RETAIN
STA. 112+10	TO STA. 115+95	440' LT.	RETAIN
STA. 114+25	TO STA. 11+65	35' RT.	RETAIN
STA. 114+65	TO STA. 115+95	195' LT. & RT.	RETAIN
STA. 115+40	TO STA. 116+32	190' LT. & RT.	RETAIN
STA. 115+44	TO STA. 116+00	80' LT.	RETAIN
STA. 116+32	TO STA. 122+16	555' LT.	RETAIN
STA. 116+50	TO STA. 116+60	105' LT.	RETAIN
STA. 118+35	TO STA. 120+05	180' LT.	RETAIN
STA. 120+20	TO STA. 122+00	200' RT.	RETAIN
STA. 114+00	TO STA. 114+00	40' RT.	RETAIN
STA. 115+30	TO STA. 115+54	60' RT.	RETAIN
SAND BAG DITCH CHECKSS (E-5)			
STA. 113+50	22 BAGS LT.	RETAIN	
STA. 118+70	22 BAGS LT.	RETAIN	
STA. 119+95	22 BAGS LT.	RETAIN	
ROCK DITCH CHECKSS (E-6)			
STA. 115+35	3 CU.YD. LT.	RETAIN	
STA. 116+40	3 CU.YD. LT.	RETAIN	
DROP INLET DITCH CHECKSS (E-7)			
STA. 112+36	20 LIN.FT. LT.	RETAIN	
SEDIMENT BASIN (E-14)			
STA. 115+70	25 CU.YD.	LT. RETAIN	
STA. 117+00	25 CU.YD.	LT. RETAIN	

EROSION CONTROL GENERAL NOTES

THE QUANTITIES AND LOCATIONS OF THE EROSION CONTROL DEVICES SHOWN IN THE PLANS ARE ESTIMATED AND MAY BE ALTERED IF AND WHERE DIRECTED BY THE ENGINEER TO MAXIMIZE THEIR EFFECTIVENESS. THE DEVICES ARE TO BE INSTALLED IN AN AREA ONLY WHEN THE SOIL DISTURBING ACTIVITY IN THAT AREA BEGINS.

REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.

REVISIONS

DATE	REVISION

* MAINTAIN ALL EROSION CONTROL DEVICES UNTIL THE END OF THE JOB UNLESS OTHERWISE SPECIFIED.

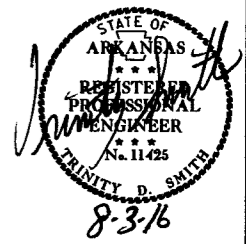
STAGE 2
TEMPORARY EROSION CONTROL DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		10	52
				JOB NO. 080444				

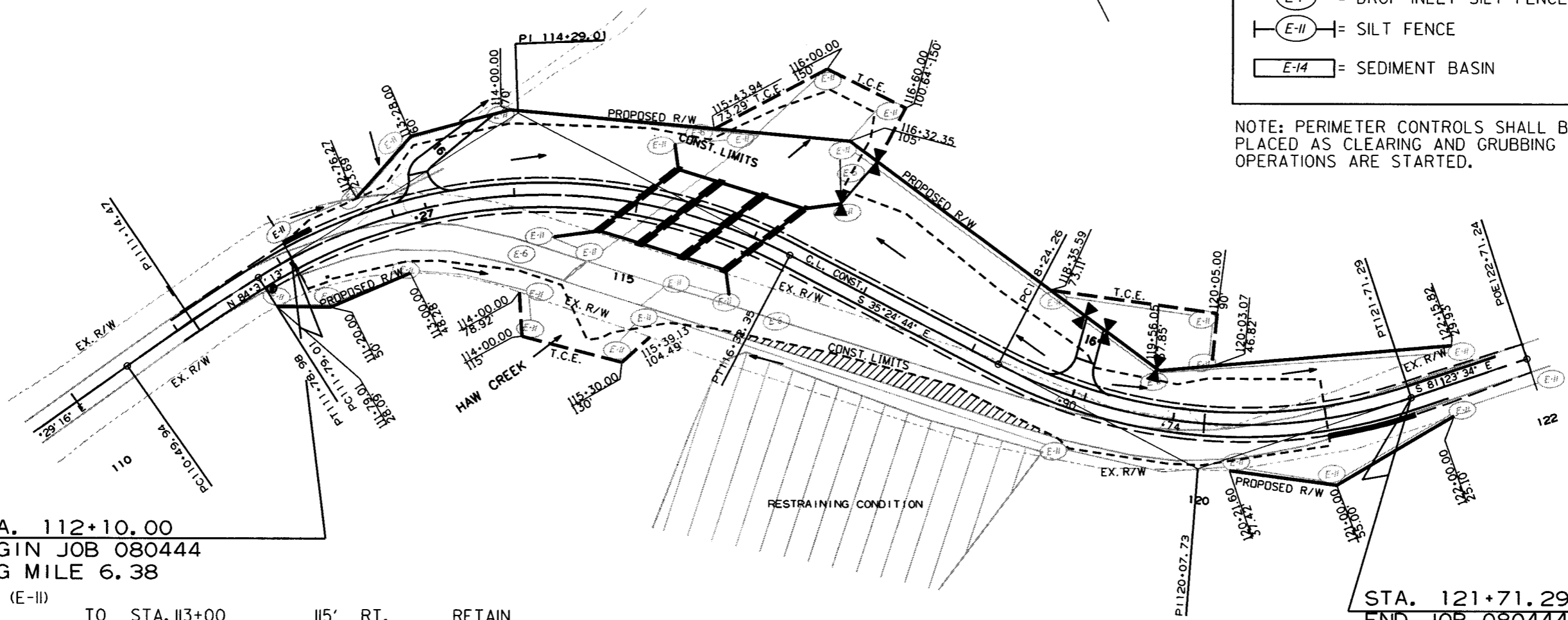
LEGEND

- (E-5) = SAND BAG DITCH CHECKS
- (E-6) = ROCK DITCH CHECKS
- (E-7) = DROP INLET SILT FENCE
- (E-11) = SILT FENCE
- (E-14) = SEDIMENT BASIN

② TEMPORARY EROSION CONTROL DETAILS



NOTE: PERIMETER CONTROLS SHALL BE PLACED AS CLEARING AND GRUBBING OPERATIONS ARE STARTED.



STA. 112+10.00
BEGIN JOB 080444
LOG MILE 6.38

STA. 121+71.29
END JOB 080444
LOG MILE 6.20

SILT FENCE (E-11)			
STA. 111+79	TO STA. 113+00	115' RT.	RETAIN
STA. 112+10	TO STA. 115+95	440' LT.	RETAIN
STA. 114+25	TO STA. 11+65	35' RT.	RETAIN
STA. 114+65	TO STA. 115+95	195' LT. & RT.	RETAIN
STA. 115+40	TO STA. 116+32	190' LT. & RT.	RETAIN
STA. 115+44	TO STA. 116+00	80' LT.	RETAIN
STA. 116+32	TO STA. 122+16	555' LT.	RETAIN
STA. 116+50	TO STA. 116+60	105' LT.	RETAIN
STA. 118+35	TO STA. 120+05	180' LT.	RETAIN
STA. 120+20	TO STA. 122+00	200' RT.	RETAIN
STA. 114+00	TO STA. 114+00	40' RT.	RETAIN
STA. 115+30	TO STA. 115+54	60' RT.	RETAIN

SAND BAG DITCH CHECKSS (E-5)			
STA. 113+50	22 BAGS LT.	RETAIN	
STA. 118+70	22 BAGS LT.	RETAIN	
STA. 119+95	22 BAGS LT.	RETAIN	

ROCK DITCH CHECKSS (E-6)			
STA. 115+35	3 CU.YD. LT.	RETAIN	
STA. 116+40	3 CU.YD. LT.	RETAIN	

DROP INLET DITCH CHECKSS (E-7)			
STA. 112+36	20 LIN.FT. LT.	RETAIN	

OBLITERATION AREA

EROSION CONTROL GENERAL NOTES

THE QUANTITIES AND LOCATIONS OF THE EROSION CONTROL DEVICES SHOWN IN THE PLANS ARE ESTIMATED AND MAY BE ALTERED IF AND WHERE DIRECTED BY THE ENGINEER TO MAXIMIZE THEIR EFFECTIVENESS. THE DEVICES ARE TO BE INSTALLED IN AN AREA ONLY WHEN THE SOIL DISTURBING ACTIVITY IN THAT AREA BEGINS.

REFER TO SECTION 110 OF THE STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.

REVISIONS

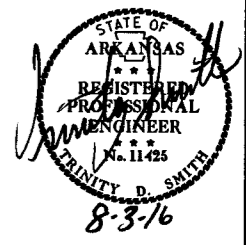
DATE	REVISION

* MAINTAIN ALL EROSION CONTROL DEVICES UNTIL THE END OF THE JOB UNLESS OTHERWISE SPECIFIED.

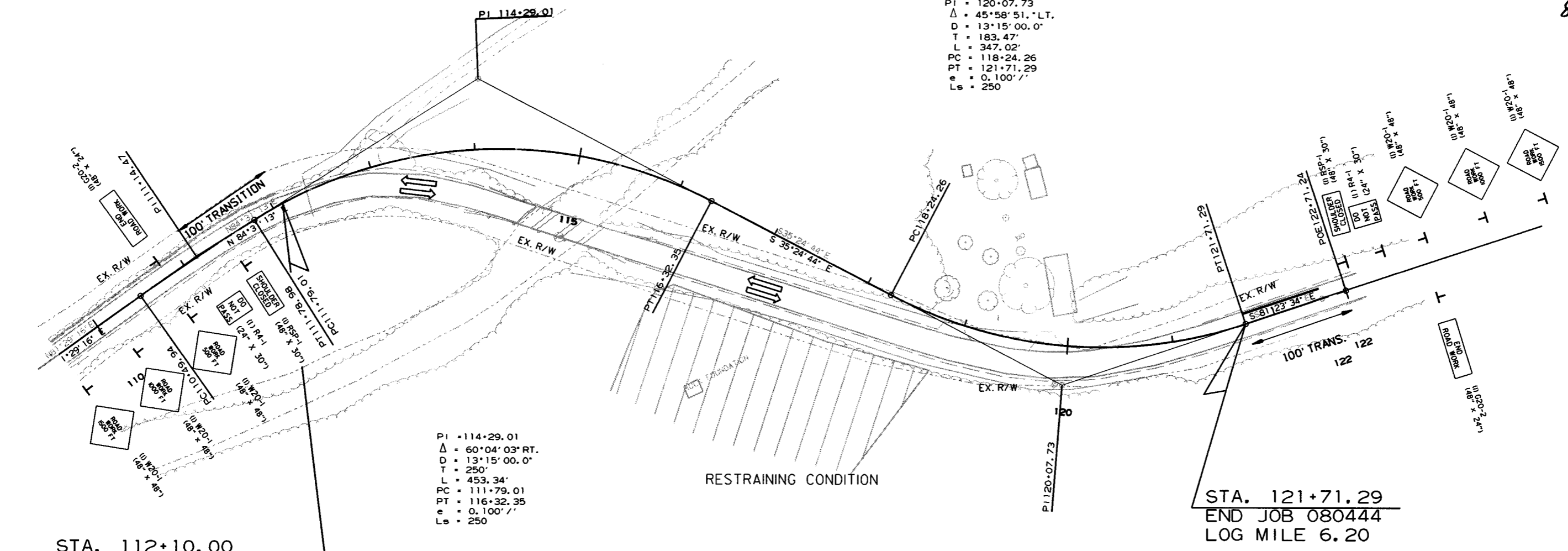
STAGE 3
TEMPORARY EROSION CONTROL DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		II	52
				JOB NO.		080444		

② MAINTENANCE OF TRAFFIC DETAILS



PI = 120+07.73
 Δ = 45°58'51" LT.
D = 13°15'00.0"
T = 183.47'
L = 347.02'
PC = 118+24.26
PT = 121+71.29
e = 0.100' / '
Ls = 250



STA. 112+10.00
BEGIN JOB 080444
LOG MILE 6.38

PI = 114+29.01
 Δ = 60°04'03" RT.
D = 13°15'00.0"
T = 250'
L = 453.34'
PC = 111+79.01
PT = 116+32.35
e = 0.100' / '
Ls = 250

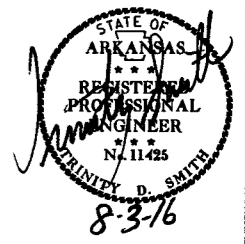
STA. 121+71.29
END JOB 080444
LOG MILE 6.20

NOTE:
ALL ADVANCE WARNING SIGNS
ARE TO BE RETAINED THROUGHOUT
ALL STAGES OF CONSTRUCTION.

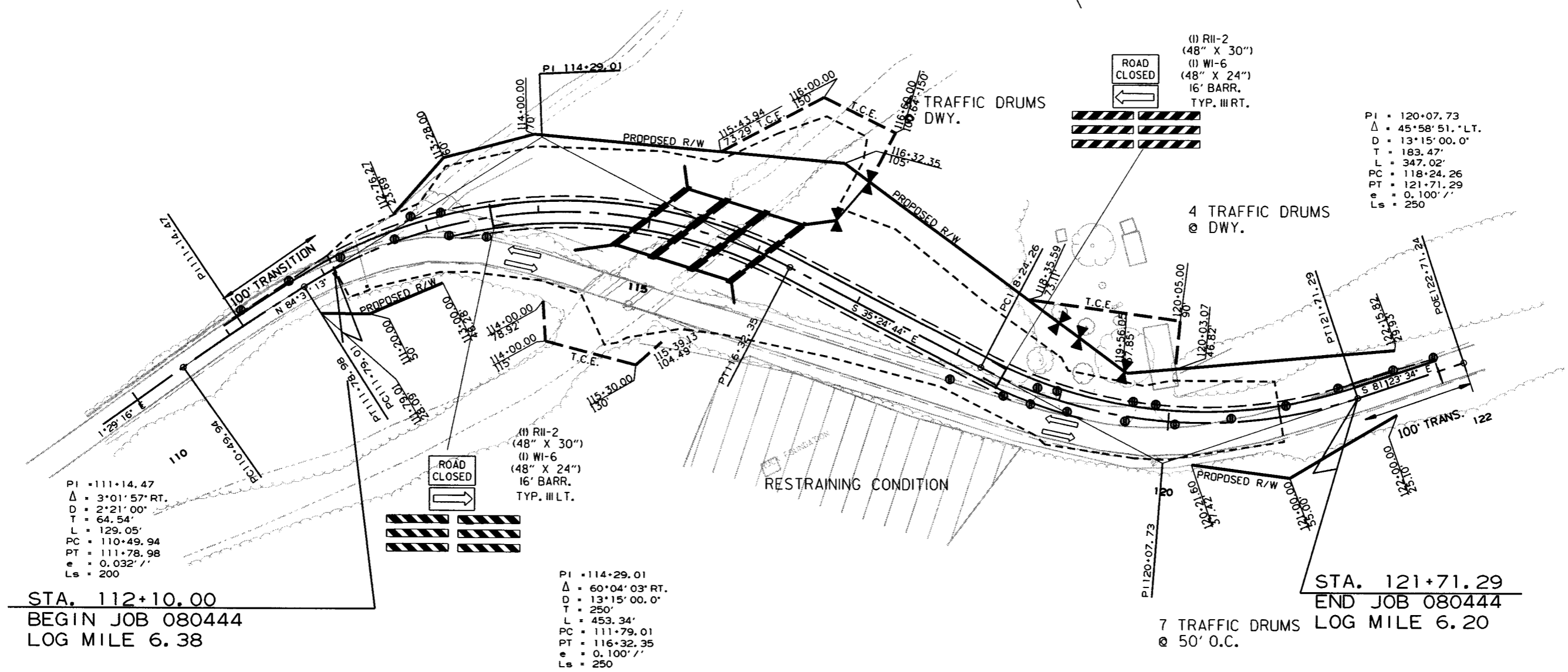
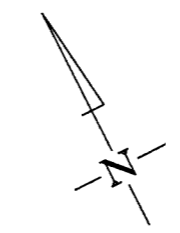
ADVANCE WARNING SIGNS
MAINTENANCE OF TRAFFIC DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		12	52
				JOB NO.	080444			

② MAINTENANCE OF TRAFFIC DETAILS



4 TRAFFIC DRUMS @ 50' O.C. 4 TRAFFIC DRUMS @ DWY.



PI = 111+14.47
 Δ = 3°01'57" RT.
D = 2°21'00"
T = 64.54'
L = 129.05'
PC = 110+49.94
PT = 111+78.98
e = 0.032' /'
Ls = 200

(1) RII-2 (48" X 30")
(1) WI-6 (48" X 24")
16' BARR.
TYP. III LT.

PI = 114+29.01
 Δ = 60°04'03" RT.
D = 13°15'00.0"
T = 250'
L = 453.34'
PC = 111+79.01
PT = 116+32.35
e = 0.100' /'
Ls = 250

(1) RII-2 (48" X 30")
(1) WI-6 (48" X 24")
16' BARR.
TYP. III RT.

PI = 120+07.73
 Δ = 45°58'51.1" LT.
D = 13°15'00.0"
T = 183.47'
L = 347.02'
PC = 118+24.26
PT = 121+71.29
e = 0.100' /'
Ls = 250

STA. 112+10.00
BEGIN JOB 080444
LOG MILE 6.38

STA. 121+71.29
END JOB 080444
LOG MILE 6.20

7 TRAFFIC DRUMS @ 50' O.C.

SEQUENCE OF CONSTRUCTION

STAGE 1 -
MAINTAIN TRAFFIC ON EXISTING ROADWAY,
CONSTRUCT 3 SIDED R.C. BOX CULVERT AND
PROPOSED ROADWAY. PLACE PERMANENT
PAVEMENT MARKINGS ON NEW ALIGNMENT.

STAGE 2 -
CONSTRUCT PERMANENT CONNECTIONS TO EXISTING
ROADWAY. PLACE PERMANENT PAVEMENT MARKINGS
AND SHIFT TRAFFIC ONTO PROPOSED ALIGNMENT.

STAGE 3 -
REMOVE EXISTING BRIDGE STRUCTURE AND
COMPLETE DITCH AND SLOPE GRADES.

STAGE 2
CONSTRUCTION PAVEMENT MARKINGS = 1732 LIN.FT.
REMOVAL OF PERMANENT PAVEMENT MARKINGS = 2020 LIN.FT.

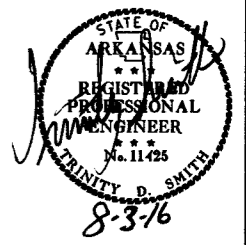
TRAFFIC DRUMS = 23 EACH

STAGE 1
MAINTENANCE OF TRAFFIC DETAILS

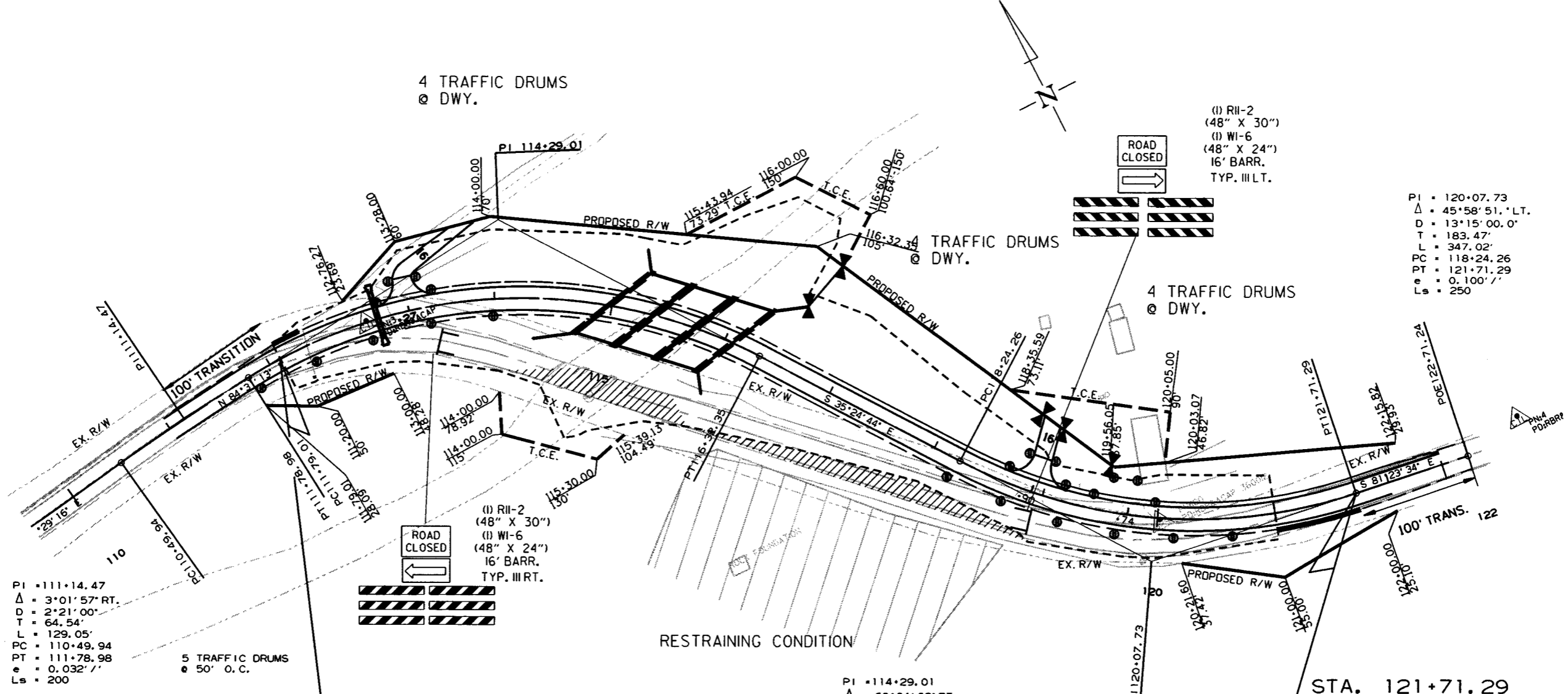
8/2/2016 R080444.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		14	52
				JOB NO.		080444		

② MAINTENANCE OF TRAFFIC DETAILS



OBLITERATION AREA



STA. 112+10.00
BEGIN JOB 080444
LOG MILE 6.38

STA. 121+71.29
END JOB 080444
LOG MILE 6.20

SEQUENCE OF CONSTRUCTION

- STAGE 1 -
MAINTAIN TRAFFIC ON EXISTING ROADWAY,
CONSTRUCT 3 SIDED R.C. BOX CULVERT AND
PROPOSED ROADWAY. PLACE PERMANENT
PAVEMENT MARKINGS ON NEW ALIGNMENT.
- STAGE 2 -
CONSTRUCT PERMANENT CONNECTIONS TO EXISTING
ROADWAY. PLACE PERMANENT PAVEMENT MARKINGS
AND SHIFT TRAFFIC ONTO PROPOSED ALIGNMENT.
- STAGE 3 -
REMOVE EXISTING BRIDGE STRUCTURE AND
COMPLETE DITCH AND SLOPE GRADES.

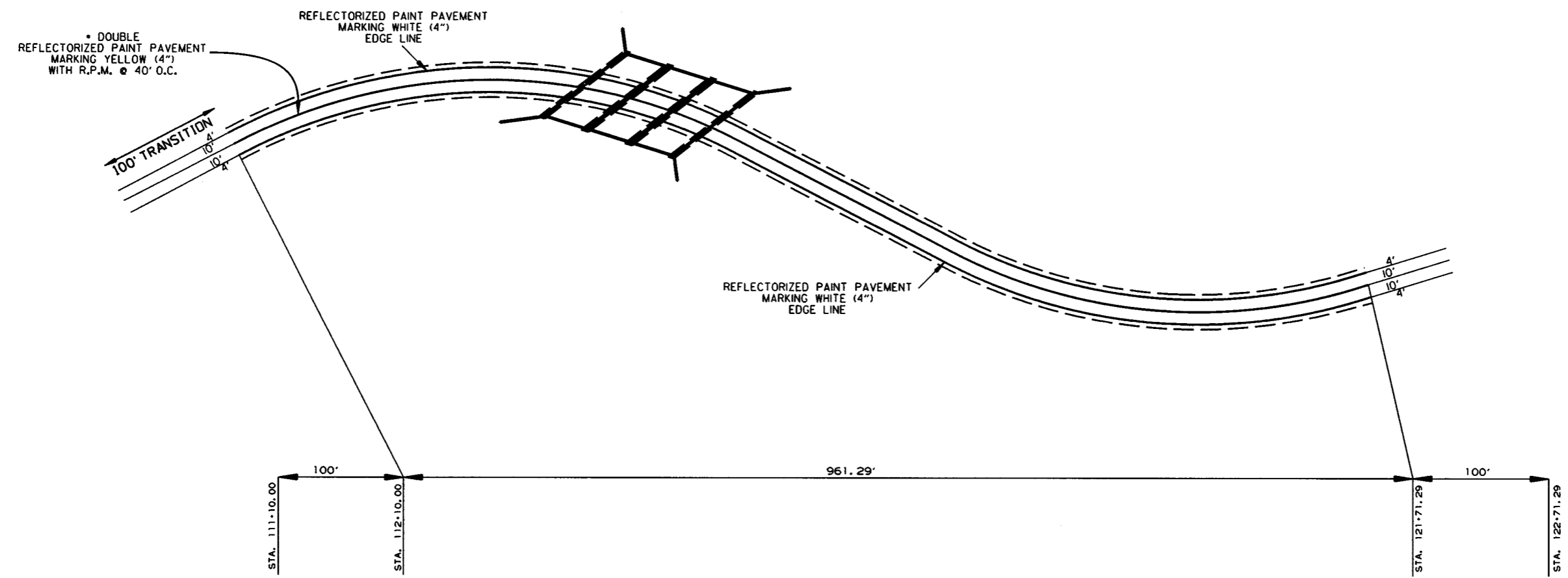
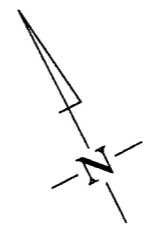
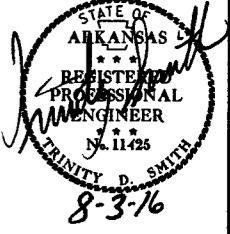
TRAFFIC DRUMS = 28 EACH

STAGE 3
MAINTENANCE OF TRAFFIC DETAILS

7/26/2016
R080444.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	080444		15	52

② PERMANENT PAVEMENT MARKING DETAILS



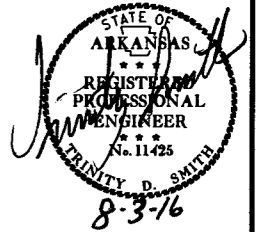
• THE 4" YELLOW STRIPING QUANTITY HAS BEEN ESTIMATED BASED ON A DOUBLE YELLOW CENTERLINE STRIPE FOR THE ENTIRE PROJECT. THE PROJECT MUST BE MARKED FOR PASSING/NO PASSING ZONES PRIOR TO THE PLACEMENT OF ANY FINAL STRIPING. CONTACT MAINTENANCE DIVISION AFTER THE FINAL LIFT OF SURFACE COURSE HAS BEEN PLACED TO SCHEDULE THE ZONING OF THE PROJECT.

FINAL STRIPING
 REFLECTORIZED PAINT PAVEMENT MARKINGS:
 RT. AND LT. EDGE LINES = 2275 LIN. FT. WHITE
 DOUBLE CENTERLINE = 2275 LIN. FT. YELLOW
 RAISED PAVEMENT MARKERS:
 TYPE 11 (YEL./YEL.) 40' O.C. CENTERLINE = 26 EACH

7/25/2016
 R080444.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	080444		16	52

② QUANTITIES



CONSTRUCTION PAVEMENT MARKINGS AND PERMANENT PAVEMENT MARKINGS

DESCRIPTION	STAGE 1	STAGE 2	STAGE 3	END OF JOB	REMOVAL OF PERMANENT PAVEMENT MARKINGS	CONSTRUCTION PAVEMENT MARKINGS	RAISED PAVEMENT MARKERS	REFLECTORIZED PAINT PAVEMENT MARKING	
							TYPE II (YEL/YEL) EACH	4"	
								WHITE	YELLOW
LIN. FT. - EACH						LIN. FT.	LIN. FT.		
REMOVAL OF PERMANENT PAVEMENT MARKINGS		2020			2020				
CONSTRUCTION PAVEMENT MARKINGS		1732				1732			
RAISED PAVEMENT MARKERS TYPE II (YEL/YEL)			26				26		
REFLECTORIZED PAINT PAVEMENT MARKING WHITE (4")			2275					2275	
REFLECTORIZED PAINT PAVEMENT MARKING YELLOW (4")			2275						2275
TOTALS:					2020	1732	26	2275	2275

NOTE: THIS IS A LOW TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

NOTE: THE 4" YELLOW STRIPING QUANTITY HAS BEEN ESTIMATED BASED ON A DOUBLE YELLOW CENTERLINE STRIPE FOR THE ENTIRE PROJECT. THE PROJECT MUST BE MARKED FOR PASSING/NO PASSING ZONES PRIOR TO THE PLACEMENT OF ANY FINAL STRIPING. CONTACT THE MAINTENANCE DIVISION AFTER THE FINAL LIFT OF SURFACE COURSE HAS BEEN PLACED TO SCHEDULE THE ZONING OF THE PROJECT.

ADVANCE WARNING SIGNS AND DEVICES

SIGN NUMBER	DESCRIPTION	SIGN SIZE	STAGE 1	STAGE 2	STAGE 3	END OF JOB	MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED		VERTICAL PANELS	TRAFFIC DRUMS	BARRICADES (TYPE III)		
								NO.	SQ. FT.			EACH	RIGHT	LEFT
													LIN. FT.	
W20-1	ROAD WORK 1500 FT.	48"x48"	2	2	2		2	2	32.0					
W20-1	ROAD WORK 1000 FT.	48"x48"	2	2	2		2	2	32.0					
W20-1	ROAD WORK 500 FT.	48"x48"	2	2	2		2	2	32.0					
G20-2	END ROAD WORK	48"x24"	2	2	2		2	2	16.0					
R11-2	ROAD CLOSED	48"x30"	2	2	2		2	2	20.0					
RSP-1	SHOULDER CLOSED	48"x30"	2	2	2		2	2	20.0					
W1-6	LARGE ARROW	48"x24"	2	2	2		2	2	16.0					
R4-1	DO NOT PASS	24"x30"	2	2	2		2	2	10.0					
	VERTICAL PANELS			15			15			15				
	TRAFFIC DRUMS		23	14	28		28				28			
	TYPE III BARRICADE-RT. (8')		2	2	2		2					16		
	TYPE III BARRICADE-LT. (8')		2	2	2		2						16	
TOTALS:									178.0	15	28	16	16	

NOTE: THIS IS A LOW TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

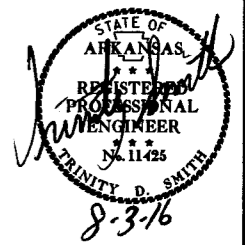
7/25/2016

R080444.DGN

QUANTITIES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	080444		17	52

② QUANTITIES



CLEARING AND GRUBBING

STATION	STATION	CLEARING	GRUBBING
STATION			
112+10	122+71	11	11
TOTALS:		11	11

REMOVAL AND DISPOSAL OF CULVERTS

STATION	DESCRIPTION	PIPE CULVERTS
		EACH
112+35	30" X 18" X 40' ARCH CM PIPE W/ FES	1
TOTAL:		1

NOTE: QUANTITY SHOWN ABOVE SHALL INCLUDE REMOVAL & DISPOSAL OF ALL HEADWALLS AND FLARED END SECTIONS IF APPLICABLE.

ACHM PATCHING OF EXISTING ROADWAY

DESCRIPTION	TON
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	10
TOTAL:	10

NOTE: QUANTITY ESTIMATED. SEE SECTION 104.03 OF THE STD. SPECS.

REMOVAL OF EXISTING BRIDGE STRUCTURE

STATION	STATION	LOCATION	LUMP SUM
114+54	115+31	77' X 22' ROADWAY W/ CONC. DECK AND 3-SPAN 6' STONE PIER BRIDGE (SITE NO. 1)	1.00
TOTAL:			

4" PIPE UNDERDRAIN

STATION	STATION	LOCATIONS	4" PIPE UNDERDRAINS	UNDERDRAIN OUTLET PROTECTORS
			LIN. FT.	EACH
* ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER			500	4
TOTALS:			500	4

* NOTE: QUANTITIES ESTIMATED. SEE SECTION 104.03 OF THE STD. SPECS.

CONCRETE COMBINATION CURB AND GUTTER

STATION	STATION	LOCATION	TYPE B-1 (1' 6")
			LIN. FT.
112+10	112+30	ON LT.	20
121+00	121+71	ON RT.	71
TOTAL:			91

SOIL LOG

STATION	LOCATION	DEPTH	LIQUID LIMIT	PLASTICITY INDEX	AASHTO CLASSIFICATION	COLOR
		FEET				
111+00	7' RT.	0-5	28	12	A-2-6(0)	BR/GR
111+00	25' RT.	0-5	21	9	A-4(0)	BROWN
119+00	25' LT.	0-5	28	12	A-6(2)	BROWN
111+00	25' RT.	0-5	19	5	A-2-4(0)	BROWN

SOIL CHARACTERISTICS TABULATED ABOVE ARE REPRESENTATIVE AT THE LOCATION OF THE SAMPLE, AND FROM SURFACE INDICATIONS ARE TYPICAL FOR THE LIMITS SHOWN. THESE DATA ARE SHOWN FOR INFORMATION ONLY. THE STATE WILL NOT BE RESPONSIBLE FOR VARIATIONS IN THE SOIL CHARACTERISTICS AND/OR EXTENT OF SAME DIFFERING FROM THE ABOVE TABULATIONS.

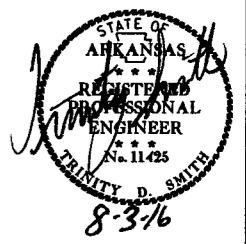
ASPHALT CONCRETE PATCHING FOR MAINTENANCE OF TRAFFIC

LOCATION	TON	TACK COAT
		GALLON
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	6	12
TOTALS:	6	12

BASIS OF ESTIMATE:
 ASPHALT CONCRETE PATCHING FOR MAINTENANCE OF TRAFFIC...25 TON/MILE
 TACK COAT FOR MAINTENANCE OF TRAFFIC.....50 GAL./MILE

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	080444		18	52

② QUANTITIES



STRUCTURES

STATION	DESCRIPTION	CROSS DRAIN ALT.		F.E.S. ALT.		SPAN	HEIGHT	THREE-SIDED PRECAST CULVERT (28' X 12')	CLASS S CONCRETE-BRIDGE	REINF. STEEL-BRIDGE (GRADE 60)	UNCL. EXC. FOR STR.-BRIDGE	BRIDGE NAME PLATE (TYPE D)	SOLID SODDING	WATER	STD. DWG. NOS.
		R.C.	C.M.	R.C.	C.M.										
		36" X 23"	35" X 24"	36" X 23"	35" X 24"										
LIN. FT.		EACH		LIN. FT.		CU.YD.	POUND	CU.YD.	EACH	SQ.YD.	M.GAL.				
112+36	CONST. W/36"X23" PIPE CULVERT W/ FES ON RT.	43	48	2	2								13	0.16	FES-1, FES-2, PCC-1, PCM-1
SUBTOTALS:		43	48	2	2								13	0.16	
STRUCTURES OVER 20' - 0" SPAN															
115+55	THREE-SIDED PRECAST CULVERT (28'X12')					28	12	249	460.00	33300	1400	1	66	0.83	PBC-1, RCB-1, RCB-2, SPECIAL DETAILS
SUBTOTALS:								249	460.00	33300	1400	1	66	0.83	
TOTALS:		43	48	2	2			249	460.00	33300	1400	1	79	0.99	

BASIS OF ESTIMATE:
WATER.....12.6 GAL. / SQ. YD. OF SOLID SODDING

NOTE: FOR R.C. PIPE CULVERT INSTALLATIONS USE TYPE 3 BEDDING UNLESS OTHERWISE SPECIFIED.

NOTE: FOR C.M. PIPE CULVERT INSTALLATIONS USE TYPE 2 BEDDING UNLESS OTHERWISE SPECIFIED.

EROSION CONTROL

STATION	STATION	LOCATION	PERMANENT EROSION CONTROL					TEMPORARY EROSION CONTROL									
			SPECIAL SEEDING	LIME	SPECIAL MULCH COVER	WATER	SPECIAL SECOND SEEDING	TEMPORARY SEEDING	SPECIAL MULCH COVER	WATER	SAND BAG DITCH CHECKS	ROCK DITCH CHECKS	SILT FENCE	SEDIMENT BASIN	OBLITERATION OF SEDIMENT BASIN	*SEDIMENT REMOVAL & DISPOSAL	
			ACRE	TON	ACRE	M.GAL.	ACRE	ACRE	ACRE	M.GAL.	(E-5) BAG	(E-6) CU.YD.	(E-11) LIN. FT.	(E-14) CU.YD.	CU.YD.	CU. YD.	
ENTIRE PROJECT		CLEARING AND GRUBBING								2.02	2.02	41.2					
ENTIRE PROJECT		STAGE 1	0.71	1.42	0.71	72.4	0.71	0.71	14.5	66	6	400	50	50		113	
ENTIRE PROJECT		STAGE 2	0.25	0.50	0.25	25.5	0.25	0.25	5.1		6	100				20	
		STAGE 3 OBLITERATION AREA	0.16	0.32	0.16	16.3	0.16									6	
*ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.			1.00	2.00	1.00	102.0	1.00										
TOTALS:			2.12	4.24	2.12	216.2	2.12	2.98	2.98	60.8	66	12	2195	50	50	139	

BASIS OF ESTIMATE:
LIME2 TONS / ACRE OF SEEDING
WATER.....102.0 M.G. / ACRE OF SEEDING
WATER.....20.4 M.G. / ACRE OF TEMPORARY SEEDING
SAND BAG DITCH CHECKS.....22 BAGS / LOCATION
ROCK DITCH CHECKS.....3 CU.YD./LOCATION

NOTE: THE TEMPORARY EROSION CONTROL DEVICES SHOWN ABOVE AND ON THE PLANS SHALL BE INSTALLED IN SUCH A SEQUENCE AS TO DETER EROSION AND SEDIMENTATION ON U.S. WATERWAYS AS EXPLAINED BY THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM PERMIT.

*QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

8/2/2016 R080444.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		19	52
				JOB NO.	080444			

FENCE ITEMS

STATION	STATION	LOCATION	WIRE FENCE (TYPE D)	* 8'-0" GATES
			LIN. FT.	EACH
116+50	116+60	LT. SIDE	45	
116+60	118+76	LT. SIDE	215	
119+00	119+55	LT. SIDE	55	
118+90		LT. SIDE		2
TOTAL:			315	2

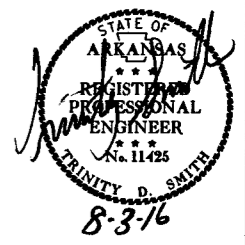
* DENOTES ALTERNATE BID ITEM.

SELECTED PIPE BEDDING

LOCATION	SELECTED PIPE BEDDING
	CU. YD.
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	50
TOTAL:	50

NOTE: QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

QUANTITIES



REMOVAL AND DISPOSAL OF FENCE

STATION	STATION	LOCATION	FENCE
			LIN. FT.
118+10	119+10	LT. SIDE	150
120+07	120+19	LT. SIDE	12
TOTALS:			162

EROSION CONTROL MATTING

STATION	STATION	LOCATION	LENGTH	CLASS 3
			LIN. FT.	SQ. YD.
113+00.00	114+40.00	RT. SIDE	140.00	124.44
116+00.00	118+00.00	RT. SIDE	200.00	177.78
116+00.00	119+00.00	LT. SIDE	300.00	266.67
TOTAL:				568.89

NOTE: AVERAGE WIDTH = 8'-0"

COLD MILLING ASPHALT PAVEMENT

STATION	STATION	LOCATION	AVG. WIDTH	COLD MILLING ASPHALT PAVEMENT
			FEET	SQ. YD.
111+14.00	113+27.00	MAIN LANES	22.00	520.67
120+80.00	122+47.00	MAIN LANES	22.00	408.22
TOTAL:				928.89

NOTE: AVERAGE MILLING DEPTH 1".

DUMPED RIPRAP AND FILTER BLANKET

STATION	LOCATION	DUMPED RIPRAP (TYPE SPECIAL)	FILTER BLANKET
		CU. YD.	SQ. YD.
113+00	OUTLET OF PIPE CULVERT	3	12
	* TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	10	25
TOTALS:		13	37

*NOTE: QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STANDARD SPECIFICATIONS

NOTE: FILTER BLANKET SHALL BE GEOTEXTILE FABRIC (TYPE 5).

REMOVAL AND DISPOSAL OF ITEMS

STATION	STATION	LOCATION	GUARDRAIL
			LIN. FT.
113+84	115+76	EXISTING ROADWAY	192
114+12	116+00	EXISTING ROADWAY	188
118+90		LT. SIDE - MAIN LANES	
119+84		LT. SIDE - MAIN LANES	
TOTALS:			380

NOTE: THE QUANTITY SHOWN ABOVE FOR THE REMOVAL AND DISPOSAL OF GUARDRAIL SHALL INCLUDE THE REMOVAL AND DISPOSAL OF ALL GUARDRAIL TERMINALS AND TERMINAL ANCHOR POSTS.

EARTHWORK

STATION	STATION	LOCATION / DESCRIPTION	UNCLASSIFIED EXCAVATION	COMPACTED EMBANKMENT	* TOPSOIL FURNISHED AND PLACED	* SOIL STABILIZATION
			CU. YD.	CU. YD.	CU. YD.	TON
ENTIRE PROJECT		STAGE 1-MAIN LANES	361	6883		
ENTIRE PROJECT		STAGE 2-MAIN LANES	133	3		
ENTIRE PROJECT		STAGE 3-OBLITERATION OF EXISTING ROADWAY	1328	5		
ENTIRE PROJECT		DRIVES		140		
ENTIRE PROJECT		CHANNEL CHANGE	2200			
ENTIRE PROJECT		TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER			500	100
TOTALS:			4022	7031	500	100

* QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

NOTE: EARTHWORK QUANTITIES SHOWN ABOVE SHALL BE PAID AS PLAN QUANTITY.

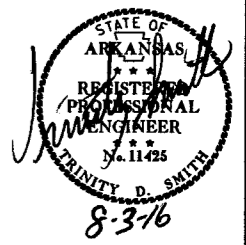
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		20	52
				JOB NO. 080444				

CONCRETE DITCH PAVING

STATION	STATION	LOCATION	LENGTH		CONCRETE DITCH PAVING	SOLID SODDING	WATER
			LIN. FT.	FEET	(TYPE B-1) SQ. YD.	SQ. YD.	M. GAL.
113+50.00	115+00.00		150.00	4.00	66.67	66.67	0.84
TOTALS:					66.67	66.67	0.84

BASIS OF ESTIMATE:
WATER.....12.6 GAL. / SQ. YD. OF SOLID SODDING.

② QUANTITIES



BENCH MARKS

STATION	LOCATION	BENCH MARKS
		EACH
114+97	RT. HEADWALL	1
TOTAL:		1

NOTE: SHOWN FOR INFORMATION ONLY. BENCH MARKS SHALL BE FURNISHED AND PLACED BY STATE FORCES.

DRIVEWAYS & TURNOUTS

STATION	SIDE	DESCRIPTION	WIDTH	ACHM SURFACE COURSE (3/8") 220 LBS. PER SQ. YD. (PG 64-22)		AGGREGATE BASE COURSE (CLASS 7)	STANDARD DRAWINGS
			FEET	SQ. YD.	TON	TON	
113+27	LT.	PRIVATE DRIVE	16	37.01	4.07	80.44	
119+74	LT.	PRIVATE DRIVE	16	37.01	4.07	61.78	
* ENTIRE PROJECT TEMPORARY DRIVES						20.00	
TOTALS:				74.02	8.14	162.22	

BASIS OF ESTIMATE:
ACHM SURFACE COURSE (3/8").....94.6% MIN. AGGR.....5.4% ASPHALT BINDER
MAXIMUM NUMBER OF GYRATIONS = 115 FOR PG 64-22

* QUANTITY ESTIMATED
SEE SECTION 104.03 OF THE STD. SPECS.
TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

PAVEMENT REPAIR OVER CULVERTS (ASPHALT)

STATION	LOCATION	WIDTH	LENGTH	TON
		FEET		
112+36	MAIN LANES	9.08	28	3
113+00	MAIN LANES	9.08	28	3
TOTAL:				6

AVG. DEPTH = 2"

BASE AND SURFACING

STATION	STATION	LOCATION	LENGTH FEET	AGGREGATE BASE COURSE (CLASS 7)		TACK COAT				ACHM SURFACE COURSE (3/8")			
				TON / STATION	TON	AVG. WID. FEET	SQ.YD.	GALLONS / SQ.YD.	GALLON	AVG. WID. FEET	SQ.YD.	POUND / SQ.YD.	TOTAL PG 64-22 TON
MAIN LANES													
111+10.00	112+10.00	100' TRANSITION	100.00	43.75	43.75	20.00	222.22	0.05	11.11	28.00	311.11	220.00	34.22
112+10.00	112+36.00	MAIN LANES - NOTCH & WIDEN	26.00	80.50	20.93	20.00	57.78	0.17	9.82	28.00	80.89	220.00	8.90
112+36.00	112+40.00	MAIN LANES - FULL DEPTH	4.00	223.00	8.92					28.00	12.44	220.00	1.37
112+40.00	121+00.00	MAIN LANES - FULL DEPTH	860.00	230.00	1978.00					28.00	2675.56	220.00	294.31
121+00.00	121+71.29	MAIN LANES - NOTCH & WIDEN	71.29	80.50	57.39	20.00	158.42	0.17	26.93	28.00	221.79	220.00	24.40
121+71.29	122+71.29	100' TRANSITION	100.00	43.75	43.75	20.00	222.22	0.05	11.11	28.00	311.11	220.00	34.22
112+10.00	112+36.00	LEVELING	26.00			20.00	57.78	0.05	2.89	20.00	57.78	220.00	6.36
121+00.00	121+71.00	LEVELING	71.00			20.00	157.78	0.05	7.89	20.00	157.78	220.00	17.36
112+78.40	114+74.30	SUPERELEVATION	195.90	VAR.	210.00								
119+77.97	119+97.78	SUPERELEVATION	19.81	VAR.	15.00								
ENTIRE	PROJECT	CONNECTIONS				20.00	511.11	0.05	25.56				150.00
TOTALS:				2377.74			1387.31		95.31		3828.46		571.14

BASIS OF ESTIMATE:
ACHM SURFACE COURSE (3/8").....94.6% MIN. AGGR.....5.4% ASPHALT BINDER
MAXIMUM NUMBER OF GYRATIONS = 115 FOR PG 64-22
TACK COAT QUANTITIES WERE CALCULATED USING THE EMULSIFIED ASPHALT RATES. REFER TO SS-400-1 FOR THE RESIDUAL ASPHALT APPLICATION RATES.

QUANTITIES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. 080444	22	52

② SURVEY CONTROL DETAILS

SURVEY CONTROL COORDINATES

Project Name: s080444
Date: 8/1/2012
Coordinate System: ARKANSAS STATE PLANE - NORTH ZONE BASED ON GPS CONTROL,
360012-360012A PROJECTED TO GROUND.
Units: U. S. SURVEY FOOT

Point Name	Northing	Easting	Elev	Feature	Description
1	493008.2318	941141.9425	767.092	CTL	5/8" Rebar with 2" Aluminum Cap
2	493149.7277	941760.9624	767.768	CTL	5/8" Rebar with 2" Aluminum Cap
3	493223.9549	942300.1862	755.677	CTL	5/8" Rebar with 2" Aluminum Cap
4	492709.1850	943133.6359	766.373	CTL	5/8" Rebar with 2" Aluminum Cap
100	492768.7797	942823.2214	758.352	GPS	AHTD CAP GPS 360012
101	492574.0370	943672.2791	766.536	GPS	AHTD CAP GPS 360012A
901	493359.7924	942538.7326	815.634	TBM	CHISELED SQUARE CENTER OF 3X4' CONCRETE
902	493359.7924	942538.7326	757.491	TBM	NO DESCRIPTION AVAILABLE
990	486370.4645	935517.9496	1007.042	BM	NO DESCRIPTION AVAILABLE
991	492875.1504	940882.4094	776.379	BM	NO DESCRIPTION AVAILABLE
992	489395.3100	936217.0912	867.590	BM	NO DESCRIPTION AVAILABLE

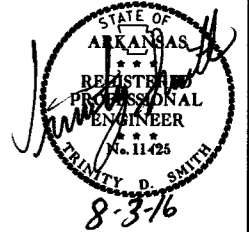
*Note - Rebar and Cap - Standard - 5/8" Rebar with 2" Aluminum Cap stamped
*(standard markings common to all caps), or as indicated
(other markings indicated in the point description of the individual point).
USE CAF = 1.0 FOR STAKEOUT FOR THIS PROJECT
A PROJECT CAF OF 0.9998990040 HAS BEEN USED TO COMPUTE THE GROUND COORDINATES LISTED ABOVE.
THIS CAF IS INTENDED FOR USE WITHIN THE PROJECT LIMITS.
GRID DISTANCE = GROUND DISTANCE X CAF.
GRID COORDINATES ARE STORED UNDER FILE NAME. S080444G1.CTL
HORIZONTAL DATUM: NAD 83 (1997)
VERTICAL DATUM: NAVD 88 POSITIONAL ACCURACY THIRD ORDER, UNLESS SPECIFIED OTHERWISE
AT A SPECIFIC POINT.

REFERENCE POINTS (1500 SERIES) ARE TO BE USED TO ESTABLISH CONTROL
IF THE PRIMARY CONTROL POINTS LISTED ABOVE HAVE BEEN DESTROYED.
REFERENCE POINTS ARE NOT TO BE USED FOR VERTICAL CONTROL

BASIS OF BEARING:
ARKANSAS STATE PLANE GRID BEARINGS - 0301-NORTH ZONE
DETERMINED FROM GPS CONTROL POINTS: 360012 - 360012A
CONVERGENCE ANGLE: 00 43 31 LEFT AT LT: 35-40-54.6 LG: 093-14-46.9
GRID AZIMUTH = ASTRONOMICAL AZIMUTH - CONVERGENCE ANGLE.

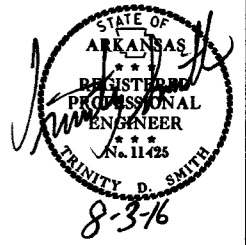
CONST.

POINT NO.	TYPE	STATION	NORTHING	EASTING
8000	POB	100+00.03	492974.53	941068.09
8001	PC	101+10.04	493043.41	941153.86
8003	PT	105+24.86	493173.30	941538.90
8004	PC	106+25.18	493170.70	941639.18
8006	PT	108+92.14	493187.04	941905.30
8007	PC	110+49.94	493210.40	942061.36
8009	PT	111+78.98	493226.11	942189.43
8010	PC	111+79.01	493226.12	942189.45
8012	PT	116+32.35	493046.24	942583.17
8013	PC	118+24.26	492889.83	942694.38
8015	PT	121+71.29	492712.84	942982.09
8017	POE	122+71.24	492697.88	943080.91

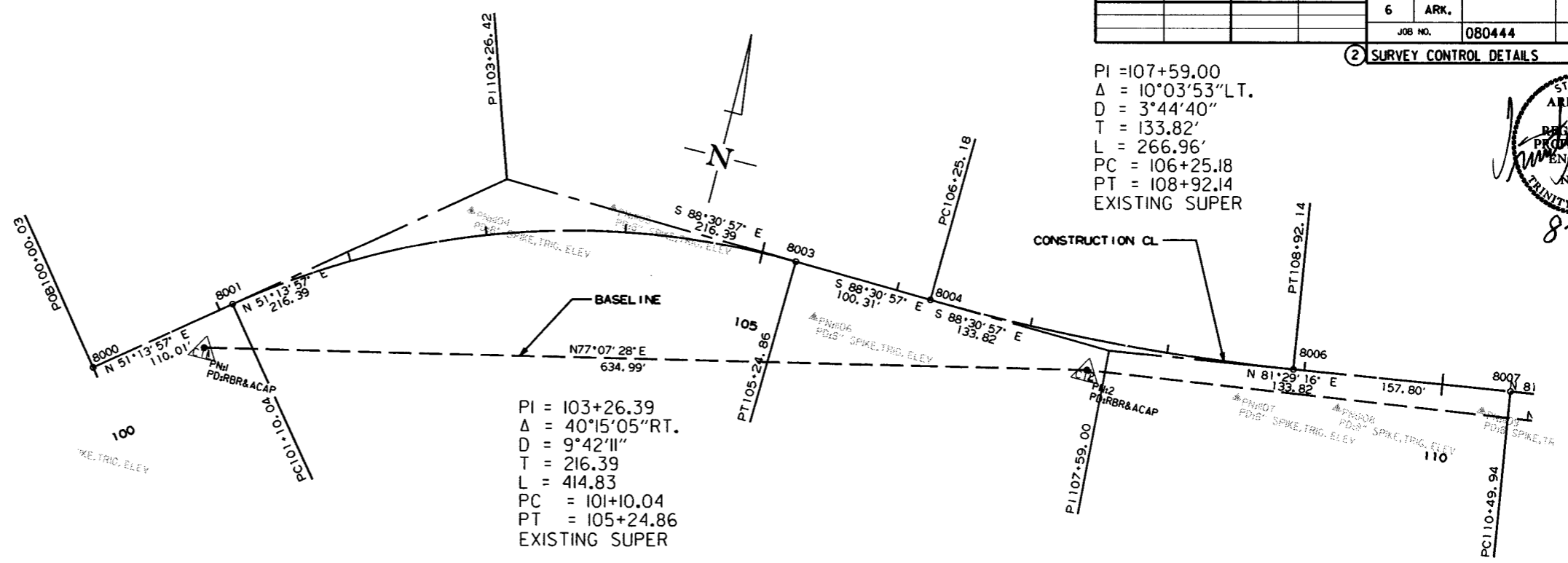


DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		23	52
				JOB NO.		080444	23	52

2 SURVEY CONTROL DETAILS

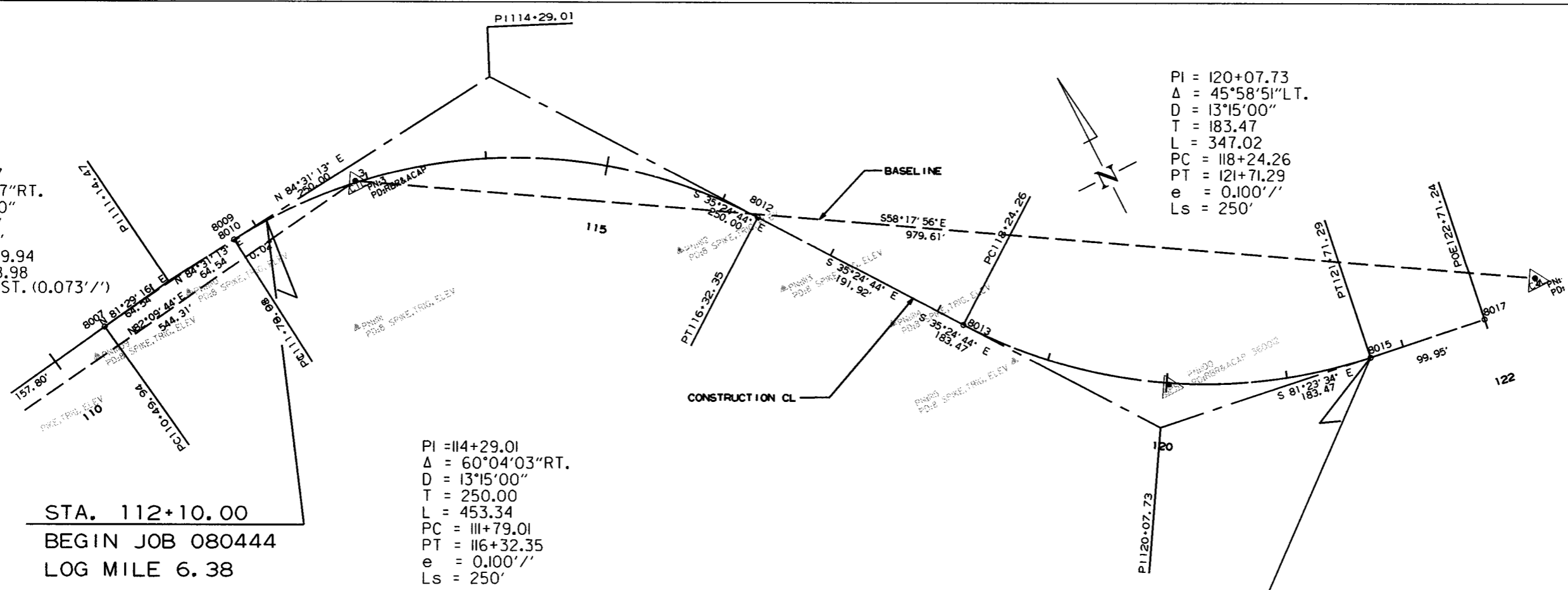


PI = 107+59.00
 $\Delta = 10^{\circ}03'53''$ LT.
 $D = 3^{\circ}44'40''$
 $T = 133.82'$
 $L = 266.96'$
 $PC = 106+25.18$
 $PT = 108+92.14$
 EXISTING SUPER



PI = 103+26.39
 $\Delta = 40^{\circ}15'05''$ RT.
 $D = 9^{\circ}42'11''$
 $T = 216.39$
 $L = 414.83$
 $PC = 101+10.04$
 $PT = 105+24.86$
 EXISTING SUPER

PI = 120+07.73
 $\Delta = 45^{\circ}58'51''$ LT.
 $D = 13^{\circ}15'00''$
 $T = 183.47$
 $L = 347.02$
 $PC = 118+24.26$
 $PT = 121+71.29$
 $e = 0.100'/'$
 $Ls = 250'$



PI = 111+14.47
 $\Delta = 3^{\circ}01'57''$ RT.
 $D = 2^{\circ}21'00''$
 $T = 64.54'$
 $L = 129.05'$
 $PC = 110+49.94$
 $PT = 111+78.98$
 MATCH EXIST. (0.073'/'')

PI = 114+29.01
 $\Delta = 60^{\circ}04'03''$ RT.
 $D = 13^{\circ}15'00''$
 $T = 250.00$
 $L = 453.34$
 $PC = 111+79.01$
 $PT = 116+32.35$
 $e = 0.100'/'$
 $Ls = 250'$

STA. 112+10.00
 BEGIN JOB 080444
 LOG MILE 6.38

STA. 121+71.29
 END JOB 080444
 LOG MILE 6.20

SURVEY CONTROL DETAILS

STA. 113+00 CONSTRUCT
 36" X 23" X 48" ARCH PIPE CULVERT
 W/F, E, S, LT. & RT.
 R.C. PIPE (CLASS 111), (TYPE 2 BEDDING) = 43 LIN. FT.
 C.M. PIPE (TYPE 2 BEDDING) = 48 LIN. FT.
 Q25 = 9.58 CFS, DA = 18.50 AC

STA. 113+27 CONSTRUCT
 DRIVEWAY ON LT.
 CONSTRUCT APPROACH = 70 CU. YDS.

STA. 112+10 TO STA. 112+30
 CONSTRUCT CURB & GUTTER ON LT.
 CURB & GUTTER (TYPE B-1) = 20 LIN. FT.

STA. 112+35.54 IN PLACE
 30" X 18" X 40" ARCH CM PIPE
 CULVERT W/FES
 REMOVE

PI = 111+14.47
 $\Delta = 3^{\circ}01'57''$ RT.
 D = 2*21'00"
 T = 64.54'
 L = 129.05'
 PC = 110+49.94
 PT = 111+78.98
 MATCH EXISTING
 SUPERELEVATION (0.073'/'')

STA. 112+10.00
 BEGIN JOB 080444
 LOG MILE 6.38

STA. 115+55.00 CONSTRUCT
 TRI. 28' X 12' X 83'
 THREE-SIDED PRECAST CULVERT
 ON 35' LT. FWD SKEW
 W/WINGS ON 3+1 LT. & RT.
 TOTAL SPAN = 110'-7 1/2"
 Q25 = 7000 CFS DA = 15.3 SQ. MI.

PI = 114+29.01
 $\Delta = 60^{\circ}04'03''$ RT.
 D = 13*15'00"
 T = 250.00'
 L = 453.34'
 PC = 111+79.01
 PT = 116+32.35
 e = 0.100'/'

STA. 114+54 TO STA. 115+31 IN PLACE
 77' X 22' BRIDGE CONSISTING OF CONC. DECK
 WITH 3 SPAN 6' CONCRETE/STONE PIER
 REMOVE AS EXISTING BRIDGE STRUCTURE (SITE NO. 1) = 1.00 LUMP SUM

PI = 120+07.73
 $\Delta = 45^{\circ}58'51''$ LT.
 D = 13*15'00"
 T = 183.47'
 L = 347.02'
 PC = 118+24.26
 PT = 121+71.29
 e = 0.100'/'

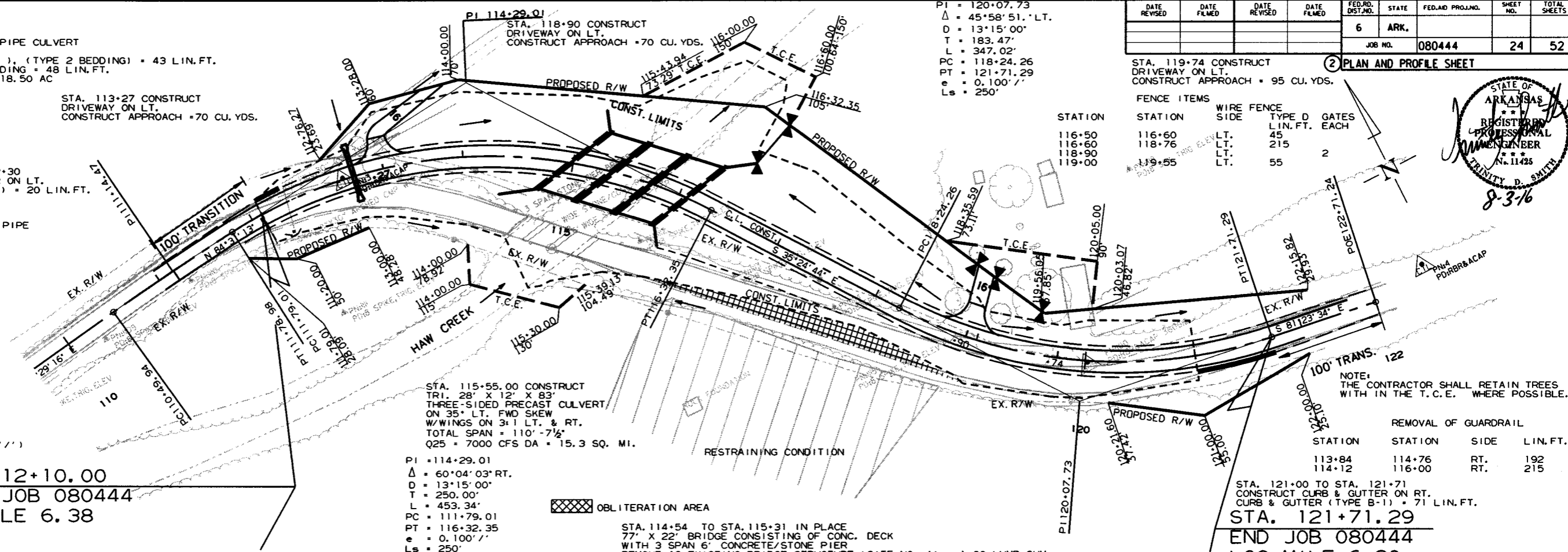
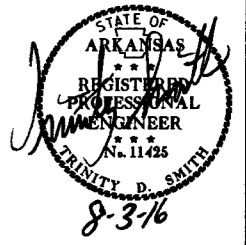
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		24	52

STA. 119+74 CONSTRUCT
 DRIVEWAY ON LT.
 CONSTRUCT APPROACH = 95 CU. YDS.

FENCE ITEMS

STATION	STATION	WIRE FENCE SIDE	TYPE D LIN. FT.	GATES EACH
116+50	116+60	LT.	45	
116+60	118+76	LT.	215	
118+90		LT.		2
119+55		LT.	55	

PLAN AND PROFILE SHEET

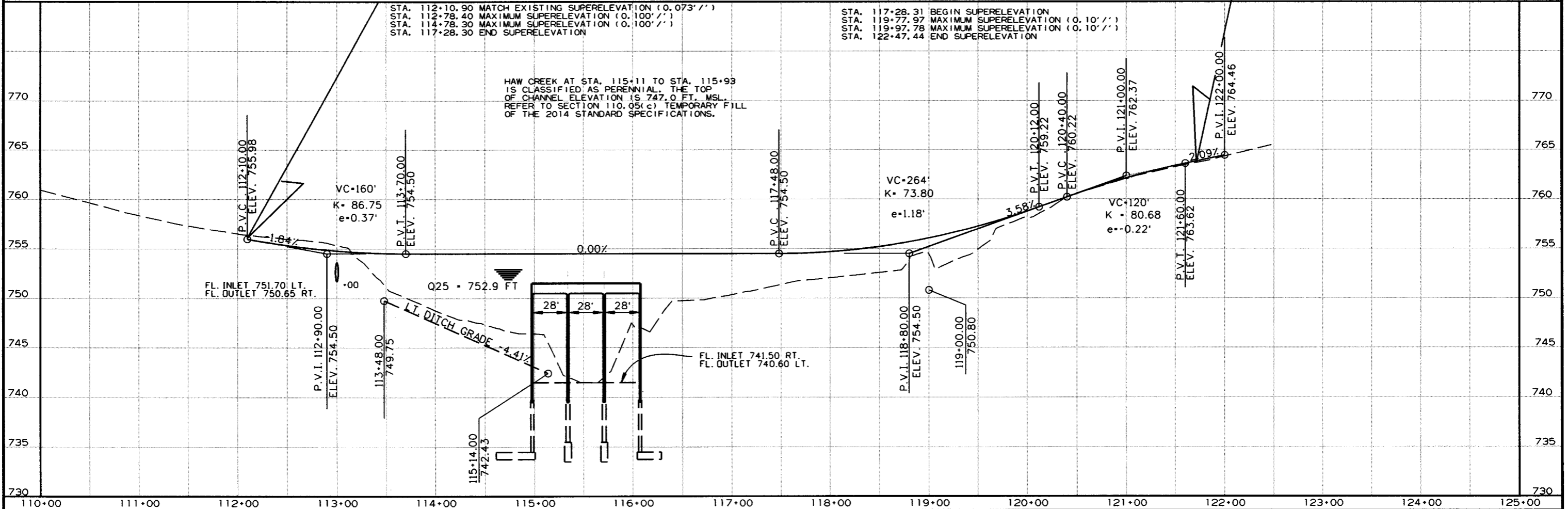


REFER TO SURVEY CONTROL DETAIL SHEETS FOR HORIZONTAL AND VERTICAL CONTROL DATA.

STA. 112+10.90 MATCH EXISTING SUPERELEVATION (0.073'/'')
 STA. 112+78.40 MAXIMUM SUPERELEVATION (0.100'/'')
 STA. 114+78.30 MAXIMUM SUPERELEVATION (0.100'/'')
 STA. 117+28.30 END SUPERELEVATION

STA. 117+28.31 BEGIN SUPERELEVATION
 STA. 119+77.97 MAXIMUM SUPERELEVATION (0.10'/'')
 STA. 119+97.78 MAXIMUM SUPERELEVATION (0.10'/'')
 STA. 122+47.44 END SUPERELEVATION

HAW CREEK AT STA. 115+11 TO STA. 115+93
 IS CLASSIFIED AS PERENNIAL. THE TOP
 OF CHANNEL ELEVATION IS 747.0 FT. MSL.
 REFER TO SECTION 110.05(c) TEMPORARY FILL
 OF THE 2014 STANDARD SPECIFICATIONS.

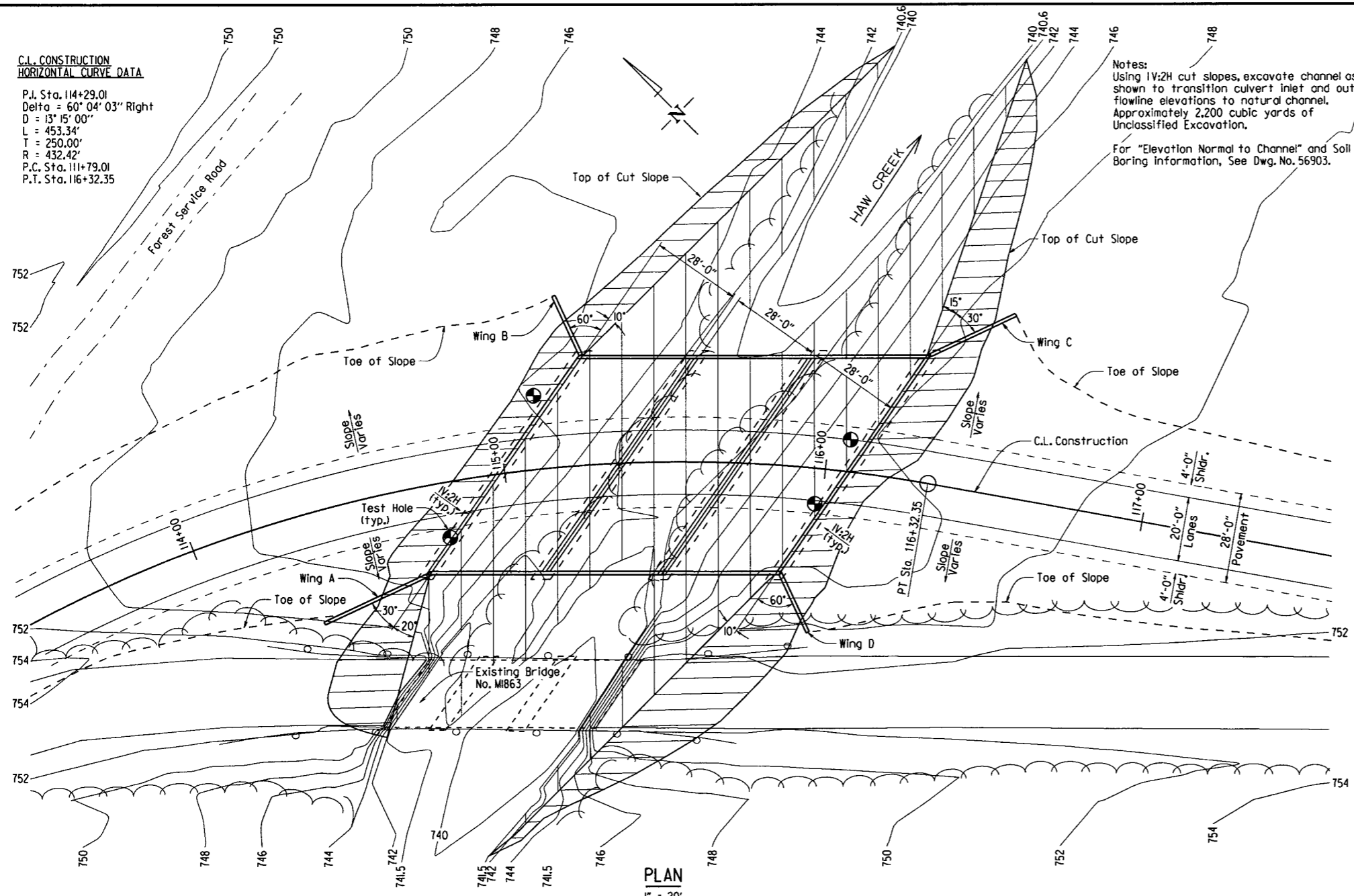


8/2/2016

RO80444.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	080444	25	52	
				07396	LAYOUT	56902		

C.L. CONSTRUCTION HORIZONTAL CURVE DATA
P.I. Sta. 114+29.01
Delta = 60° 04' 03" Right
D = 13' 15" 00"
L = 453.34'
T = 250.00'
R = 432.42'
P.C. Sta. 111+79.01
P.T. Sta. 116+32.35



Notes:
Using 1V:2H cut slopes, excavate channel as shown to transition culvert inlet and outlet flowline elevations to natural channel. Approximately 2,200 cubic yards of Unclassified Excavation.
For "Elevation Normal to Channel" and Soil Boring Information, See Dwg. No. 56903.

GENERAL NOTES

BENCHMARK: Vertical control data is shown on the Survey Control Data Sheets.

CONSTRUCTION SPECIFICATIONS: Arkansas State Highway and Transportation Department Standard Specifications for Highway Construction (2014 edition) with applicable Supplemental Specifications and Special Provisions. Unless otherwise noted in the plans, Section and Subsection refer to the Standard Construction Specifications.

DESIGN SPECIFICATIONS: AASHTO LRFD Bridge Design Specifications, Sixth Edition (2012), with 2013 Interim Revisions.

LIVE LOADING: HL-93
SEISMIC PERFORMANCE ZONE: I

BORING LOGS: Boring logs may be obtained from the Construction Contract Procurement Section of the Program Management Division.

FOOTINGS: Where rock is encountered at or above Elev. 734.50, footings shall be keyed a minimum of 1'-0" into material designated as Sandstone or Shale with Sandstone Seams on the boring legend. Where rock is encountered below Elev. 734.50, the top of the footings shall be set at Elev. 734.50. The allowable bearing capacity for footings keyed a minimum of 1'-0" into Shale with Sandstone Seams is 20,000 psf. The allowable bearing capacity for footings seated on Sand with Clay Seams and Cobbles is 6,000 psf. Foundations for footings shall be prepared in accordance with Subsection 80L04. Rock excavations shall be made to neat lines of the concrete footings. Care shall be exercised to avoid shattering of rock faces by excessive blasting. Concrete in footings keyed into rock shall be poured directly against excavated surfaces of rock. Excavations shall be backfilled and compacted to the channel flow line grade in accordance with Subsection 80L08.

THREE-SIDED PRECAST CULVERT: Precast Culvert shall be fabricated and constructed in accordance with the details shown and Job Special Provision "Three-Sided Precast Culverts".

EXISTING BRIDGE: Existing Bridge No. M1863, (L.M. 6.52) is 22.3' wide and 66' long. The existing bridge consists of corrugated metal decking with asphalt overlay on steel beams supported by rock masonry piers and abutments.

REMOVAL AND SALVAGE: After the new structure is open to traffic, existing Bridge No. M1863 shall be removed in accordance with Section 205. All material from the existing bridge shall become the property of the Contractor, except for the rails from the existing guardrail and the steel beams, which shall become the property of the State.

The Contractor shall provide temporary storage and on site loading onto AHTD equipment for removal of salvaged items from the site. Payment for this work shall be considered incidental to "Removal of Existing Bridge Structure".

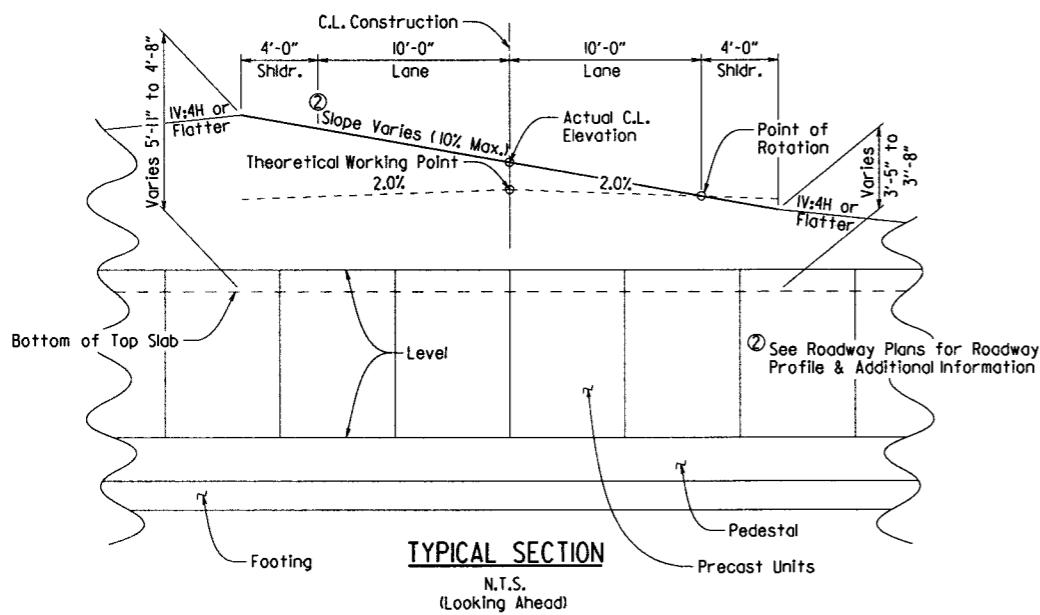
MAINTENANCE OF TRAFFIC: See Roadway Plans.

HYDRAULIC DATA

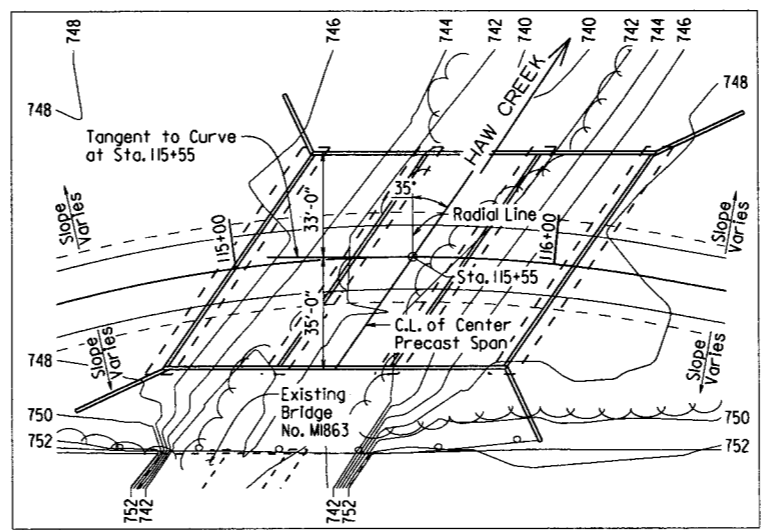
FLOOD DESCRIPTION	FREQUENCY YEARS	DISCHARGE CFS	NATURAL WATER SURFACE ELEVATION	WATER SURFACE ELEV. WITH BACKWATER
			FEET	FEET
Design	25	7,000	752.2	752.9
Base	100	10,100	753.4	755.8
Extreme	500	15,000	754.4	756.9
Overtopping	50	8,800	753.0	755.2

① Unconstricted water surface without structure or roadway approaches.
② 0100 backwater elevation for existing structure = 754.8
Hydraulic Model Proposed Low Chord Elev. = 749.60
Drainage area = 15.3 square miles
Historical H.W. Elev = 753.5

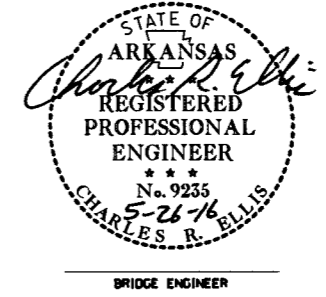
PLAN
1" = 20'



TYPICAL SECTION
N.T.S.
(Looking Ahead)



LOCATION SKETCH
1" = 30'



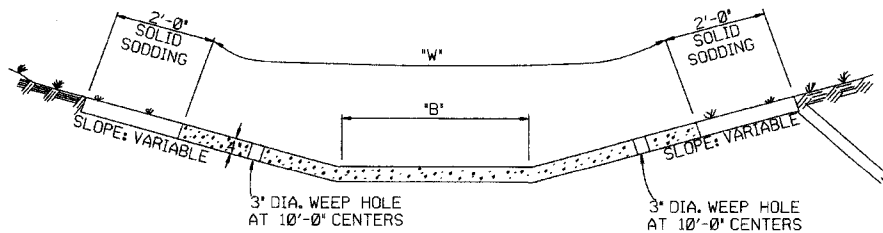
SHEET 1 OF 2
LAYOUT OF
THREE-SIDED PRECAST CULVERT
OVER HAW CREEK
HAW CREEK STR. & APPRS. (S)
JOHNSON COUNTY
ROUTE 123 SEC. 3
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.
BRIDGE NO. 07396
DRAWING NO. 56902

DRAWN BY: ACP DATE: 09-25-13 FILENAME: b080444_ll.dgn
CHECKED BY: AMS DATE: 11-21-14 SCALE: As Shown
DESIGNED BY: ACP DATE: 04-13
BRIDGE NO. 07396 DRAWING NO. 56902

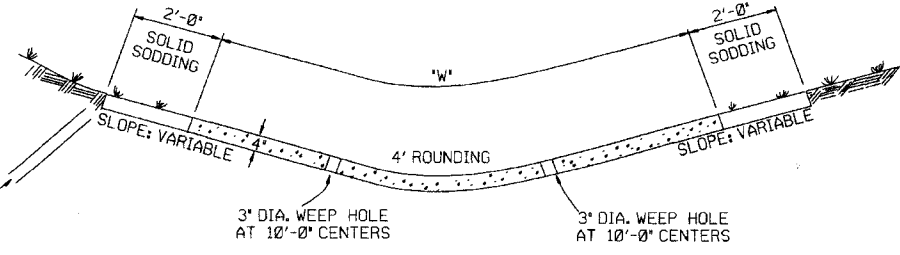
PRINT DATE: 7/7/2016

REFER TO TABULATION OF QUANTITIES FOR 'W' & 'B' DIMENSIONS

REFER TO TABULATION OF QUANTITIES FOR 'W' DIMENSIONS

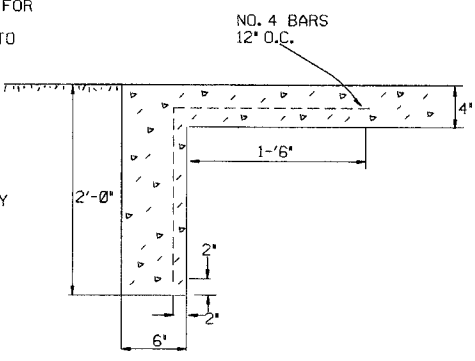


TYPE A



TYPE B

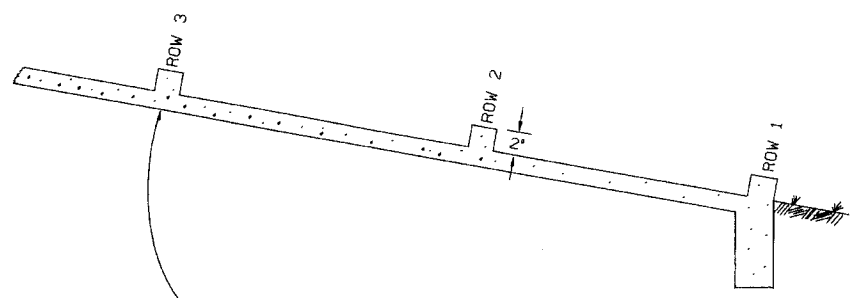
THE STEEL AND ADDITIONAL CONCRETE FOR THE WALLS SHALL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR 'CONCRETE DITCH PAVING.'



TOE WALL DETAIL FOR CONCRETE DITCH PAVING

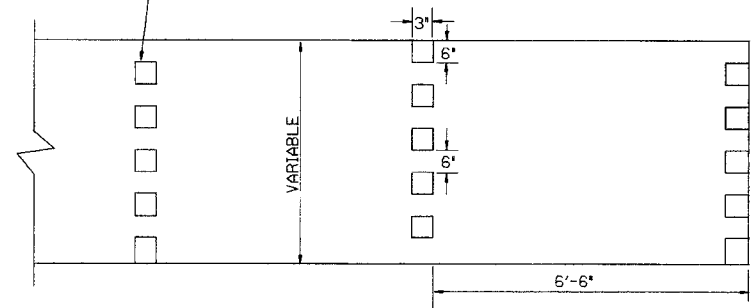
GENERAL NOTES:

- THE FULL WIDTH OF EACH SECTION SHALL BE POURED MONOLITHICALLY.
- TOE WALLS TO BE CONSTRUCTED FULL WIDTH AT EACH END OF DITCH PAVING, AND POURED MONOLITHICALLY.
- SOLID SOD ALONG DITCH PAVING TO BE PLACED WITHIN 14 DAYS OF DITCH PAVING CONSTRUCTION.
- 1" WIDE TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE DITCH PAVING AT 45' INTERVALS. THE SPACE SHALL BE FILLED WITH APPROVED JOINT FILLER COMPLYING WITH AASHTO M213.



NUMBER OF ELEMENTS PER ROW VARIES WITH WIDTH OF PAVING SPECIFIED

ENERGY DISSIPATORS TO BE USED FOR THE ENTIRE LENGTH OF DITCH WHEN SLOPE OF DITCH PAVING EXCEEDS 7%. THE DISSIPATORS WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE UNINCLUDED IN THE PRICE BID FOR CONCRETE DITCH PAVING.



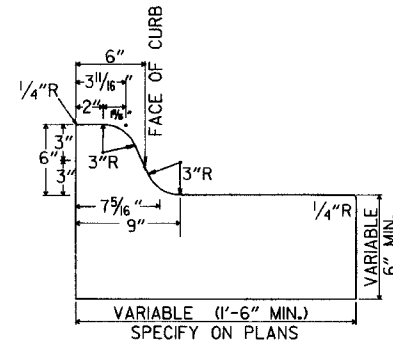
ENERGY DISSIPATORS (NO SCALE)

11-17-10	ADDED GENERAL NOTE	
6-2-94	ADDED GENERAL NOTE ABOUT SOLID SODDING	
11-30-8	ELIMINATED MIN. ROWS OF ELEMENTS	111-30-89
7-15-88	REVISED DISSIPATOR NOTE	653-7-15-88
4-3-87	REVISED ENERGY DISSIPATOR	671-4-3-87
1-9-87	MODIFIED NOTE ON ENERGY DISS.	532-1-9-87
11-3-86	ADDED NOTE TO ENERGY DISS.	596-12-1-86
11-1-84	ENERGY DISSIPATOR DETAILS ADDED	508-11-1-84
11-1-84	EXCAVATION DETAILS ADDED	
	TYPED A & B	
10-2-72	REVISED AND REDRAWN	508-10-2-72
DATE	REVISION	DATE FILM'D

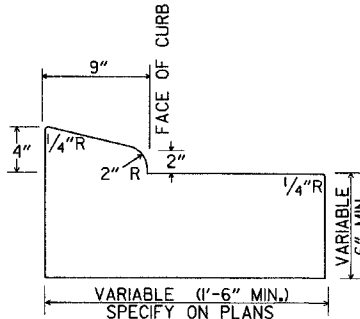
ARKANSAS STATE HIGHWAY COMMISSION

CONCRETE DITCH PAVING

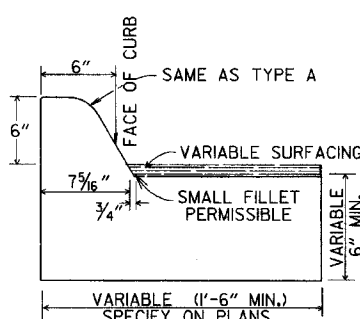
STANDARD DRAWING CDP-1



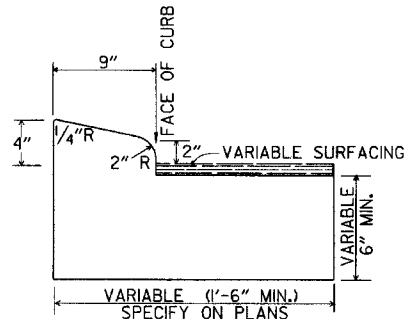
TYPE A



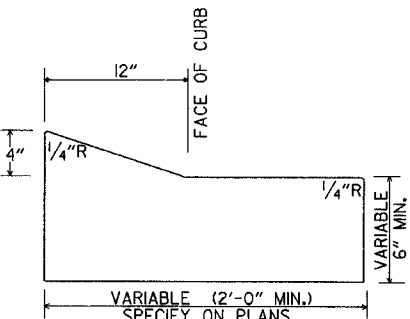
TYPE B-1



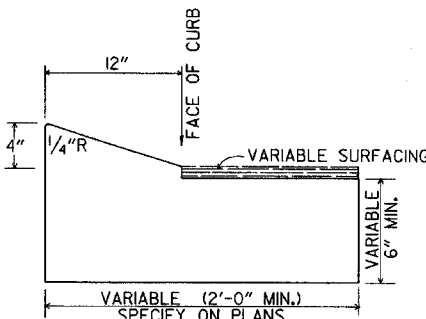
TYPE C



TYPE B-2

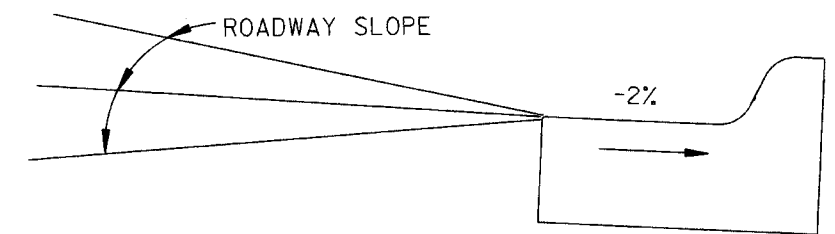


TYPE E-1

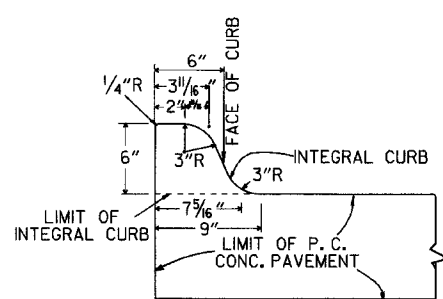


TYPE E-2

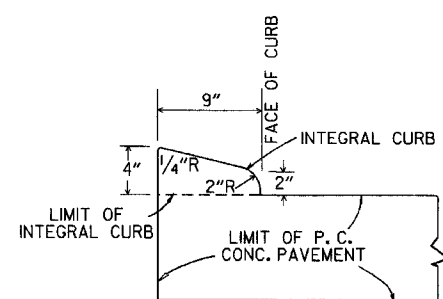
CONCRETE COMBINATION CURB AND GUTTER



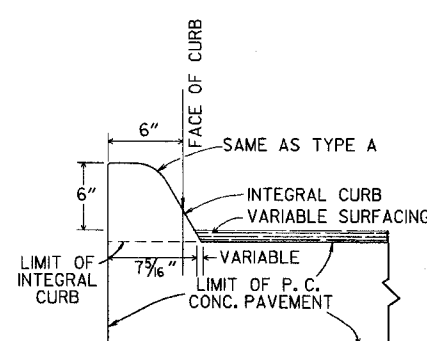
DETAIL OF GUTTER SLOPE
GUTTER SHALL BE CONSTRUCTED ON 2% SLOPE AWAY FROM ROADWAY, REGARDLESS OF ROADWAY SLOPE.



TYPE A

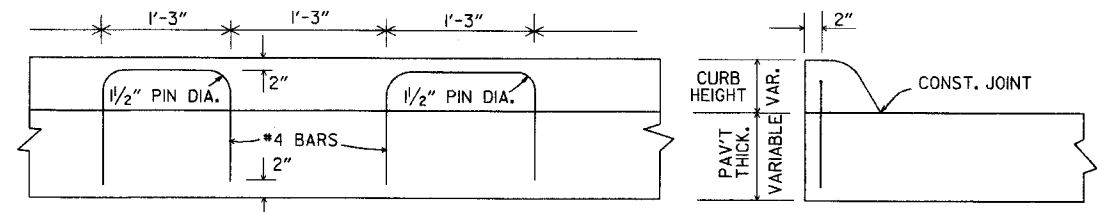


TYPE B



TYPE C

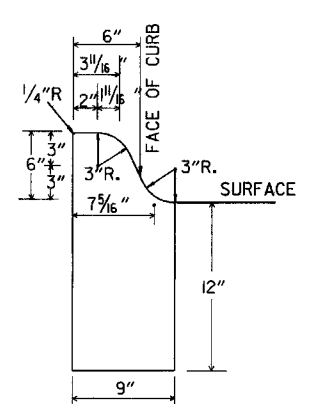
INTEGRAL CURB



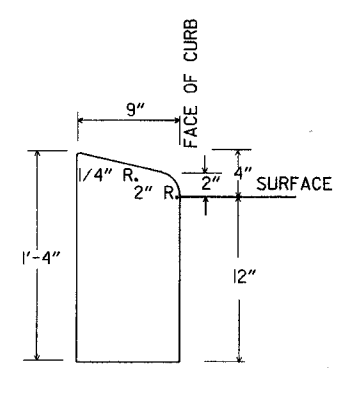
LONGITUDINAL SECTION

ELEVATION

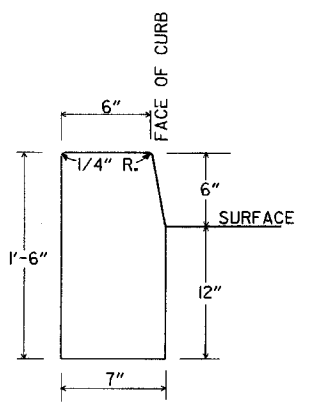
ALTERNATE CONSTRUCTION METHOD FOR INTEGRAL CURB



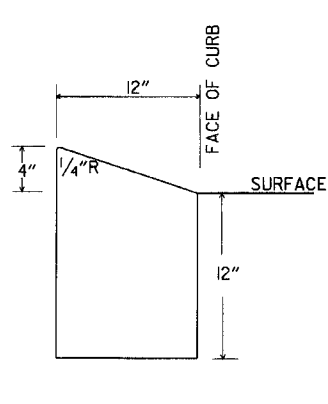
TYPE A



TYPE B

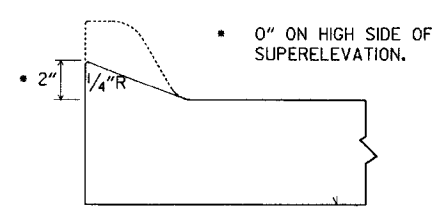


TYPE D



TYPE E

CONCRETE CURB



NOTE: USE MODIFIED CURB AS SPECIFIED ON STD. DR-1. COMPENSATION FOR MODIFIED CURB WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE TYPE OF CURB OR CURB AND GUTTER SPECIFIED.

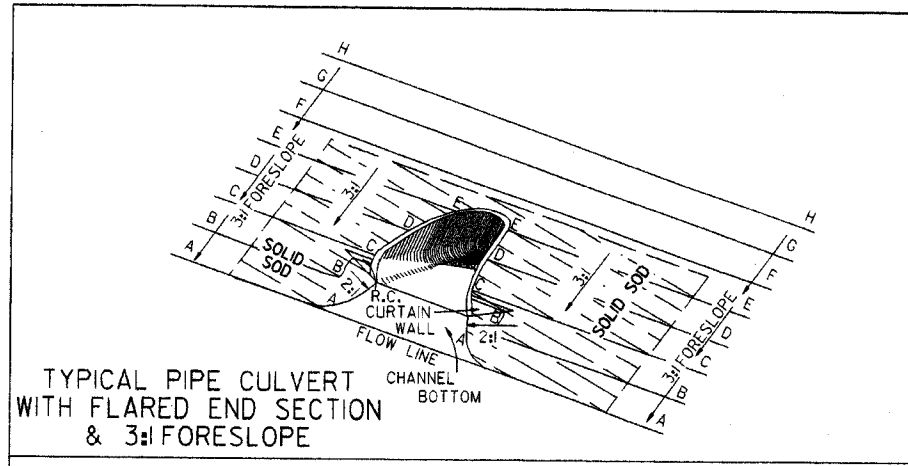
DETAILS OF MODIFIED CURB

DATE	REVISION	DATE FILMED
11-29-07	REVISED GUTTER SLOPE & MODIFIED CURB DETAILS	
11-10-05	ADDED DETAILS OF TYPE E CURBS	
11-16-01	REVISED CONCRETE CURB TYPE B	
11-18-98	REVISED MODIFIED CURB	
6-2-94	ADDED NOTE TO SPECIAL MODIFIED CURB	
8-5-93	CORRECTED GUTTER SLOPE	8-5-93
10-1-92	ADDED DETAILS OF GUTTER SLOPE	10-1-92
5-24-90	ADDED DETAILS OF MODIFIED CURB	5-24-90
11-30-89	VARIABLE DEPTH TYPE A & B 1	11-30-89
7-15-88	REVISED MODIFIED CURB	630-7-15-88
11-1-73	REVISED MODIFIED CURB	500-11-1-73
10-2-72	REVISED AND REDRAWN	512-10-2-72

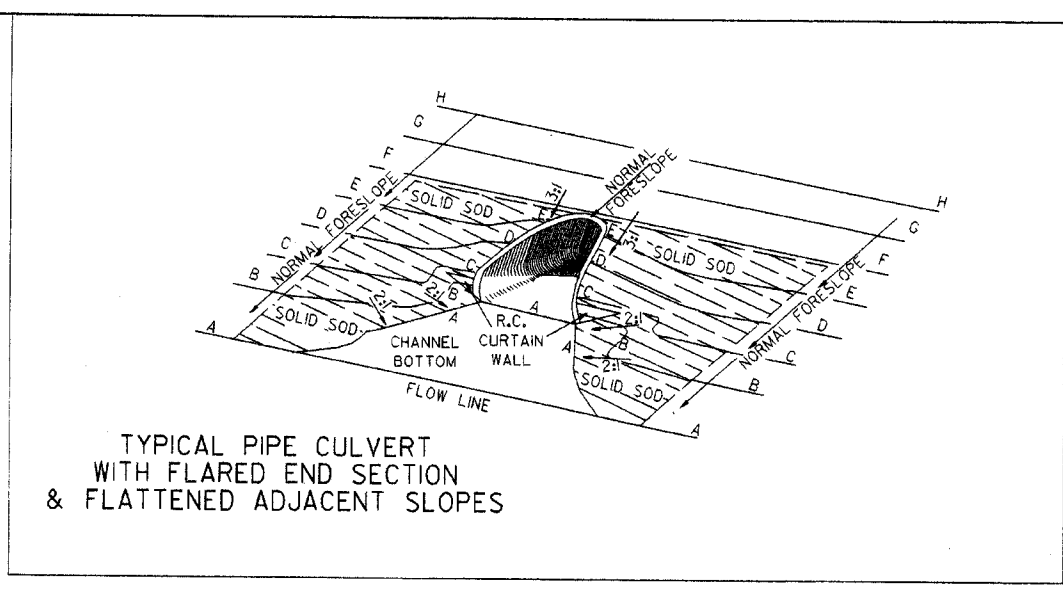
ARKANSAS STATE HIGHWAY COMMISSION

CURBING DETAILS

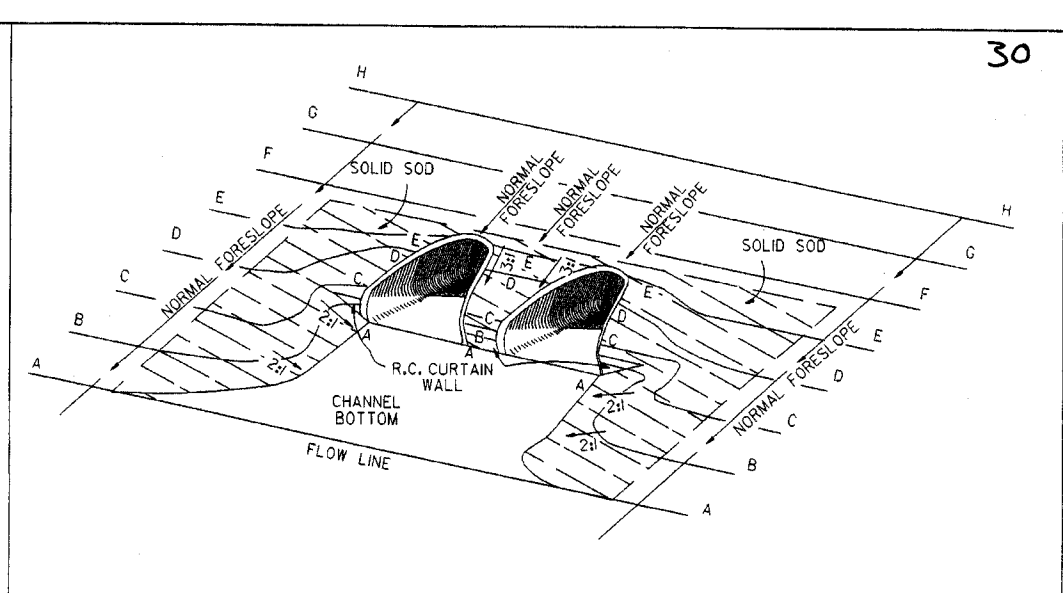
STANDARD DRAWING CG-1



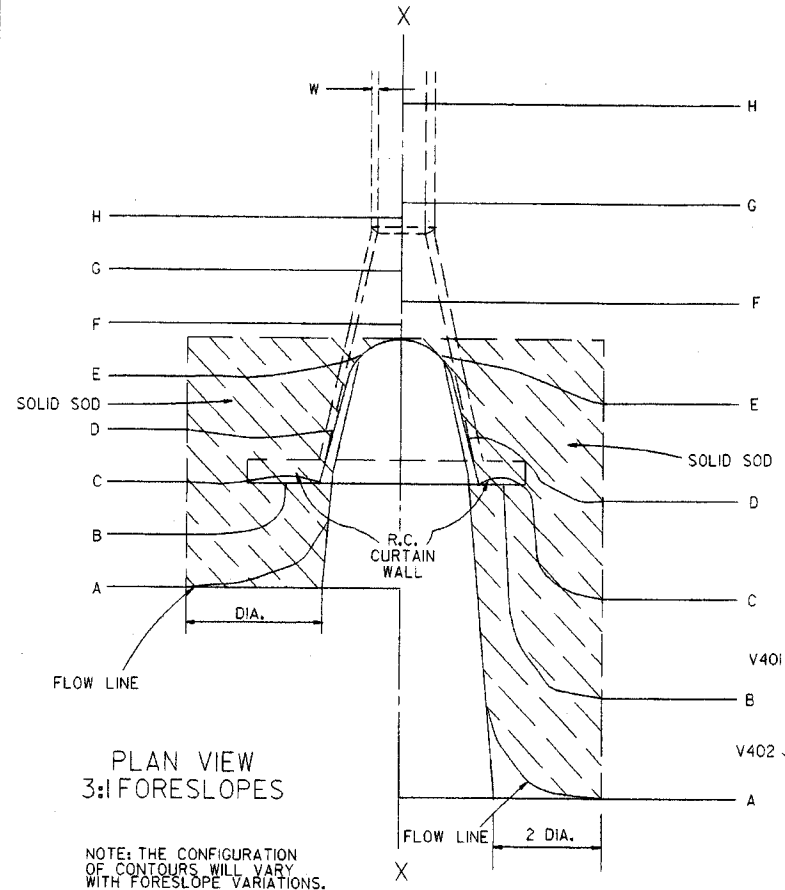
TYPICAL PIPE CULVERT WITH FLARED END SECTION & 3:1 FORESLOPE



TYPICAL PIPE CULVERT WITH FLARED END SECTION & FLATTENED ADJACENT SLOPES



TYPICAL MULTIPLE PIPE CULVERT WITH FLARED END SECTIONS & FLATTENED ADJACENT SLOPES



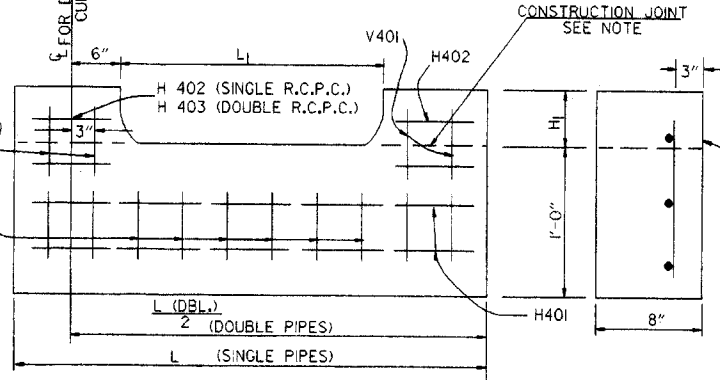
PLAN VIEW 3:1 FORESLOPES

PLAN VIEW FLATTENED FORESLOPES

R.C. CURTAIN WALL DIMENSIONS & QUANTITIES

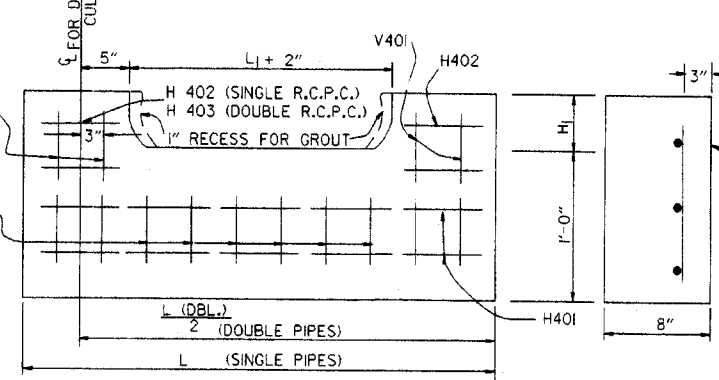
PIPE DIA.	H ₁	L ₁	L	L (DBL.) / 2	SINGLE R.C.P.C.		DOUBLE R.C.P.C.	
					CONC.	REINF. STEEL	CONC.	REINF. STEEL
					CU. YDS.	LBS.	CU. YDS.	LBS.
18"	11 1/2"	3'-5"	8'-0"	6'-3"	0.31	27.7	0.45	39.5
24"	1'-0 1/2"	4'-6"	9'-6"	7'-6"	0.37	33.4	0.53	48.0
30"	1'-3 1/2"	5'-7"	11'-0"	9'-0"	0.45	39.0	0.67	59.0
36"	1'-7"	6'-8"	13'-0"	10'-6"	0.58	52.6	0.83	73.9
42"	2'-1 1/2"	7'-3"	15'-6"	12'-0"	0.82	77.1	1.10	100.7
48"	2'-5"	7'-10"	17'-0"	13'-0"	0.98	94.9	1.27	120.4
54"	2'-9 1/2"	8'-5"	18'-6"	14'-0"	1.16	115.8	1.47	143.7
60"	3'-4"	9'-0"	20'-6"	15'-6"	1.47	149.7	1.84	180.3
72"	4'-5"	10'-2"	25'-6"	18'-6"	2.31	232.6	2.73	271.0

NOTE: QUANTITIES SHOWN ARE FOR ONE (1) CURTAIN WALL.



CAST-IN-PLACE

NOTE: THE PORTION OF THE R.C. CURTAIN WALL BENEATH THE FLARED END SECTION (LOWER 1'-0") SHALL BE PLACED MONOLITHICALLY. THE FLARED END SECTION SHALL THEN BE SET IN PLACE & THE REMAINING PORTIONS OF THE R.C. CURTAIN WALL PLACED.



PRECAST

NOTE: THE PRECAST CURTAIN WALL WILL BE SET AND BACKFILLED WITH COMPACTED MATERIAL. THE FLARED END SECTION SHALL THEN BE SET IN PLACE AND THE 1\"/>

REINFORCING STEEL SCHEDULE

PIPE DIA.	SINGLE R.C. PIPE CULVERT								DOUBLE R.C. PIPE CULVERT									
	H401		H402		V401		V402		H401		H402		H403		V401		V402	
	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.
18"	7'-8"	2	1'-11 1/2"	4	1'-7 1/2"	8	8"	8	12'-2"	2	1'-11 1/2"	4	8"	2	1'-7 1/2"	10	8"	14
24"	9'-2"	2	2'-2"	4	1'-8 1/2"	10	8"	9	14'-8"	2	2'-2"	4	8"	2	1'-8 1/2"	12	8"	22
30"	10'-8"	2	2'-4 1/2"	4	1'-11 1/2"	10	8"	12	17'-8"	2	2'-4 1/2"	4	8"	2	1'-11 1/2"	14	8"	28
36"	12'-8"	2	2'-10"	6	2'-3"	12	8"	14	20'-8"	2	2'-10"	6	8"	3	2'-3"	14	8"	28
42"	15'-2"	2	3'-9 1/2"	8	2'-9 1/2"	16	8"	15	23'-8"	2	3'-9 1/2"	8	8"	4	2'-9 1/2"	18	8"	30
48"	16'-8"	2	4'-3"	10	3'-1"	18	8"	16	25'-8"	2	4'-3"	10	8"	5	3'-1"	20	8"	32
54"	18'-2"	2	4'-8 1/2"	12	3'-5 1/2"	20	8"	17	27'-8"	2	4'-9"	12	8"	6	3'-5 1/2"	22	8"	34
60"	20'-2"	2	5'-5"	14	4'-0"	24	8"	18	30'-8"	2	5'-5"	14	8"	7	4'-0"	26	8"	36
72"	25'-2"	2	7'-4"	18	5'-1"	30	8"	20	36'-8"	2	7'-4"	18	8"	9	5'-1"	33	8"	40

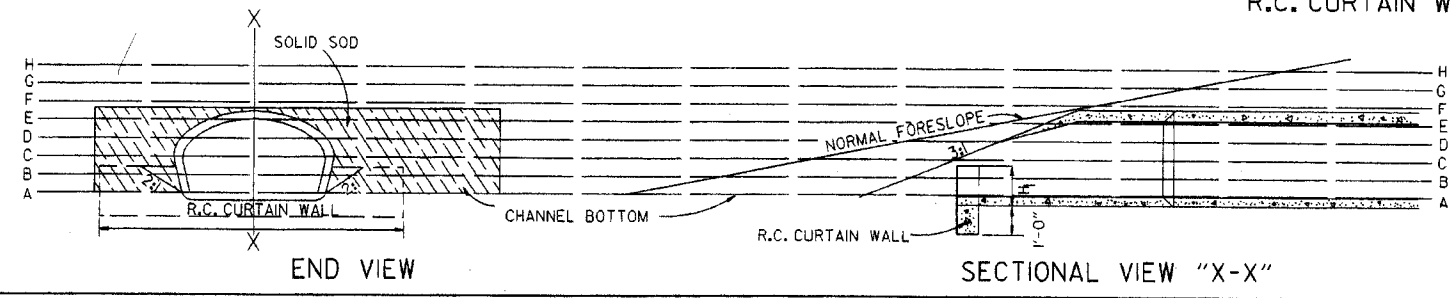
ALL REINFORCING STEEL #4 BARS @ 6" O.C.

SOLID SODDING

PIPE DIA.	SINGLE R.C.P.C.						DOUBLE R.C.P.C.					
	3:1	4:1	6:1	3:1	4:1	6:1	3:1	4:1	6:1	3:1	4:1	6:1
	SQ. YDS.						SQ. YDS.					
18"	5	7	12	6	8	13	5	7	12	6	8	13
24"	8	12	19	9	13	20	8	12	19	9	13	20
30"	13	18	29	14	19	30	13	18	29	14	19	30
36"	17	26	41	18	28	43	17	26	41	18	28	43
42"	23	35	55	25	37	57	23	35	55	25	37	57
48"	29	46	68	31	48	70	29	46	68	31	48	70
54"	37	51	85	37	59	87	37	51	85	37	59	87
60"	45	62	104	48	65	107	45	62	104	48	65	107
72"	64	92	156	67	95	159	64	92	156	67	95	159

NOTE: QUANTITIES SHOWN ABOVE ARE FOR ONE (1) END OF F.E.S.

- GENERAL NOTES
- A CAST-IN-PLACE OR PRECAST CURTAIN WALL MAY BE USED. PAYMENT FOR THE CURTAIN WALL SHALL BE CONSIDERED TO BE INCLUDED IN THE UNIT PRICE BID EACH FOR FLARED END SECTIONS OF THE SEVERAL SIZES, WHICH PRICE SHALL BE FULL COMPENSATION FOR FURNISHING ALL MATERIALS INCLUDING REINFORCING STEEL AND CONCRETE; FOR FORMS, MIXING AND PLACING; FOR EXCAVATION AND BACKFILL, AND FOR ALL LABOR, TOOLS, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE WORK.
 - ALL EXPOSED EDGES SHALL BE CHAMFERED 3/4".
 - CONCRETE FOR CURTAIN WALL SHALL MEET THE REQUIREMENTS FOR CLASS A OR S CONCRETE AS PROVIDED IN SECTION 802 OF THE STANDARD SPECIFICATIONS OR FOR PAVING CONCRETE AS PROVIDED IN SECTION 501 OF THE STANDARD SPECIFICATIONS.
 - WELDED WIRE MESH 3 x 3 W/10 x W/10 MAY BE USED IN LIEU OF REINFORCING BARS.

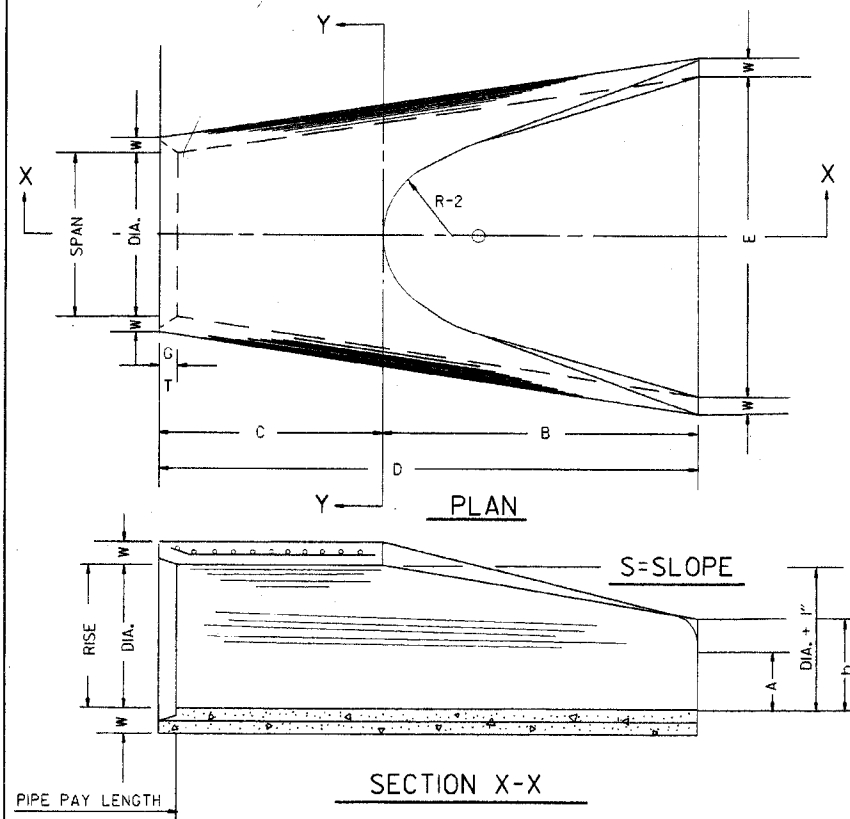


END VIEW

SECTIONAL VIEW "X-X"

R.C. CURTAIN WALL DETAILS

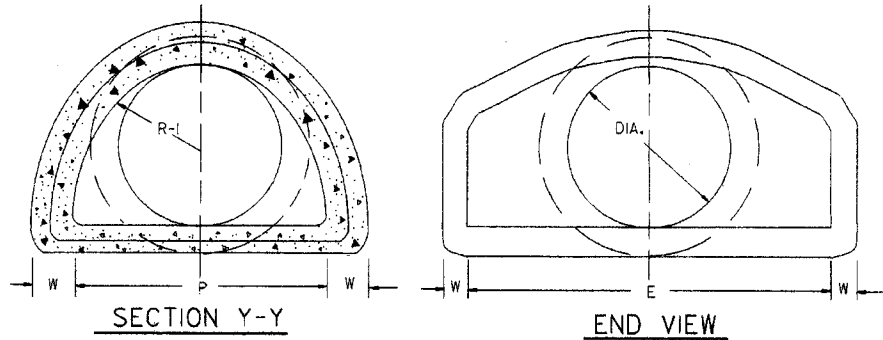
10-18-96	ADDED NOTE TO SOLID SODDING		ARKANSAS STATE HIGHWAY COMMISSION
10-12-95	CORRECTED SPELLING		
11-3-94	ADDED GENERAL NOTE NO. 4		
8-15-91	REV. CURTAIN WALL QUANT. STEEL SCH. & SOLID SOD QUANT.		
3-2-81	ALLOW PRECAST IN 2 OR MORE PIECES CHAMFER EDGES		
5-15-80	ADDED PRECAST WALL & GENERAL NOTES		
10-2-72	REVISED AND REDRAWN		
DATE	REVISION	FILMED	STANDARD DRAWING FES-1



END SECTION FOR REINFORCED CONCRETE PIPE CULVERTS

TABLE OF DIMENSIONS

DIA.	WALL	A	B	C	D	E	S	DIA. + 1"	P	R-1	R-2	G-T	WT.	h
18"	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	3:1	19"	29"	15 1/2"	12"	2"	1000	1'-0 1/2"
24"	3"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	3:1	25"	33 3/8"	16 1/8"	14"	2 1/2"	1600	1'-1 1/2"
30"	3 1/2"	1'-0"	4'-6"	1'-7 1/2"	6'-1 1/2"	5'-0"	3:1	31"	37"	18 1/2"	15"	3 1/4"	1940	1'-4 3/4"
36"	4"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	6'-0"	3:1	37"	47 1/8"	24 3/8"	20"	3 1/2"	4100	1'-8"
42"	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	3:1	43"	53 1/2"	27 1/2"	22"	3 1/2"	5380	2'-2 1/2"
48"	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	3:1	49"	56 1/2"	28 1/2"	22"	3 1/2"	6550	2'-6"
54"	5 1/2"	2'-4"	6'-6"	1'-10"	8'-4"	7'-6"	3:1	55"	65 1/2"	33 1/8"	24"	4"	8750	2'-10 1/2"
60"	6"	2'-10"	6'-6"	1'-10"	8'-4"	8'-0"	3:1	61"	72 1/2"	36 1/8"	24"	4"	9270	3'-5"
72"	7"	3'-10"	6'-6"	1'-10"	8'-4"	9'-0"	3:1	73"	77 3/8"	38 3/8"	24"	5"	13250	4'-6"

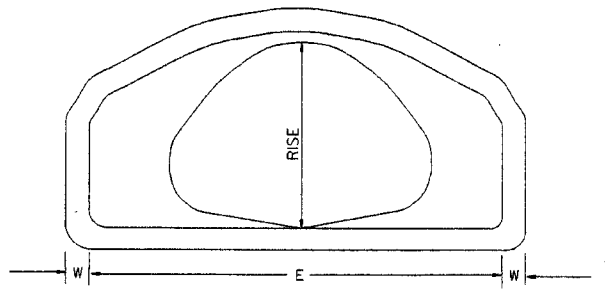


NOTE: TONGUE END ON UPSTREAM SECTION GROOVE END ON DOWNSTREAM SECTION

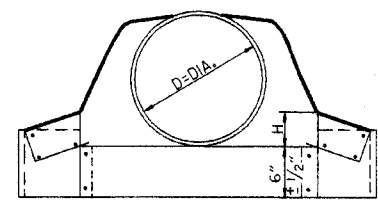
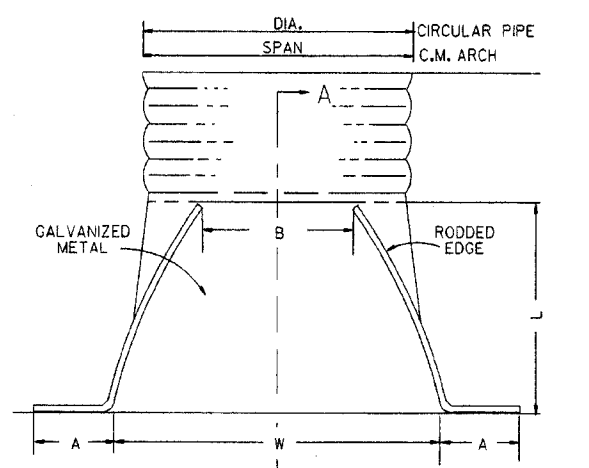
ARCH PIPE

EQUIV. DIA.	SPAN		RISE		W	A	B	C	D	E	P	R2	G-T	S
	AASHTO M 206	AHD NOMINAL	AASHTO M 206	AHD NOMINAL										
INCHES														
15	18	18	11	11	2"	4"	2'-0"	4'-0"	6'-0"	3'-0"	29"	12"	1 1/2"	2 1/2:1
18	22	22	13 1/2	14	2 1/2"	5"	2'-0"	4'-1"	6'-1"	3'-0"	32 1/8"	13"	2 1/2"	2 1/2:1
21	26	26	15 1/2	16	2 3/4"	7"	2'-3"	3'-10"	6'-1"	4'-0"	34 1/8"	14"	2 1/2"	2 1/2:1
24	28 1/2	29	18	18	3"	9"	2'-3"	3'-10"	6'-1"	5'-0"	36 1/8"	15"	2 1/2"	2 1/2:1
30	36 1/4	36	22 1/2	23	3 1/2"	10"	3'-1"	3'-0 1/2"	6'-1 1/2"	6'-0"	47 1/8"	20"	3"	2 1/2:1
36	43 3/4	44	26 3/8	27	4"	10 1/2"	4'-0"	2'-11 1/2"	6'-1 1/2"	6'-6"	54 1/8"	22"	3 1/2"	2 1/2:1
42	51 1/8	51	31 3/8	31	4 1/2"	11 1/2"	4'-7"	1'-10 1/4"	6'-5 1/4"	7'-2"	59 1/2"	23"	3 3/4"	2 1/2:1
48	58 1/2	59	36	36	5"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	7'-10"	70 3/8"	24"	4 1/4"	2 1/2:1
54	65	65	40	40	5 1/2"	1'-7"	5'-3"	2'-11"	8'-2"	8'-6"	72 1/8"	24"	4 3/4"	2 1/2:1
60	73	73	45	45	6"	1'-10"	5'-6"	2'-8"	8'-2"	9'-0"	77 3/8"	24"	5"	2 1/2:1

* THE MEASURED SPAN AND RISE SHALL NOT VARY MORE THAN ± 2 PER CENT FROM THE VALUES SPECIFIED BY AASHTO M 206.



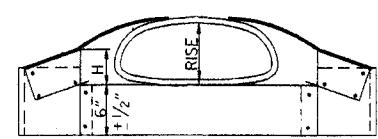
END VIEW CONCRETE ARCH PIPE



CIRCULAR PIPE

CIRCULAR PIPE

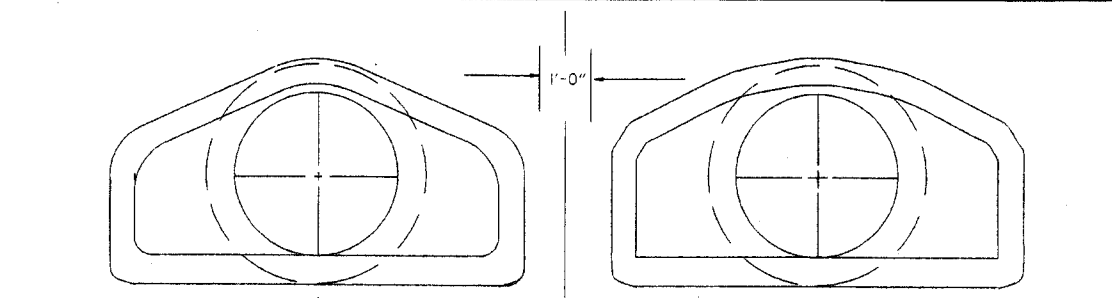
D. DIA.	GAUGE	A 1" ±	B. MAX.	H 1" ±	L 1 1/2" ±	W 2" ±	S
12	16	6	6	6	21	24	2 1/2:1
15	16	7	8	6	26	30	2 1/2:1
18	16	8	10	6	31	36	2 1/2:1
21	16	9	12	6	36	42	2 1/2:1
24	16	10	13	6	41	48	2 1/2:1
30	14	12	16	8	51	60	2 1/2:1
36	14	14	19	9	60	72	2 1/2:1
42	12	16	22	11	69	84	2 1/2:1
48	12	18	27	12	78	90	2 1/2:1
54	12	18	30	12	84	102	2:1
60	12	18	33	12	87	114	1 3/4:1
66	12	18	36	12	87	120	1 1/2:1
72	12	18	39	12	87	126	1 1/3:1



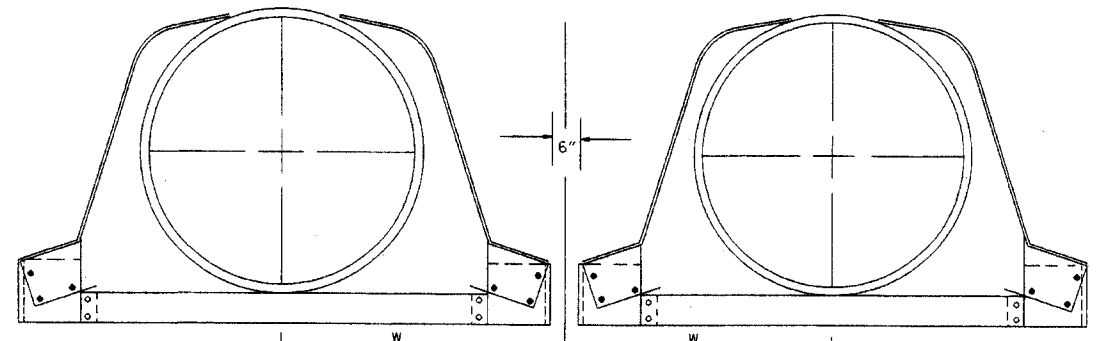
C.M. ARCH PIPE

C.M. ARCH PIPE

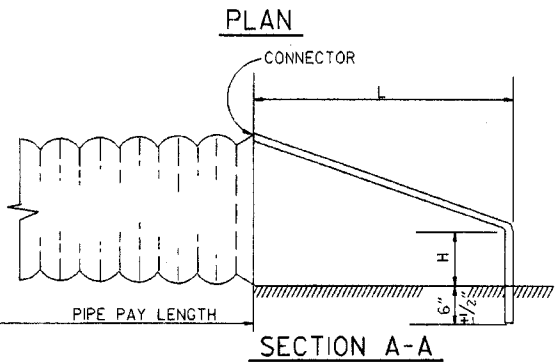
EQUIV. DIA.	SPAN	RISE	A 1" ±	B MAX.	H 1" ±	L 1 1/2" ±	W 2" ±	S	GAUGE
15"	17	13	7	9	6	19	30	2 1/2:1	16
18"	21	15	7	10	6	23	36	2 1/2:1	16
21"	24	18	8	12	6	28	42	2 1/2:1	16
24"	28	20	9	14	6	32	48	2 1/2:1	16
30"	35	24	10	16	6	39	60	2 1/2:1	14
36"	42	29	12	18	8	46	75	2 1/2:1	14
42"	49	33	13	21	9	53	85	2 1/2:1	12
48"	57	38	18	26	12	63	90	2 1/2:1	12
54"	64	43	18	30	12	70	102	2 1/4:1	12
60"	71	47	18	33	12	77	114	2 1/4:1	12



MULTIPLE R.C. PIPE CULVERTS



MULTIPLE C.M. PIPE CULVERTS



SECTION A-A

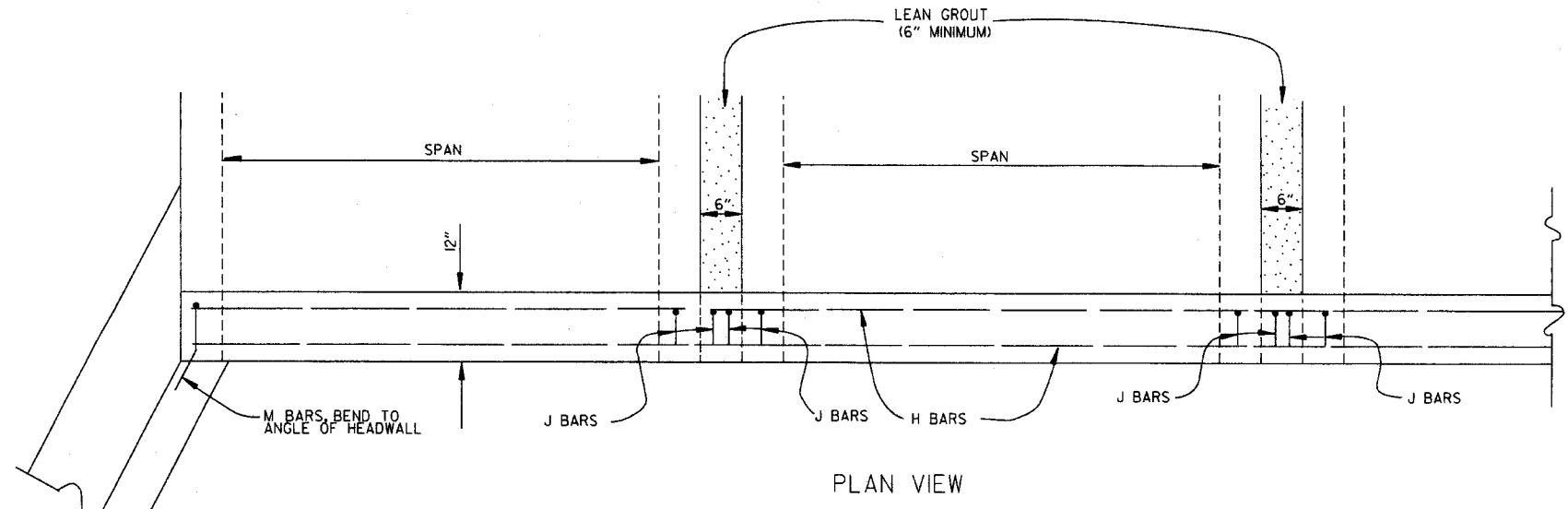
NOTE: ALTERNATE CONNECTIONS TO THE PIPE CULVERTS, IN ACCORDANCE WITH MANUFACTURER'S STANDARD PRACTICES, MAY BE MADE SUBJECT TO THE APPROVAL OF THE ENGINEER.

END SECTIONS FOR CORRUGATED METAL PIPE CULVERTS

10-18-96	REVISED ASTM REF. TO AASHTO	10-18-96	ARKANSAS STATE HIGHWAY COMMISSION
5-15-80	REVISED DISTANCE BETWEEN MULTIPLE R.C.P. F.E.S.	664-5-15-80	
7-14-78	C.M. ARCH SIZES TO CONFORM WITH AASHTO SIZES	752-7-14-78	
8-22-75	ADDED MULTIPLE PIPE CULVERTS	517-8-22-75	
12-5-74	REMOVED NOTE RE REINF. FOR R.C. F.E.S.	500-12-5-74	
5-24-73	CMP END SECTION, SHOW PIPE PAY LENGTH	627-5-24-73	
10-2-72	REVISED AND REDRAWN	760-10-2-72	
DATE	REVISION	FILED	

FLARED END SECTION

STANDARD DRAWING FES-2



BAR LIST

BAR	NO.	SIZE	LENGTH	BAR BENDING DIAGRAM
H	2	#4	•	
I	•	#4	•	
J	•	#4	1'-5"	
L	•	#4	3'-2"	
M	•	#4	1'-8"	

• NOTE: LENGTH AND NUMBER OF BARS VARIES WITH SIZE OF CULVERT

GENERAL NOTES

WINGS, CURTAIN WALLS AND APRONS SHALL BE TIED TO THE PRECAST CULVERT SECTION BY CASTING BARS IN CULVERT END SECTIONS AS SHOWN OR BY DOWELING AND GROUTING. J BARS AND M BARS SHALL BE EMBEDDED A MINIMUM OF 10" IN PRECAST BOX.

WINGS, FOOTINGS, APRONS AND CURTAIN WALLS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPLICABLE WING DRAWING. STEEL AND CONCRETE QUANTITIES WILL BE ADJUSTED TO FIT THE IN-PLACE WIDTH & HEIGHT OF THE PRECAST CONCRETE BOX CULVERTS.

ALL EXPOSED CORNERS TO HAVE 3/4" CHAMFERS.

WINGWALLS AND FOOTINGS MAY BE ADJUSTED IN THE FIELD AS DIRECTED BY THE ENGINEER.

ALL CONCRETE, REINFORCING STEEL, LEAN GROUT, MEMBRANE WATERPROOFING, DRAINAGE FILL MATERIAL, GEOTEXTILE FILTER FABRIC, LABOR, MATERIALS AND EQUIPMENT REQUIRED FOR INSTALLING PRECAST BOX CULVERTS WILL NOT BE PAID FOR DIRECTLY BUT WILL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR THE ITEMS AS SPECIFIED IN SECTION 607 OF THE STANDARD SPECIFICATIONS.

LEAN GROUT SHALL CONSIST OF A SAND CEMENT MIXTURE MEETING THE FOLLOWING REQUIREMENTS:
 PORTLAND CEMENT SHALL BE TYPE I AND SHALL MEET THE REQUIREMENTS OF AASHTO M 85.
 SAND SHALL MEET THE REQUIREMENTS OF FINE AGGREGATE AS SPECIFIED IN SECTION 802.02 OF THE STANDARD SPECIFICATIONS.
 THE SAND CEMENT MIXTURE SHALL CONSIST OF NOT LESS THAN 1.5 SACKS OF PORTLAND CEMENT PER TON OF MATERIAL MIXTURE. THE MIXTURE SHALL CONTAIN SUFFICIENT WATER TO HYDRATE THE CEMENTS. THE SAND CEMENT MIXTURE SHALL BE PLACED IN MAXIMUM 8 INCH THICK LIFTS, LOOSE MEASURE, AND THOROUGHLY RODDED AND TAMPED AROUND BOX TO THOROUGHLY FILL ALL VOIDS.

MEMBRANE WATERPROOFING CONFORMING TO THE REQUIREMENTS OF SECTION 815 OF THE STANDARD SPECIFICATIONS SHALL BE APPLIED TO ALL BOX CULVERT JOINTS.

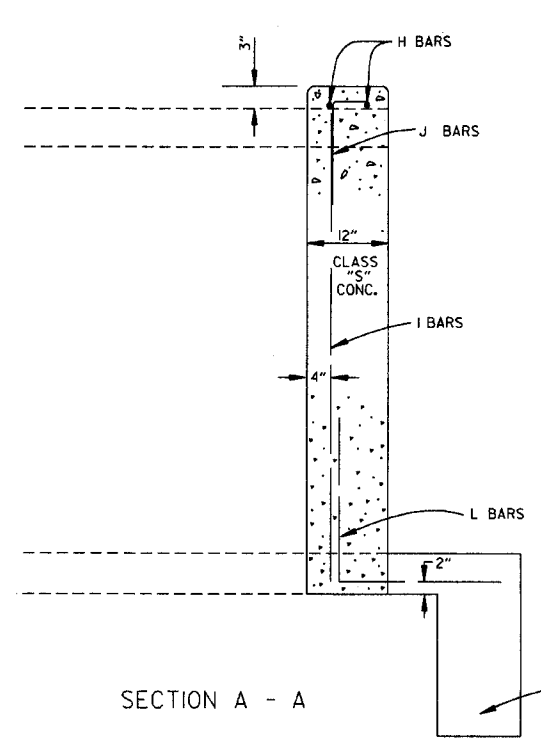
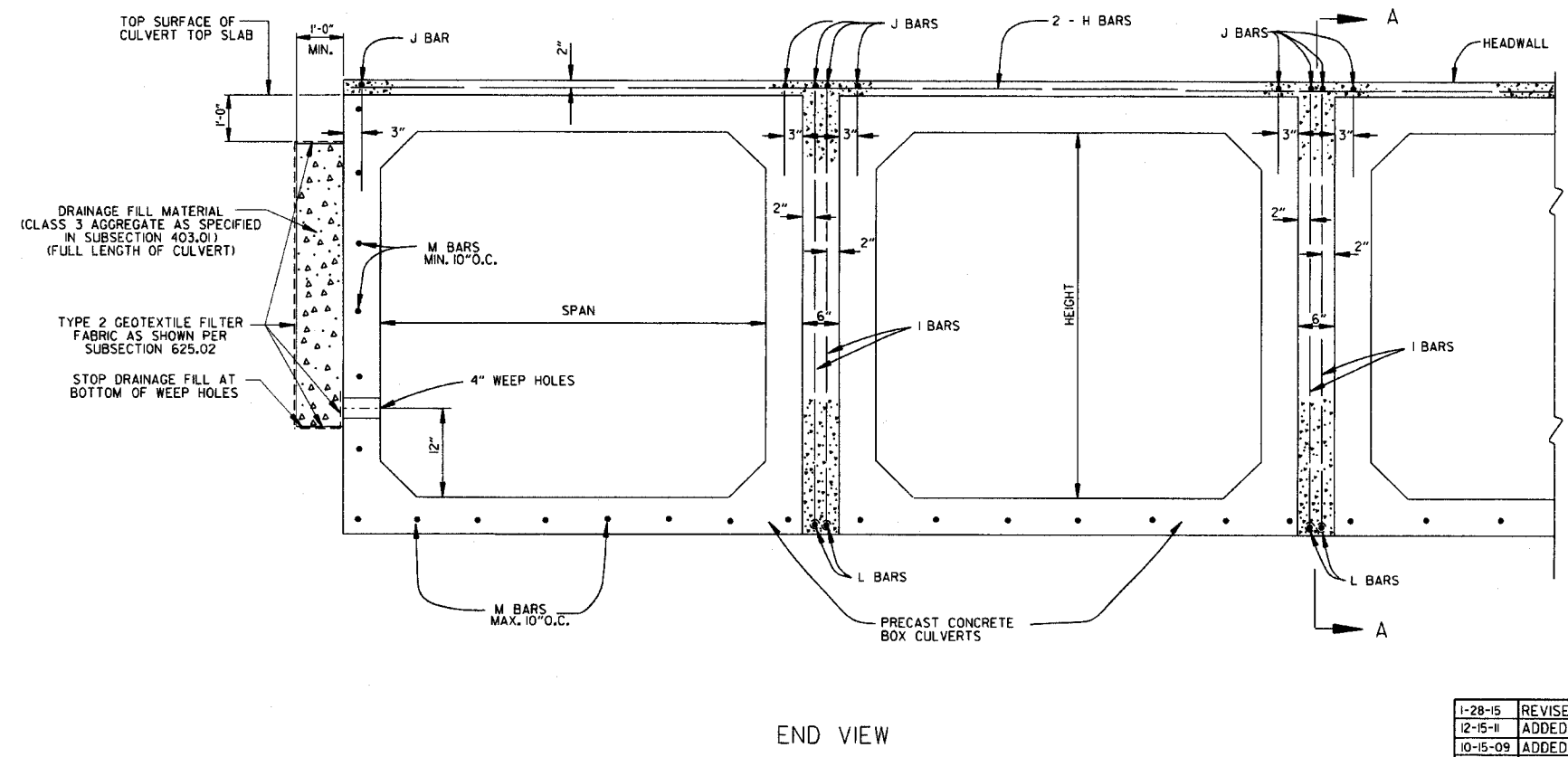
THE MEMBRANE WATERPROOFING WILL BE REQUIRED ON THE TOP EXTERNAL JOINT AND SHALL EXTEND 1 FOOT DOWN THE SIDES OF THE CULVERT.

IN OUTER BARRELS, ONE WEEP HOLE IS REQUIRED IN EXTERIOR WALLS OF EACH PRECAST CULVERT SECTION. WEEP HOLES SHALL HAVE A MAXIMUM HORIZONTAL SPACING OF 10'-0" IN THE ASSEMBLED CULVERT AND SHALL BE SPACED TO CLEAR ALL REINFORCING STEEL. THE DRAIN OPENING SHALL BE 4" DIAMETER AND SHALL BE PLACED 12" ABOVE THE TOP OF THE BOTTOM SLAB.

DRAINAGE FILL MATERIAL WITH GEOTEXTILE FABRIC IS REQUIRED AT THE EXTERIOR WALLS OF THE ASSEMBLED CULVERT, SEE DETAILS ON THIS DRAWING.

MINIMUM WIDTH SHALL BE 12" (6" ON EACH SIDE OF JOINT). ON MULTIPLE BARREL CULVERTS, MEMBRANE WATERPROOFING SHALL BE APPLIED TO EACH BARREL AS DESCRIBED ABOVE.

WITH THE APPROVAL OF THE ENGINEER, THE CONTRACTOR WILL BE ALLOWED TO SUBSTITUTE, AT NO ADDITIONAL COST TO THE DEPARTMENT, FLOWABLE SELECT MATERIAL CONFORMING TO SECTION 206 OF THE STANDARD SPECIFICATIONS IN LIEU OF LEAN GROUT.



DATE	REVISION	DATE FILMED
1-28-15	REVISED GEOTEXTILE FABRIC PLACEMENT	
12-15-11	ADDED NOTE & DTLS FOR WEEP HOLE AND DRAINAGE FILL	
10-15-09	ADDED GENERAL NOTE	
11-10-05	REVISED SPACING OF "M" BARS	
4-10-03	REVISED GENERAL NOTES	
10-18-96	CORRECTED AASHTO REF.	
10-1-92	ADDED NOTE FOR MEMBRANE WATERPROOFING	
8-15-91	ADDED NOTE FOR LEAN GROUT	
11-8-90	REVISED FOR 1991 SPECS	
11-30-89	ISSUED, JABE	

ARKANSAS STATE HIGHWAY COMMISSION

PRECAST CONCRETE BOX CULVERTS

STANDARD DRAWING PBC-1

REINFORCED CONCRETE ARCH PIPE DIMENSIONS

EQUIV. DIA. INCHES	SPAN		RISE	
	AASHTO M 206	AHTD NOMINAL	AASHTO M 206	AHTD NOMINAL
15	18	18	11	11
18	22	22	13½	14
21	26	26	15½	16
24	28½	29	18	18
30	36¼	36	22½	23
36	43¾	44	26¾	27
42	51½	51	31¾	31
48	58½	59	36	36
54	65	65	40	40
60	73	73	45	45
72	88	88	54	54
84	102	102	62	62
90	115	115	72	72
96	122	122	77½	77
108	138	138	87½	87
120	154	154	96¾	97
132	168¾	169	106½	107

THE MEASURED SPAN AND RISE SHALL NOT VARY MORE THAN ± 2 PERCENT FROM THE VALUES SPECIFIED BY AASHTO M206.

REINFORCED CONCRETE HORIZONTAL ELLIPTICAL PIPE DIMENSIONS

EQUIV. DIA. INCHES	AASHTO M 207	
	SPAN	RISE
18	23	14
24	30	19
27	34	22
30	38	24
33	42	27
36	45	29
39	49	32
42	53	34
48	60	38
54	68	43
60	76	48
66	83	53
72	91	58
78	98	63
84	106	68

THE MEASURED SPAN AND RISE SHALL NOT VARY MORE THAN ± 2 PERCENT FROM THE VALUES SPECIFIED BY AASHTO M207.

CONSTRUCTION SEQUENCE

1. PLACE STRUCTURAL BEDDING MATERIAL TO GRADE. DO NOT COMPACT.
2. INSTALL PIPE TO GRADE.
3. COMPACT STRUCTURAL BEDDING OUTSIDE THE MIDDLE THIRD OF THE PIPE.
4. PLACE AND COMPACT THE HAUNCH AREA UP TO THE MIDDLE OF THE PIPE.
5. COMPLETE BACKFILL ACCORDING TO SUBSECTION 606.03.(F)(II).

NOTE: HAUNCH AND STRUCTURAL BEDDING MATERIAL WILL NOT BE PAID FOR SEPARATELY, BUT COMPENSATION WILL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID PER LINEAR FOOT OF CONCRETE PIPE.

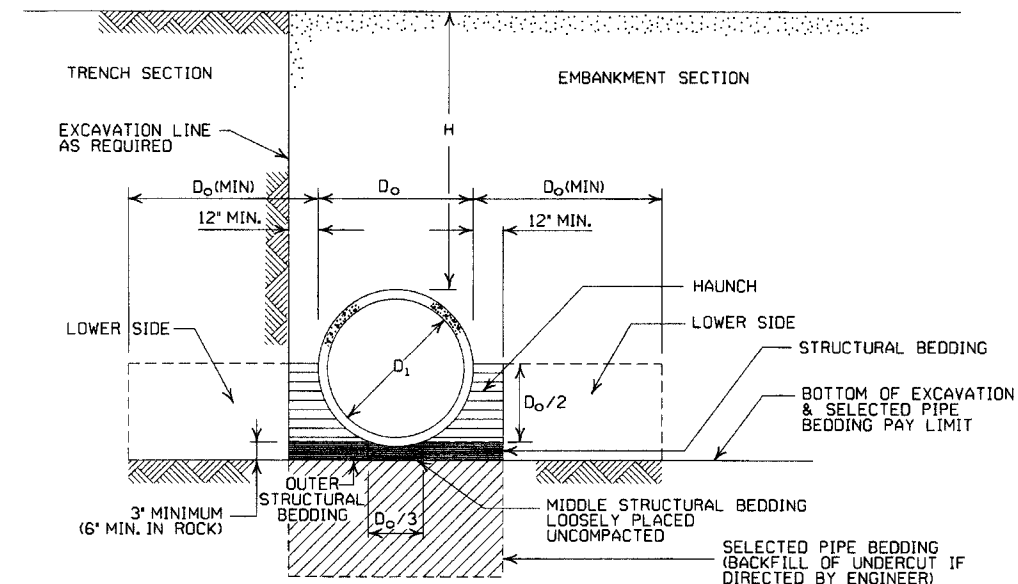
- LEGEND -

- D_i = NORMAL INSIDE DIAMETER OF PIPE
- D_o = OUTSIDE DIAMETER OF PIPE
- H = FILL COVER HEIGHT OVER PIPE (FEET)
- MIN. = MINIMUM
- UNDISTURBED SOIL

INSTALLATION TYPE	MATERIAL REQUIREMENTS FOR HAUNCH AND STRUCTURAL BEDDING
TYPE 1	AGGREGATE BASE COURSE (CLASS 5 OR CLASS 7)
TYPE 2	SELECTED MATERIALS (CLASS SM-1, SM-2, OR SM-4) OR TYPE 1 INSTALLATION MATERIAL*
TYPE 3**	AASHTO CLASSIFICATION A-1 THRU A-6 SOIL OR TYPE 1 OR 2 INSTALLATION MATERIAL

* SM-3 WILL NOT BE ALLOWED.

** MATERIALS SHALL NOT INCLUDE ORGANIC MATERIALS OR STONES LARGER THAN 3 INCHES.



EMBANKMENT AND TRENCH INSTALLATIONS

1. MATERIAL IN THE HAUNCH AND OUTER STRUCTURAL BEDDING SHALL BE COMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OR CLASS OF MATERIAL USED.
2. FOR TRENCHES WITH WALLS OF NATURAL SOIL, THE DENSITY OF THE SOIL IN THE LOWER SIDE ZONE SHALL BE AS FIRM AS THE 95% DENSITY REQUIRED FOR THE HAUNCH. IF THE EXISTING SOIL DOES NOT MEET THIS CRITERIA, IT SHALL BE REMOVED AND RECOMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OF MATERIAL USED.
3. FOR EMBANKMENTS, THE MATERIAL IN THE LOWER SIDE ZONE SHALL BE COMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OR CLASS OF MATERIAL USED.

GENERAL NOTES

1. CONCRETE PIPE CULVERT CONSTRUCTION SHALL CONFORM TO ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION), WITH APPLICABLE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS, UNLESS OTHERWISE NOTED IN THE PLANS, SECTION AND SUBSECTION REFER TO THE STANDARD CONSTRUCTION SPECIFICATIONS.
2. CONCRETE PIPE CULVERT DESIGN SHALL CONFORM TO AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, FIFTH EDITION (2010) WITH 2010 INTERIMS.
3. ALL PIPE SHALL CONFORM TO SECTION 606. CIRCULAR R.C. PIPE CULVERTS SHALL CONFORM TO AASHTO M10. R.C. ARCH PIPE CULVERTS SHALL CONFORM TO AASHTO M206 AND HORIZONTAL ELLIPTICAL PIPE CULVERTS SHALL CONFORM TO AASHTO M207.
4. ALL PIPE SHALL BE PROTECTED DURING CONSTRUCTION BY A COVER SUFFICIENT TO PREVENT DAMAGE FROM PASSAGE OF EQUIPMENT.
5. THE MINIMUM TRENCH WIDTH SHALL BE THE OUTSIDE DIAMETER OF THE PIPE PLUS 24 INCHES. THE MAXIMUM ALLOWABLE TRENCH WIDTH SHALL BE THE MINIMUM WIDTH PRACTICABLE FOR WORKING CONDITIONS.
6. MULTIPLE PIPE CULVERTS SHALL BE INSTALLED WITH A MINIMUM CLEARANCE OF 24 INCHES BETWEEN STRINGS OF PIPE. REFER TO STD. DWG. FES-2 FOR MINIMUM CLEARANCE WHERE FLARED END SECTIONS ARE USED.
7. IMPERVIOUS MATERIAL SHOULD BE PLACED AS DIRECTED BY THE ENGINEER AT THE ENDS OF THE CULVERT TO PREVENT LOSS OF STRUCTURAL BEDDING WHEN PERVIOUS MATERIAL IS USED FOR STRUCTURAL BEDDING AND/OR BACKFILL.
8. NOT MORE THAN ONE LIFTING HOLE MAY BE PROVIDED IN CONCRETE PIPE TO FACILITATE HANDLING. HOLE MAY BE CAST IN PLACE, CUT INTO THE FRESH CONCRETE AFTER FORMS ARE REMOVED, OR DRILLED. THE HOLE SHALL NOT BE MORE THAN TWO INCHES IN DIAMETER OR TWO INCHES SQUARE. CUTTING OR DISPLACEMENT OF REINFORCEMENT WILL NOT BE PERMITTED. SPALLED AREAS AROUND THE HOLE SHALL BE REPAIRED IN A WORKMANLIKE MANNER. LIFTING HOLE SHALL BE FILLED WITH MORTAR, CONCRETE, OR OTHER METHOD AS APPROVED BY THE ENGINEER.
9. WHEN DIRECTED BY THE ENGINEER, UNSUITABLE MATERIAL THAT IS ENCOUNTERED AT THE BOTTOM OF THE EXCAVATED TRENCH (BELOW THE AREA IDENTIFIED AS "STRUCTURAL BEDDING" ABOVE) WILL BE EXCAVATED AND REPLACED WITH SELECTED PIPE BEDDING. THE QUANTITY OF MATERIAL REQUIRED TO BACKFILL THE UNDERCUT AREA UP TO THE SELECTED PIPE BEDDING PAY LIMIT DESIGNATED ABOVE WILL BE MEASURED AND PAID FOR AS "SELECTED PIPE BEDDING."
10. WHEN THE EXISTING MATERIAL EXCAVATED FOR THE PIPE TRENCH IS DETERMINED BY THE ENGINEER TO BE UNSUITABLE FOR BACKFILLING THE PIPE (ABOVE THE AREA IDENTIFIED ABOVE AS THE HAUNCH), BORROW MATERIAL OR MATERIAL FROM THE ROADWAY EXCAVATION WILL BE USED TO BACKFILL THE PIPE. IF SUITABLE MATERIAL IS NOT AVAILABLE, THE ENGINEER MAY AUTHORIZE THE USE OF "SELECTED PIPE BACKFILL."

MINIMUM HEIGHT OF FILL "H" OVER CIRCULAR R.C. PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE			
	CLASS III	CLASS IV	CLASS V	ALL
PIPE ID (IN.)	FEET			
12-15	2	2.5	2	1
18-24	2.5	3	2	1
27-33	3	4	2	1
36-42	3.5	5	2	1
48	4.5	5.5	2	1
54-60	5	7	2	1
66-78	6	8	2	1
84-108	7.5	8	2	1

NOTE: FOR MINIMUM COVER VALUES, "H" SHALL INCLUDE A MINIMUM OF 12" OF PAVEMENT AND/OR BASE.

MAXIMUM HEIGHT OF FILL "H" OVER CIRCULAR R.C. PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE		
	CLASS III	CLASS IV	CLASS V
TYPE 1	21	32	50
TYPE 2	16	25	39
TYPE 3	12	20	30

NOTE: IF FILL HEIGHT EXCEEDS 50 FEET, A SPECIAL DESIGN CONCRETE PIPE WILL BE REQUIRED USING TYPE 1 INSTALLATION.

MINIMUM HEIGHT OF FILL "H" OVER R.C. ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE	
	CLASS III	CLASS IV
TYPE 2 OR TYPE 3	2.5	1.5

NOTE: TYPE 1 INSTALLATION WILL NOT BE ALLOWED FOR ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS.

NOTE: FOR MINIMUM COVER VALUES, "H" SHALL INCLUDE A MINIMUM OF 12" OF PAVEMENT AND/OR BASE.

MAXIMUM HEIGHT OF FILL "H" OVER R.C. ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE	
	CLASS III	CLASS IV
TYPE 2	13	21
TYPE 3	10	16

NOTE: TYPE 1 INSTALLATION WILL NOT BE ALLOWED FOR ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS.

DATE	REVISION	DATE FILMED
2-27-14	REVISED GENERAL NOTE 1.	
12-15-11	REVISED FOR LRFD DESIGN SPECIFICATIONS	
5-18-00	REVISED TYPE 3 BEDDING & ADDED NOTE	
3-30-00	REVISED INSTALLATIONS	
11-06-97	ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION

CONCRETE PIPE CULVERT FILL HEIGHTS & BEDDING

STANDARD DRAWING PCC-1



CORRUGATED STEEL PIPE (ROUND)

PIPE DIAMETER (INCHES)	① MINIMUM COVER TOP OF PIPE TO TOP OF GROUND "H" (FEET)	MAX. FILL HEIGHT "H" ABOVE TOP OF PIPE (FEET)				
		METAL THICKNESS (INCHES)				
		0.064	0.079	0.109	0.138	0.168
2 3/8 INCH BY 1/2 INCH CORRUGATION RIVETED, WELDED, OR HELICAL LOCK-SEAM						
12	1	84	91			
15	1	67	73			
18	1	56	61			
24	1	42	46	59		
30	2	34	36	47		
36	2		30	39	41	
42	2		43	67	70	73
48	2		37	58	61	64
② 3 INCH BY 1 INCH OR 5 INCH BY 1 INCH CORRUGATION RIVETED, WELDED, BOLTED, OR HELICAL LOCK-SEAM						
36	1	48	60	88	111	118
42	1	41	51	72	90	102
48	1	36	45	64	77	85
54	2	32	40	59	71	79
60	2	29	36	53	64	71
66	2	26	33	47	58	64
72	2	24	30	44	53	59
78	2		28	41	49	54
84	2		26	38	45	51
90	2		24	35	43	45
96	2		22	33	40	44
102	2			31	38	42
108	2			30	35	39
114	2			28	34	37
120	2			27	32	35

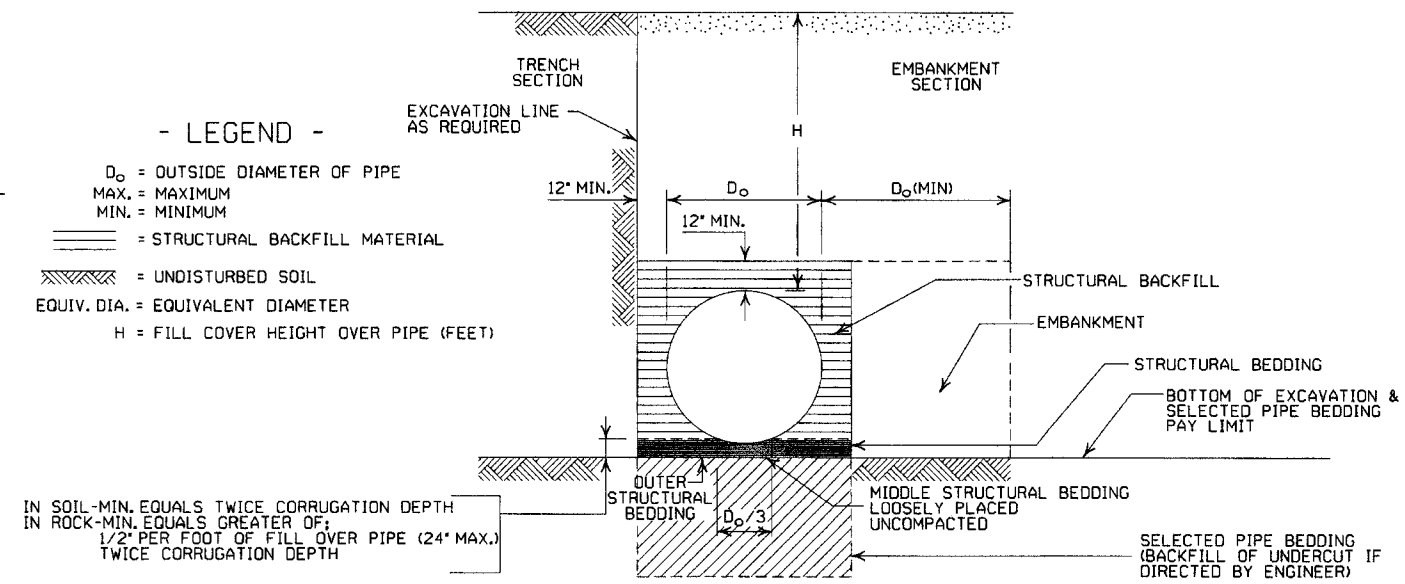
CONSTRUCTION SEQUENCE

1. PLACE STRUCTURAL BEDDING MATERIAL TO GRADE. DO NOT COMPACT.
2. INSTALL PIPE TO GRADE.
3. COMPACT STRUCTURAL BEDDING OUTSIDE THE MIDDLE THIRD OF THE PIPE.
4. COMPLETE STRUCTURAL BACKFILL OPERATION BY WORKING FROM SIDE TO SIDE OF THE PIPE. THE SIDE TO SIDE STRUCTURAL BACKFILL DIFFERENTIAL SHALL NOT EXCEED 24 INCHES OR 1/3 THE SIZE OF THE PIPE, WHICHEVER IS LESS.

NOTE: STRUCTURAL BACKFILL AND STRUCTURAL BEDDING MATERIAL WILL NOT BE PAID FOR SEPARATELY, BUT COMPENSATION WILL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID PER LINEAR FOOT OF METAL PIPE.

INSTALLATION TYPE	MATERIAL REQUIREMENTS FOR STRUCTURAL BACKFILL AND STRUCTURAL BEDDING
TYPE 1	AGGREGATE BASE COURSE (CLASS 4, 5, 6, OR 7)
TYPE 2	SELECTED MATERIALS (CLASS SM-1, SM-2, OR SM-4) OR TYPE 1 INSTALLATION MATERIAL ③

③ SM-3 WILL NOT BE ALLOWED.



EMBANKMENT AND TRENCH INSTALLATIONS

1. STRUCTURAL BACKFILL, EMBANKMENT, AND OUTER STRUCTURAL BEDDING MATERIAL SHALL BE COMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OR CLASS OF MATERIAL USED.
2. INSTALLATION TYPE 1 OR 2 MAY BE USED FOR CORRUGATED STEEL OR ALUMINUM PIPE (ROUND).
3. INSTALLATION TYPE 1 SHALL BE USED FOR CORRUGATED STEEL OR ALUMINUM PIPE ARCHES WITH 2 3/8" X 1/2" CORRUGATION.
4. INSTALLATION TYPE 1 OR 2 MAY BE USED FOR CORRUGATED STEEL OR ALUMINUM PIPE ARCHES WITH 3" X 1" OR 5" X 1" CORRUGATION.

CORRUGATED ALUMINUM PIPE (ROUND)

PIPE DIAMETER (INCHES)	① MINIMUM COVER TOP OF PIPE TO TOP OF GROUND "H" (FEET)	MAX. FILL HEIGHT "H" ABOVE TOP OF PIPE (FEET)				
		METAL THICKNESS IN INCHES				
		0.060	0.075	0.105	0.135	0.164
2 3/8 INCH BY 1/2 INCH CORRUGATION RIVETED OR HELICAL LOCK-SEAM						
12	1	45	45			
18	2	30	30	52		
24	2	22	22	39	41	
30	2		18	31	32	34
36	2.5		15	26	27	28
42	2			43	43	44
48	2			40	41	43
54	2			35	37	38
60	2				33	34
66	2					31
72	2					29

EQUIVALENT METAL THICKNESSES AND GAUGES

METAL THICKNESS IN INCHES			GAUGE NUMBER	
STEEL				
ZINC COATED	UNCOATED	ALUMINUM		
0.064	0.0598	0.060		16
0.079	0.0747	0.075		14
0.109	0.1046	0.105		12
0.138	0.1345	0.135		10
0.168	0.1644	0.164	8	

CORRUGATED METAL PIPE ARCHES

EQUIV. DIA. (INCHES)	PIPE DIMENSION SPAN X RISE (INCHES)	MINIMUM CORNER RADIUS (INCHES)	STEEL				ALUMINUM			
			MIN. THICKNESS REQUIRED INCHES	① MIN. HEIGHT OF FILL, "H" (FT.)		MIN. THICKNESS REQUIRED INCHES	① MIN. HEIGHT OF FILL, "H" (FT.)			
				INSTALLATION			INSTALLATION			
				TYPE 1	TYPE 1		TYPE 1	TYPE 1		
2 3/8 INCH BY 1/2 INCH CORRUGATION RIVETED, WELDED, OR HELICAL LOCK-SEAM										
15	17x13	3	0.064	2	15	0.060	2	15		
18	21x15	3	0.064	2	15	0.060	2	15		
21	24x18	3	0.064	2.25	15	0.060	2.25	15		
24	28x20	3	0.064	2.5	15	0.075	2.5	15		
30	35x24	3	0.079	3	12	0.075	3	12		
36	42x29	3 1/2	0.079	3	12	0.105	3	12		
42	49x33	4	0.079	3	12	0.105	3	12		
48	57x38	5	0.109	3	13	0.135	3	13		
54	64x43	6	0.109	3	14	0.135	3	14		
60	71x47	7	0.138	3	15	0.135	3	14		
66	77x52	8	0.168	3	15					
72	83x57	9	0.168	3	15	0.164	3	15		
② 3 INCH BY 1 INCH OR 5 INCH BY 1 INCH CORRUGATION RIVETED, WELDED, OR HELICAL LOCK-SEAM										
			INSTALLATION		INSTALLATION					
			TYPE 2	TYPE 1	TYPE 2	TYPE 1				
36	40x31	5	0.079	3	2	12	15			
42	46x36	6	0.079	3	2	13	15			
48	53x41	7	0.079	3	2	13	15			
54	60x46	8	0.079	3	2	13	15			
60	66x51	9	0.079	3	2	13	15			
66	73x55	12	0.079	3	2	15	15			
72	81x59	14	0.079	3	2	15	15			
78	87x63	14	0.079	3	2	15	15			
84	95x67	16	0.109	3	2	15	15			
90	103x71	16	0.109	3	2	15	15			
96	112x75	18	0.109	3	2	15	15			
102	117x79	18	0.109	3	2	15	15			
108	128x83	18	0.138	3	2	15	15			

① FOR MINIMUM COVER VALUES, "H" SHALL INCLUDE A MINIMUM 12" OF PAVEMENT AND/OR BASE.

② WHERE THE STANDARD 2 2/3" X 1/2" CORRUGATION AND GAUGE IS SPECIFIED FOR A GIVEN DIAMETER, A PIPE OF THE SAME DIAMETER WITH A 3" X 1" OR 5" X 1" CORRUGATION MAY BE SUBSTITUTED, PROVIDING IT IS GAUGED FOR A FILL HEIGHT CONDITION EQUAL TO OR GREATER THAN THE MAXIMUM FILL HEIGHT CONDITION FOR THE SPECIFIED GAUGE AND CORRUGATION.

GENERAL NOTES


1. METAL PIPE CULVERT CONSTRUCTION SHALL CONFORM TO ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION), WITH APPLICABLE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS. UNLESS OTHERWISE NOTED IN THE PLANS, SECTION AND SUBSECTION REFER TO THE STANDARD CONSTRUCTION SPECIFICATIONS.
2. METAL PIPE CULVERT DESIGN SHALL CONFORM TO AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, FIFTH EDITION (2010) WITH 2010 INTERIMS.
3. METAL PIPE CULVERT MATERIALS AND INSTALLATIONS SHALL CONFORM TO SECTION 606 AND JOB SPECIAL PROVISION "METAL PIPE".
4. ALL PIPE SHALL BE PROTECTED DURING CONSTRUCTION BY A COVER SUFFICIENT TO PREVENT DAMAGE FROM PASSAGE OF EQUIPMENT.
5. THE MINIMUM TRENCH WIDTH SHALL BE THE OUTSIDE DIAMETER OF THE PIPE PLUS 24 INCHES. THE MAXIMUM ALLOWABLE TRENCH WIDTH SHALL BE THE MINIMUM WIDTH PRACTICABLE FOR WORKING CONDITIONS.
6. MULTIPLE PIPE CULVERTS SHALL BE INSTALLED WITH A MINIMUM CLEARANCE OF 24 INCHES BETWEEN STRINGS OF PIPE. REFER TO STD. DWG. FES-2 FOR MINIMUM CLEARANCE WHERE FLARED END SECTIONS ARE USED.
7. IMPERVIOUS MATERIAL SHOULD BE PLACED AS DIRECTED BY THE ENGINEER AT THE ENDS OF THE CULVERT TO PREVENT LOSS OF STRUCTURAL BEDDING WHEN PERVIOUS MATERIAL IS USED FOR STRUCTURAL BEDDING AND/OR BACKFILL.
8. WHEN DIRECTED BY THE ENGINEER, UNSUITABLE MATERIAL THAT IS ENCOUNTERED AT THE BOTTOM OF THE EXCAVATED TRENCH (BELOW THE AREA IDENTIFIED AS "STRUCTURAL BEDDING" ABOVE) WILL BE EXCAVATED AND REPLACED WITH SELECTED PIPE BEDDING. THE QUANTITY OF MATERIAL REQUIRED TO BACKFILL THE UNDERCUT AREA UP TO THE SELECTED PIPE BEDDING PAY LIMIT DESIGNATED ABOVE WILL BE MEASURED AND PAID FOR AS "SELECTED PIPE BEDDING."
9. WHEN THE EXISTING MATERIAL EXCAVATED FOR THE PIPE TRENCH IS DETERMINED BY THE ENGINEER TO BE UNSUITABLE FOR BACKFILLING THE PIPE (ABOVE THE AREA IDENTIFIED AS STRUCTURAL BACKFILL), BORROW MATERIAL OR MATERIAL FROM THE ROADWAY EXCAVATION WILL BE USED TO BACKFILL THE PIPE. IF SUITABLE MATERIAL IS NOT AVAILABLE, THE ENGINEER MAY AUTHORIZE THE USE OF "SELECTED PIPE BACKFILL."

DATE	REVISION	DATE FILMED
2-27-14	REVISED GENERAL NOTE 1	
12-15-11	REVISED FOR LRFD DESIGN SPECS	
3-30-00	REVISED INSTALLATIONS	
11-06-97	ISSUED	

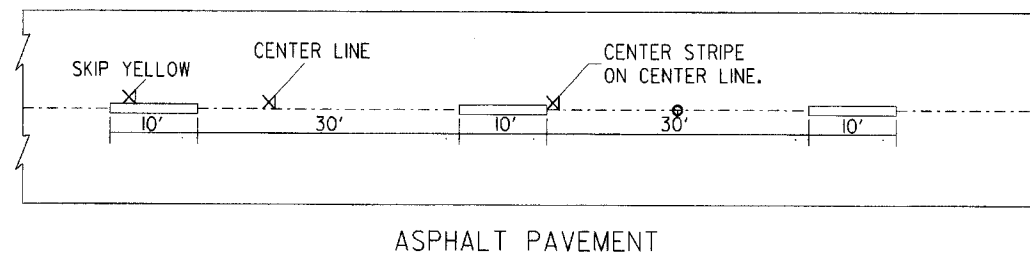
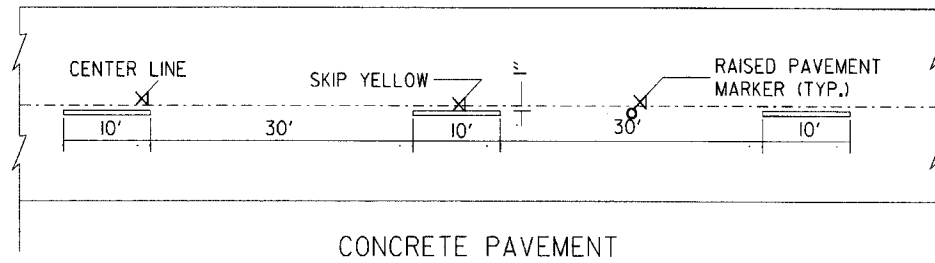
ARKANSAS STATE HIGHWAY COMMISSION

METAL PIPE CULVERT
FILL HEIGHTS & BEDDING

STANDARD DRAWING PCM-1



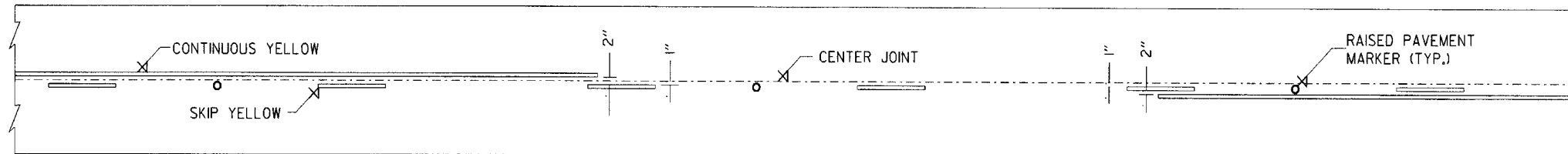
- NOTES:
1. REFER TO THE STRIPING DETAILS FOR PAVEMENT MARKING LINE WIDTHS.
 2. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
 3. RAISED PAVEMENT MARKERS SHALL BE PLACED ON AN 80 FEET SPACING UNLESS OTHERWISE SHOWN IN THE PLANS.



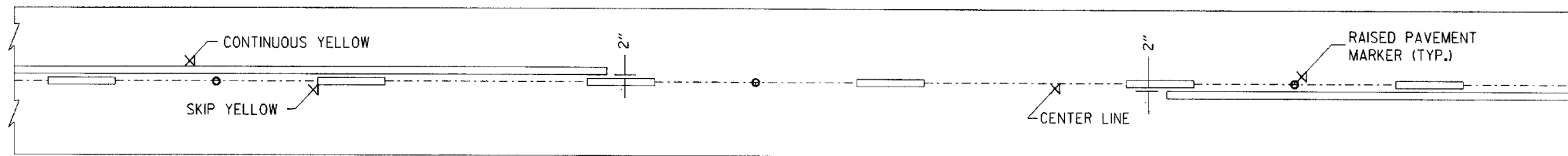
CONCRETE PAVEMENT

ASPHALT PAVEMENT

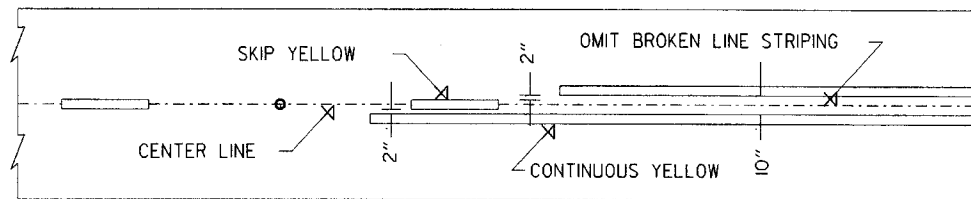
BROKEN LINE STRIPING



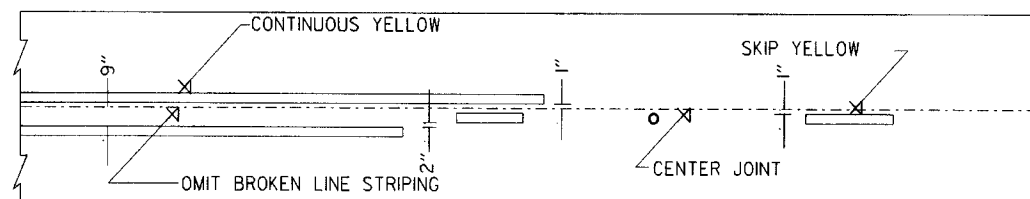
SOLID LINE STRIPING ON CONCRETE PAVEMENT



SOLID LINE STRIPING ON ASPHALT PAVEMENT

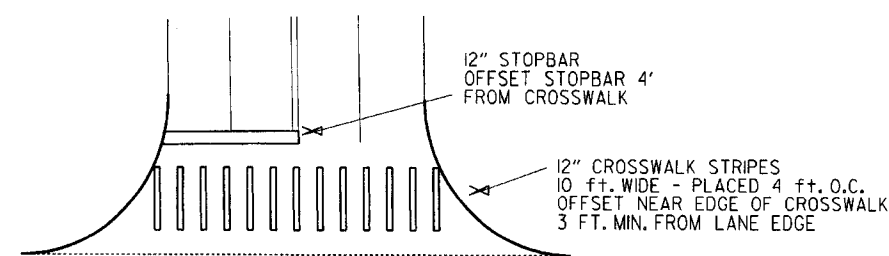


ASPHALT PAVEMENT



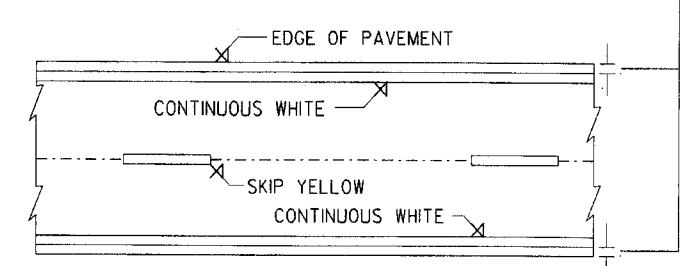
CONCRETE PAVEMENT

STRIPING AT ADJACENT NO PASSING LANES

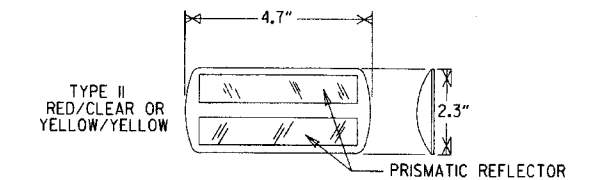


CROSSWALK AND STOPBAR DETAILS

2" FOR ASPHALT OR CONCRETE PAVEMENT
6" FOR BITUMINOUS SURFACE TREATMENT



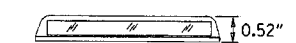
PAVEMENT EDGE LINE MARKING



TYPE II RED/CLEAR OR YELLOW/YELLOW

PRISMATIC REFLECTOR

NOTE: THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

NOTE: DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

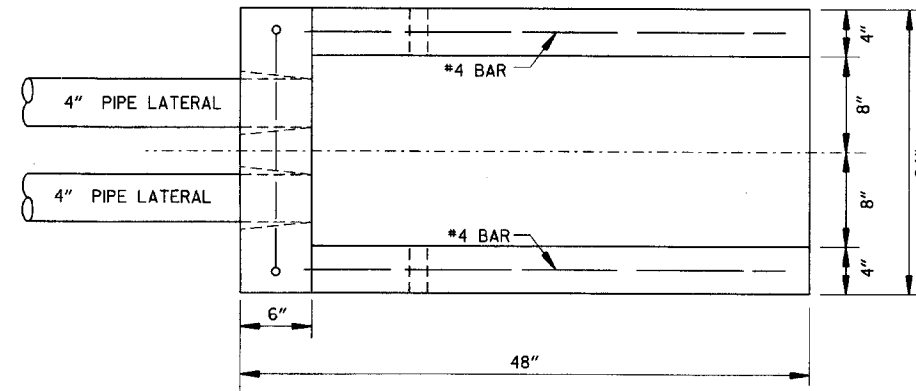
5-12-16	REVISED LINE WIDTHS, SPACING, & NOTES	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REVISED GENERAL NOTES & REMOVED PLOWABLE PAV'T. MRKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTLS.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION

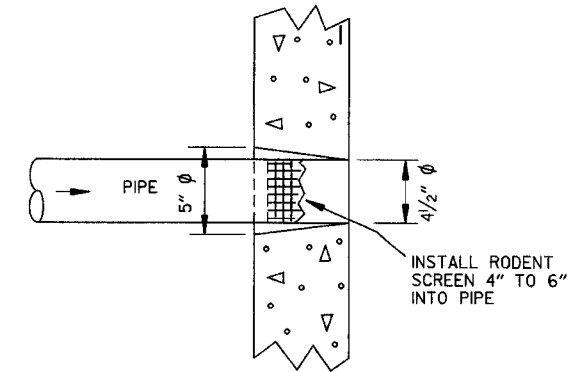
PAVEMENT MARKING DETAILS

STANDARD DRAWING PM-1

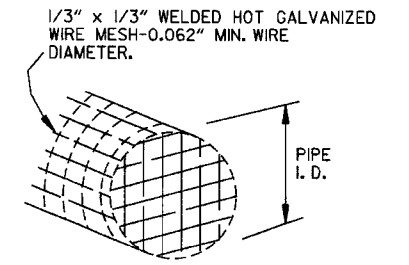
NOTE:
 1. GRANULAR BACKFILL TO BE SUBSIDIARY TO PIPE UNDERDRAIN.
 2. UNLESS OTHERWISE SPECIFIED ON THE PLANS, THE UNDERDRAIN COVER SHALL BE THOROUGHLY COMPACTED EARTH AND SHALL BE SUBSIDIARY TO PIPE UNDERDRAIN.
 3. GRANULAR MATERIAL SHALL BE WRAPPED WITH GEOTEXTILE FABRIC, LAP FABRIC 12" OR THE WIDTH OF THE TRENCH AT THE TOP.



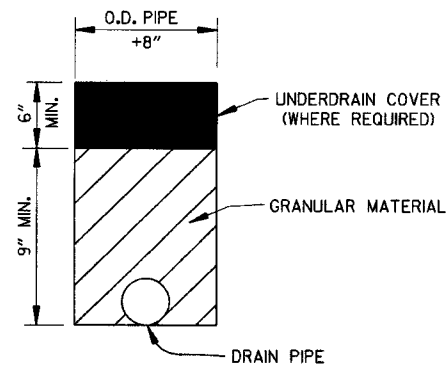
PLAN VIEW



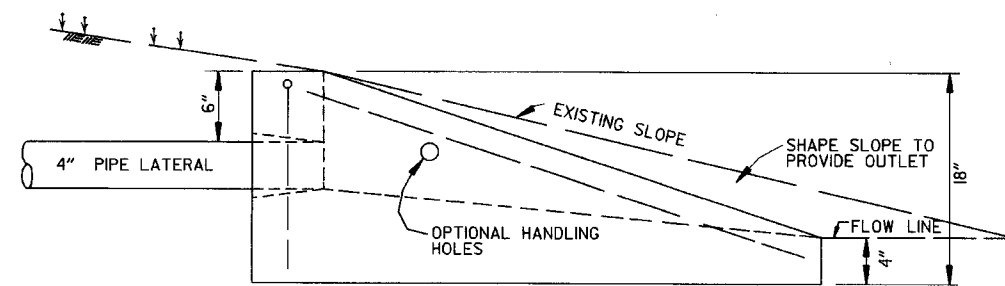
DETAIL OF HOLE FOR 4" PIPE



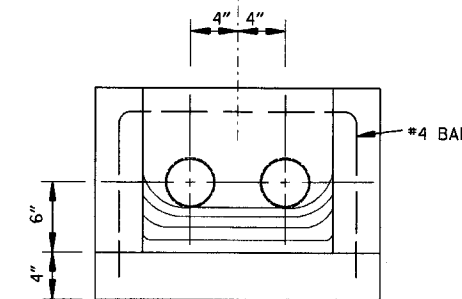
DETAIL OF RODENT SCREEN



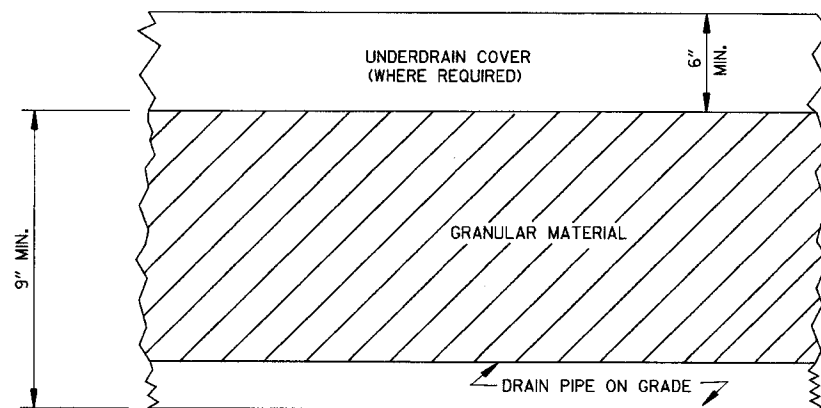
DRAIN PIPE



SIDE VIEW



FRONT VIEW

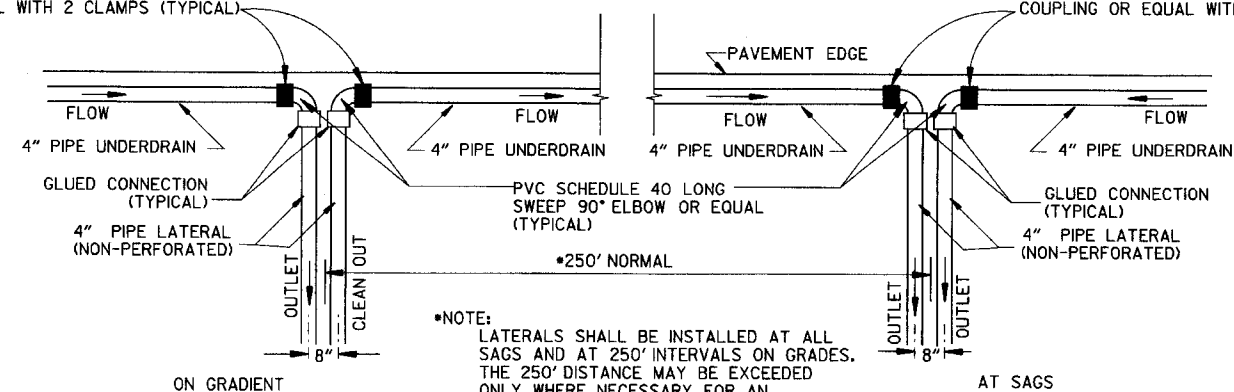


DETAILS OF PIPE UNDERDRAIN

FERNCO 1056-44 (4" CI/PLASTIC) OR FERNCO 1051-44 (4" AC/DIOR 4" CI/PLASTIC) COUPLING OR EQUAL WITH 2 CLAMPS (TYPICAL)

UNDERDRAIN OUTLET PROTECTORS

FERNCO 1056-44 (4" CI/PLASTIC) OR FERNCO 1051-44 (4" AC/DIOR 4" CI/PLASTIC) COUPLING OR EQUAL WITH 2 CLAMPS (TYPICAL)



*NOTE: LATERALS SHALL BE INSTALLED AT ALL SAGS AND AT 250' INTERVALS ON GRADES. THE 250' DISTANCE MAY BE EXCEEDED ONLY WHERE NECESSARY FOR AN ACCEPTABLE OUTLET.

DETAIL OF PIPE UNDERDRAIN LATERALS WHEN PLACED ALONG PAVEMENT EDGE

NOTE: PVC PIPE FOR LATERALS SHALL MEET THE REQUIREMENTS OF ASTM D 1785 (LATEST REVISION) FOR SCHEDULE 40 PIPE.

4-10-03	REVISED NOTE 3	
1-12-00	REVISED DETAIL OF UNDERDRAIN LATERALS	
11-18-98	REVISED NOTE	
10-18-96	REVISED MIN. DEPTH & GEOTEXTILE FABRIC	
4-26-96	ADDED LATERAL NOTE; 5 1/2" TO 5"	
11-22-95	REVISED LATERALS	
7-20-95	REVISED LATERALS & ADDED NOTE	
11-3-94	REVISED FOR DUAL LATERALS	11-3-94
10-1-92	SUBSTITUTED GEOTEXTILE	10-1-92
8-15-91	ADDED POLYETHYLENE PIPE	8-15-91
11-8-90	DELETED ALTERNATE NOTE	11-8-90
1-25-90	ADDED 4" SNAP ADAPTER	1-25-90
11-30-89	DEL. (SUBGRADE); ADDED (WHERE REQUIRED)	11-30-89
7-15-88	ISSUED P.L.M.	647-7-15-88
DATE	REVISION	DATE FILMED

ARKANSAS STATE HIGHWAY COMMISSION

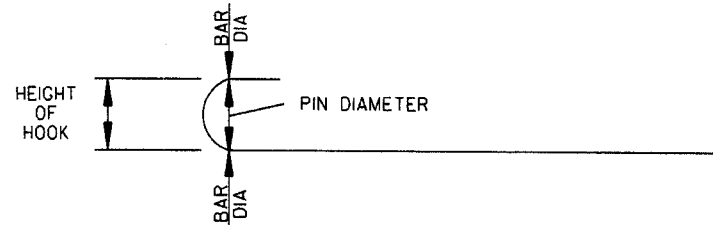
DETAILS OF PIPE UNDERDRAIN

STANDARD DRAWING PU-1

STEEL FABRICATION: REINFORCING STEEL FABRICATION SHALL CONFORM TO THE DIMENSIONS LISTED IN THE TABLE BELOW:

BAR SIZE	PIN DIAMETER	HOOK EXTENSION "K"
3	2 1/4"	4"
4	3"	4 1/2"
5	3 3/4"	5"
6	4 1/2"	6"
7	5 1/4"	7"
8	6"	8"

IF THE OVERALL HEIGHT OF THE HOOK (SEE DIAGRAM BELOW) FOR A "b", "bi", "b2" or "b3" BENT BAR IS GREATER THAN THE CORRESPONDING TOP OR BOTTOM SLAB THICKNESS, LESS 2 3/4 INCHES, EACH BENT BAR SHALL BE REPLACED WITH ONE HOOKED BAR AND ONE STRAIGHT BAR, USING LENGTHS AS SHOWN IN THE TABLE BELOW. THE TWO BARS SHALL BE THE SAME DIAMETER AS, AND PLACED AT THE SAME SPACING AS, THE "b", "bi", "b2" OR "b3" BENT BARS THEY REPLACE.



NOTE: DIMENSIONS OF BARS ARE MEASURED OUT TO OUT OF BARS.

OVERALL HEIGHT OF HOOKED BAR DIAGRAM

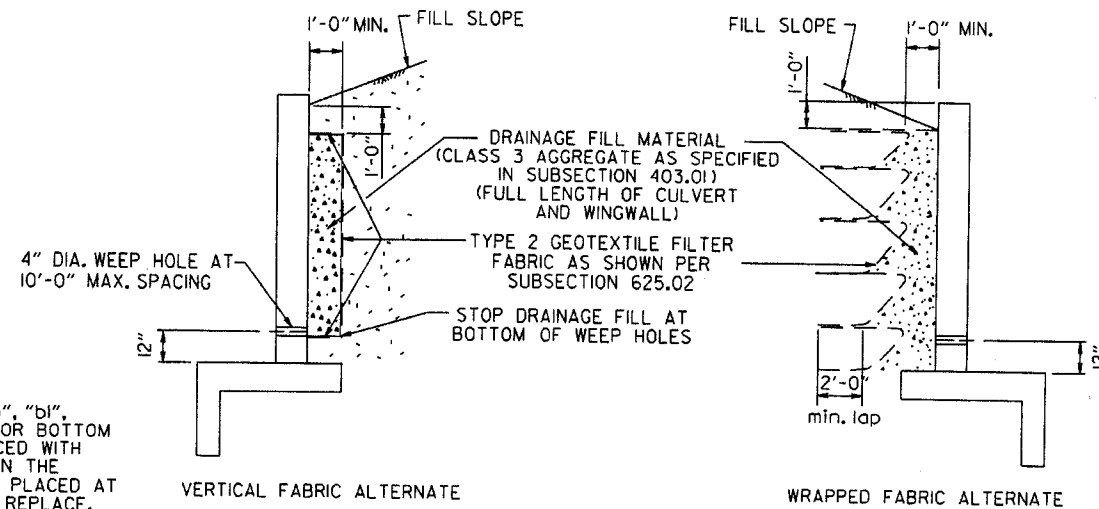
THE HOOKED BARS SHALL BE PLACED IN THE BOTTOM OF THE TOP SLAB AND THE TOP OF THE BOTTOM SLAB. THE STRAIGHT BARS SHALL BE PLACED IN THE TOP OF THE TOP SLAB AND THE BOTTOM OF THE BOTTOM SLAB. SEE TABLE BELOW FOR LENGTHS OF REPLACEMENT HOOKED AND STRAIGHT BARS.

FOR SKEWED CULVERTS, THE REPLACEMENT STRAIGHT BAR MAY HAVE TO BE CUT IN FIELD TO FIT.

REPLACEMENT BAR LENGTHS TABLE

BAR SIZE: "b", "bi", "b2" OR "b3"	LENGTH OF HOOKED BAR	LENGTH OF STRAIGHT BAR
#4	L + 1' - 0"	SEE "c" BAR LENGTH
#5	L + 1' - 2"	SEE "c" BAR LENGTH
#6	L + 1' - 4"	SEE "c" BAR LENGTH
#7	L + 1' - 8"	SEE "c" BAR LENGTH
#8	L + 1' - 10"	SEE "c" BAR LENGTH
#9	L + 2' - 6"	SEE "c" BAR LENGTH

L = "OW" - 3 INCHES



WINGWALL & CULVERT DRAINAGE DETAIL

REINFORCED CONCRETE BOX CULVERT GENERAL NOTES

CONCRETE SHALL BE CLASS S WITH A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 3500 PSI. REINFORCING STEEL SHALL BE AASHTO M 31 OR M 53, GRADE 60.

CONSTRUCTION AND MATERIALS FOR WINGWALL & CULVERT DRAINAGE, INCLUDING WEEP HOLES AND GRANULAR MATERIAL, SHALL BE SUBSIDIARY TO THE BID ITEM, "CLASS S CONCRETE".

MEMBRANE WATERPROOFING SHALL CONFORM TO THE REQUIREMENTS OF SECTION 815 OF THE STANDARD SPECIFICATIONS.

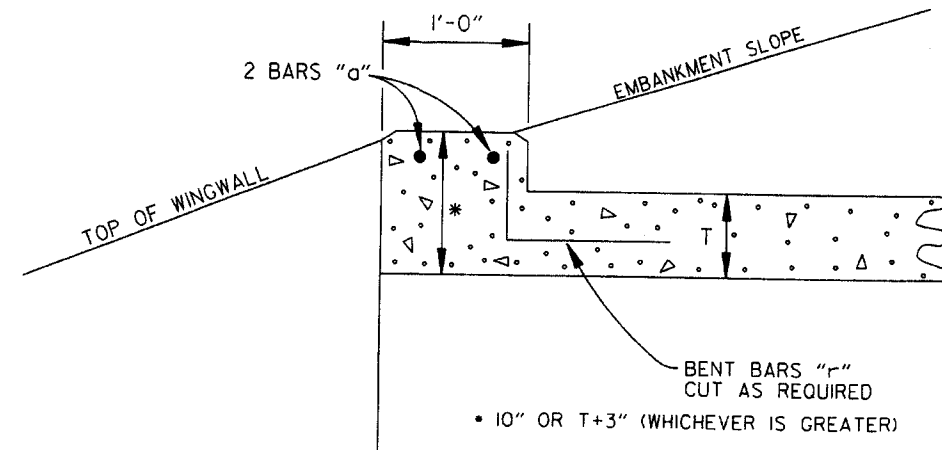
MEMBRANE WATERPROOFING SHALL BE APPLIED TO ALL CONSTRUCTION JOINTS IN THE TOP SLAB AND THE SIDEWALLS OF R.C. BOX CULVERTS AS DIRECTED BY THE ENGINEER. NO PAYMENT SHALL BE MADE FOR THIS ITEM, BUT PAYMENT WILL BE CONSIDERED TO BE INCLUDED IN THE VARIOUS ITEMS BID FOR THE R.C. BOX CULVERT.

REINFORCING STEEL TOLERANCES: THE TOLERANCES FOR REINFORCING STEEL SHALL MEET THOSE LISTED IN "MANUAL OF STANDARD PRACTICE" PUBLISHED BY CONCRETE REINFORCING STEEL INSTITUTE (CRSI) EXCEPT THAT THE TOLERANCE FOR TRUSS BARS SUCH AS FIGURE 3 ON PAGE 7-4 OF THE CRSI MANUAL SHALL BE MINUS ZERO TO PLUS 1/2 INCH.

WEEP HOLES IN BOX CULVERT WALLS SHALL HAVE A MAXIMUM HORIZONTAL SPACING OF 10'-0" AND SHALL BE SPACED TO CLEAR ALL REINFORCING STEEL. THE DRAIN OPENING SHALL BE 4" DIAMETER AND SHALL BE PLACED 12" ABOVE THE TOP OF THE BOTTOM SLAB.

WEEP HOLES IN WINGWALLS SHALL HAVE A MAXIMUM HORIZONTAL SPACING OF 10'-0" AND SHALL BE SPACED TO CLEAR ALL REINFORCING STEEL. THERE SHALL BE A MINIMUM OF TWO (2) WEEP HOLES IN EACH WINGWALL. THE DRAIN OPENING SHALL BE 4" DIAMETER AND SHALL BE PLACED 12" ABOVE THE TOP OF THE WINGWALL FOOTING.

THE REQUIREMENTS SHOWN ON THIS DRAWING SHALL SUPERCEDE THE CORRESPONDING REQUIREMENTS ON ALL REINFORCED CONCRETE BOX CULVERT STANDARD DRAWINGS.



NOTE: FOR ALL SKEWED R.C. BOX CULVERTS THE LENGTH "K" OF THE MODIFIED HEADWALL SHALL BE EQUAL TO THE ROADWAY LENGTH "RL". THE ENDS OF THE HEADWALL SHALL BE CONSTRUCTED PARALLEL TO THE SKEW ANGLE OF THE BOX CULVERT.

R.C. BOX CULVERT HEADWALL MODIFICATIONS

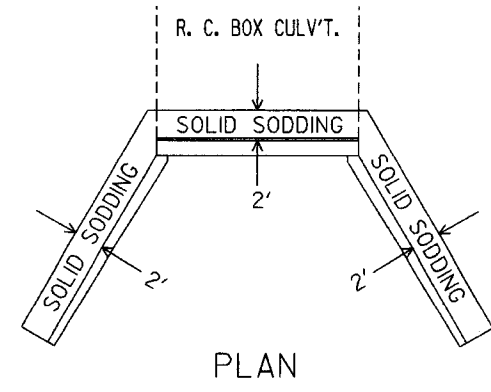
DATE	REVISION	DATE FILMED
7/26/12	REV. DRAINAGE FILL MATERIAL & DETAIL	
12/15/11	REQUIRE WEEP HOLES IN BOX CULVERT WALLS	
5-25-06	REV. GEN. NOTES AND DETAILS FOR WEEP HOLES; BAR DIAGRAM	
11-16-01	ADDED WINGWALL DRAINAGE DETAIL/EDITED GEN. NOTES	
10-18-96	REV. ASTM REF. TO AASHTO & ADDED BAR DIAGRAM	
10-12-95	MOVED SOLID SODDING DETAIL TO RCB-2	
6-2-94	ADDED SOLID SODDING PLAN DETAIL	
8-5-93	REVISED PIN DIAMETER TO SPECS.	
8-15-91	DRAWN AND ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION

REINFORCED CONCRETE BOX CULVERT DETAILS

STANDARD DRAWING RCB-1

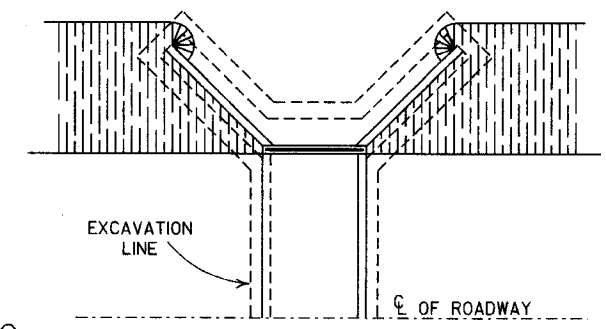
SOLID SODDING



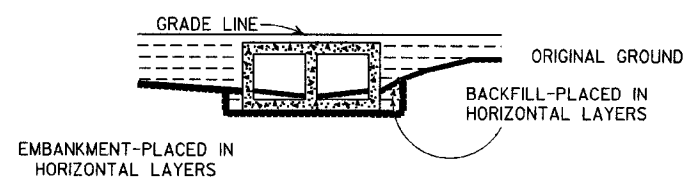
PLAN

PARTIAL SECTION SHOWING SOLID SODDING AT HEADWALLS AND WING WALLS

NOTE: LENGTH MEASURED ALONG THE CENTER OF 2' STRIP OF SOLID SODDING.

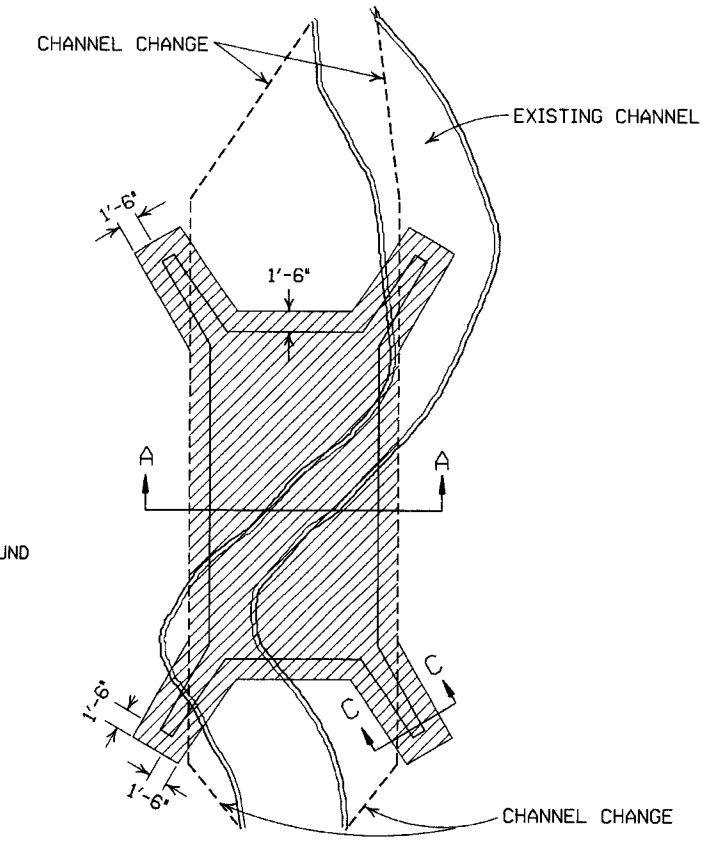


PLAN

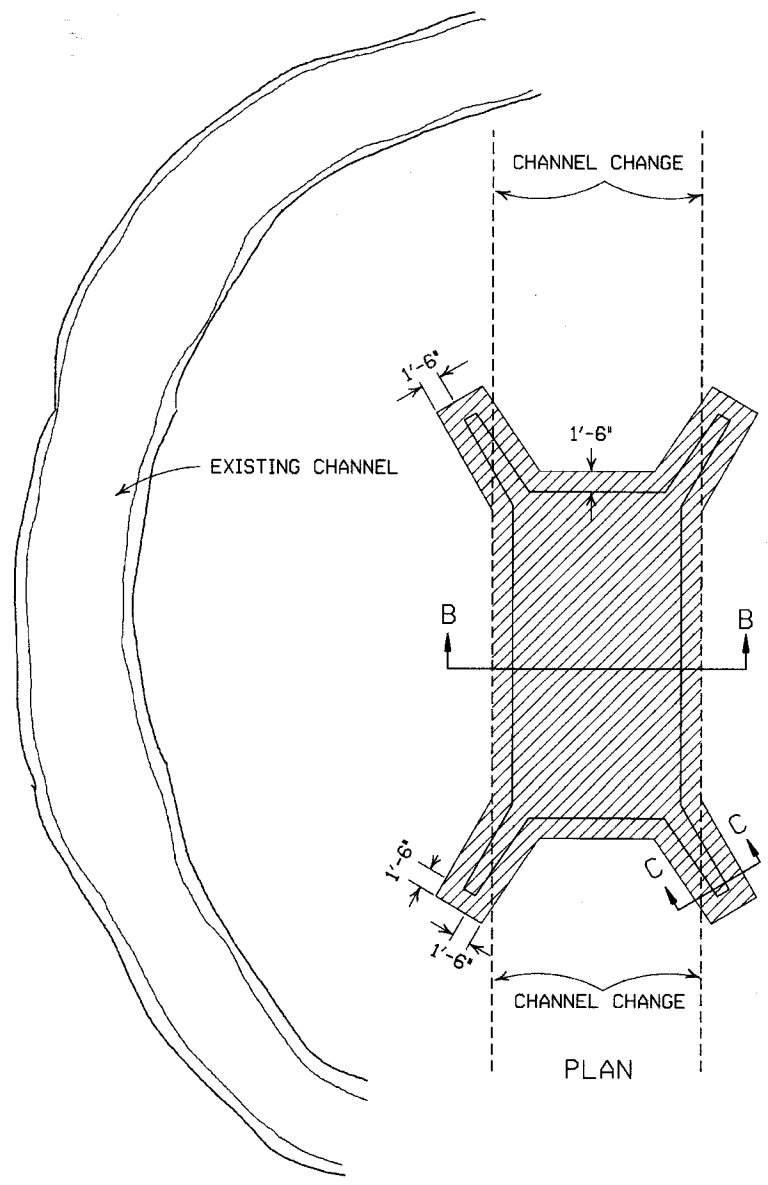


LONGITUDINAL SECTION

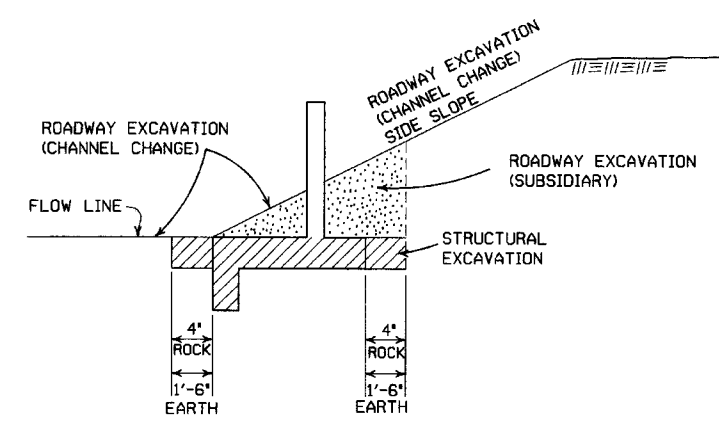
BACKFILL DETAILS FOR BOX CULVERT



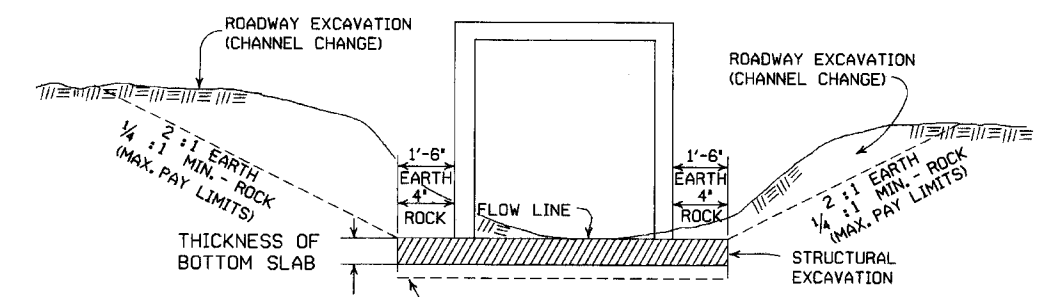
PLAN



PLAN

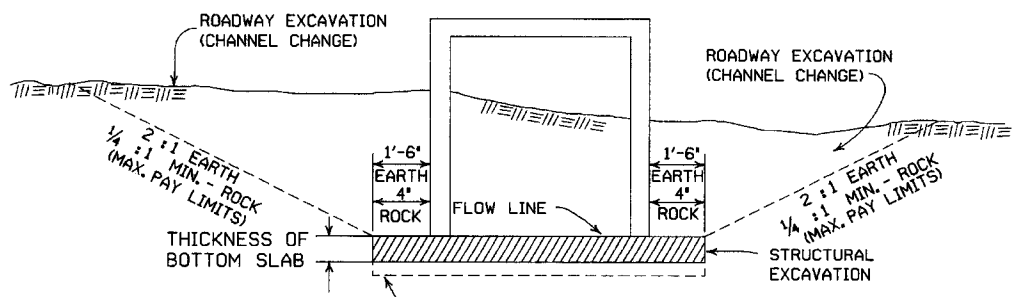


SECTION C-C



SECTION A-A

DETAILS THROUGH EXISTING CHANNELS



UNDERCUT SHALL BE MEASURED AND PAID FOR ACCORDING TO SECTIONS 801.10 AND 801.11, RESPECTIVELY, OF THE STANDARD SPECIFICATIONS.

SECTION B-B
DETAILS FOR NEW CHANNELS

GENERAL NOTES:

ROADWAY EXCAVATION (CHANNEL CHANGE) WILL BE PAID FOR AT R.C. BOX CULVERT LOCATIONS. IT WILL BE PAID TO THE LIMITS ACTUALLY CUT AND WILL BE CONFINED TO THAT PORTION OF THE INDICATED AREA THAT IS ABOVE THE FLOW LINE. ROADWAY EXCAVATION (CHANNEL CHANGE) SHALL BE MEASURED BY CROSS SECTIONS AND VOLUMES COMPUTED BY AVERAGE END AREA METHOD. ALL CHANNEL CHANGES SHALL BE BROUGHT TO GRADE PRIOR TO MAKING ANY EXCAVATION FOR STRUCTURES.

EXCAVATION FOR STRUCTURES WILL BE PAID FOR AT ALL R.C. BOX CULVERT LOCATIONS. IT WILL BE PAID TO THE LIMITS SHOWN AND SHALL BE CONFINED TO THAT PORTION OF THE INDICATED AREA THAT IS BELOW THE CHANNEL FLOW LINE.

ROADWAY EXCAVATION SHOWN IN SECTION C-C ABOVE AS SUBSIDIARY WILL NOT BE MEASURED OR PAID FOR DIRECTLY, BUT PAYMENT WILL BE CONSIDERED TO BE INCLUDED IN THE VARIOUS ITEMS OF EXCAVATION.

11-20-03	REVISED SECTION A-A NOTE	
8-22-02	REVISED SECTION B-B NOTE	
10-12-95	COMBINED 1891B AND 1888A	
1-4-83	REVISED GENERAL NOTES AND ADDED MAXIMUM PAY LIMIT NOTES.	674-1-4-83
2-2-76	EXCAV. PAY LIMITS	917-2-2-76
10-2-72	REVISED AND REDRAWN	564-10-16-72
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION

EXCAVATION PAY LIMITS, BACKFILL, & SOLID SODDING FOR BOX CULVERTS

STANDARD DRAWING RCB-2

SUPERELEVATION TABLE FOR TWO - WAY TRAFFIC

DEGREE OF CURVE	30 MPH		40 MPH		50 MPH		55 MPH		60 MPH		70 MPH			
	Ls (FT)		Ls (FT)		Ls (FT)		Ls (FT)		Ls (FT)		Ls (FT)			
e	MINIMUM	DESIRABLE	e	MINIMUM	DESIRABLE	e	MINIMUM	DESIRABLE	e	MINIMUM	DESIRABLE	e	MINIMUM	DESIRABLE
0° 15'	N.C.		N.C.			N.C.			N.C.			N.C.		
0° 30'	N.C.		N.C.			N.C.			N.C.			N.C.		
0° 45'	N.C.		N.C.			N.C.			N.C.			N.C.		
1° 00'	N.C.		N.C.			N.C.			N.C.			N.C.		
1° 15'	N.C.		N.C.			N.C.			N.C.			N.C.		
1° 30'	N.C.		N.C.			N.C.			N.C.			N.C.		
1° 45'	N.C.		N.C.			N.C.			N.C.			N.C.		
2° 00'	R.C.		0.021	175		0.021	200		0.023	250		0.028	275	300
2° 15'	R.C.		0.025			0.026			0.030			0.037		
2° 30'	R.C.		0.028			0.031			0.037			0.046		
2° 45'	R.C.		0.031			0.034			0.043			0.054		
3° 00'	0.021		0.034			0.040			0.049			0.062		
3° 15'	0.023	150	0.037			0.043			0.055			0.070		
3° 30'	0.025		0.040			0.046			0.058			0.075	300	
3° 45'	0.027		0.043			0.049			0.061			0.078		350
4° 00'	0.029		0.046			0.052			0.064			0.081		400
4° 15'	0.031	200	0.049			0.055			0.067			0.085		
4° 30'	0.033		0.051			0.057			0.072			0.091		
4° 45'	0.037		0.056			0.061			0.077			0.096		
5° 00'	0.040		0.061			0.066	205		0.082	260		0.100		
5° 15'	0.043		0.066	185		0.072			0.088	275		0.108		
5° 30'	0.046		0.070			0.076			0.093	295		0.115		
5° 45'	0.050		0.074			0.080			0.099	315		0.122		
6° 00'	0.053		0.078			0.083	215		0.103	335		0.130		
6° 15'	0.058		0.081			0.087			0.106	355		0.138		
6° 30'	0.063	160	0.084			0.091			0.110	375		0.145		
6° 45'	0.068		0.087	250		0.094			0.114	395		0.152		
7° 00'	0.073		0.089			0.096	205		0.118	415		0.160		
7° 15'	0.078		0.091			0.098			0.122	435		0.168		
7° 30'	0.083		0.094			0.101			0.126	455		0.175		
7° 45'	0.088		0.096			0.104			0.130	475		0.182		
8° 00'	0.091		0.098			0.106	215		0.134	495		0.190		
8° 15'	0.093		0.100			0.108			0.138	515		0.198		
8° 30'	0.095		0.102			0.110			0.142	535		0.205		
8° 45'	0.097		0.104			0.112			0.146	555		0.212		
9° 00'	0.099		0.106			0.114			0.150	575		0.220		
9° 15'	0.101		0.108			0.116			0.154	595		0.228		
9° 30'	0.103		0.110			0.118			0.158	615		0.235		
9° 45'	0.105		0.112			0.120			0.162	635		0.242		
10° 00'	0.107		0.114			0.122			0.166	655		0.250		
10° 15'	0.109		0.116			0.124			0.170	675		0.258		
10° 30'	0.111		0.118			0.126			0.174	695		0.265		
10° 45'	0.113		0.120			0.128			0.178	715		0.272		
11° 00'	0.115		0.122			0.130			0.182	735		0.280		
11° 15'	0.117		0.124			0.132			0.186	755		0.288		
11° 30'	0.119		0.126			0.134			0.190	775		0.295		
11° 45'	0.121		0.128			0.136			0.194	795		0.302		
12° 00'	0.123		0.130			0.138			0.198	815		0.310		
12° 15'	0.125		0.132			0.140			0.202	835		0.318		
12° 30'	0.127		0.134			0.142			0.206	855		0.325		
12° 45'	0.129		0.136			0.144			0.210	875		0.332		
13° 00'	0.131		0.138			0.146			0.214	895		0.340		
13° 15'	0.133		0.140			0.148			0.218	915		0.348		
13° 30'	0.135		0.142			0.150			0.222	935		0.355		
13° 45'	0.137		0.144			0.152			0.226	955		0.362		
14° 00'	0.139		0.146			0.154			0.230	975		0.370		
14° 15'	0.141		0.148			0.156			0.234	995		0.378		
14° 30'	0.143		0.150			0.158			0.238	1015		0.385		
14° 45'	0.145		0.152			0.160			0.242	1035		0.392		
15° 00'	0.147		0.154			0.162			0.246	1055		0.400		
15° 15'	0.149		0.156			0.164			0.250	1075		0.408		
15° 30'	0.151		0.158			0.166			0.254	1095		0.415		
15° 45'	0.153		0.160			0.168			0.258	1115		0.422		
16° 00'	0.155		0.162			0.170			0.262	1135		0.430		
16° 15'	0.157		0.164			0.172			0.266	1155		0.438		
16° 30'	0.159		0.166			0.174			0.270	1175		0.445		
16° 45'	0.161		0.168			0.176			0.274	1195		0.452		
17° 00'	0.163		0.170			0.178			0.278	1215		0.460		
17° 15'	0.165		0.172			0.180			0.282	1235		0.468		
17° 30'	0.167		0.174			0.182			0.286	1255		0.475		
17° 45'	0.169		0.176			0.184			0.290	1275		0.482		
18° 00'	0.171		0.178			0.186			0.294	1295		0.490		
18° 15'	0.173		0.180			0.188			0.298	1315		0.498		
18° 30'	0.175		0.182			0.190			0.302	1335		0.505		
18° 45'	0.177		0.184			0.192			0.306	1355		0.512		
19° 00'	0.179		0.186			0.194			0.310	1375		0.520		
19° 15'	0.181		0.188			0.196			0.314	1395		0.528		
19° 30'	0.183		0.190			0.198			0.318	1415		0.535		
19° 45'	0.185		0.192			0.200			0.322	1435		0.542		
20° 00'	0.187		0.194			0.202			0.326	1455		0.550		
20° 15'	0.189		0.196			0.204			0.330	1475		0.558		
20° 30'	0.191		0.198			0.206			0.334	1495		0.565		
20° 45'	0.193		0.200			0.208			0.338	1515		0.572		
21° 00'	0.195		0.202			0.210			0.342	1535		0.580		
21° 15'	0.197		0.204			0.212			0.346	1555		0.588		
21° 30'	0.199		0.206			0.214			0.350	1575		0.595		
21° 45'	0.201		0.208			0.216			0.354	1595		0.602		
22° 00'	0.203		0.210			0.218			0.358	1615		0.610		
22° 15'	0.205		0.212			0.220			0.362	1635		0.618		
22° 30'	0.207		0.214			0.222			0.366	1655		0.625		
22° 45'	0.209		0.216			0.224			0.370	1675		0.632		
23° 00'	0.211		0.218			0.226			0.374	1695		0.640		
23° 15'	0.213		0.220			0.228			0.378	1715		0.648		
23° 30'	0.215		0.222			0.230			0.382	1735		0.655		
23° 45'	0.217		0.224			0.232			0.386	1755		0.662		
24° 00'	0.219		0.226			0.234			0.390	1775		0.670		
24° 15'	0.221		0.228			0.236			0.394	1795		0.678		
24° 30'	0.223		0.230			0.238			0.398	1815		0.685		
24° 45'	0.225		0.232			0.240			0.402	1835		0.692		

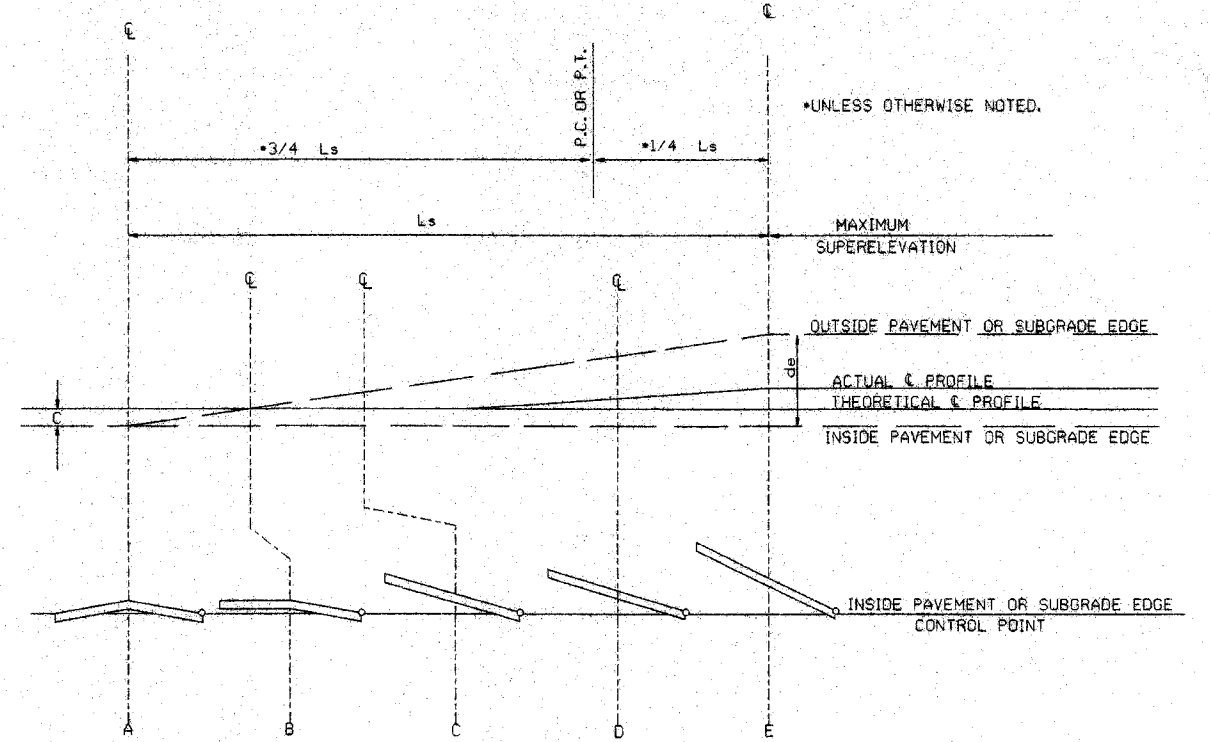
ABBREVIATIONS

- NC - NORMAL CROWN
- RC - REVERSE CROWN, SUPERELEVATION AT NORMAL CROWN SLOPE
- e - RATE OF SUPERELEVATION (FT. PER FT.)
- Ls - LENGTH OF SUPERELEVATION TRANSITION (FT.)
- L - DISTANCE FROM BEGINNING OF SUPERELEVATION TRANSITION TO ANY POINT (FT.)
- d - WIDTH OF PAVEMENT (FT.) OR WIDTH OF SUBGRADE (FT.)
- C - NORMAL CROWN (FT.)

GENERAL NOTES

- ON PAVEMENT WITH TWO-WAY TRAFFIC, THE SUPERELEVATION SHALL BE REVOLVED ON THE INSIDE PAVEMENT EDGE UNLESS OTHERWISE NOTED ON THE PLANS
- SUPERELEVATION VALUES SHOWN ON THE CROSS SECTIONS ARE VALUES (+) OR (-) TO BE ADDED TO OR SUBTRACTED FROM THE POINT OF CONTROL.
- LENGTHS FOR L MAY BE ROUNDED IN MULTIPLES OF 25 FT. OR 50 FT. TO PERMIT SIMPLER CALCULATIONS.
- PAVEMENTS WIDER THAN 2 LANES SHALL HAVE ADDITIONAL TRANSITION LENGTHS AS FOLLOWS:
 - 3 LANE UNDIVIDED - - - - +20%
 - 4 LANE UNDIVIDED - - - - +50%
 - 5 LANE UNDIVIDED - - - - +80%
 - 6 LANE UNDIVIDED - - - - +100%

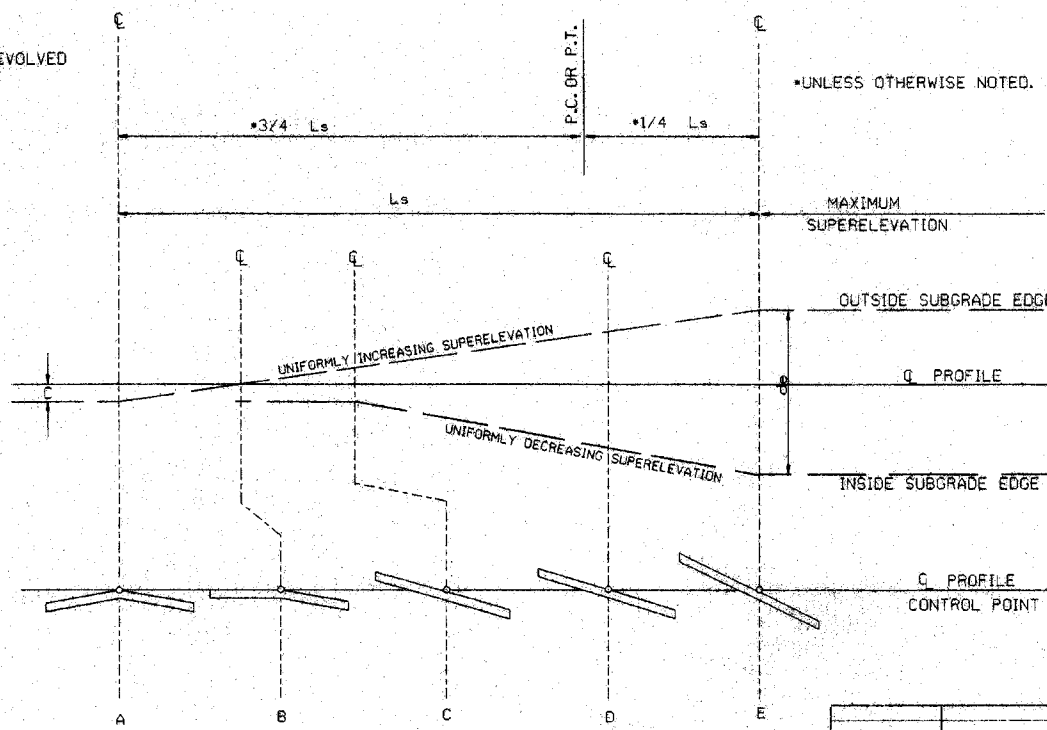
NOTE: MAINTAIN NORMAL CROWN ON INSIDE UNTIL SUPERELEVATION EXCEEDS 2C.
RATE OF SUPERELEVATION SHALL BE COMPUTED ON STRAIGHT LINE METHOD USING APPLICABLE Ls.



STANDARD METHOD WHEN SUPERELEVATION REVOLVES AROUND INNER SUBGRADE POINT OR INNER PAVEMENT EDGE

NOTE: MAINTAIN NORMAL CROWN ON INSIDE UNTIL SUPERELEVATION EXCEEDS 2C.

SUPERELEVATION FORMULA = $\frac{Lde}{Ls}$



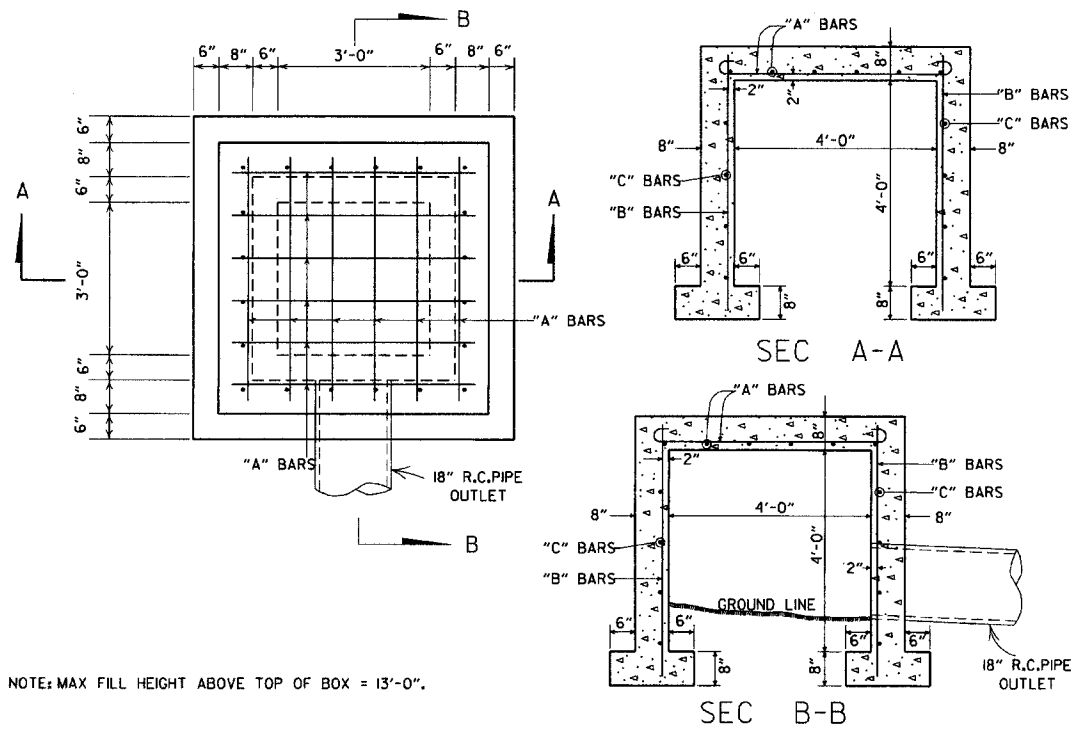
STANDARD METHOD WHEN SUPERELEVATION REVOLVES AROUND CENTER LINE

10-18-96	ADDED FORMULA	10-18-96
01-09-87	ISSUED	534-1-9-87
DATE	REVISION	DATE FILMED

ARKANSAS STATE HIGHWAY COMMISSION

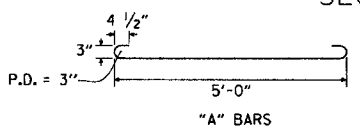
TABLES AND METHOD OF SUPERELEVATION FOR TWO-WAY TRAFFIC

STANDARD DRAWING SE-2



NOTE: MAX FILL HEIGHT ABOVE TOP OF BOX = 13'-0".

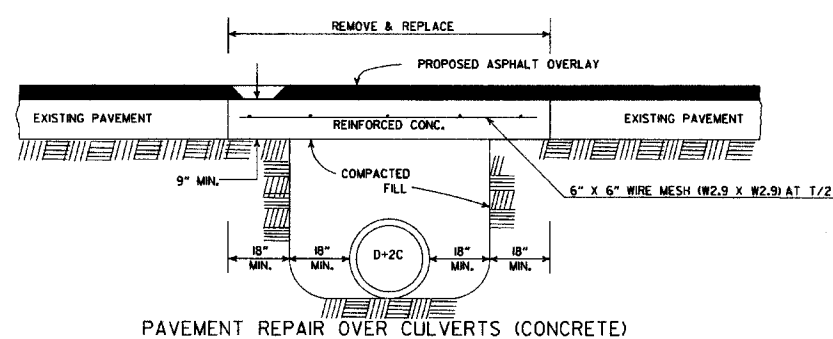
STEEL SCHEDULE			
BAR	NUMBER	LENGTH	SPACING
"A"	12	6'-0"	10"
"B"	20	5'-0"	10 1/2"
"C"	16	5'-0"	12"



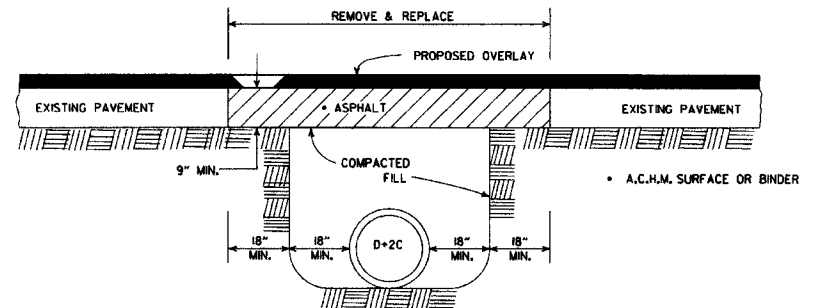
QUANTITIES
CONCRETE 3.31 CU. YDS.
REINFORCING STEEL 168 LB.

GENERAL NOTE:
THE PAY ITEMS FOR REINFORCED CONCRETE SPRING BOXES SHALL BE FOR THE QUANTITIES OF CONCRETE OF THE CLASS SPECIFIED, REINFORCING STEEL, EXCAVATION FOR STRUCTURES AND 18" R.C. PIPE CULVERT.

REINFORCED CONCRETE SPRING BOX

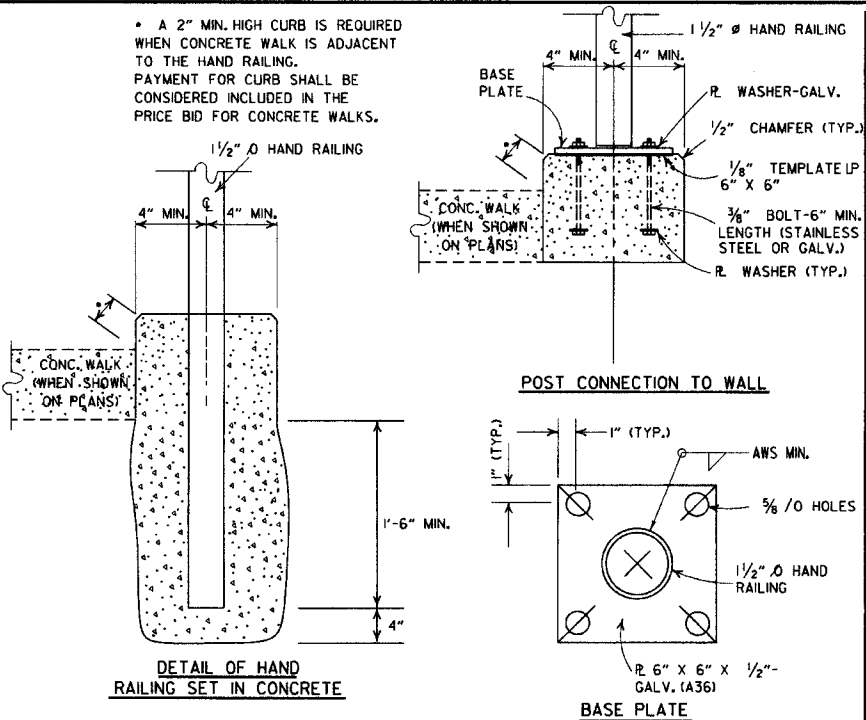


PAVEMENT REPAIR OVER CULVERTS (CONCRETE)

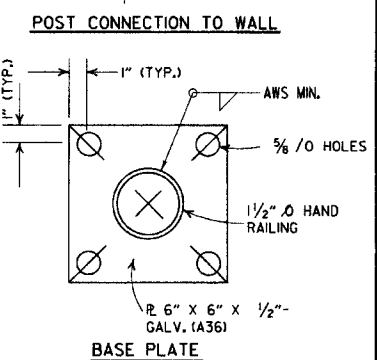


PAVEMENT REPAIR OVER CULVERTS (ASPHALT)

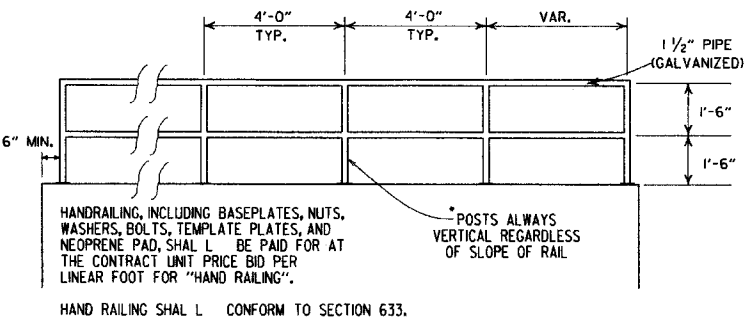
DETAIL SHOWING REPAIR OF EXISTING PAVEMENT AT CULVERT INSTALLATIONS



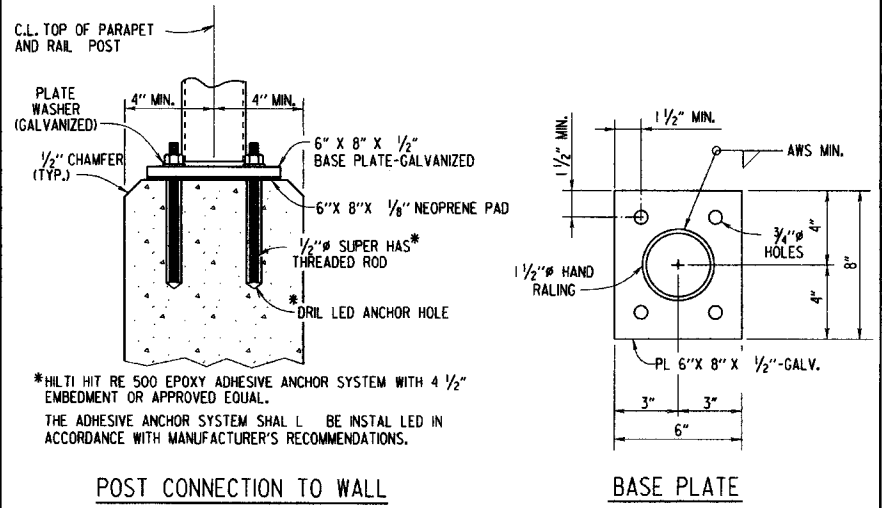
DETAIL OF HAND RAILING SET IN CONCRETE



POST CONNECTION DETAILS



HAND RAILING SHALL CONFORM TO SECTION 633.

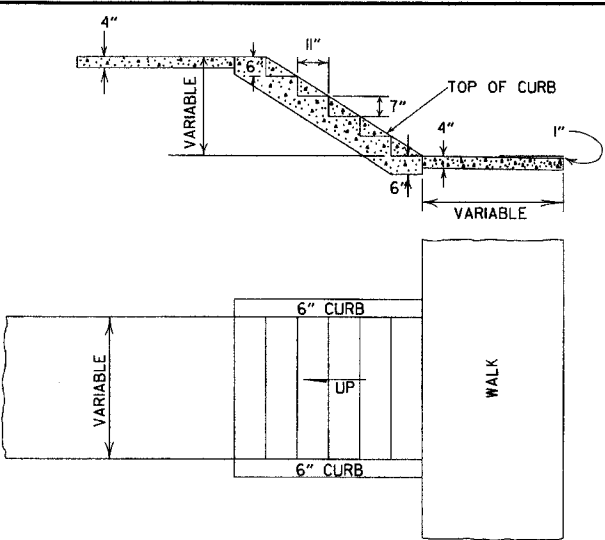


POST CONNECTION TO WALL

BASE PLATE

DETAILS OF ALTERNATE POST ANCHOR SYSTEM (EPOXY ADHESIVE ANCHORS)

HAND RAILING DETAILS



DETAILS OF CONCRETE STEPS & WALKS


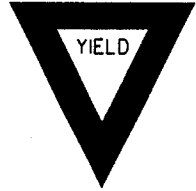
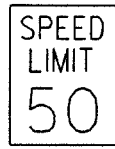



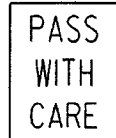


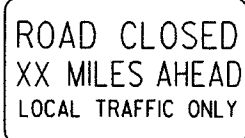
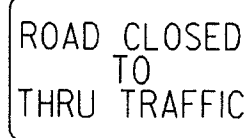
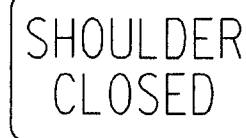
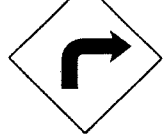
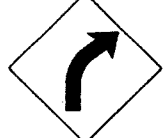


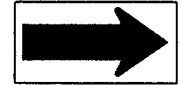
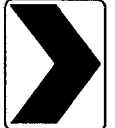
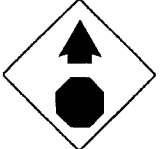

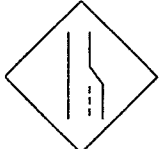









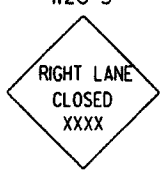


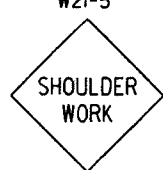
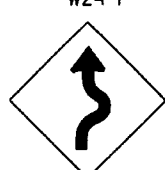



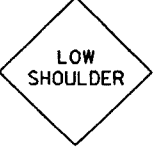
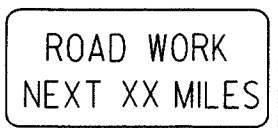
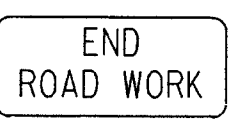
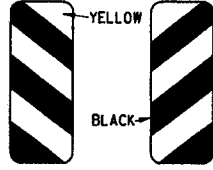


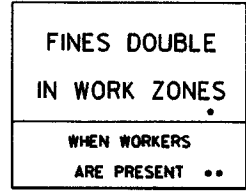
GENERAL NOTES
1. RISE AND TREAD DIMENSIONS OF STEPS MAY BE VARIED AS DIRECTED BY THE ENGINEER, HOWEVER, TREAD WIDTHS SHALL BE 11" MIN. ALL STEPS IN A FLIGHT SHALL HAVE CONSISTENT TREAD & RISER DIMENSIONS.
2. 1" TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE WALKS AT 45' INTERVALS.

DATE	REVISION	DATE FILMED
9-12-13	REVISED REINFORCED CONCRETE SPRING BOX	
7-26-12	REMOVED RETAINING WALL DETAILS & REVISED HAND RAILING DETAILS	
4-17-08	REV. JOINT & FOOTING STEP DETAILS	
11-29-07	REVISED RETAINING WALL DRAINAGE	
5-25-06	REVISED PVMT REPAIR OVER CULVERTS (CONC); REVISED REINFORCED CONC SPRING BOX	
10-9-03	REVISED PIPE RAILING DETAILS TO HAND RAILING DETAILS	
4-10-03	REVISED RETAINING WALL DRAWING	
8-22-02	ADDED HAND RAILING DETAIL	
11-16-01	REVISED PVMT REPAIR OVER CULVERTS (CONC); CORRECTED SPELLING IN GENERAL NOTES	
11-18-98	ADDED GENERAL NOTES TO CONCRETE STEPS & WALKS	
7-02-98	ENLARGED PIPE	
4-03-97	ADDED NOTE TO STEEL BAR SCHED.	
10-18-96	CORRECTED SPELLING	
4-26-96	ADD WEEP HOLE; REV. JOINT SPACING IN RET. WALL	
6-2-94	CHANGED CONST. TO CONTRACTION JOINT	
10-1-92	CHANGED MESH FABRIC TO WIRE MESH	10-1-92
8-15-91	DELETED HDWL MODIFICATION DETAIL	8-15-91
11-8-90	DELETED COLD MIX FROM CULV'T. REPAIR	11-8-90
11-30-89	REV. RETAINING WALL STEEL SCHEDULE	11-30-89
11-17-88	V. BARS BEHIND ARROW	665-11-17-88
7-15-88	REV. PAVEMENT REPAIR	649-7-15-88
11-1-84	ADDED HDWL. MODS. DEL. PIPE UNDERDRAINS	
1-4-83	REV. TRENCH FOR PIPE UNDERDRAIN	510-11-1-84
3-2-81	ELIMINATED CONC. CLASS & ADDED CHAMFER NOTE	682-1-4-83
4-20-79	SPELLING OF "UNDERDRAIN"	721-3-2-81
2-2-76	REV. UNDERDRAIN DET & PAVEMENT REPAIR	674-4-20-79
4-10-75	12" MIN. GRAN. MAT'L. OVER PIPE	919-2-2-76
5-22-74	REV. SPECS. FOR GRAN. MAT'L.	568-4-10-75-853
10-2-72	GRANULAR MAT'L. TO BE SB-3	567-5-22-74-740
	REVISED AND REDRAWN	564-10-16-72

ARKANSAS STATE HIGHWAY COMMISSION

DETAILS OF SPECIAL ITEMS

STANDARD DRAWING SI - 1

<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>RSP-1</p>  <p>48"x30"</p>	<p>WI-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>WI-3</p>  <p>STD. 48"x48"</p>	<p>WI-4</p>  <p>STD. 48"x48"</p>	<p>WI-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>WI-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>W1-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60" • USE 6" C LETTERS •• USE 4" D LETTERS</p>

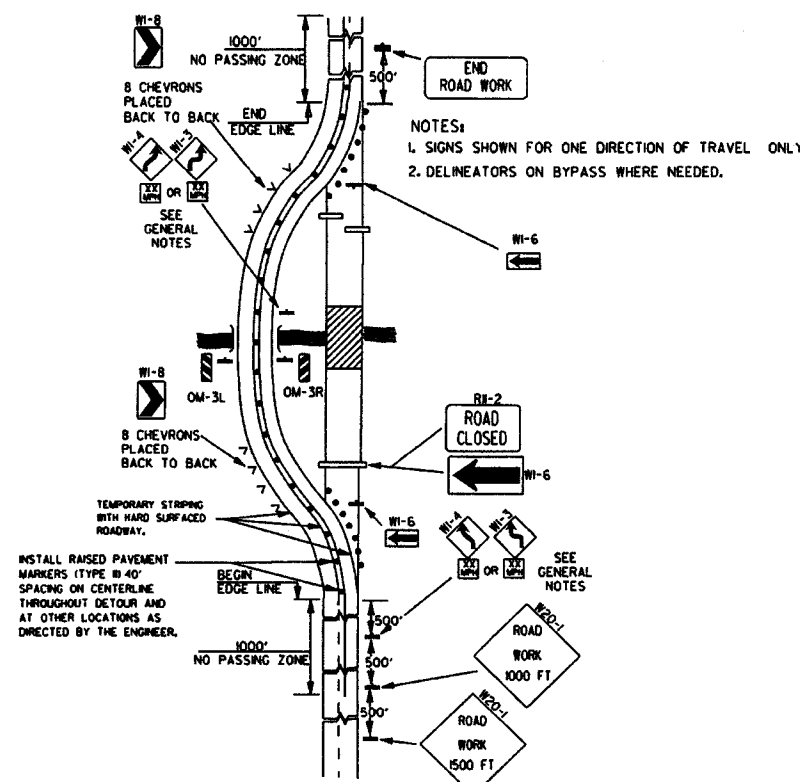
ADVANCE DISTANCES (XXXX) 41

500 FT 1/2 MILE
1000 FT 3/4 MILE
1500 FT 1 MILE
 AHEAD

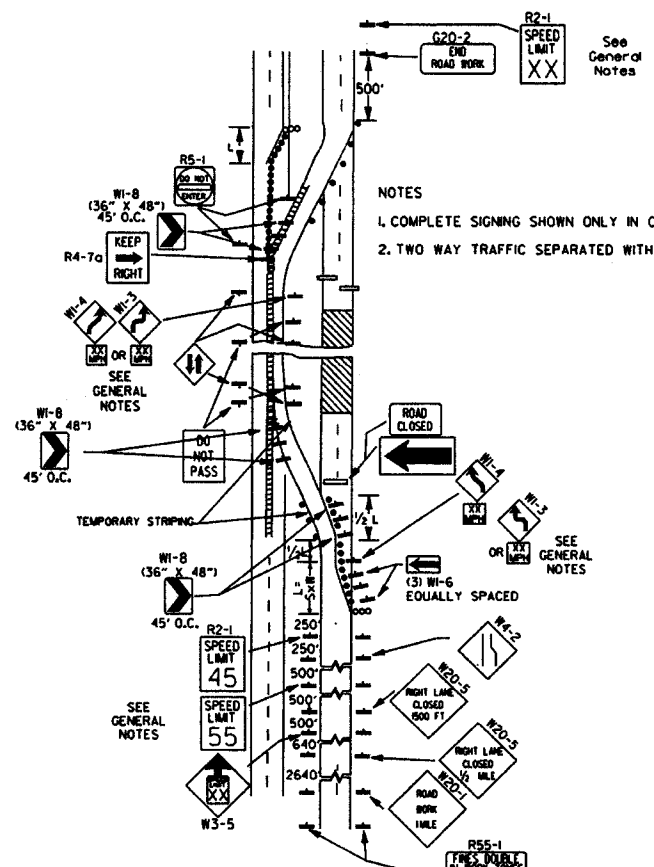
- GENERAL NOTES:
- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
 - TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
 - EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
 - SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
 - SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
 - POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
 - ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
 - FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
 - MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT, HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
 - R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.
- NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

9-2-95	REVISED REDUCED SPEED LIMIT AHEAD SIGNS	
	REVISED ROAD WORK NEXT XX MILES	
12-5-94	REVISED W24-1	
11-17-90	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-10-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-93	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED

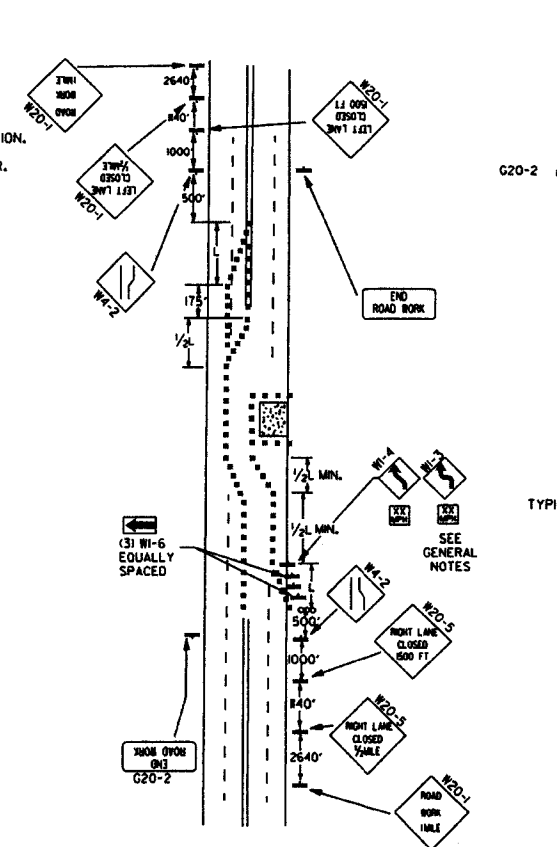
ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-1



(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.

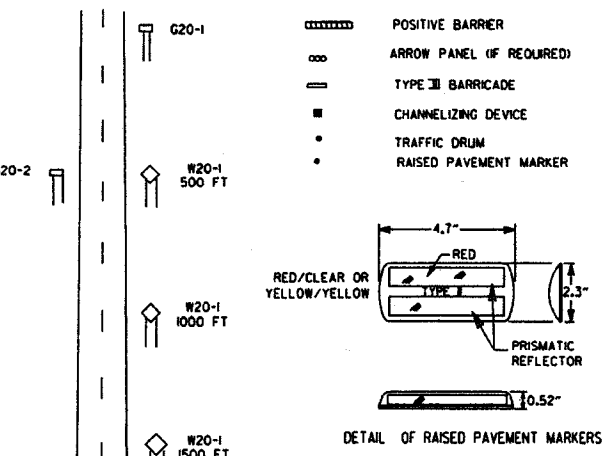


(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

- KEY:
- FLAGGER
 - POSITIVE BARRIER
 - ARROW PANEL (IF REQUIRED)
 - TYPE III BARRICADE
 - CHANNELIZING DEVICE
 - TRAFFIC DRUM
 - RAISED PAVEMENT MARKER



TAPER FORMULAE:

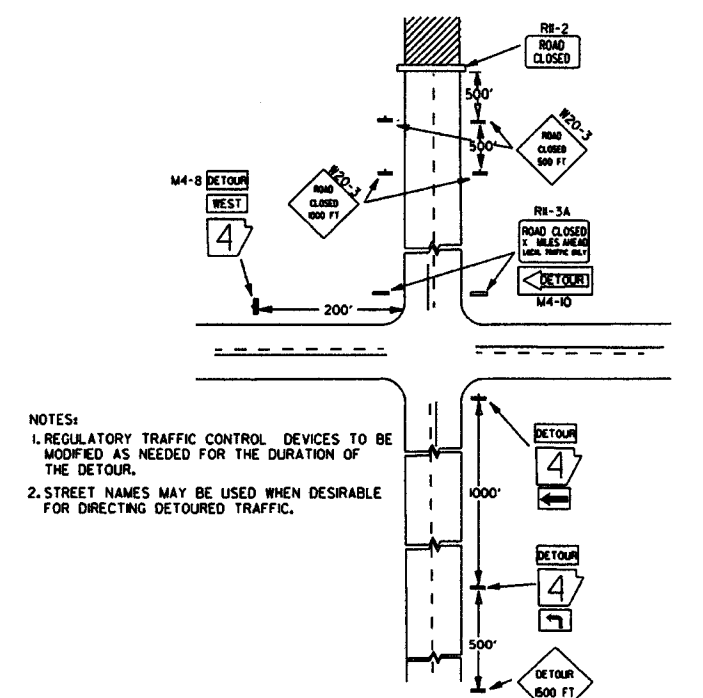
$L = SXW$ FOR SPEEDS OF 45MPH OR MORE.

$L = \frac{WS^2}{60}$ FOR SPEEDS OF 40MPH OR LESS.

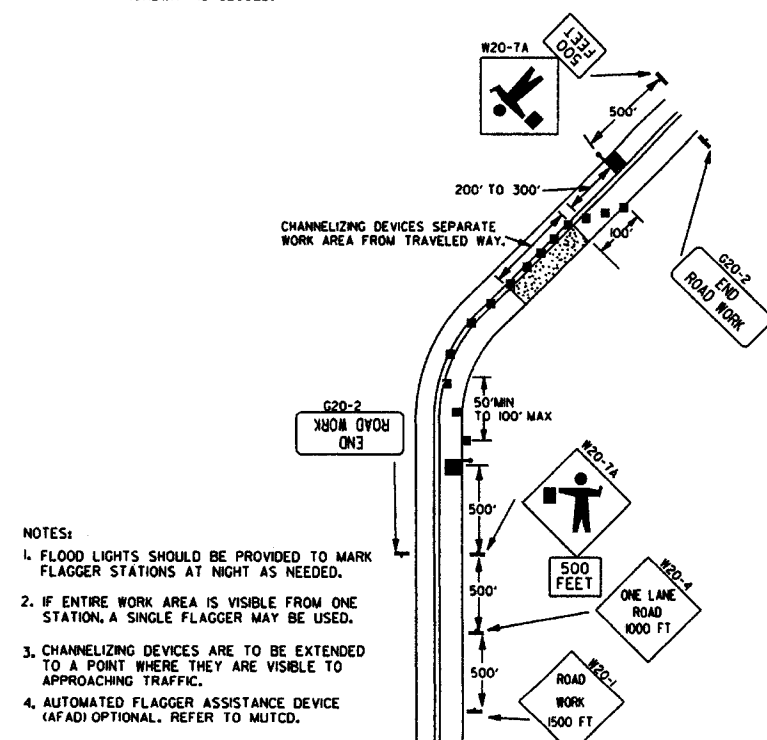
WHERE:
L = MINIMUM LENGTH OF TAPER.
S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.
W = WIDTH OF OFFSET.

- GENERAL NOTES:
- ADVISORY SPEED POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS TO BE DETERMINED AT SITE. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
 - WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-K45I SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-145MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-KXXI SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 - WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-K45I SHALL BE OMITTED. ADDITIONAL R2-155MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-KXXI SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 - THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
 - WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
 - PAYMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 - TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.
 - DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

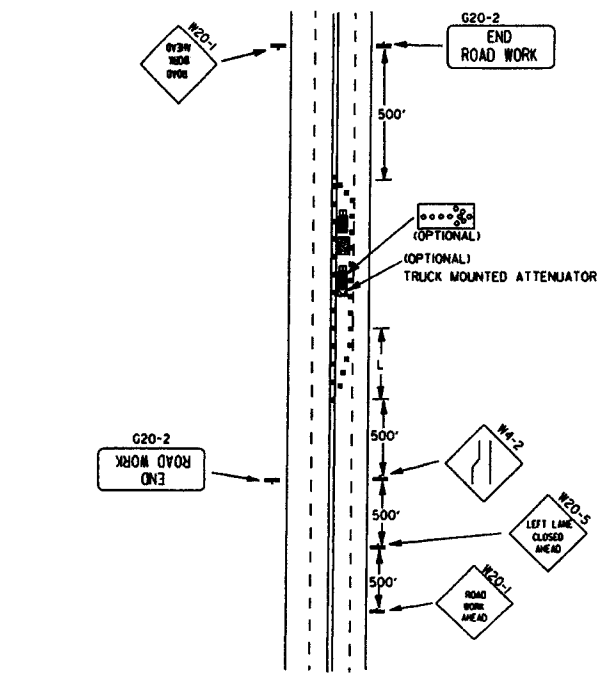
DATE	REVISION	FILED
9-2-15	REVISED NOTE 2, ADDED NOTE B, REVISED DRAWING (A) & REPLACED R2-5A WITH W3-5	
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-8-10	ADDED (AFAD)	
8-20-08	REVISED SIGN DESIGNATIONS	
8-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-28-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-94	DRAWN AND PLACED IN USE	
DATE	REVISION	FILED



(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.

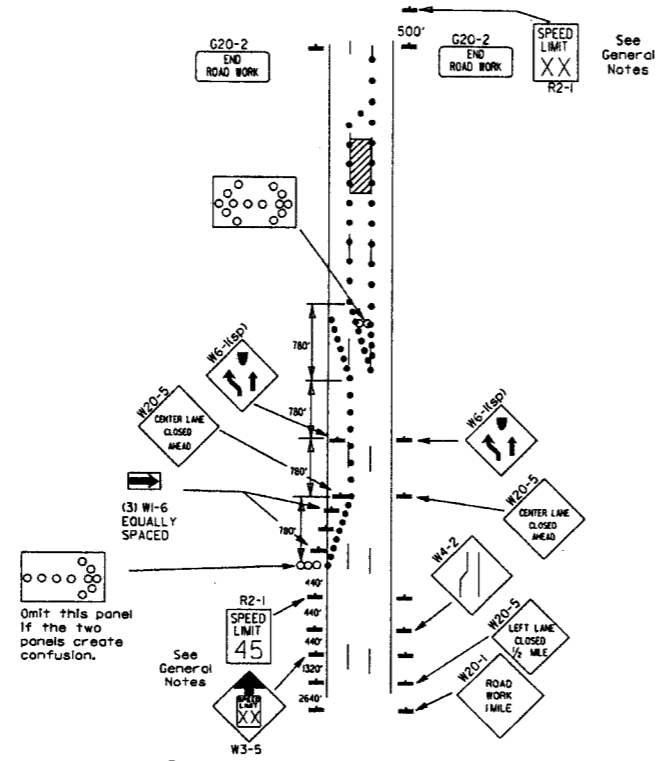


(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.

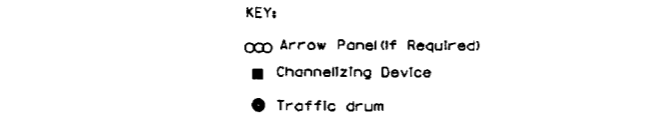


(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

Channelizing devices



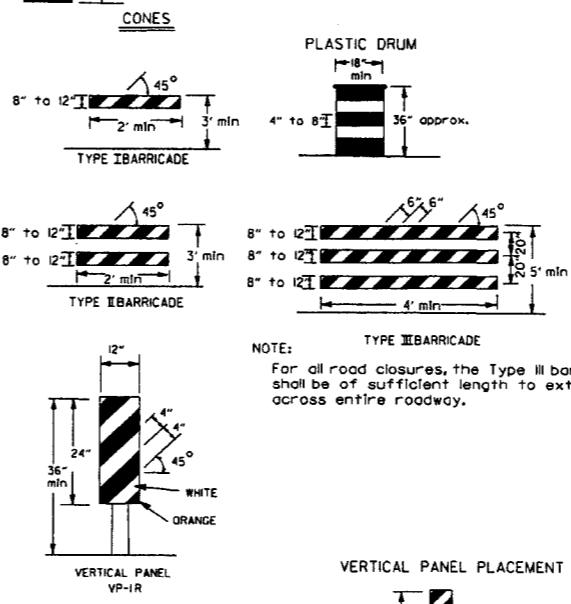
(B) Typical application - 3-lane oneway roadway where center lane is closed.



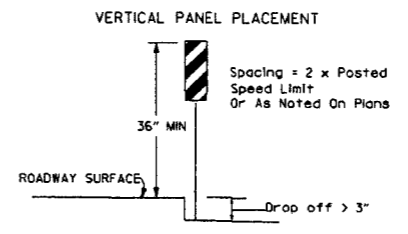
- GENERAL NOTES:
1. A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
 2. When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the W3-5 shall be installed at that location. Additional R2-1(45) speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
 3. When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(65) shall be omitted. Additional R2-1(55) speed limit signs shall be installed at a maximum of 1 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
 4. The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
 5. Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
 6. Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
 7. The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 125' in advance of the job limit. Additional W20-1 (1 MILE) signs are not required in advance of lane closures that begin inside the project limits.
 8. Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
 9. All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual For Assessing Safety Hardware (MASH).
 10. Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.

When cones are used on freeways and multi-lane highways, they shall be 28" min. During hours of darkness, 28" cones shall be used on all roadways, and shall be reflectorized in accordance with the M.U.T.C.D.



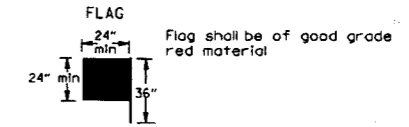
NOTE: For all road closures, the Type III barricades shall be of sufficient length to extend across entire roadway.



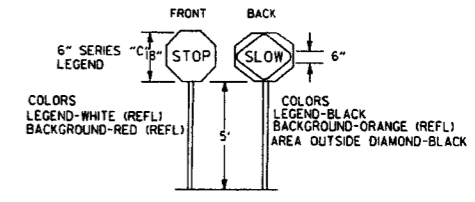
TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	W8-11
1" to 3"	Edge of shoulder	W8-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-land vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

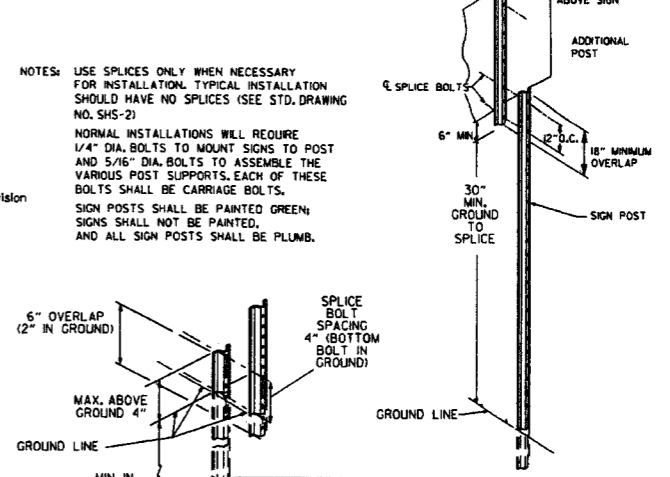
* When shown on the plans concrete barrier will be used. When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.



STOP SLOW PADDLE



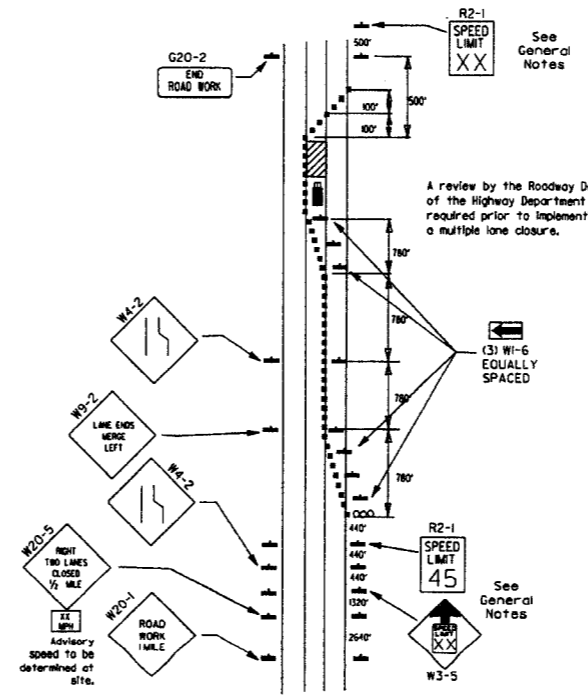
DETAIL OF SPLICES



NOTES: USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2) NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS. SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED. AND ALL SIGN POSTS SHALL BE PLUMB.

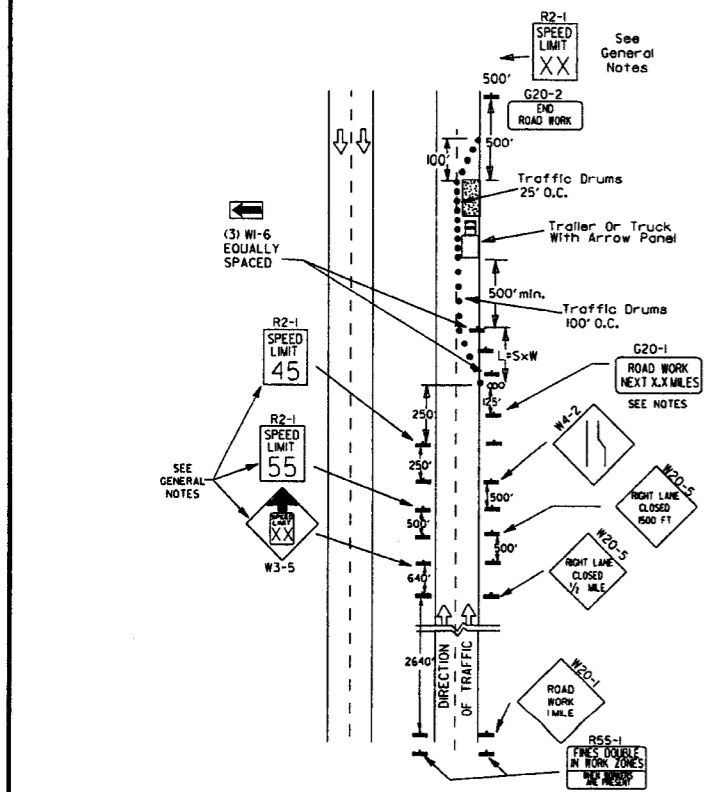
DATE	REVISION	FILMED
9-2-15	REVISED NOTE 2 & REPLACED R2-5A WITH W3-5	
10-15-09	ADDED REFERENCE TO MASH	
8-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION
STANDARD DRAWING TC-3



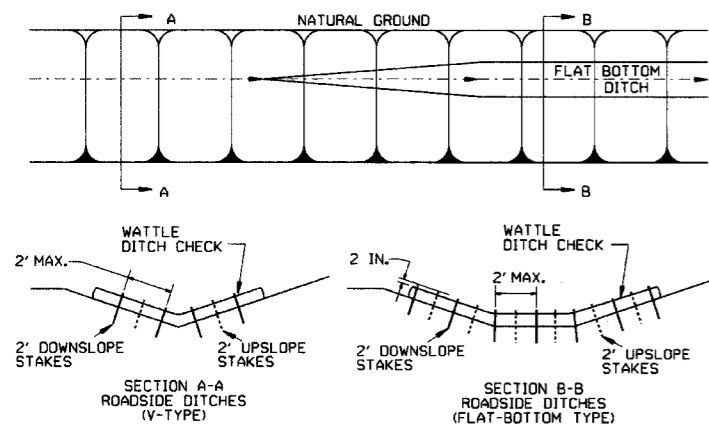
(D) Typical application - closing multiple lanes of a multilane highway.

(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.

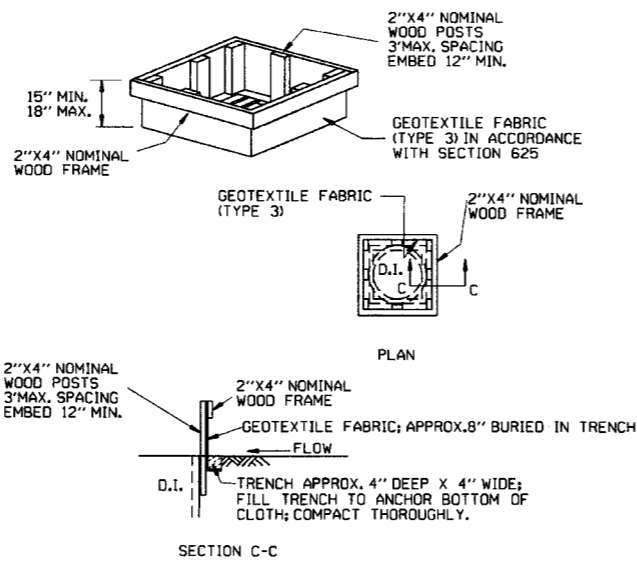


(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.

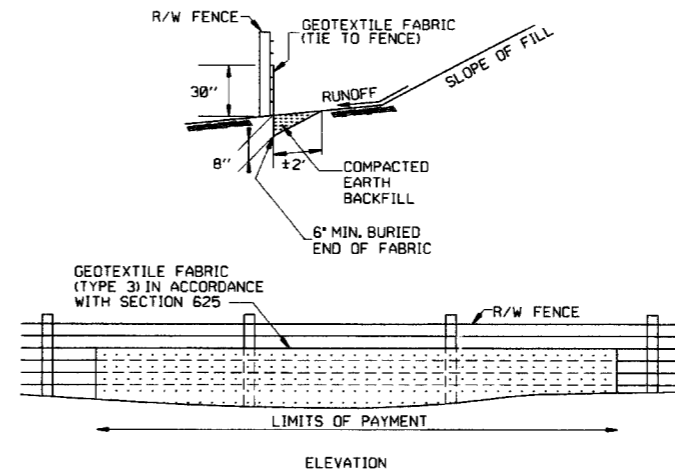
GENERAL NOTES
INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.



WATTLE DITCH CHECK (E-1)



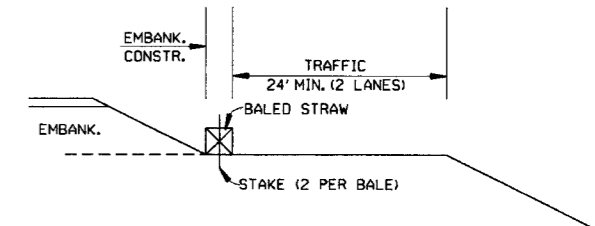
DROP INLET SILT FENCE (E-7)



SILT FENCE ON R/W FENCE (E-4)

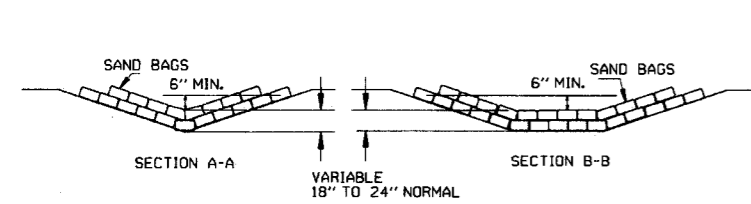
GENERAL NOTES
GEOTEXTILE FABRIC SHALL BE SPLICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST, OR TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.

GENERAL NOTES
1. STRAW BALES SHALL BE INSTALLED SO THAT THE BINDINGS ARE ORIENTED AROUND THE SIDES RATHER THAN ALONG THE TOPS AND BOTTOMS OF THE BALES. THE BALES SHALL BE A MINIMUM OF 30 INCHES IN LENGTH.
2. NO GAPS SHALL BE LEFT BETWEEN BALES.
3. BALED STRAW FILTER BARRIERS COMPLETED AND ACCEPTED WILL BE MEASURED BY THE BALE IN PLACE AS AUTHORIZED BY THE ENGINEER AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID PER BALE FOR BALED STRAW DITCH CHECKS.

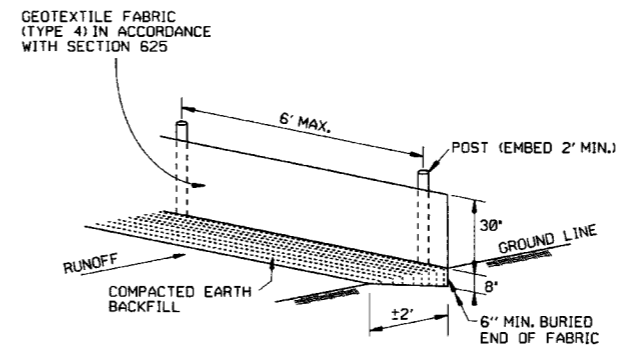


BALED STRAW FILTER BARRIER (E-2)

NUMBER OF SAND BAGS AND ARRANGEMENT VARIABLE WITH ON-SITE CONDITIONS. PLACE SAND BAGS AT BASE OF DITCH CHECK IN AREA OF OVERFLOW.

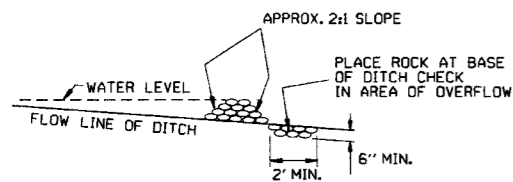


SAND BAG DITCH CHECK (E-5)

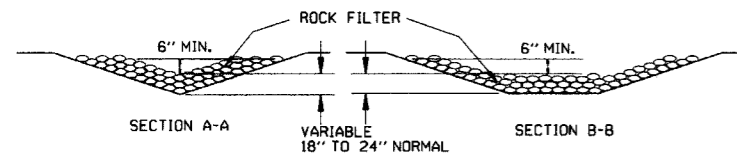


SILT FENCE (E-11)

GENERAL NOTES
GEOTEXTILE FABRIC SHALL BE SPLICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST OR TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.



ROCK DITCH CHECK (E-6)

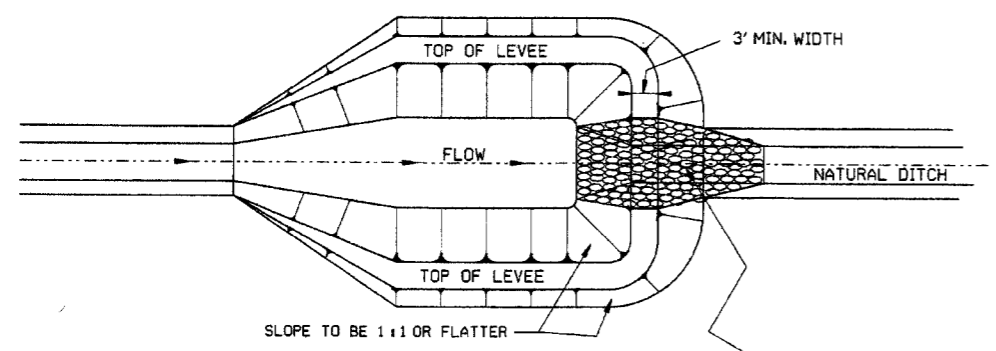


12-15-11	DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK	
11-18-98	ADDED NOTES	
7-02-98	ADDED BALED STRAW FILTER BARRIER (E-2)	
7-20-95	REVISED SILT FENCE E-4 AND E-11	7-20-95
7-15-94	REV. E-4 & E-11 MIN. 13\"/>	
6-2-94	REVISED E-1, 4, 7 & 11; DELETED E-2 & 3	6-2-94
4-1-93	REDRAWN	
10-1-92	REDRAWN	
8-2-76	ISSUED R.D.M.	298-7-28-76
DATE	REVISION	FILMED

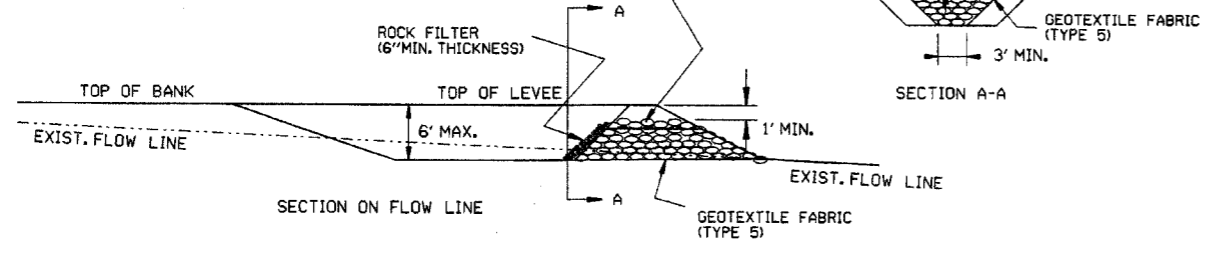
ARKANSAS STATE HIGHWAY COMMISSION

TEMPORARY EROSION CONTROL DEVICES

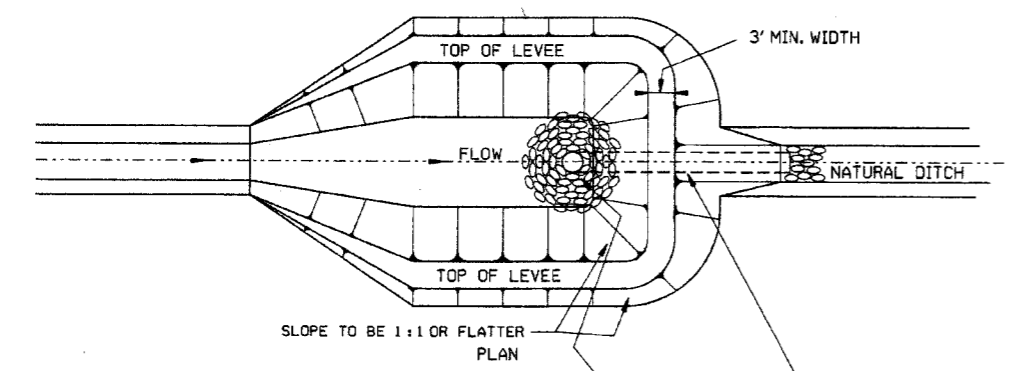
STANDARD DRAWING TEC-1



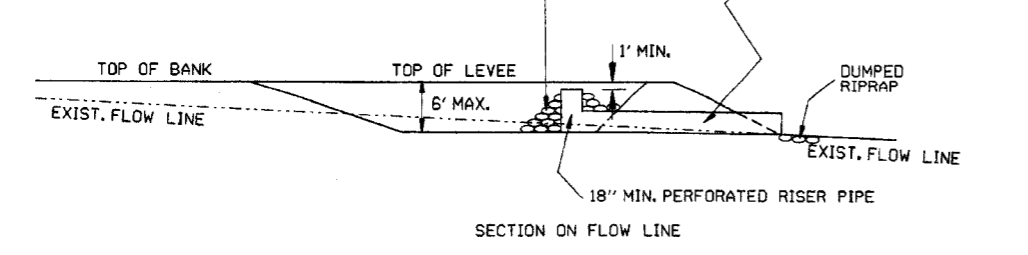
NOTE:
SIZE OF BASIN TO BE DETERMINED
BY VOLUME REQUIRED; HOWEVER
A MINIMUM LENGTH-TO-WIDTH
RATIO OF 2:1 SHALL BE USED.



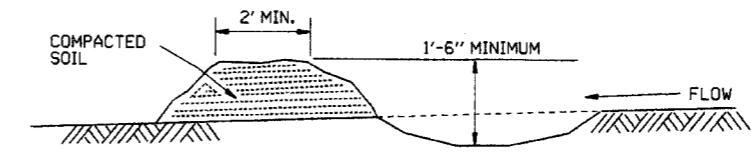
SEDIMENT BASIN WITH RIPRAP OUTLET (E-9)



NOTE:
SIZE OF BASIN TO BE DETERMINED
BY VOLUME REQUIRED; HOWEVER
A MINIMUM LENGTH-TO-WIDTH
RATIO OF 2:1 SHALL BE USED.

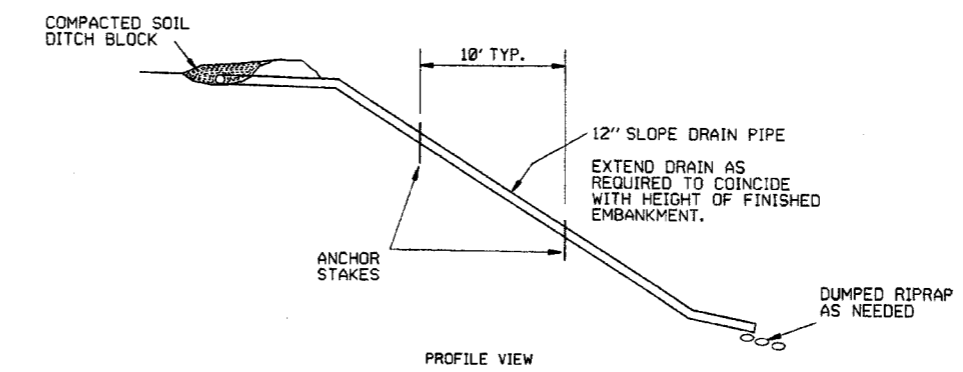
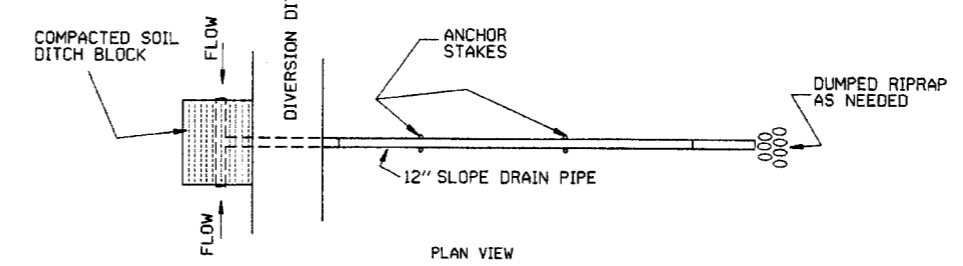


SEDIMENT BASIN WITH PIPE OUTLET (E-10)

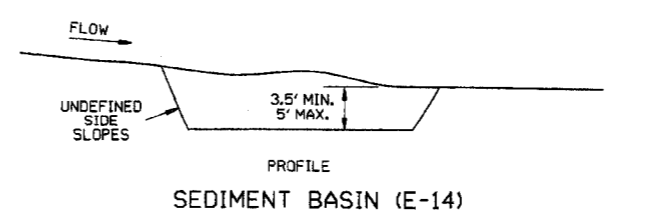
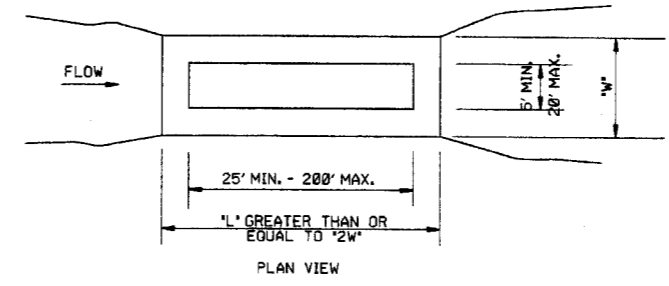


DIVERSION DITCH (E-8)

NOTE:
A T-SECTION SHALL BE USED AT THE INLET
FOR TWO-DIRECTIONAL FLOW.
AN ELBOW SHALL BE USED FOR
ONE-DIRECTIONAL FLOW.



SLOPE DRAIN (E-12)



SEDIMENT BASIN (E-14)

6-2-94	Revised E-8 & E-12; Added E-14 & Deleted E-13
4-1-93	ISSUED
DATE	REVISION
	FILMED

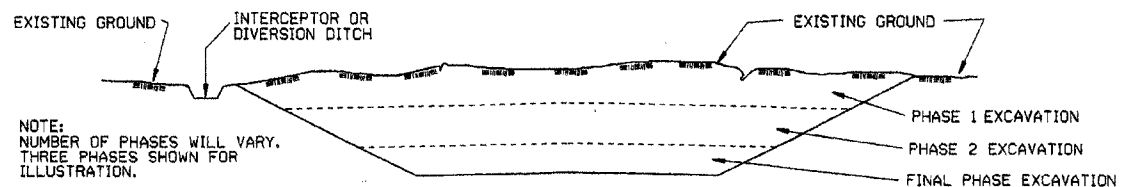
ARKANSAS STATE HIGHWAY COMMISSION
 TEMPORARY EROSION
 CONTROL DEVICES
 STANDARD DRAWING TEC-2

CLEARING AND GRUBBING

CONSTRUCTION SEQUENCE

1. PLACE PERIMETER CONTROLS (I.E. SILT FENCES, DIVERSION DITCHES, SEDIMENT BASINS, ETC.)
2. PERFORM CLEARING AND GRUBBING OPERATION.

EXCAVATION



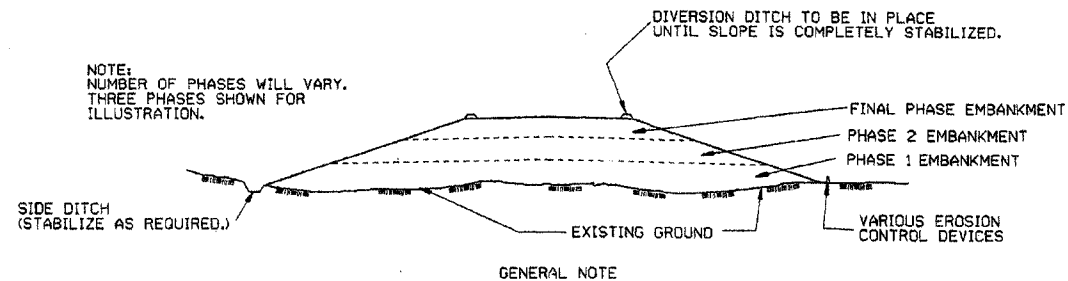
GENERAL NOTE

ALL CUT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE EXCAVATED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

CONSTRUCTION SEQUENCE

1. EXCAVATE AND STABILIZE INTERCEPTOR AND/OR DIVERSION DITCHES.
2. PERFORM PHASE 1 EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING.
3. PERFORM PHASE 2 EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING.
4. PERFORM FINAL PHASE OF EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING. STABILIZE DITCHES. CONSTRUCT DITCH CHECKS, DIVERSION DITCHES, SEDIMENT BASINS, OR OTHER EROSION CONTROL DEVICES AS REQUIRED.

EMBANKMENT



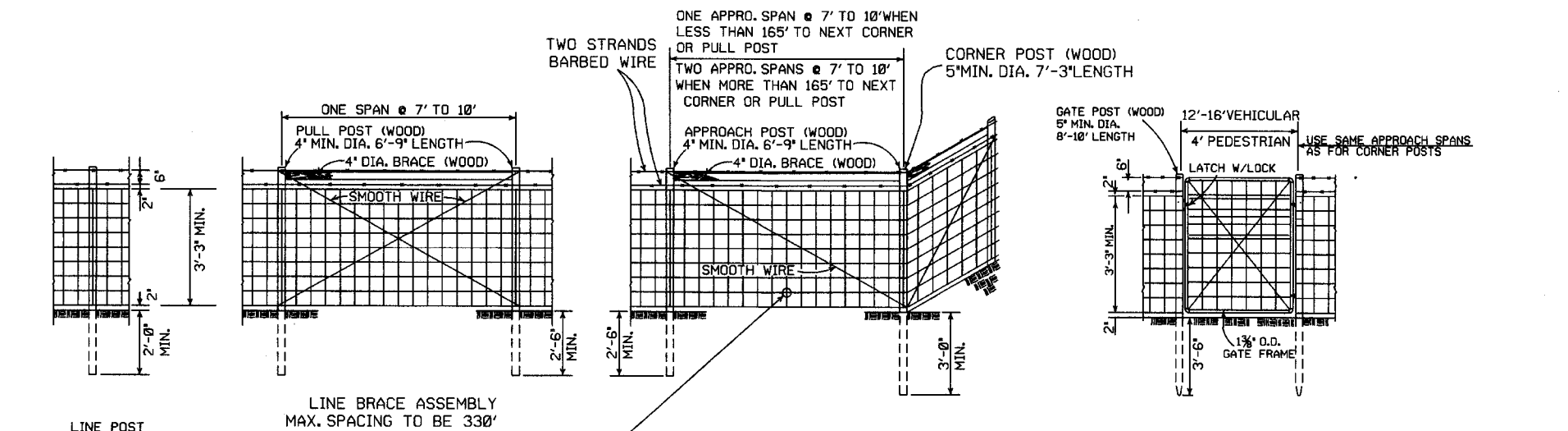
GENERAL NOTE

ALL EMBANKMENT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE CONSTRUCTED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

CONSTRUCTION SEQUENCE

1. CONSTRUCT DIVERSION DITCHES, DITCH CHECKS, SEDIMENT BASINS, SILT FENCES, OR OTHER EROSION CONTROL DEVICES AS SPECIFIED.
2. PLACE PHASE 1 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
3. PLACE PHASE 2 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
4. PLACE FINAL PHASE OF EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PLACE DIVERSION DITCHES AND SLOPE DRAINS AND MAINTAIN UNTIL ENTIRE SLOPE IS STABILIZED.

		ARKANSAS STATE HIGHWAY COMMISSION	
		TEMPORARY EROSION CONTROL DEVICES	
		STANDARD DRAWING TEC-3	
11-03-94	CORRECTED SPELLING		
6-2-94	Drawn & Issued		6-2-94
DATE	REVISION		FILMED

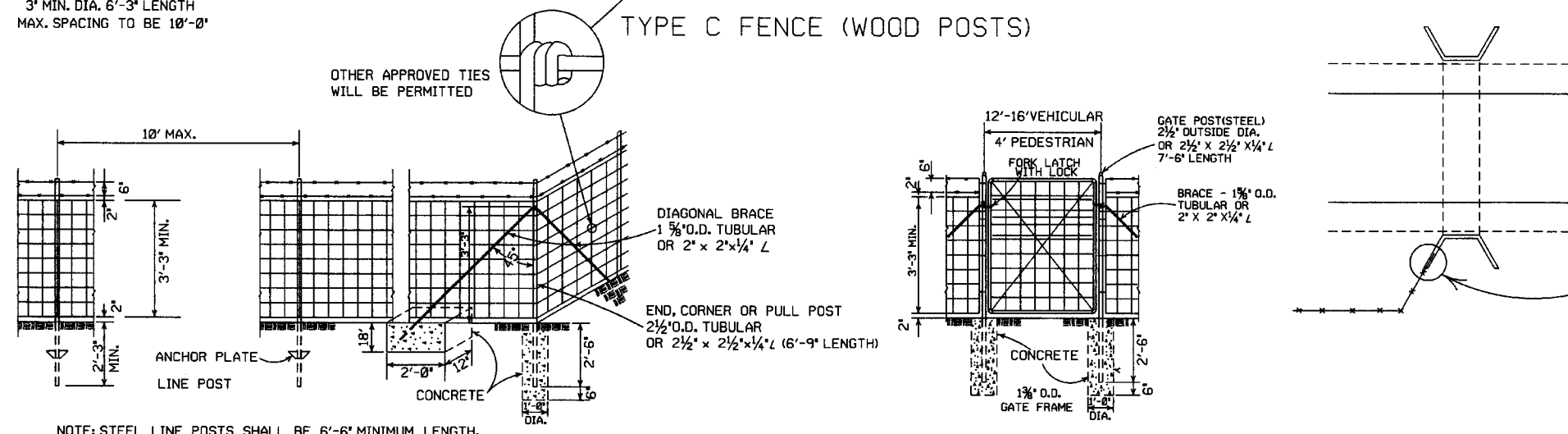


GENERAL NOTES:
 STEEL LINE POSTS SHALL BE PAINTED OR GALVANIZED. TUBULAR END, CORNER, PULL, OR DIAGONAL BRACES MUST CONFORM TO THE DIMENSIONS AND WEIGHTS SPECIFIED ON STANDARD DRAWING WF-3 (CHAIN LINK). APPROVED ALTERNATES ARE ACCEPTABLE. AN ACCEPTABLE TOLERANCE IN LENGTH OF TUBULAR OR WOODEN POSTS SHALL BE -1\"/>

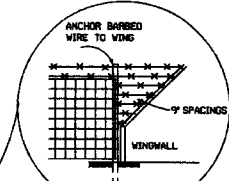
THE CONTRACTOR SHALL FURNISH AT LEAST 25% OF TIMBER LINE POSTS OF 7 FOOT LENGTHS IN ORDER TO PROVIDE SUFFICIENT SET IN SOFT GROUND OR SMALL DEPRESSIONS.

DRIVEWAY GATES, EITHER SINGLE 12' TO 16' OR DOUBLE 6' TO 8' OPENING OF THE SAME TYPE AS THE PEDESTRIAN GATE, SHALL BE INSTALLED ON THE RIGHT SIDE OF EACH THROUGH LANE ROAD AT LARGE CULVERTS OR BRIDGE CROSS FENCE, FOR USE OF MAINTENANCE EQUIPMENT. LOCATION OF GATES TO BE SHOWN ON PLANS OR AS DESIGNATED BY THE ENGINEER.

AT STREAM CROSSINGS, THE FENCE SHALL NOT BE CONSTRUCTED ACROSS LARGE STREAMS. WHERE CLEARANCE IS SUFFICIENT FROM THE TOP OF THE BANK TO THE BRIDGE STRUCTURE A CROSS CONNECTION SHALL BE CONSTRUCTED BETWEEN THE FENCE ON EACH SIDE OF THE ROAD. WHERE THE CLEARANCE IS NOT SUFFICIENT, THE FENCE SHALL BE TERMINATED WITH CROSS CONNECTIONS AND END POSTS ADJACENT TO BRIDGE ABUTMENTS OR CULVERT WINGWALLS.



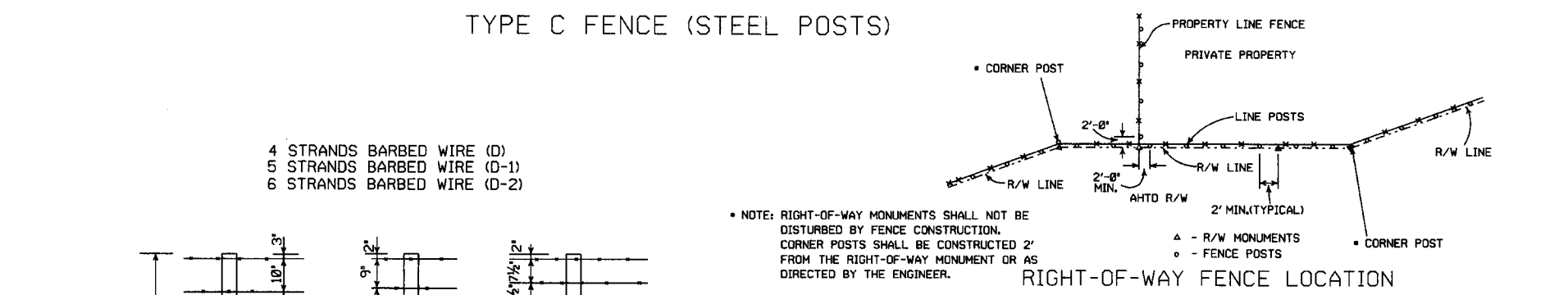
NOTE: USE 3/8\"/>



SPLICE FOR BARBED WIRE BETWEEN PULL POST ASSEMBLY SHALL BE BY THE 'EYE METHOD' AS DESCRIBED AS FOLLOWS: THE ENDS OF THE BARBED WIRE SHALL BE BENT TO FORM A LOOP. THE LOOPS SHALL BE CONNECTED. AFTER THE LOOPS ARE CONNECTED THE ENDS OF THE WIRE SHALL BE WRAPPED AROUND THE PROJECTING WIRES A MINIMUM OF 4 TIMES FOR EACH WIRE LOOP.

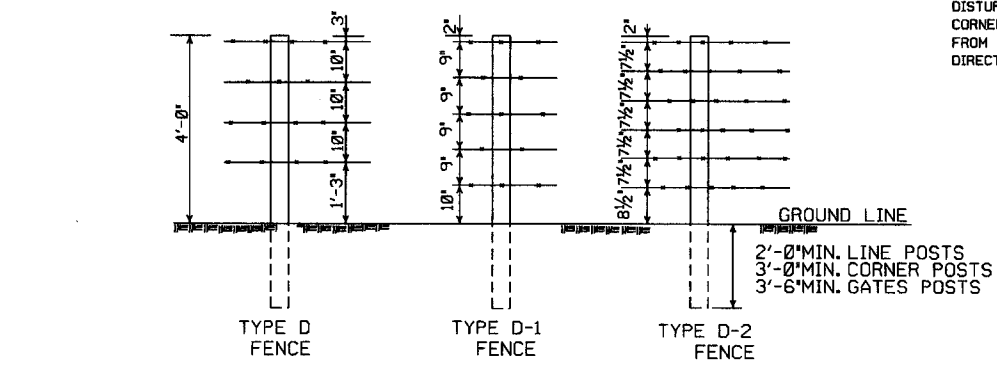
SPLICE FOR WOVEN WIRE BETWEEN PULL POST SHALL BE BY THE 'WESTERN UNION METHOD' AS DESCRIBED AS FOLLOWS: THE VERTICAL WIRES FOR EACH END OF THE FENCE FABRIC SHALL BE PLACED SIDE BY SIDE AND THE PROJECTING HORIZONTAL WIRES SHALL BE WRAPPED A MINIMUM OF 4 TIMES AROUND THE HORIZONTAL WIRES OF THE FIRST WEB.

STAPLE AT LEAST TOP, BOTTOM AND ALTERNATE WIRES OF WOVEN FABRIC FOR WOOD LINE POSTS.

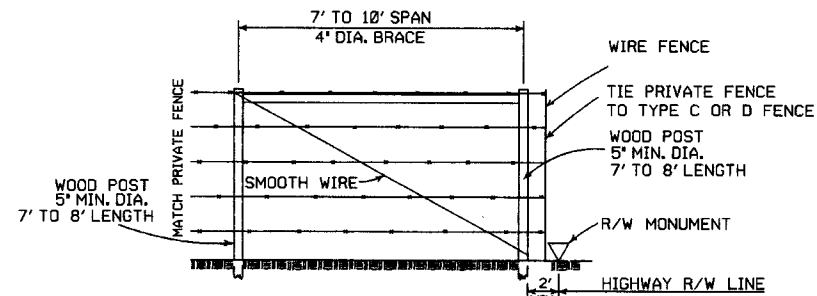


- 4 STRANDS BARBED WIRE (D)
- 5 STRANDS BARBED WIRE (D-1)
- 6 STRANDS BARBED WIRE (D-2)

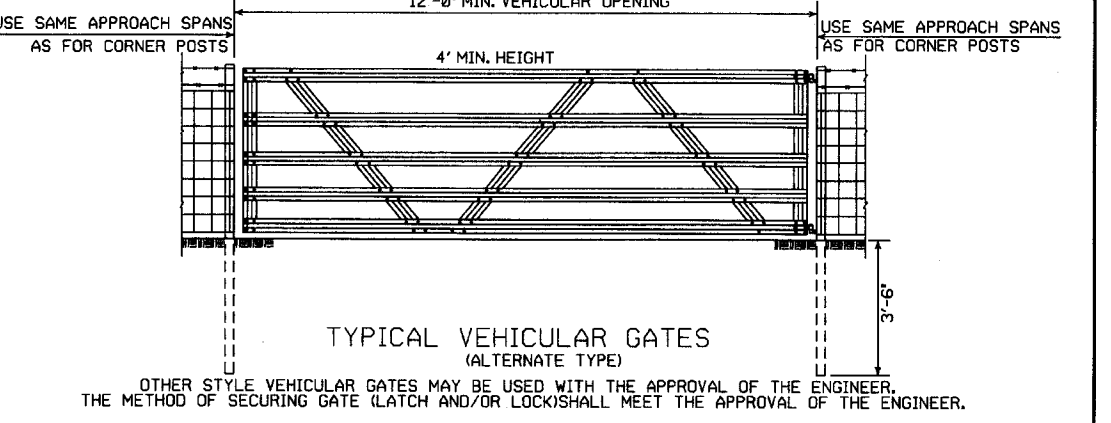
NOTE: RIGHT-OF-WAY MONUMENTS SHALL NOT BE DISTURBED BY FENCE CONSTRUCTION. CORNER POSTS SHALL BE CONSTRUCTED 2' FROM THE RIGHT-OF-WAY MONUMENT OR AS DIRECTED BY THE ENGINEER.



NOTE: SPACING AND SIZE (EXCEPT LENGTH) OF POSTS, APPROACH SPANS, PULL POST ASSEMBLIES, AND CORNER BRACING FOR TYPE D FENCE SHALL CONFORM TO TYPE C FENCE. USE GALVANIZED STAPLES ON WOOD POSTS AND APPROVED FASTENERS ON STEEL POSTS.



PRIVATE FENCE TERMINAL INSTALLATION WHERE EXISTING FENCE CONSISTS OF STEEL POSTS, USE END POST ASSEMBLY AS SHOWN IN TYPE C FENCE OR OTHER END POST ASSEMBLY AS APPROVED BY THE ENGINEER.



OTHER STYLE VEHICULAR GATES MAY BE USED WITH THE APPROVAL OF THE ENGINEER. THE METHOD OF SECURING GATE (LATCH AND/OR LOCK) SHALL MEET THE APPROVAL OF THE ENGINEER.

8-22-82	REVISED GENERAL NOTES	
10-18-96	REVISED AASHTO	
11-22-95	REVISED R-O-W LOCATION DETAIL	
6-2-94	REVISED BARB WIRE AND ANCHOR BARBED WIRE TO VINYL	6-2-94
	ADDED CORNER POST NOTES	
8-5-93	REVISED R/W INSTALLATION FENCE	8-5-93
10-1-92	ADDED STAPLE NOTE	10-1-92
8-15-91	ADDED TYPE D-2 FENCE	8-15-91
11-30-89	DELETED CLASS CONCRETE	11-30-89
7-15-88	ADDED SPLICE NOTE	700-7-15-88
10-30-87	GENERAL REVISIONS	549-10-30-87
11-1-84	MAX. POST SPACING MIN. WIRE GAUGE	507-11-1-84
1-4-83	MIN. DIA. LINE POST	648-1-4-83
3-2-81	TOLERANCE FOR POST LENGTH	722-3-2-81
12-1-72	ADDED D-1 & FENCE INSTALLATION	564-12-1-72
10-2-72	REVISED AND REDRAWN	540-10-2-72
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION

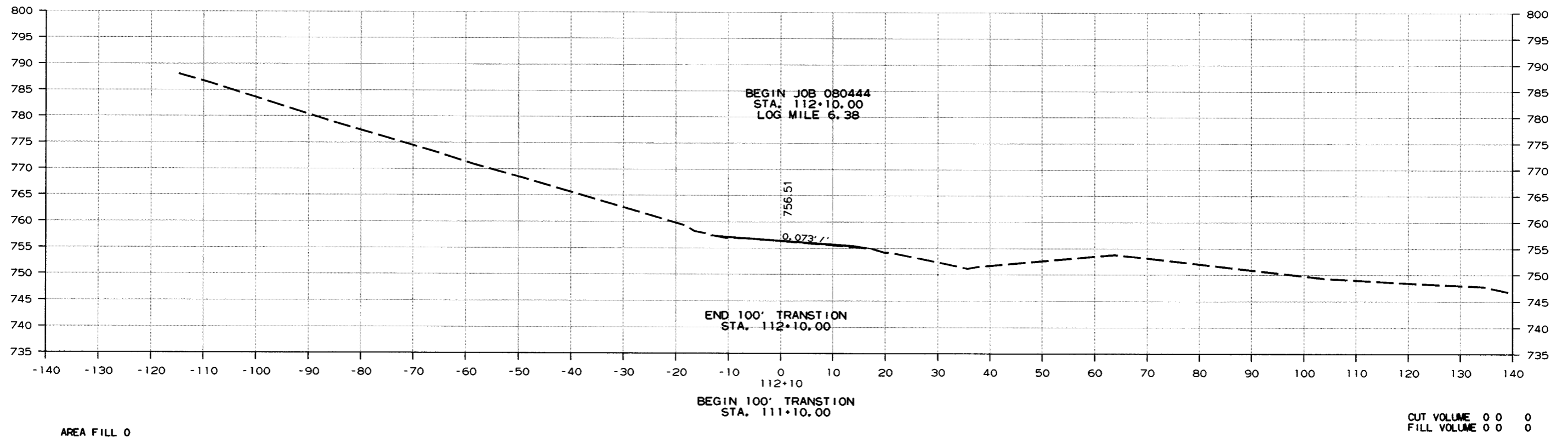
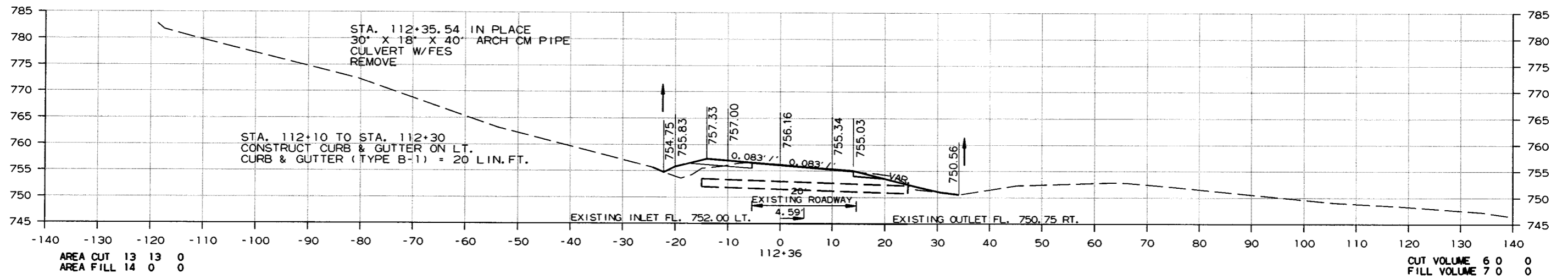
WIRE FENCE
TYPE C AND D

STANDARD DRAWING WF-4

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. 080444							48	52

STAGE 1 STAGE 2 STAGE 3

STAGE 1 STAGE 2 STAGE 3 ② CROSS SECTIONS



CROSS SECTION STA. 112+00 TO STA. 112+36

6/7/2016

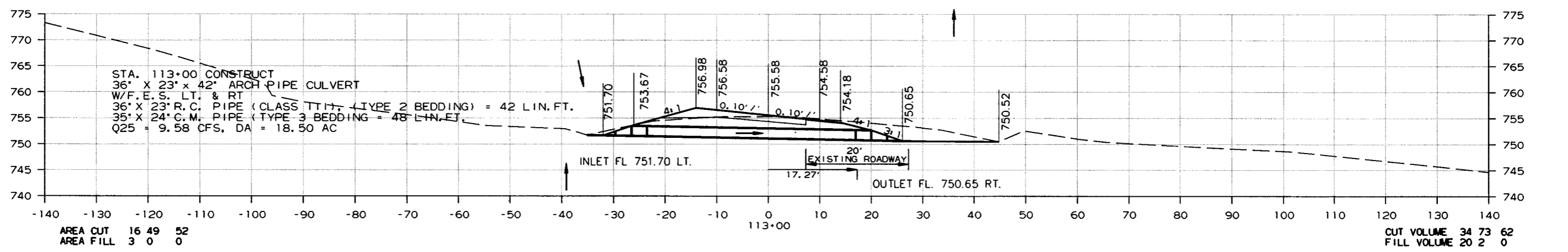
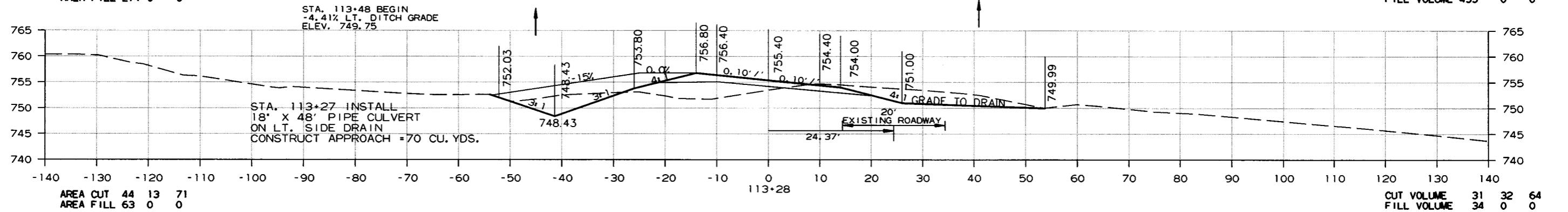
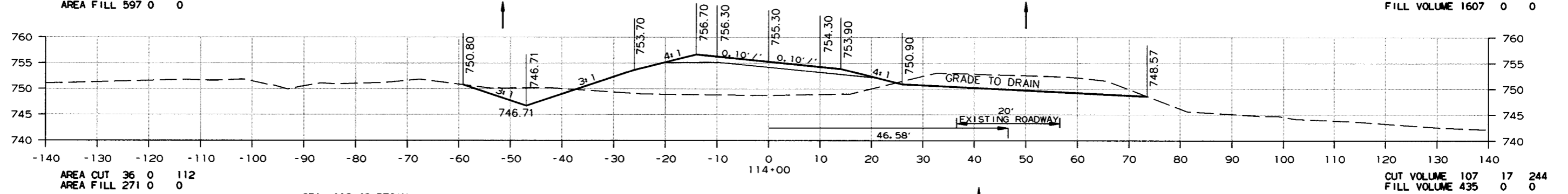
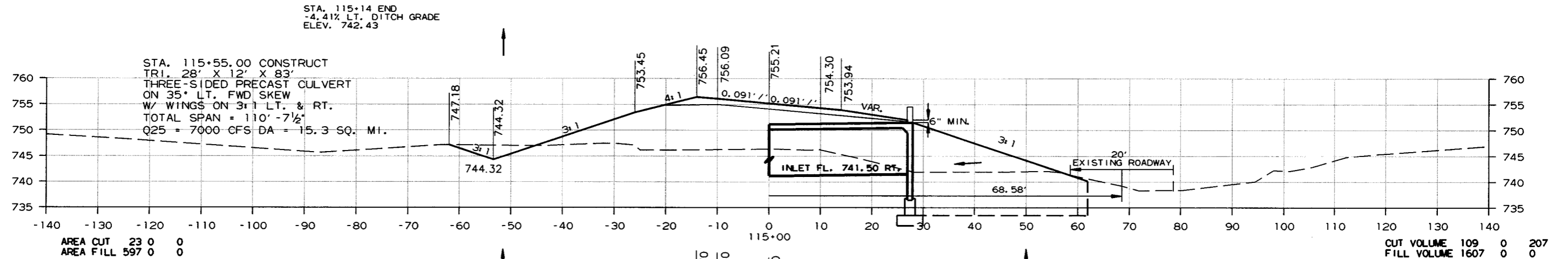
R080444.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		49	52
						JOB NO. 080444		

② CROSS SECTIONS

STAGE 1 STAGE 2 STAGE 3

STAGE 1 STAGE 2 STAGE 3

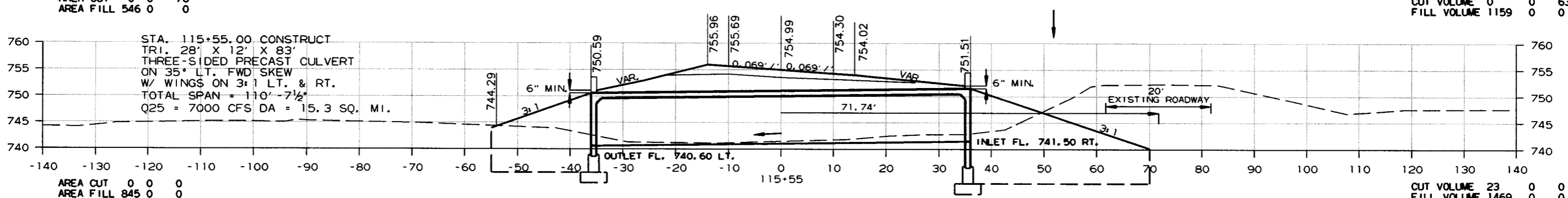
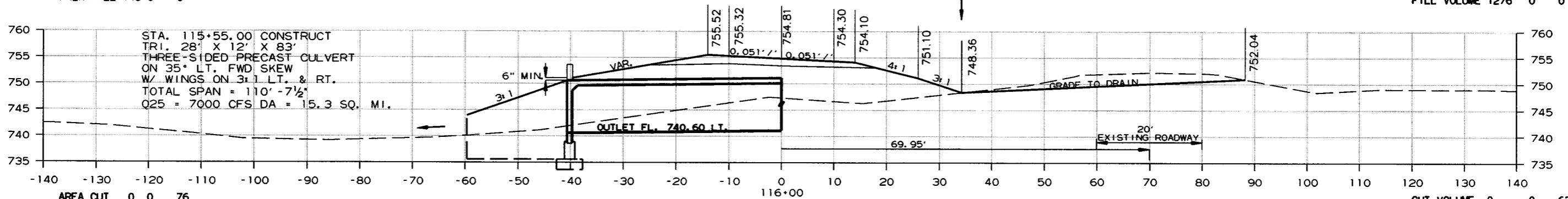
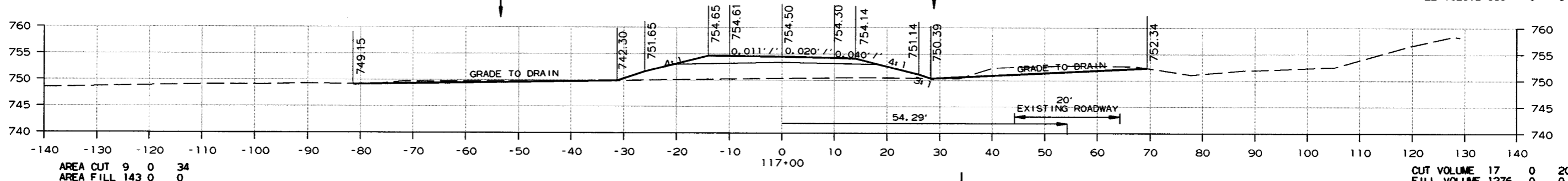
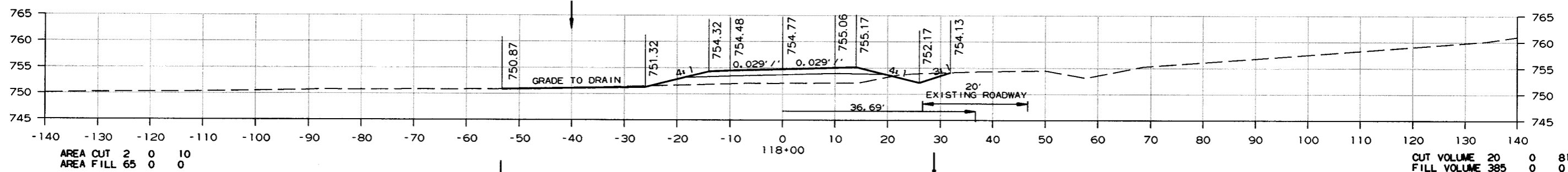


CROSS SECTION STA. 113+00 TO STA. 115+00

6/7/2016 R080444.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. 080444	50	52

② CROSS SECTIONS



CROSS SECTION STA. 115+55 TO STA. 118+00

6/7/2016 R080444.DGN