



Latitude:36.05698, Longitude:-91.19358

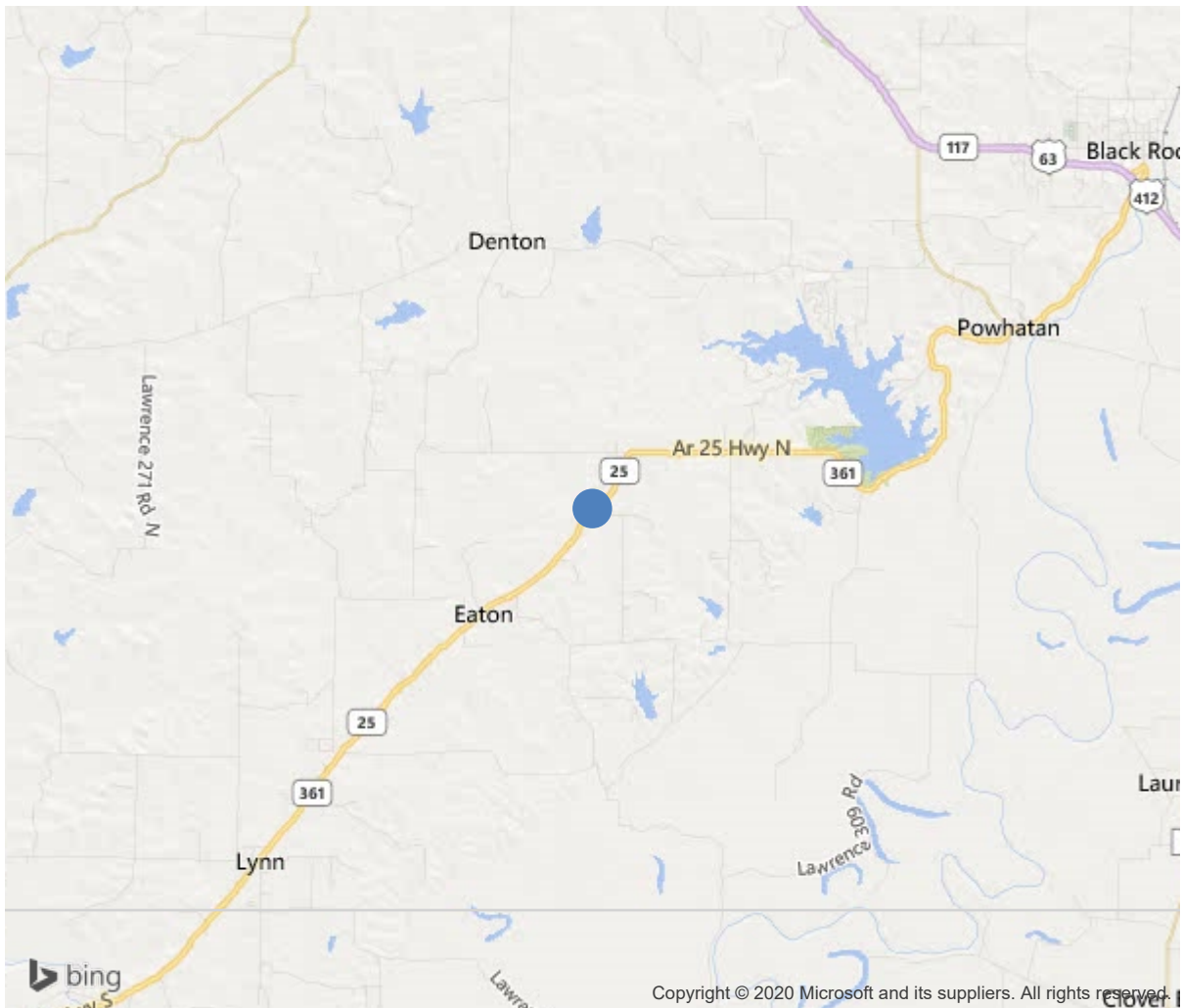
Route:25 Section:06 Log:15.85

Arnold Road ID:38x25x6xA, Arnold Log mile:15.843

District 10, Lawrence County

Owner: 1-State Highway Agency

8.58 M SW OF US 63



36.05698, -91.19358



Bridge #M0614(Routine)
SH 25-06- LM 15.85 over CROW CREEK
Location: 8.58 M SW OF US 63

Team Lead: Richard Jones **Inspection Date:** September 19, 2017

IDENTIFICATION	
(1) State Names	Arkansas
(8) Structure Number	M0614
(5) Inventory Route	25
(2) Highway Agency District	10
(3) County Code	75-Lawrence County, Arkansas
(4) Place Code	0
(6) Features Intersected	CROW CREEK
(7) Facility Carried	SH 25-06- LM 15.85
(9) Location	8.58 M SW OF US 63
(11) Mile Point	15.85 mi
(12) Base Highway Network	Yes
(13) LRS Inventory Rte & Subrte	0000025060
(16) Latitude	36.05698
(17) Longitude	-91.19358
(98) Border Bridge State Code	
(99) Border Bridge Structure No.	
STRUCTURE TYPE AND MATERIAL	
(43) Main Structure Type	122
Material	1-Concrete
Type	22-Channel beam
(44) Approach Structure Type	00
Material	0-Other
Type	0-Other
(45) No. of Spans in Main Unit	2
(46) No. of Approach Spans	0
(107) Deck Structure Type	2-Concrete Precast Panels
(108) Wearing Surface/Protective System	
Type of Wearing Surface	6-Bituminous
Type of Membrane	0-None
Type of Deck Protection	0-None
AGE AND SERVICE	
(27) Year Built	1962
(106) Year Reconstructed	0
(42) Type of Service	15
On	1-Highway
Under	5-Waterway
(28) Lane	
On	2
Under	0
(29) Average Daily Traffic	1800
(30) Year of ADT	2018
(109) Truck ADT	1 %
(19) Bypass, Detour Length	4 mi
GEOMETRIC DATA	
(48) Length of Maximum Span	19 ft
(49) Structure Length	38 ft
(50) Curb or Sidewalk Width	
Left	0.5 ft
Right	0.5 ft
(51) Bridge Roadway Width Curb to Curb	24.2 ft
(52) Deck Width Out to Out	25.3 ft
(32) Approach Roadway Width (W/Shoulders)	27.9 ft
(33) Bridge Median	0-No median
(34) Skew	0 Deg
(35) Structure Flared	No flare
(10) Inventory Route Min Vert Clear	99.99 ft
(47) Inventory Route Total Horiz Clear	25.6 ft
(53) Min Vert Clear Over Bridge Rdwy	99.99 ft
(54) Min Vert Underclear	0 ft
Ref:	
(55) Min Lat Underclear RT	99.9 ft
Ref:	
(56) Min Lat Underclear LT	0 ft
NAVIGATION DATA	
(38) Navigation Control	0-No navigation control on water
(111) Pier Protection	5-None present but re-evaluation
(39) Navigation Vertical Clearance	0 ft
(116) Vert-Lift Bridge Nav Min Vert Clear	0 ft
(40) Navigation Horizontal Clearance	0 ft

CLASSIFICATION			
(112) NBIS Bridge Length			Y
(104) Highway System			0
(26) Functional Class			6-Rural Minor Arterial
(100) Defense Highway			0-The inventory route is not a S
(101) Parallel Structure			N-No parallel structure exists.
(102) Direction of Traffic			2 - way traffic
(103) Temporary Structure			
(105) Federal Lands Highways			0-N/A
(110) Designated National Network			0-The inventory route is not part of
(20) Toll			3-On free road. The structure is toll-
(21) Maintain			1-State Highway Agency
(22) Owner			1-State Highway Agency
(37) Historical Significance			5-Bridge is not eligible for the NRHP
CONDITION			
(58) Deck			5
(59) Superstructure			5
(60) Substructure			6
(61) Channel & Channel Protection			5
(62) Culverts			N
LOAD RATING AND POSTING			
(31) Design Load			2-M 13.5 / H 15
(63) Operating Rating Method			1
(64) Operating Rating			
Type			1-Load Factor(LF)
Rating			60
(65) Inventory Rating Method			1-Load Factor(LF)
(66) Inventory Rating			
Type			2
Rating			36
(70) Bridge Posting			5-Equal to or above legal loads
(41) Structure Open/Posted/Closed			A-Open, no restriction
APPRAISAL			
(67) Structural Evaluation			5
(68) Deck Geometry			4
(69) Clearances, Vertical/Horizontal			N
(71) Waterway Adequacy			6
(72) Approach Roadway Alignment			8
(36) Traffic Safety Features			0000
A) Bridge Railings			0-Inspected feature does not meet cur
B) Transitions			0-Inspected feature does not meet cur
C) Approach Guardrail			0-Inspected feature does not meet cur
D) Approach Guardrail Ends			0-Inspected feature does not meet cur
(113) Scour Critical Bridges			5-Bridge foundations determined to be
PROPOSED IMPROVEMENTS			
(75) Type of Work			
(76) Length of Structure Improvement			0 ft
(94) Bridge Improvement Cost			\$ 0
(95) Roadway Improvement Cost			\$ 0
(96) Total Project Cost			\$ 0
(97) Year of Improvement Cost Estimate			
(114) Future ADT			1499
(115) Year of Future ADT			2028
INSPECTIONS			
(90) Inspection Date			201909
(91) Frequency			24 Months
(92) Critical Feature Inspection	Done	Freq. (Mon)	Date
A: Fracture Critical Detail	No	24	
B: Underwater Inspection	No	0	
C: Other Special Inspection	No	0	

SUFFICIENCY RATING	67.4
STATUS (SD/FO/None)	Not Deficient



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ELEM	DESCRIPTION	UNITS	TOTAL	CS1	CS2	CS3	CS4
16	Reinforced Concrete Top Flange	SF	962	962	0	0	0
510	Wearing Surfaces	SF	920	730	0	190	0
3220	Crack (Wearing Surface)	SF	190	0	0	190	0
110	Reinforced Concrete Open Girder/Beam	LF	266	127	0	139	0
1090	Exposed Rebar	LF	11	0	0	11	0
1130	Cracking (RC and Other)	LF	128	0	0	128	0
215	Reinforced Concrete Abutment	LF	56	36	0	20	0
6000	Scour	LF	20	0	0	20	0
228	Timber Pile	EA	9	0	5	4	0
1150	Check/Shake	EA	5	0	5	0	0
6000	Scour	EA	4	0	0	4	0
234	Reinforced Concrete Pier Cap	LF	78	78	0	0	0
330	Metal Bridge Railing	LF	76	75	0	1	0
7000	Damage	LF	1	0	0	1	0



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Maintenance Needs



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Deck Notes

Roadway settlement both bridge ends, 1in.-1 1/2in. typical. Left bridge rail corner post is broken with exposed rebar over Bent 1. Top of Deck with Asphalt Overlay has moderate size cracks mainly over joints & between deck units.

Superstructure Notes

Girder Portion of majority of Deck Units has longitudinal cracking, 2in. up from bottom of girder typical with some rebar exposed: Span 1 Unit 1 has 5ft. of crack. Span 1 Unit 2 has 3ft. of crack. Span 1 Unit 3 has 10ft. of crack. Span 1 Unit 4 has 3ft. of crack. Span 1 Unit 5 has 1ft. of rebar exposed and 5ft. of cracks. Span 1 Unit 6 has 19ft. of crack. Span 1 Unit 7 is in good condition. Span 2 Unit 1 has 10ft. of crack. Span 2 Unit 2 has 4ft. of crack. Span 2 Unit 3 has 19ft. of crack. Span 2 Unit 4 has 10ft. of rebar exposed and 9ft. of crack. Span 2 Unit 5 has 15ft. of cracks. Span 2 Unit 6 has 15ft. of crack. Span 2 Unit 7 has 10ft. of crack. Diaphragms at ends of a few units have spalls with rebar exposed, see 2015 photo.

Substructure Notes

Bent 1 Abutment is undermining under repairs 1ft. deep-1ft. back under concrete, see 2017 photo. Bent 3 Abutment is undermining 1ft.-1.5ft. deep and 1ft-1.5ft back under cap, see 2017 photo. Timber Piles have some outside decay, checks and shakes. Drift Bent 2 has caused embankment erosion to Bents 1&3. Note: Taken Underwater off due to all elements are above water and the ditch is dry this inspection.