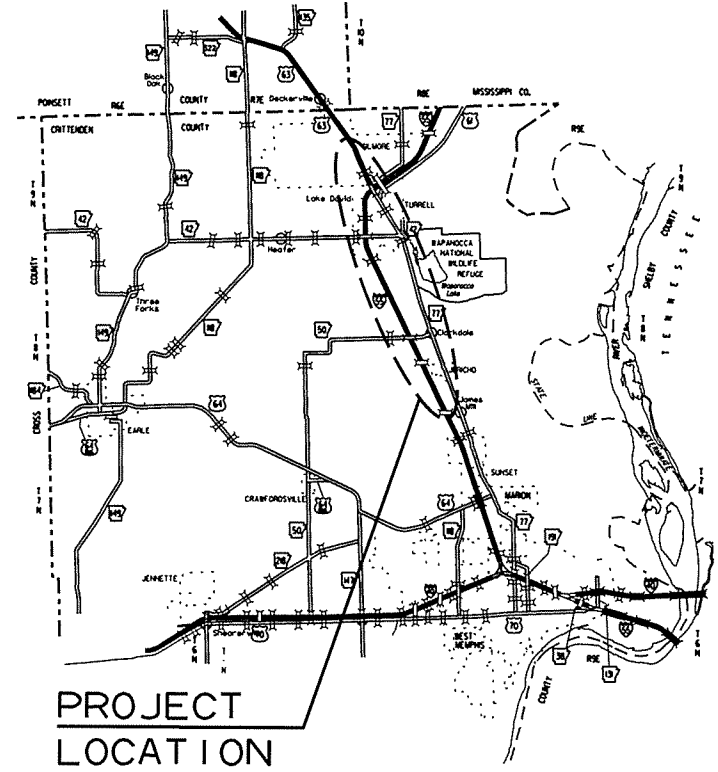


"A FULLY CONTROLLED ACCESS FACILITY"  
 ARKANSAS STATE HIGHWAY AND TRANSPORTATION DEPARTMENT  
 CONSTRUCTION PLANS FOR STATE HIGHWAY

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		BB0110	1	38
				2		JERICHO-LAKE DAVID (S)		



VICINITY MAP

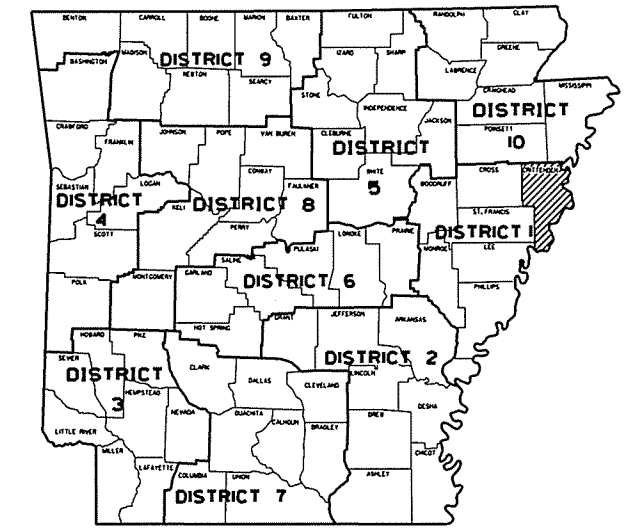
# JERICHO-LAKE DAVID (S)

CRITTENDEN COUNTY

ROUTE 55 SECTION II

## JOB NO. BB0110

FED. AID PROJ. B1 M-B55-0(202)



ARK. HWY. DIST. NO. 1

PROJECT LOCATION

0 1/4 1/2 1 MILE  
BAR SCALE

STA. 1040+00.00  
END JOB BB0110  
LOG MILE 23.05

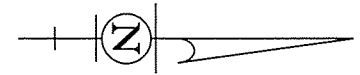
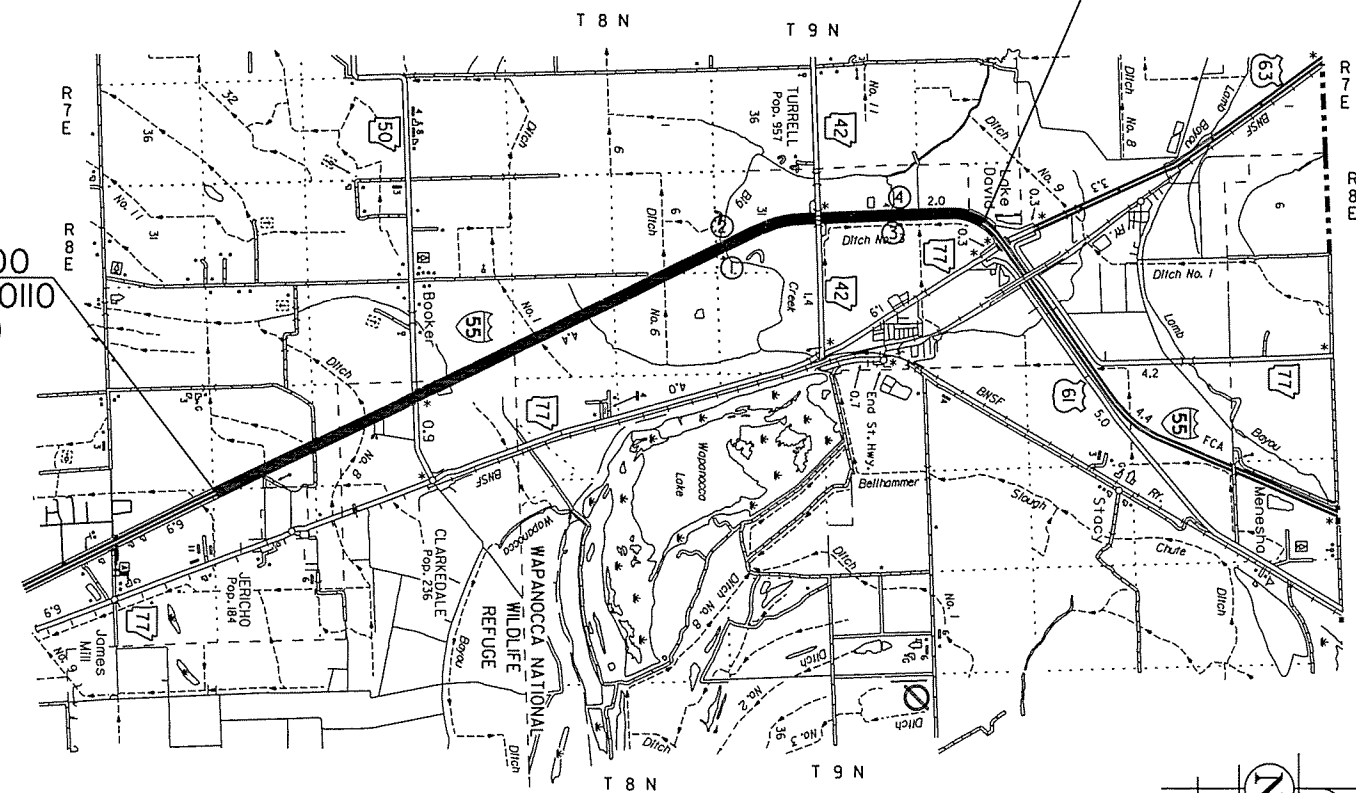
DESIGN TRAFFIC DATA

DESIGN YEAR	2034
2034 ADT	30,000
2034 ADT	39,000
2034 DHV	4,290
DIRECTIONAL DISTRIBUTION	0.60
TRUCKS	44%
DESIGN SPEED	70 MPH

BRIDGE DATA

- ① STA. 907+76 BR. END  
EXISTING 162' BRIDGE O2809  
40'-0" CLEAR ROADWAY  
STA. 909+38 BR. END  
REMOVE ACHM OVERLAY  
PLACE POLYMER OVERLAY
- ② STA. 907+76 BR. END  
EXISTING 162' BRIDGE A2810  
40'-0" CLEAR ROADWAY  
STA. 909+38 BR. END  
REMOVE ACHM OVERLAY  
PLACE POLYMER OVERLAY
- ③ STA. 998+14 BR. END  
EXISTING 81' BRIDGE A2810  
40'-0" CLEAR ROADWAY  
STA. 998+95 BR. END  
REMOVE ACHM OVERLAY  
PLACE POLYMER OVERLAY
- ④ STA. 998+14 BR. END  
EXISTING 81' BRIDGE O2810  
40'-0" CLEAR ROADWAY  
STA. 998+95 BR. END  
REMOVE ACHM OVERLAY  
PLACE POLYMER OVERLAY

STA. 620+00.00  
BEGIN JOB BB0110  
LOG MILE 15.10



APPROVED



*Ralph J. Hall*  
DEPUTY DIRECTOR  
AND CHIEF ENGINEER

BEGINNING OF PROJECT	MID POINT OF PROJECT	END OF PROJECT
LATITUDE = N 35°16'46"	LATITUDE = N 35°19'58"	LATITUDE = N 35°23'16"
LONGITUDE = W 90°14'12"	LONGITUDE = W 90°15'54"	LONGITUDE = W 90°16'42"

GROSS LENGTH OF PROJECT	42000.00	FEET	OR	7.955	MILES
NET " " ROADWAY	41757.00	" "	" "	7.909	" "
NET " " BRIDGE	243.00	" "	" "	0.046	" "
NET " " PROJECT	42000.00	" "	" "	7.955	" "

P.E. BB0110  
NON-PART.

2/12/2014

RB0110.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	BB0110	2 38

② INDEX OF SHEETS, GOV. SPECS., & GEN NOTES



**INDEX OF SHEETS**

SHEET NO.	TITLE	BRIDGE NO.	DRWG. NO.	DATE
1	TITLE SHEET			
2	INDEX OF SHEETS, GOVERNING SPECIFICATIONS, AND GENERAL NOTES			
3	TYPICAL SECTIONS OF IMPROVEMENT			
4 - 5	SPECIAL DETAILS			
6 - 10	MAINTENANCE OF TRAFFIC			
11 - 12	QUANTITIES			
13	SUMMARY OF QUANTITIES AND REVISIONS			
14 - 28	PLAN SHEETS			
29	LAYOUT OF BRIDGES OVER BIG CREEK (FOR INFORMATION ONLY)	02809, A2809	55512	
30	DETAILS OF 81'-0" CONTINUOUS R.C. SLAB UNIT (FOR INFORMATION ONLY)	02809, A2809	55513	
31	LAYOUT OF BRIDGES OVER DRY BAYOU (FOR INFORMATION ONLY)	02810, A2810	55514	
32	DETAILS OF 81'-0" CONTINUOUS R.C. SLAB UNIT (FOR INFORMATION ONLY)	02810, A2810	55515	
33	PAVEMENT MARKING DETAILS		PM-1	9-12-13
34	PERMANENT PAVEMENT MARKING ON ACCESS CONTROLLED ROADWAYS		PM-2	9-12-13
35	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION		TC-1	12-15-11
36	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION		TC-2	9-12-13
37	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION		TC-3	10-15-09
38	DETAILS OF STANDARD TURNOUT FOR ENTRANCE & EXIT RAMP (NON-REINFORCED)		TR-1A	8-22-02

**GOVERNING SPECIFICATIONS**

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

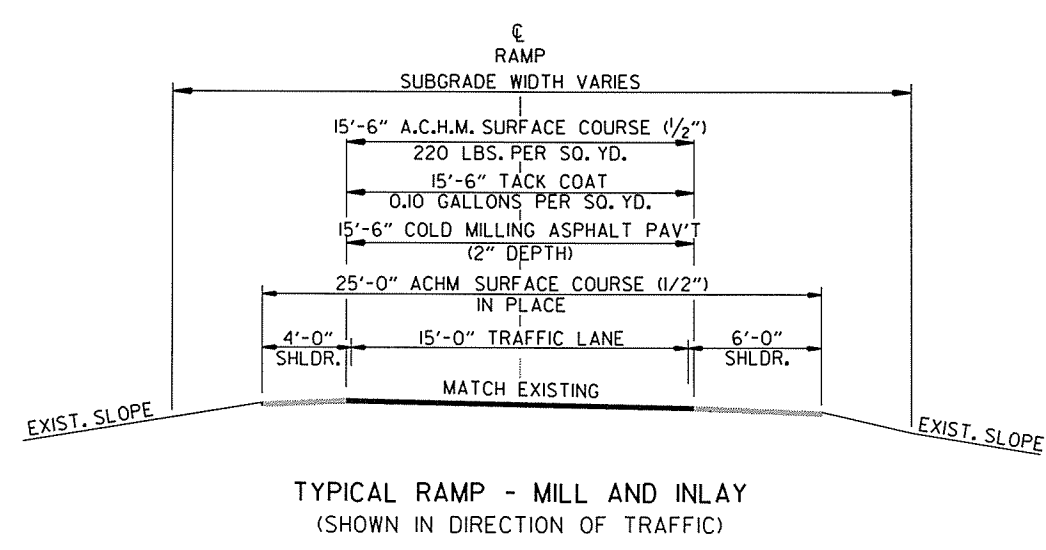
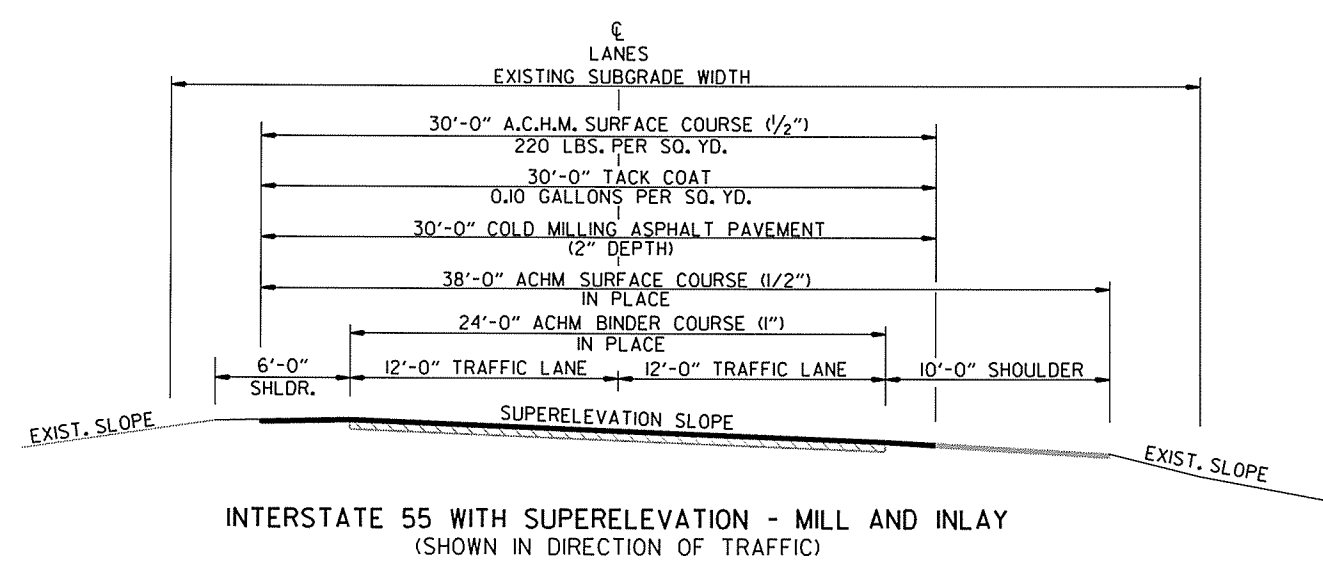
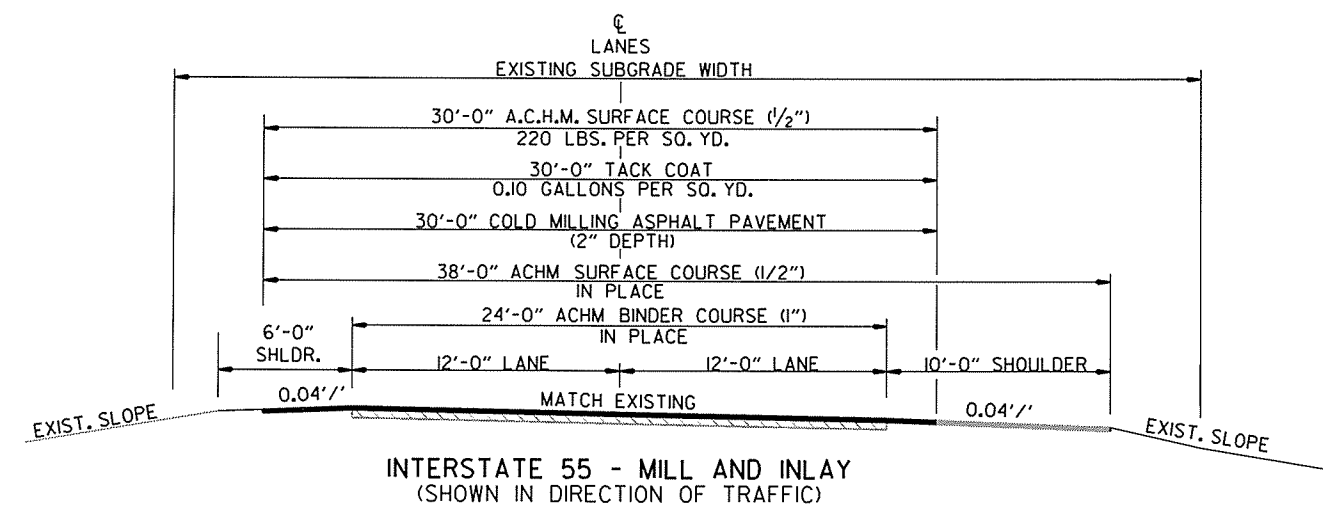
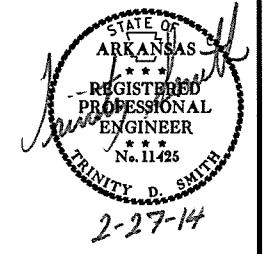
NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
108-1	LIQUIDATED DAMAGES
410-1	CONSTRUCTION REQUIREMENTS AND ACCEPTANCE OF ASPHALT CONCRETE PLANT MIX COURSES
JOB BB0110	ASSESSMENT OF WORKING DAYS-SATURDAYS
JOB BB0110	BRIDGE DECK REPAIR FOR POLYMER OVERLAYS
JOB BB0110	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB BB0110	ELECTRONIC SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB BB0110	EMPLOYMENT REPORTING
JOB BB0110	GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION
JOB BB0110	HIGH PERFORMANCE PAVEMENT MARKING
JOB BB0110	MAINTENANCE OF TRAFFIC
JOB BB0110	MANDATORY USE OF INTERNET BIDDING
JOB BB0110	PARTNERING REQUIREMENTS
JOB BB0110	PERCENT WITHIN LIMITS
JOB BB0110	POLYMER OVERLAY
JOB BB0110	PORTABLE CHANGEABLE MESSAGE SIGNS (TRUCK MOUNTED)
JOB BB0110	SITE USE (A + C METHOD)
JOB BB0110	TEMPORARY PORTABLE RUMBLE STRIPS
JOB BB0110	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB BB0110	UTILITY ADJUSTMENTS
JOB BB0110	VALUE ENGINEERING
JOB BB0110	WARM MIX ASPHALT

**GENERAL NOTES**

- GRADE LINE DENOTES FINISHED GRADE WHERE SHOWN ON PLANS.
- ALL PIPE LINES, POWER, TELEPHONE AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING U. S. MAILBOXES WITHIN THE PROJECT LIMITS IN SUCH A MANNER THAT THE PUBLIC MAY RECEIVE CONTINUED MAIL SERVICE. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE OF THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0110		3	38

2 TYPICAL SECTIONS OF IMPROVEMENT

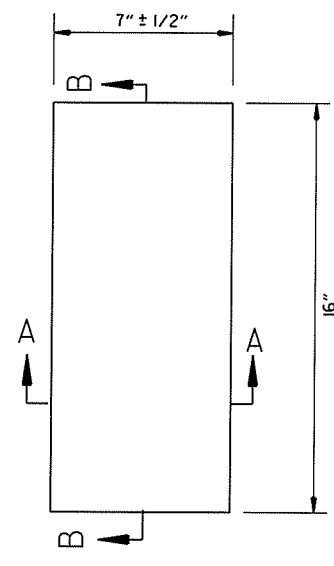


TYPICAL SECTIONS OF IMPROVEMENT

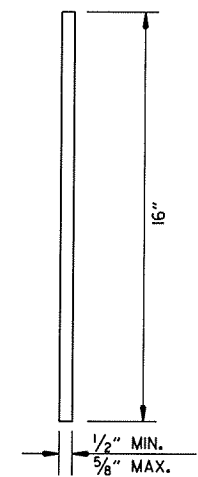
2/24/2014  
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				6	ARK.				
							JOB NO. BB0110	4	38

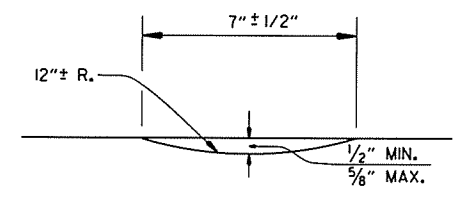
2 SPECIAL DETAILS



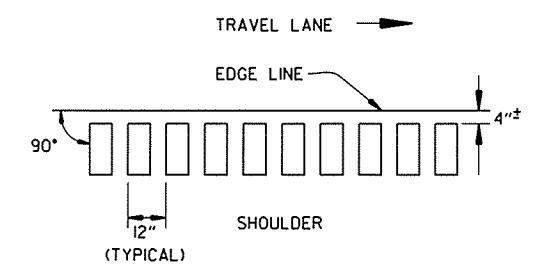
PLAN



SECTION B-B

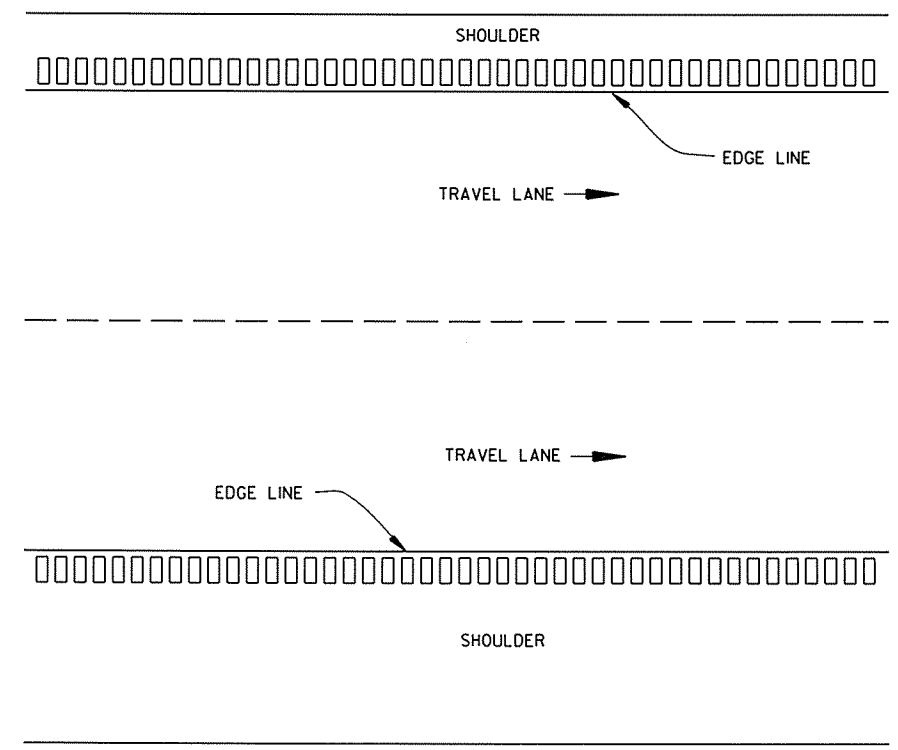


SECTION A-A



LOCATION PLAN OF RUMBLE STRIPS  
LEFT OR RIGHT SHOULDER

DETAILS OF RUMBLE STRIPS



PLAN VIEW

NOTES:

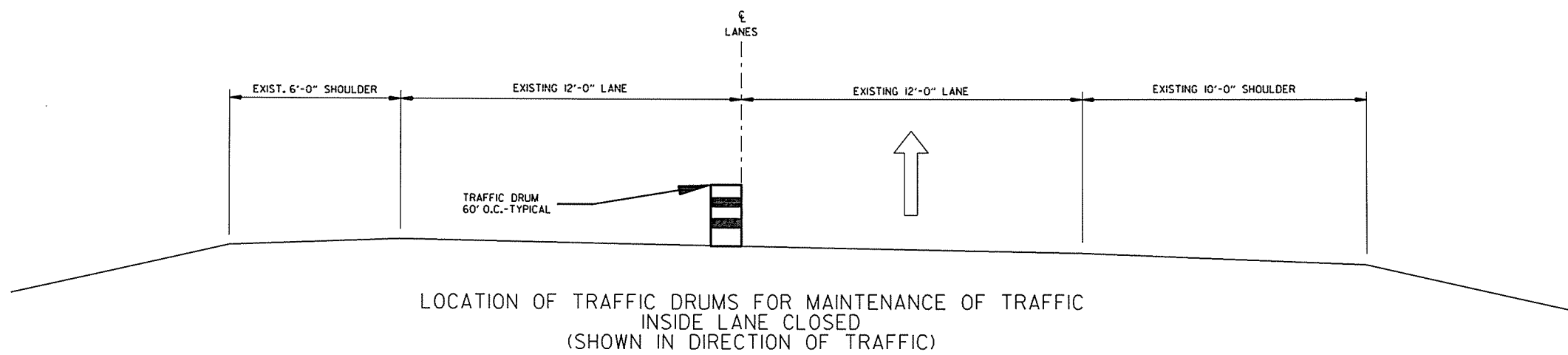
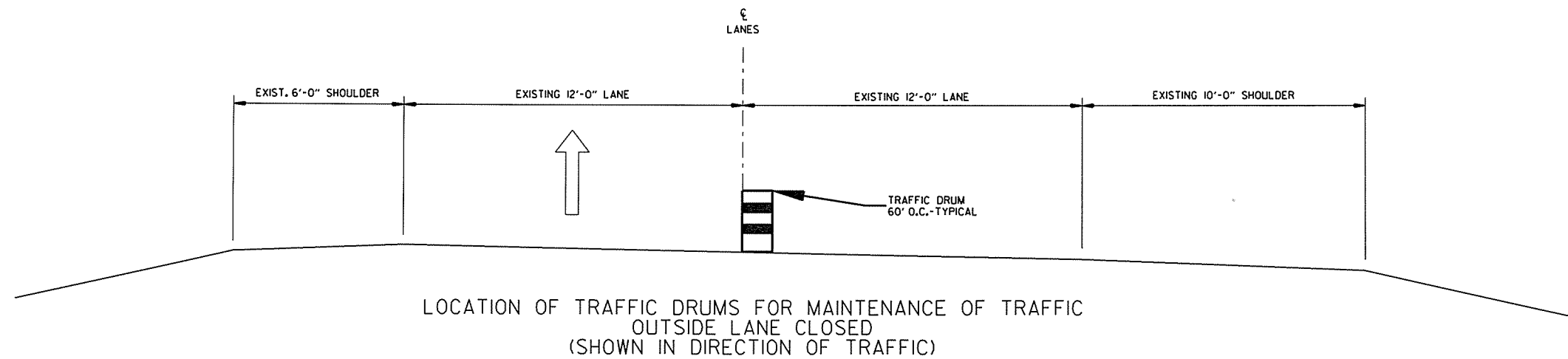
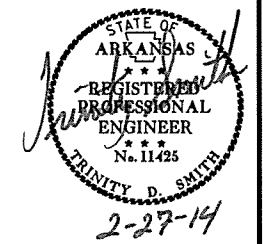
1. ALIGNMENT OF RUMBLE STRIPS SHALL GENERALLY BE STRAIGHT AND OFFSET APPROXIMATELY 4" FROM THE OUTER EDGE OF THE EDGE LINE. THIS OFFSET MAY BE ADJUSTED TO ACCOMMODATE VARIATIONS IN THE EDGE LINE.
2. THE 1/2" DEPTH SHALL GENERALLY APPLY FOR THE ENTIRE 16" LENGTH. SOME VARIATION TO SUIT SHOULDER SLOPE BREAKS MAY BE NECESSARY.
3. RUMBLE STRIPS SHALL NOT BE INSTALLED ON BRIDGE DECKS, APPROACH SLABS, OR ACROSS TRANSVERSE JOINTS OF CONCRETE SHOULDERS.

SPECIAL DETAILS

2/12/2014  
RB0110.DCN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0110							5	38

② SPECIAL DETAILS



2/12/2014  
RB0110.DCN

SPECIAL DETAILS

CONSTRUCTION PAVEMENT MARKINGS:  
 APPLY CONSTRUCTION PAVEMENT MARKINGS  
 ACCORDING TO STD. DWG. PM-2

- 4" YELLOW - 85700 LIN. FT.
- 4" (SKIP LINE) WHITE - 2100 LIN. FT.
- 4" WHITE - 84175 LIN. FT.
- 8" WHITE - 4187 LIN. FT.

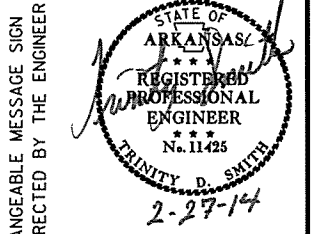
PERMANENT PAVEMENT MARKINGS:  
 APPLY PERMANENT PAVEMENT MARKINGS  
 ACCORDING TO STD. DWG. PM-2

- 4" YELLOW = 85700 LIN. FT.
- 4" (SKIP LINE) WHITE = 20880
- 4" WHITE = 84175 LIN. FT.
- 8" WHITE = 4187 LIN. FT.
- 4" WHITE CONTRAST = 120 LIN. FT.
- RAISED PAV'T MARKINGS (TYPE II) = 1736 EACH
- 80' SPACING (EXCEPT WHERE SHOWN ON STD. DWG. PM-2)

NOTE:  
 CONSTRUCTION PAVEMENT MARKINGS  
 QUANTITY BASED ON ONE APPLICATION  
 OF EXISTING PAVEMENT MARKINGS.  
 FOR ADDITIONAL INFORMATION,  
 SEE STD. DRG. PM-2.

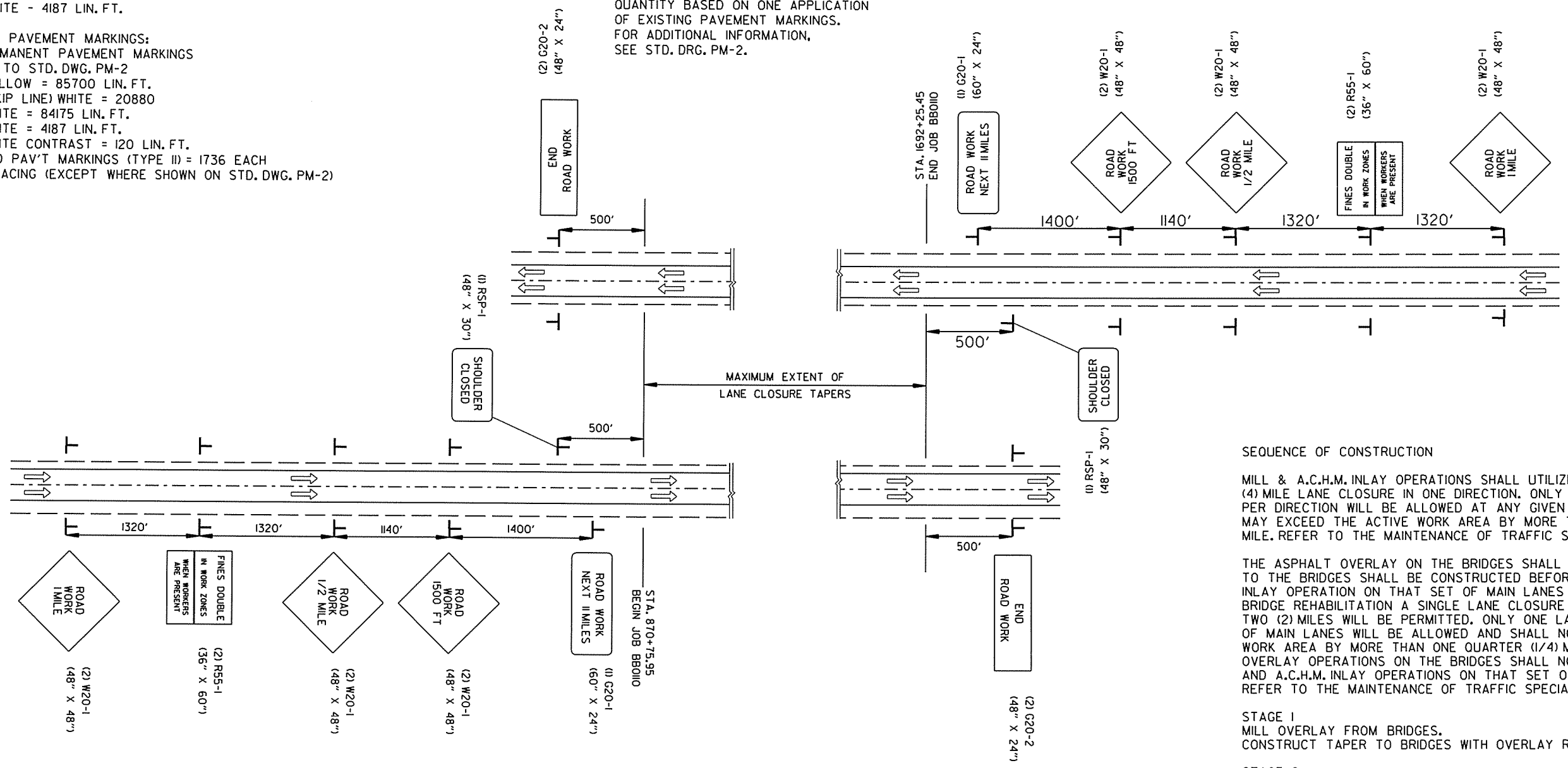
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				6	ARK.			
JOB NO. BB0110							6	38

② MAINTENANCE OF TRAFFIC



PORTABLE CHANGEABLE MESSAGE SIGN  
 PLACED AS DIRECTED BY THE ENGINEER

PORTABLE CHANGEABLE MESSAGE SIGN  
 PLACED AS DIRECTED BY THE ENGINEER



SEQUENCE OF CONSTRUCTION

MILL & A.C.H.M. INLAY OPERATIONS SHALL UTILIZE A SINGLE FOUR (4) MILE LANE CLOSURE IN ONE DIRECTION. ONLY ONE LANE CLOSURE PER DIRECTION WILL BE ALLOWED AT ANY GIVEN TIME. NO LANE CLOSURE MAY EXCEED THE ACTIVE WORK AREA BY MORE THAN ONE QUARTER (1/4) MILE. REFER TO THE MAINTENANCE OF TRAFFIC SPECIAL PROVISION.

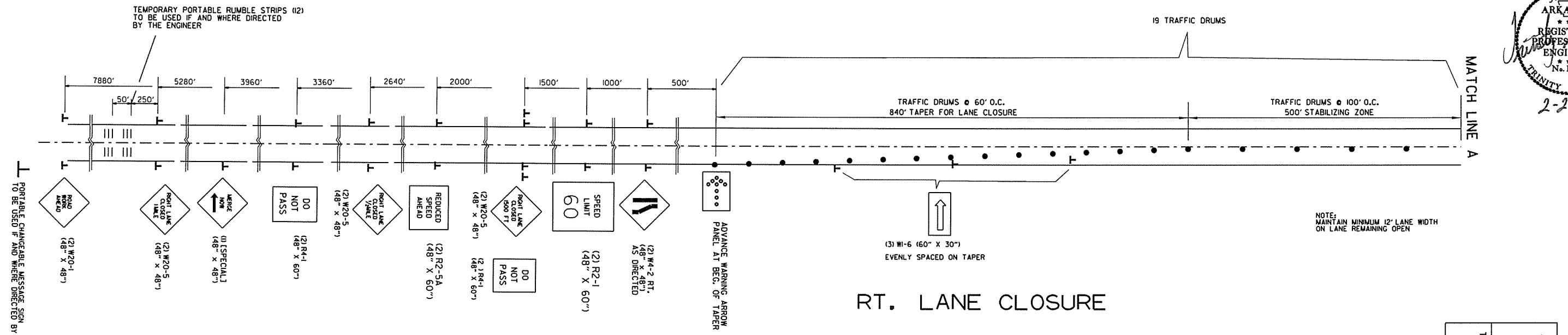
THE ASPHALT OVERLAY ON THE BRIDGES SHALL BE REMOVED AND TAPERS TO THE BRIDGES SHALL BE CONSTRUCTED BEFORE THE MILL & A.C.H.M. INLAY OPERATION ON THAT SET OF MAIN LANES ARE STARTED. FOR BRIDGE REHABILITATION A SINGLE LANE CLOSURE OF NO MORE THAN TWO (2) MILES WILL BE PERMITTED. ONLY ONE LANE CLOSURE PER SET OF MAIN LANES WILL BE ALLOWED AND SHALL NOT EXCEED THE ACTIVE WORK AREA BY MORE THAN ONE QUARTER (1/4) MILE. THE POLYMER OVERLAY OPERATIONS ON THE BRIDGES SHALL NOT START UNTIL THE MILL AND A.C.H.M. INLAY OPERATIONS ON THAT SET OF LANES IS COMPLETE. REFER TO THE MAINTENANCE OF TRAFFIC SPECIAL PROVISION.

- STAGE 1  
MILL OVERLAY FROM BRIDGES.  
CONSTRUCT TAPER TO BRIDGES WITH OVERLAY REMOVED.
- STAGE 2  
MILL & A.C.H.M. INLAY OPERATIONS FOR MAIN LANES.
- STAGE 3  
POLYMER OVERLAY OPERATIONS ON BRIDGES.

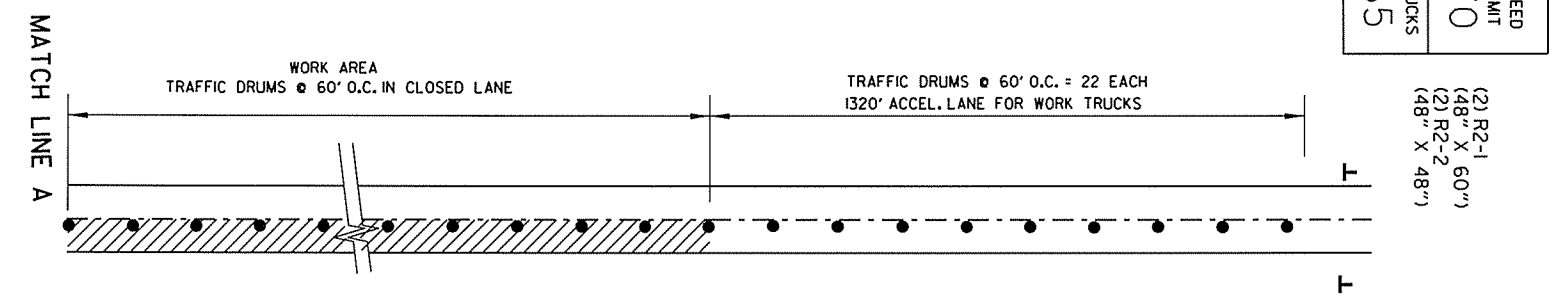
MAINTENANCE OF TRAFFIC  
 ADVANCE SIGNS AT JOB ENDS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0110							7	38

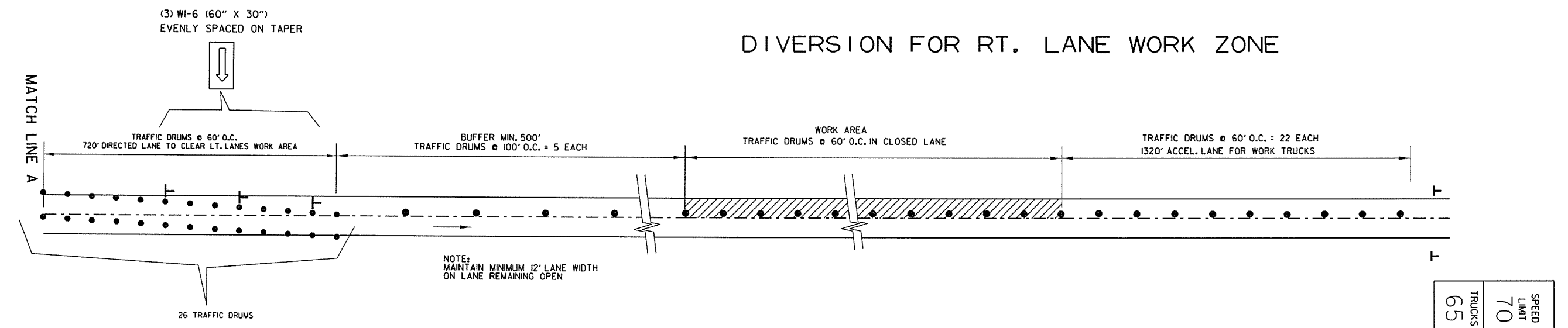
② MAINTENANCE OF TRAFFIC



RT. LANE CLOSURE



DIVERSION FOR RT. LANE WORK ZONE



DIVERSION FOR LT. LANE WORK ZONE

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. BB0110	8	38

ADVANCE WARNING SIGNS FOR ENTRANCE AND EXIT RAMPS  
 ROAD WORK AHEAD (2) = 32 SQ. FT.  
 END ROAD WORK (2) = 16 SQ. FT.

② MAINTENANCE OF TRAFFIC

(1) W20-1  
(48" X 48")



250'

BEGIN MILL & INLAY

END MILL & INLAY



(1) G20-2  
(48" X 24")

250'



2-27-14

DETAIL OF ENTRANCE AND EXIT RAMPS  
 EXIT 17 & 21

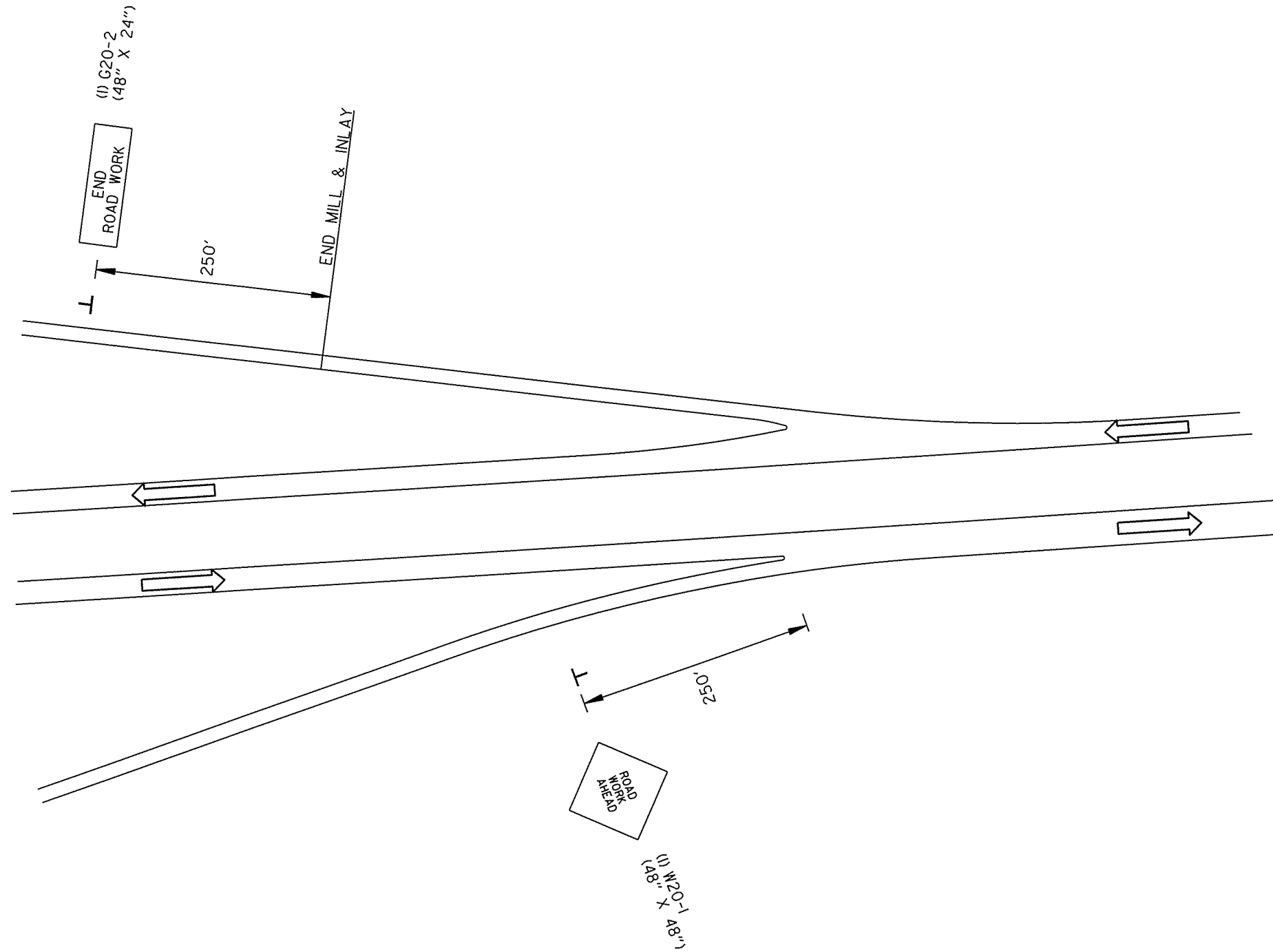
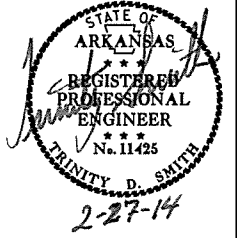
MAINTENANCE OF TRAFFIC  
 DETAILS OF RAMPS



ADVANCE WARNING SIGNS FOR ENTRANCE AND EXIT RAMP  
 ROAD WORK AHEAD (1) = 16 SQ. FT.  
 END ROAD WORK (1) = 8 SQ. FT.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	BB0110	9 38

② MAINTENANCE OF TRAFFIC

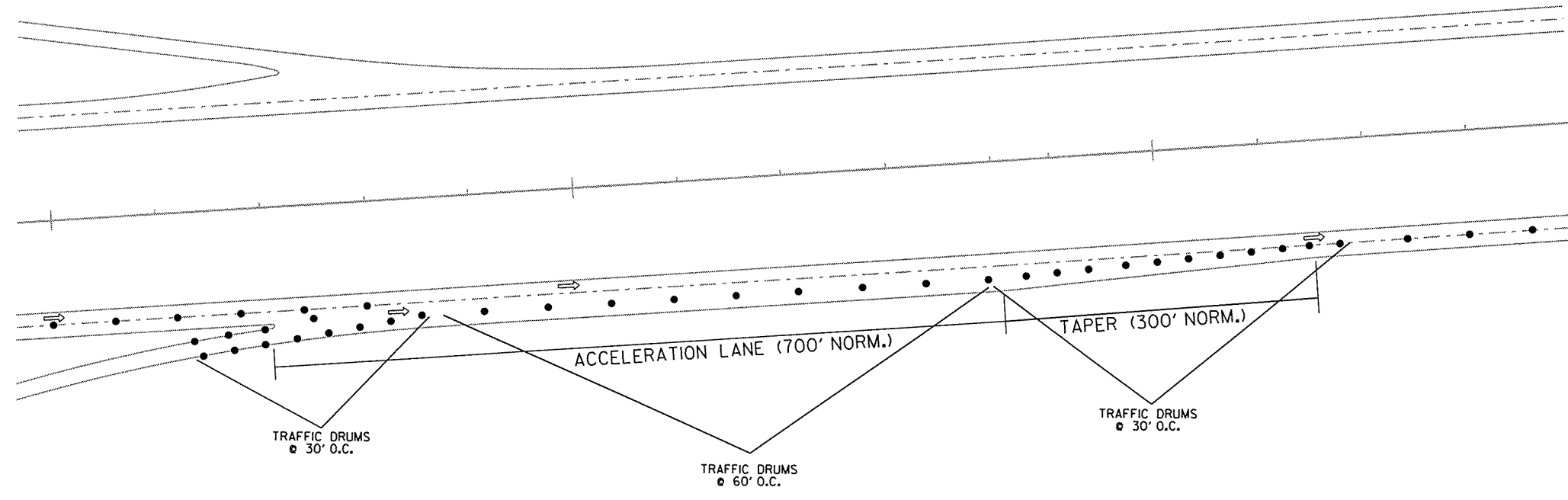
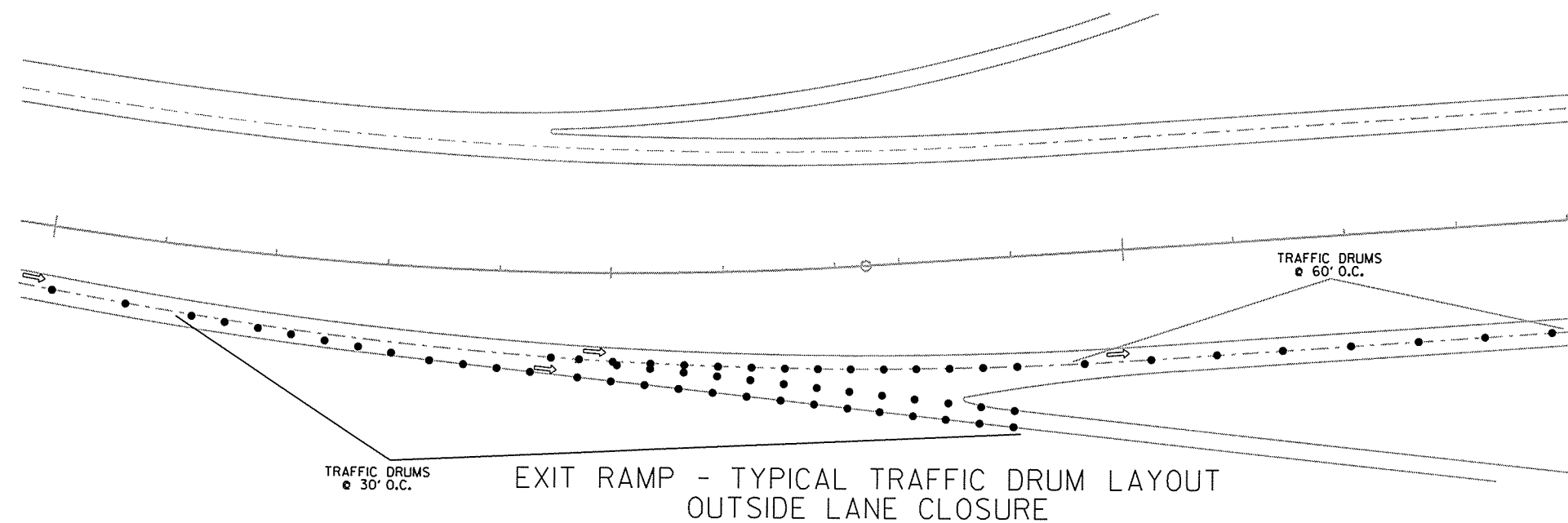


DETAIL OF ENTRANCE AND EXIT RAMP  
 EXIT 23

MAINTENANCE OF TRAFFIC  
 DETAIL OF RAMPS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO.							BB0110	38

② MAINTENANCE OF TRAFFIC



ENTRANCE RAMP - TYPICAL TRAFFIC DRUM LAYOUT  
ACCELERATION LANE CLOSURE

EXIT 17:  
NORTHBOUND EXIT = 40 TRAFFIC DRUMS  
NORTHBOUND ENTRANCE = 17 TRAFFIC DRUMS  
  
SOUTHBOUND EXIT = 40 TRAFFIC DRUMS  
SOUTHBOUND ENTRANCE = 17 TRAFFIC DRUMS

EXIT 21:  
NORTHBOUND EXIT = 40 TRAFFIC DRUMS  
NORTHBOUND ENTRANCE = 17 TRAFFIC DRUMS  
  
SOUTHBOUND EXIT = 40 TRAFFIC DRUMS  
SOUTHBOUND ENTRANCE = 17 TRAFFIC DRUMS

EXIT 23:  
NORTHBOUND EXIT = 40 TRAFFIC DRUMS

MAINTENANCE OF TRAFFIC  
DETAIL OF RAMPS WITH LANE CLOSURE

2/12/2014

RB0110.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0110		11	38

② QUANTITIES



CONSTRUCTION PAVEMENT MARKINGS AND PERMANENT PAVEMENT MARKINGS

DESCRIPTION	ENTIRE PROJECT LIN. FT. - EACH	CONSTRUCTION PAVEMENT MARKINGS LIN. FT.	RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) EACH	HIGH PERFORMANCE CONTRAST PAVEMENT MARKING			
				4"		8"	
				WHITE	(SKIP LINE) WHITE	WHITE	WHITE
				LIN. FT.			
CONSTRUCTION PAVEMENT MARKINGS	195062	195062					
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED)	1736		1736				
HIGH PERFORMANCE CONTRAST PAVEMENT MARKING WHITE (4")	120			120			
HIGH PERFORMANCE PAVEMENT MARKING WHITE (4")	84175				84175		
HIGH PERFORMANCE PAVEMENT MARKING (SKIP LINE) WHITE (4")	20880					20880	
HIGH PERFORMANCE PAVEMENT MARKING WHITE (8")	4187						4187
HIGH PERFORMANCE PAVEMENT MARKING YELLOW (4")	85700						85700
<b>TOTALS:</b>		195062	1736	120	84175	20880	85700

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.

ADVANCE WARNING SIGNS AND DEVICES

SIGN NUMBER	DESCRIPTION	SIGN SIZE	ENTIRE PROJECT LIN. FT. - EACH	MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED		TRAFFIC DRUMS EACH	ADVANCE WARNING ARROW PANEL DAY	PORTABLE CHANGEABLE MESSAGE SIGN WEEK	TEMPORARY PORTABLE RUMBLE STRIP EACH	PORTABLE CHANGEABLE MESSAGE SIGNS (TRUCK MOUNTED) WEEK
					NO.	SQ. FT.					
W20-1	ROAD WORK 1 MILE	48"x48"	4	4	4	64.0					
W20-1	ROAD WORK 1/2 MILE	48"x48"	4	4	4	64.0					
W20-1	ROAD WORK 1500 FT.	48"x48"	4	4	4	64.0					
W20-1	ROAD WORK AHEAD	48"x48"	7	7	7	112.0					
G20-2	END ROAD WORK	48"x24"	7	7	7	56.0					
G20-1	ROAD WORK NEXT xx MILES	60"x24"	2	2	2	20.0					
W20-5	RIGHT LANE CLOSED 1 MILE	48"x48"	4	4	4	64.0					
W20-5	RIGHT LANE CLOSED 1/2 MILE	48"x48"	4	4	4	64.0					
W20-5	RIGHT LANE CLOSED 1500 FT.	48"x48"	4	4	4	64.0					
R55-1	FINES DOUBLE IN WORK ZONES	30"x60"	4	4	4	50.0					
SPECIAL	MERGE NOW W/ ARROW	48"x48"	2	2	2	32.0					
R2-5A	REDUCE SPEED AHEAD	48"x60"	4	4	4	80.0					
W1-6	LARGE ARROW	48"x24"	12	12	12	96.0					
R4-1	DO NOT PASS	48"x60"	8	8	8	160.0					
R2-1	SPEED LIMIT 60 MPH	48"x60"	7	7	7	140.0					
R2-1	SPEED LIMIT 70 MPH	48"x60"	4	4	4	80.0					
R2-2	TRUCK SPEED LIMIT 65 MPH	48"x60"	4	4	4	80.0					
RSP-1	SHOULDER CLOSED	48"x30"	2	2	2	20.0					
W4-2 RT	MERGE RIGHT	48"x48"	4	4	4	64.0					
	TRAFFIC DRUMS		769	769			769				
	ADVANCE WARNING ARROW PANEL		2	2				60			
	PORTABLE CHANGEABLE MESSAGE SIGN		4	4					32		
	TEMPORARY PORTABLE RUMBLE STRIP		24	24						24	
	PORTABLE CHANGEABLE MESSAGE SIGNS (TRUCK MOUNTED)		2	2							16
<b>TOTALS:</b>						1374.0	769	60	32	24	16

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.

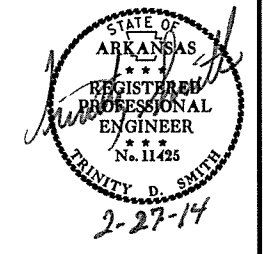
NOTE: THE QUANTITY OF TRAFFIC DRUMS PROVIDED IS FOR BOTH SIDES OF THE ROADWAY FOR ONE WORK AREA OF TWO MILES AND ONE WORK AREA OF FOUR MILES. HOWEVER, THE INSTALLATION OF TRAFFIC DRUMS SHALL NEVER EXCEED THE ACTUAL WORK AREA BY MORE THAN 1/4 MILE, UNLESS APPROVED BY THE ENGINEER.

2/12/2014

RB60110.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0110		12	38

2 QUANTITIES



### COLD MILLING ASPHALT PAVEMENT

STATION	STATION	LOCATION	COLD MILLING ASPHALT PAVEMENT	
			AVG. WIDTH FEET	SQ. YD.
** 905+76	907+76	RT. MAIN LANES	40	888.89
** 907+76	909+38	RT. MAIN LANES	40	720.00
** 909+38	911+38	RT. MAIN LANES	40	888.89
** 905+76	907+76	LT. MAIN LANES	40	888.89
** 907+76	909+38	LT. MAIN LANES	40	720.00
** 909+38	911+38	LT. MAIN LANES	40	888.89
** 996+14	998+14	RT. MAIN LANES	40	888.89
** 998+14	998+95	RT. MAIN LANES	40	360.00
** 998+95	1000+95	RT. MAIN LANES	40	888.89
** 996+14	998+14	LT. MAIN LANES	40	888.89
** 998+14	998+95	LT. MAIN LANES	40	360.00
** 998+95	1000+95	LT. MAIN LANES	40	888.89
620+00	907+76	RT. MAIN LANES	30	95920.00
909+38	998+14	RT. MAIN LANES	30	29586.67
998+95	1040+00	RT. MAIN LANES	30	13683.33
620+00	907+76	LT. MAIN LANES	30	95920.00
909+38	998+14	LT. MAIN LANES	30	29586.67
998+95	1040+00	LT. MAIN LANES	30	13683.33
ADDITIONAL FOR EXIT AND ENTRANCE RAMP				
714+31	721+86	EXIT 17 RT. MAIN LANES - EXIT RAMP	VARIABLES	874.32
714+39	723+19	EXIT 17 LT. MAIN LANES - ACCEL. LANE	VARIABLES	994.62
723+19	726+66	EXIT 17 LT. MAIN LANES - ENTRANCE RAMP	VARIABLES	467.16
738+85	742+34	EXIT 17 RT. MAIN LANES - ENTRANCE RAMP	VARIABLES	466.75
742+34	751+01	EXIT 17 RT. MAIN LANES - ACCEL. LANE	VARIABLES	930.06
737+68	745+23	EXIT 17 LT. MAIN LANES - EXIT RAMP	VARIABLES	887.26
952+77	956+16	EXIT 21 RT. MAIN LANES - EXIT RAMP	VARIABLES	400.10
938+55	950+25	EXIT 21 LT. MAIN LANES - ACCEL. LANE	VARIABLES	1130.81
950+25	953+40	EXIT 21 LT. MAIN LANES - ENTRANCE RAMP	VARIABLES	443.73
968+86	972+12	EXIT 21 RT. MAIN LANES - ENTRANCE RAMP	VARIABLES	430.02
972+12	980+91	EXIT 21 RT. MAIN LANES - ACCEL. LANE	VARIABLES	992.32
965+39	973+77	EXIT 21 LT. MAIN LANES - EXIT RAMP	VARIABLES	976.10
1021+64	1026+98	EXIT 23 RT. MAIN LANES - TURNOUT	VARIABLES	902.27
<b>TOTAL:</b>				<b>297546.64</b>

NOTE: AVERAGE MILLING DEPTH 2"  
 \* REMOVE OVERLAY ON BRIDGE, APPROX. DEPTH 2"  
 \*\* TAPER TO BRIDGE (WITH OVERLAY REMOVED)

### RUMBLE STRIPS IN ASPHALT SHOULDERS

STATION	STATION	LOCATION	* RUMBLE STRIPS IN ASPHALT SHOULDERS
			LIN. FT.
620+00	717+02	RIGHT OF RIGHT MAIN LANES	9702
717+02	742+32	RIGHT OF RIGHT MAIN LANES	2530
742+32	907+76	RIGHT OF RIGHT MAIN LANES	16544
909+38	953+99	RIGHT OF RIGHT MAIN LANES	4461
953+99	972+12	RIGHT OF RIGHT MAIN LANES	1813
972+12	998+14	RIGHT OF RIGHT MAIN LANES	2602
998+95	1025+90	RIGHT OF RIGHT MAIN LANES	2695
1026+98	1040+00	RIGHT OF RIGHT MAIN LANES	1302
620+00	907+76	LEFT OF RIGHT MAIN LANES	28776
909+38	998+14	LEFT OF RIGHT MAIN LANES	8876
998+95	1040+00	LEFT OF RIGHT MAIN LANES	4105
620+00	907+76	RIGHT OF LEFT MAIN LANES	28776
909+38	998+14	RIGHT OF LEFT MAIN LANES	8876
998+95	1040+00	RIGHT OF LEFT MAIN LANES	4105
620+00	723+19	LEFT OF LEFT MAIN LANES	10319
723+19	742+52	LEFT OF LEFT MAIN LANES	1933
742+52	907+76	LEFT OF LEFT MAIN LANES	16524
909+38	950+25	LEFT OF LEFT MAIN LANES	4087
950+25	970+72	LEFT OF LEFT MAIN LANES	2047
970+72	998+14	LEFT OF LEFT MAIN LANES	2742
998+95	1040+00	LEFT OF LEFT MAIN LANES	4105
<b>TOTAL:</b>			<b>166920</b>

\* QUANTITY ESTIMATED.  
 SEE SECTION 104.03 OF THE STD. SPECS.  
 TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

### ACHM PATCHING OF EXISTING ROADWAY

DESCRIPTION	TON
ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	50
<b>TOTAL:</b>	<b>50</b>

NOTE: QUANTITY IS ESTIMATED  
 SEE SECTION 104.03 OF THE STD. SPECS.

### BRIDGE QUANTITIES - JOB BB0110

STRUCTURE NO.	POLYMER OVERLAY	BRIDGE DECK REPAIR	REINFORCING STEEL-BRIDGE (GRADE 60)
	SQ. YD.	SQ. FT.	POUND
02809	702	320	80
A2809	702	320	80
02810	351	160	40
A2810	351	160	40
<b>TOTALS:</b>	<b>2106</b>	<b>960</b>	<b>240</b>

NOTE: EXISTING BRIDGE DECKS HAVE A 2"± ASPHALT OVERLAY.

### BASE AND SURFACING

STATION	STATION	LOCATION	LENGTH FEET	TACK COAT				ACHM SURFACE COURSE (1/2")				
				AVG. WID. FEET	SQ. YD.	GALLONS / SQ. YD.	GALLON	AVG. WID. FEET	SQ. YD.	POUND / SQ. YD.	PG 76-22 TON	
												AVG. WID. FEET
MAIN LANES												
620+00	714+31	RIGHT MAIN LANES	9431.0	30.0	31436.7	0.10	3143.7	30.0	31436.7	220.0	3458.0	
714+31	717+02	RIGHT MAIN LANES	271.0	30.0	903.3	0.10	90.3	30.0	1132.7	220.0	124.6	
717+02	742+32	RIGHT MAIN LANES	2530.0	30.0	8433.3	0.10	843.3	30.0	8433.3	220.0	927.7	
742+32	751+01	RIGHT MAIN LANES	869.0	30.0	2896.7	0.10	289.7	30.0	3647.9	220.0	401.3	
751+01	907+76	RIGHT MAIN LANES	15675.0	30.0	52250.0	0.10	5225.0	30.0	52250.0	220.0	5747.5	
909+38	952+77	RIGHT MAIN LANES	4339.0	30.0	14463.3	0.10	1446.3	30.0	14463.3	220.0	1591.0	
952+77	953+99	RIGHT MAIN LANES	122.0	30.0	406.7	0.10	40.7	30.0	509.0	220.0	56.0	
953+99	972+12	RIGHT MAIN LANES	1813.0	30.0	6043.3	0.10	604.3	30.0	6043.3	220.0	664.8	
972+12	980+91	RIGHT MAIN LANES	879.0	30.0	2930.0	0.10	293.0	30.0	3745.2	220.0	412.0	
980+91	998+14	RIGHT MAIN LANES	1723.0	30.0	5743.3	0.10	574.3	30.0	5743.3	220.0	631.8	
998+95	1021+64	RIGHT MAIN LANES	2269.0	30.0	7563.3	0.10	756.3	30.0	7563.3	220.0	832.0	
1021+64	1026+98	RIGHT MAIN LANES	534.0	30.0	1780.0	0.10	178.0	30.0	2563.6	220.0	282.0	
1026+98	1040+00	RIGHT MAIN LANES	1302.0	30.0	4340.0	0.10	434.0	30.0	4340.0	220.0	477.4	
620+00	714+39	LEFT MAIN LANES	9439.0	30.0	31463.3	0.10	3146.3	30.0	31463.3	220.0	3461.0	
714+39	723+19	LEFT MAIN LANES	880.0	30.0	2933.3	0.10	293.3	30.0	2933.3	220.0	322.7	
723+19	742+52	LEFT MAIN LANES	1933.0	30.0	6443.3	0.10	644.3	30.0	6443.3	220.0	708.8	
742+52	745+23	LEFT MAIN LANES	271.0	30.0	903.3	0.10	90.3	30.0	903.3	220.0	99.4	
745+23	907+76	LEFT MAIN LANES	16253.0	30.0	54176.7	0.10	5417.7	30.0	54176.7	220.0	5959.4	
909+38	938+55	LEFT MAIN LANES	2917.0	30.0	9723.3	0.10	972.3	30.0	9723.3	220.0	1069.6	
938+55	950+25	LEFT MAIN LANES	1170.0	30.0	3900.0	0.10	390.0	30.0	3900.0	220.0	429.0	
950+25	953+40	LEFT MAIN LANES	315.0	30.0	1050.0	0.10	105.0	30.0	1050.0	220.0	115.5	
953+40	970+72	LEFT MAIN LANES	1732.0	30.0	5773.3	0.10	577.3	30.0	5773.3	220.0	635.1	
970+72	973+77	LEFT MAIN LANES	305.0	30.0	1016.7	0.10	101.7	30.0	1016.7	220.0	111.8	
973+77	998+14	LEFT MAIN LANES	2437.0	30.0	8123.3	0.10	812.3	30.0	8123.3	220.0	893.6	
998+95	1040+00	LEFT MAIN LANES	4105.0	30.0	13683.3	0.10	1368.3	30.0	13683.3	220.0	1505.2	
ADDITIONAL FOR RAMP												
714+31	721+86	EXIT 17 RT. MAIN LANES - EXIT RAMP	755.0	VAR.	874.3	0.10	87.4	VAR.	874.3	220.0	96.2	
738+85	742+34	EXIT 17 RT. MAIN LANES - ENTRANCE RAMP	349.0	VAR.	466.8	0.10	46.7	VAR.	466.8	220.0	51.3	
742+34	751+01	EXIT 17 RT. MAIN LANES - ACCEL. LANE	867.0	VAR.	930.1	0.10	93.0	VAR.	930.1	220.0	102.3	
952+77	956+16	EXIT 21 RT. MAIN LANES - EXIT RAMP	339.0	VAR.	400.1	0.10	40.0	VAR.	400.1	220.0	44.0	
968+86	972+12	EXIT 21 RT. MAIN LANES - ENTRANCE RAMP	326.0	VAR.	430.0	0.10	43.0	VAR.	430.0	220.0	47.3	
972+12	980+91	EXIT 21 RT. MAIN LANES - ACCEL. LANE	879.0	VAR.	992.3	0.10	99.2	VAR.	992.3	220.0	109.2	
1021+64	1026+98	EXIT 23 RT. MAIN LANES - TURNOUT	534.0	VAR.	902.3	0.10	90.2	VAR.	902.3	220.0	99.3	
714+39	723+19	EXIT 17 LT. MAIN LANES - ACCEL. LANE	880.0	VAR.	994.6	0.10	99.5	VAR.	994.6	220.0	109.4	
723+19	726+66	EXIT 17 LT. MAIN LANES - ENTRANCE RAMP	347.0	VAR.	467.2	0.10	46.7	VAR.	467.2	220.0	51.4	
737+68	745+23	EXIT 17 LT. MAIN LANES - EXIT RAMP	755.0	VAR.	887.3	0.10	88.7	VAR.	887.3	220.0	97.6	
938+55	950+25	EXIT 21 LT. MAIN LANES - ACCEL. LANE	1170.0	VAR.	1103.8	0.10	110.4	VAR.	1130.8	220.0	124.4	
950+25	953+40	EXIT 21 LT. MAIN LANES - ENTRANCE RAMP	315.0	VAR.	443.7	0.10	44.4	VAR.	443.7	220.0	48.8	
965+39	973+77	EXIT 23 LT. MAIN LANES - EXIT RAMP	838.0	VAR.	976.1	0.10	97.6	VAR.	976.1	220.0	107.4	
<b>TOTALS:</b>					<b>288248.3</b>		<b>28824.5</b>		<b>290957.0</b>		<b>32005.8</b>	

BASIS OF ESTIMATE:  
 ACHM SURFACE COURSE (1/2").....94.8% MIN. AGGR.....5.2% ASPHALT BINDER  
 MAXIMUM NUMBER OF GYRATIONS = 205 FOR PG 76-22

2/12/2014 BB0110.DGN

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO. BB0110		13		38

2 SUMMARY OF QUANTITIES AND REVISIONS



SUMMARY OF QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
401	TACK COAT	28825	GAL.
SP, SS, & 407	MINERAL AGGREGATE IN ACHM SURFACE COURSE (1/2")	30342	TON
SP, SS, & 407	ASPHALT BINDER (PG 76-22) IN ACHM SURFACE COURSE (1/2")	1664	TON
412	COLD MILLING ASPHALT PAVEMENT	297547	SQ. YD.
SP & 415	ACHM PATCHING OF EXISTING ROADWAY	50	TON
601	MOBILIZATION	1.00	LUMP SUM
SP & 603	MAINTENANCE OF TRAFFIC	1.00	LUMP SUM
604	SIGNS	1374	SQ. FT.
604	TRAFFIC DRUMS	769	EACH
604	CONSTRUCTION PAVEMENT MARKINGS	195062	LIN. FT.
604	ADVANCE WARNING ARROW PANEL	60	DAY
SP & 604	PORTABLE CHANGEABLE MESSAGE SIGN	32	WEEK
SP & 604	PORTABLE CHANGEABLE MESSAGE SIGN (TRUCK MOUNTED)	16	WEEK
635	ROADWAY CONSTRUCTION CONTROL	1.00	LUMP SUM
642	RUMBLE STRIPS IN ASPHALT SHOULDERS	166920	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING WHITE (4") (ALTERNATE NO. 1)	84175	LIN. FT.
* SP	HIGH PERFORMANCE MARKING TAPE WHITE (4") (ALTERNATE NO. 2)	84175	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING (SKIP LINE) WHITE (4") (ALTERNATE NO. 1)	20880	LIN. FT.
* SP	HIGH PERFORMANCE MARKING TAPE (SKIP LINE) WHITE (4") (ALTERNATE NO. 2)	20880	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING YELLOW (4") (ALTERNATE NO. 1)	85700	LIN. FT.
* SP	HIGH PERFORMANCE MARKING TAPE YELLOW (4") (ALTERNATE NO. 2)	85700	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC PAVEMENT MARKING WHITE (8") (ALTERNATE NO. 1)	4187	LIN. FT.
* SP	HIGH PERFORMANCE MARKING TAPE WHITE (8") (ALTERNATE NO. 2)	4187	LIN. FT.
* SP & 719	INVERTED PROFILE THERMOPLASTIC CONTRAST PAVEMENT MARKING WHITE (4") (ALTERNATE NO. 1)	120	LIN. FT.
* SP	HIGH PERFORMANCE CONTRAST MARKING TAPE WHITE (4") (ALTERNATE NO. 2)	120	LIN. FT.
721	RAISED PAVEMENT MARKERS (TYPE II)	1736	EACH
SP	TEMPORARY PORTABLE RUMBLE STRIPS	24	EACH
<b>STRUCTURES OVER 20' SPAN</b>			
636	BRIDGE CONSTRUCTION CONTROL	1.00	LUMP SUM
804	REINFORCING STEEL-BRIDGE (GRADE 60)	240	POUND
SP	BRIDGE DECK REPAIR	960	SQ. FT.
SP	POLYMER OVERLAY	2106	SQ. YD.

\* DENOTES ALTERNATE BID ITEMS

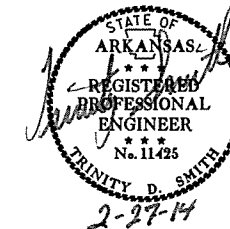
REVISIONS

DATE	REVISION	SHEET NUMBER

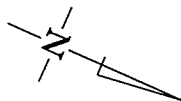
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 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
				6	ARK.				
JOB NO.							BB0110	14	38

2 PLAN SHEETS



STA. 620+00.00  
 BEGIN JOB BB0110  
 LOG MILE 15.10



615

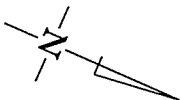
620

625

630

N 23°53'06" W

EXIST. R/W



EXIST. R/W

630

635

640

645

N 23°53'06" W

EXIST. R/W

STA. 643+71 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 4'-8" X 4'-0"  
 RETAIN

PLAN SHEETS

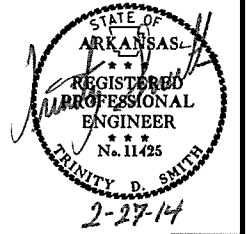
2/12/2014

RB0110.DGN

STA. 654+58 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		15	38
				JOB NO.	BB0110			

② PLAN SHEETS



EXIST. R/W

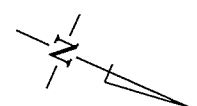
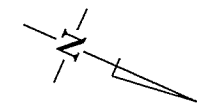
EXIST. R/W

EXIST. R/W

EXIST. R/W

STA. 669+04 - IN PLACE  
 TYPE RM MODIFIED DROP INLET  
 IN MEDIAN  
 4'-0" X 4'-0"  
 RETAIN

PLAN SHEETS



645

650

655

660

N 23°53'06" W

660

665

670

675

N 23°53'06" W

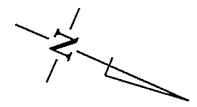
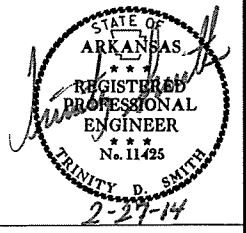
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2/12/2014

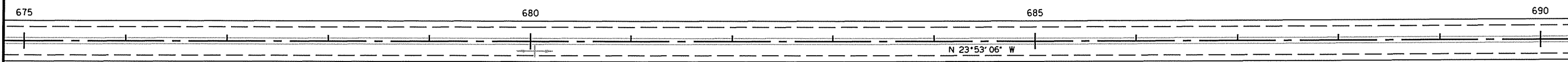
RB0110.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
				6	ARK.				
JOB NO.							BB0110	16	38

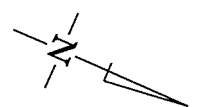
2 PLAN SHEETS



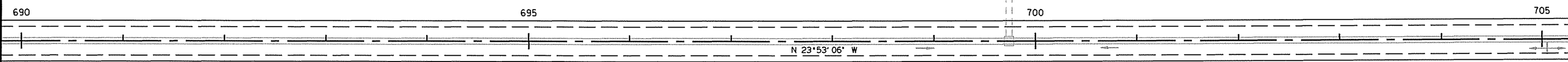
EXIST. R/W



EXIST. R/W



EXIST. R/W



EXIST. R/W

STA. 699+74 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

PLAN SHEETS

2/12/2014

RB00110.DGN

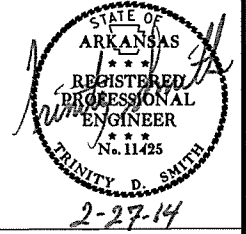


STA. 707+19 - IN PLACE  
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 4'-8" X 4'-0"  
 RETAIN

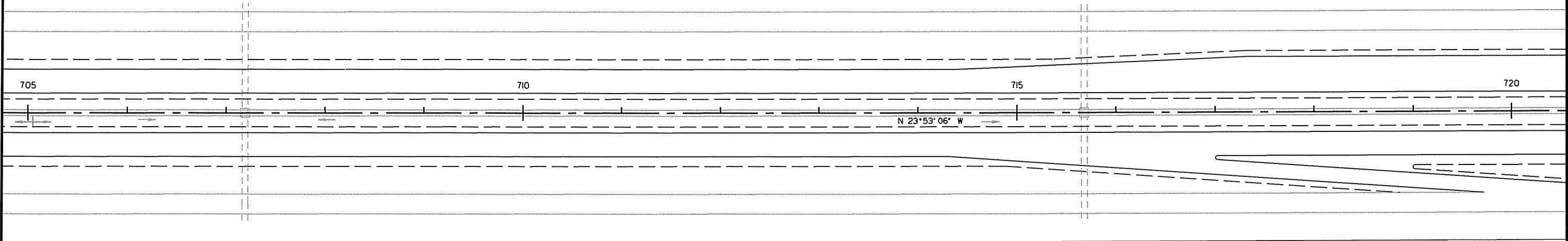
STA. 715+68 - IN PLACE  
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 4'-8" X 4'-0"  
 RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0110							17	38

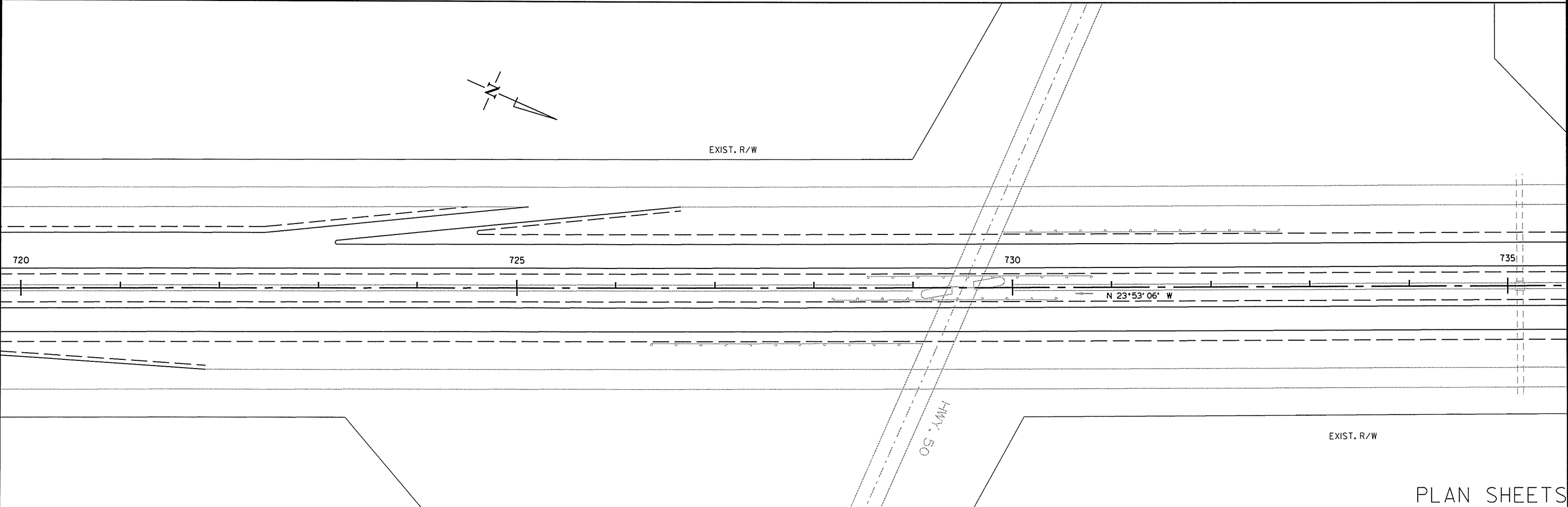
② PLAN SHEETS



EXIST. R/W



EXIST. R/W



EXIST. R/W

PLAN SHEETS

2/12/2014

RB0110.DGN

STA. 735+12 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 5'-6" X 4'-0"  
 RETAIN

STA. 744+04 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 3'-0"  
 RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0110							18	38

② PLAN SHEETS



EXIST. R/W

EXIST. R/W

EXIST. R/W

EXIST. R/W

STA. 754+04 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

PLAN SHEETS

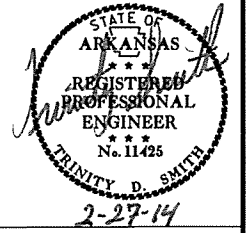
2/12/2014

RB0110.DGN

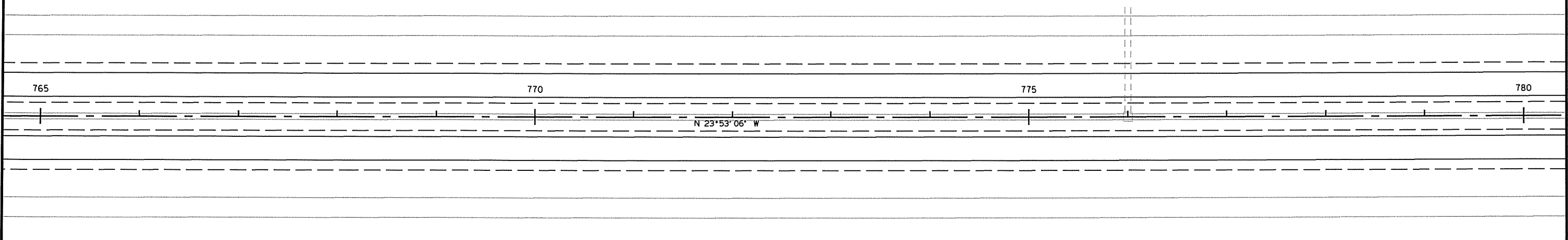
STA. 776+02 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS	
				6	ARK.				
JOB NO.							BB0110	19	38

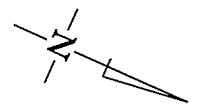
② PLAN SHEETS



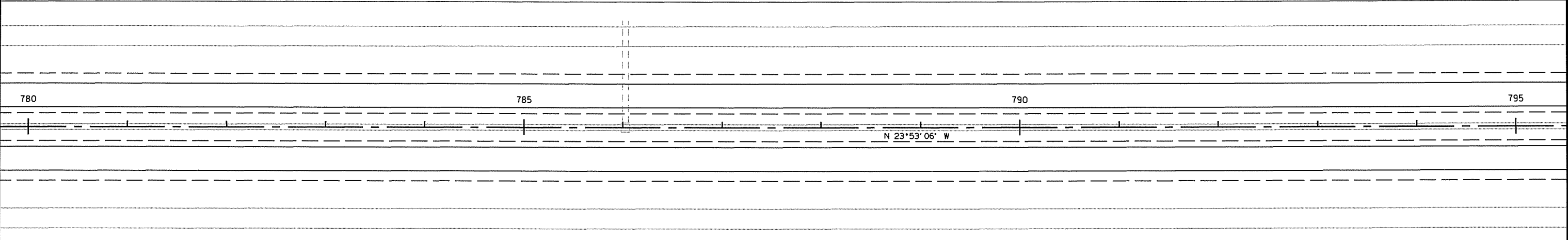
EXIST. R/W



EXIST. R/W



EXIST. R/W



EXIST. R/W

STA. 786+07 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

PLAN SHEETS

2/12/2014

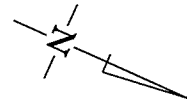
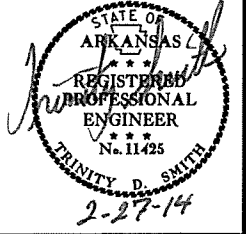
RB0110.DGN

STA. 800+04 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

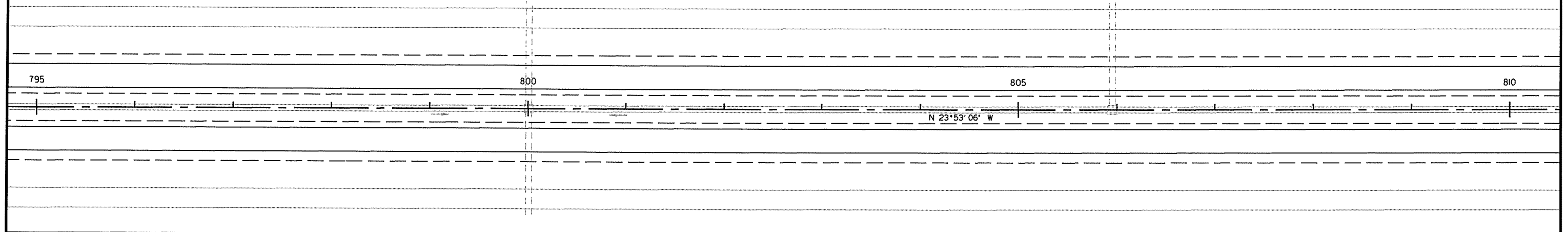
STA. 805+93 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0110							20	38

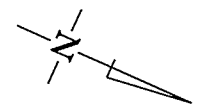
2 PLAN SHEETS



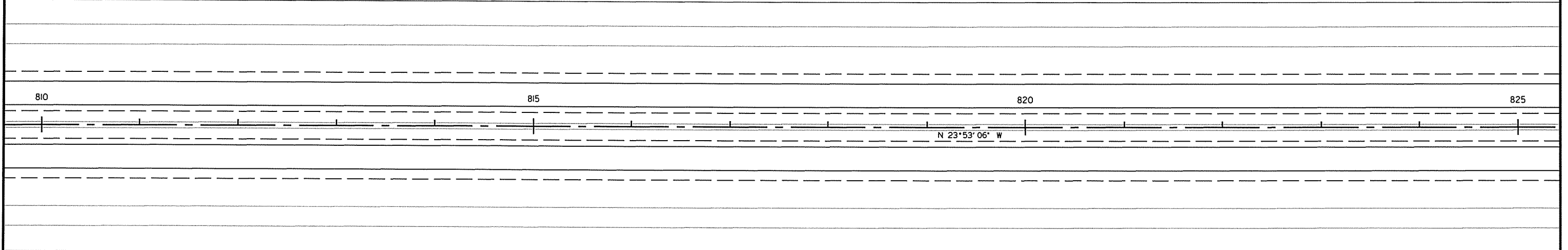
EXIST. R/W



EXIST. R/W



EXIST. R/W



EXIST. R/W

2/12/2014

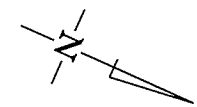
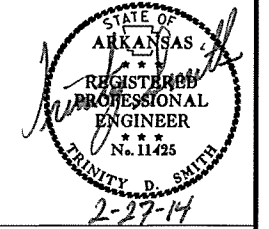
RB0110.DGN

PLAN SHEETS

STA. 829+70 - IN PLACE  
TYPE RM MODIFIED DROP INLET IN MEDIAN  
4'-8" X 4'-0"  
RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO. BB0110	21	38

② PLAN SHEETS



EXIST. R/W

825

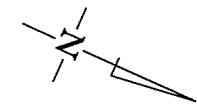
830

835

840

N 23°53'06" W

EXIST. R/W



EXIST. R/W

840

845

850

855

N 23°53'06" W

EXIST. R/W

STA. 852+28 - IN PLACE  
TYPE RM MODIFIED DROP INLET IN MEDIAN  
5'-6" X 4'-0"  
RETAIN

PLAN SHEETS

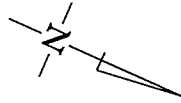
2/12/2014

RB0110.DGN

STA. 862+93 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. AID DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0110							22	38

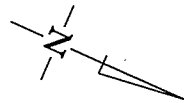
② PLAN SHEETS



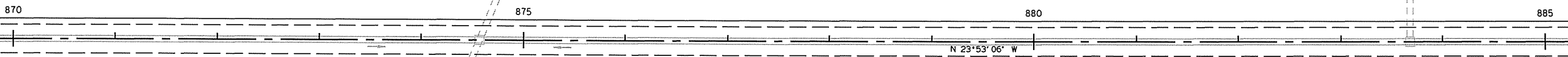
EXIST. R/W



EXIST. R/W



EXIST. R/W



EXIST. R/W

STA. 874+56 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 6'-8" X 4'-0"  
 RETAIN

STA. 883+69 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

PLAN SHEETS

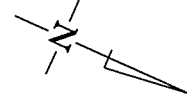
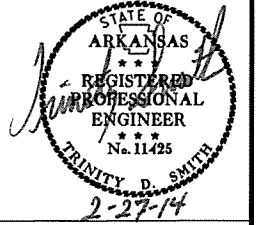
2/12/2014

RB0110.DGN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0110							23	38

STA. 897+08 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 4'-8" X 4'-0"  
 RETAIN

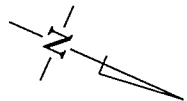
② PLAN SHEETS



EXIST. R/W

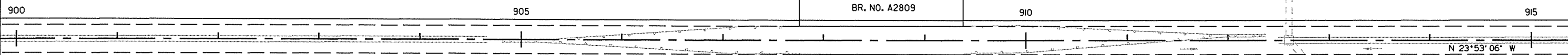


EXIST. R/W



STA. 907+76 BR. END  
 EXISTING 162' BRIDGE A2809  
 39'-0" CLEAR ROADWAY  
 STA. 909+38 BR. END  
 REMOVE ACHM OVERLAY  
 PLACE POLYMER OVERLAY

EXIST. R/W



EXIST. R/W

EXIST. R/W

STA. 907+76 BR. END  
 EXISTING 162' BRIDGE O2809  
 39'-0" CLEAR ROADWAY  
 STA. 909+38 BR. END  
 REMOVE ACHM OVERLAY  
 PLACE POLYMER OVERLAY

STA. 912+60 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 6'-8" X 4'-0"  
 RETAIN

PLAN SHEETS

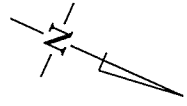
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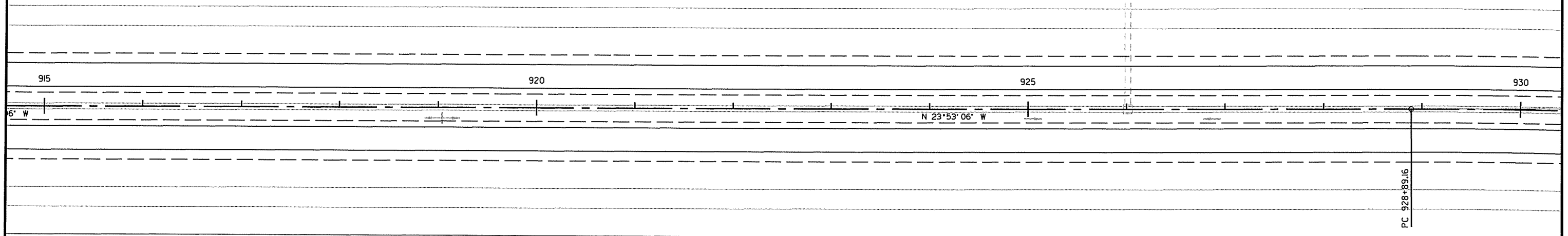
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. AID DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0110							24	38

2 PLAN SHEETS

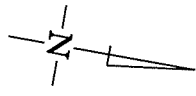
STA. 926+02 - IN PLACE  
TYPE RM MODIFIED DROP INLET IN MEDIAN  
3'-0" X 4'-0"  
RETAIN



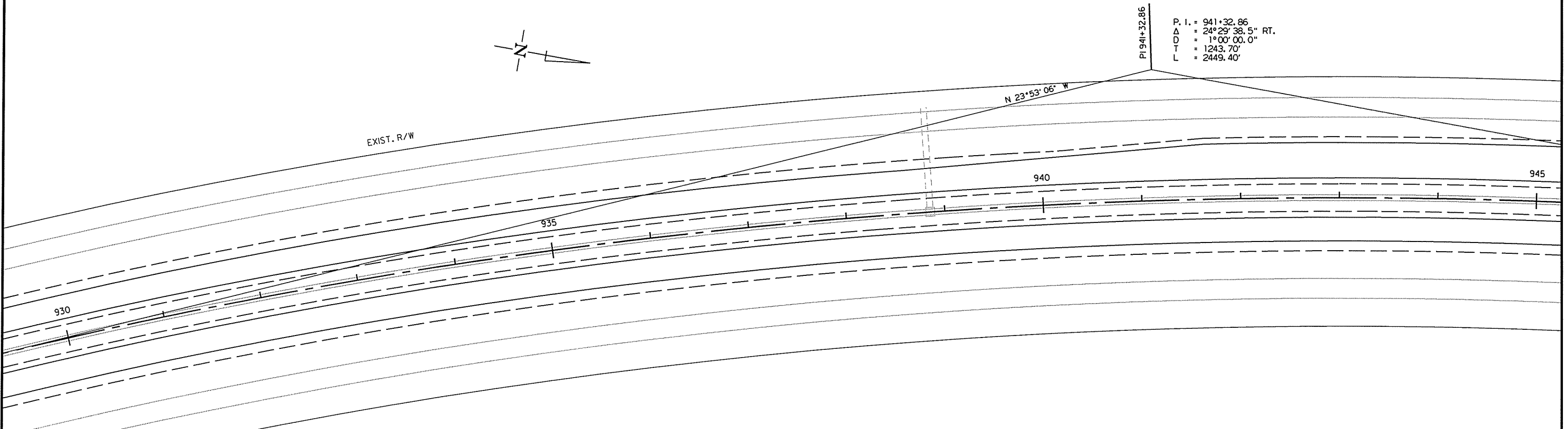
EXIST. R/W



EXIST. R/W



PI 941+32.86  
P. I. = 941+32.86  
Δ = 24°23'38.5" RT.  
D = 1°00'00.0"  
T = 1243.70'  
L = 2449.40'



EXIST. R/W

STA. 938+85 - IN PLACE  
TYPE RM MODIFIED DROP INLET IN MEDIAN  
3'-0" X 4'-0"  
RETAIN

PLAN SHEETS

2/12/2014

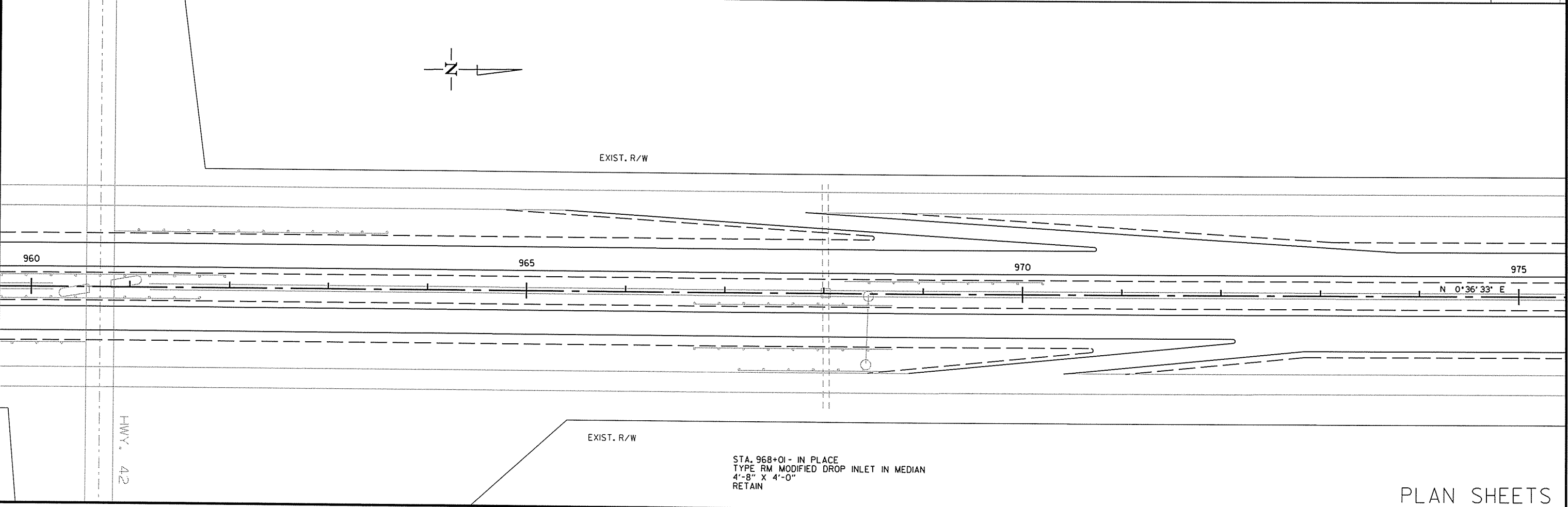
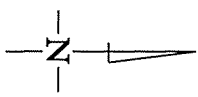
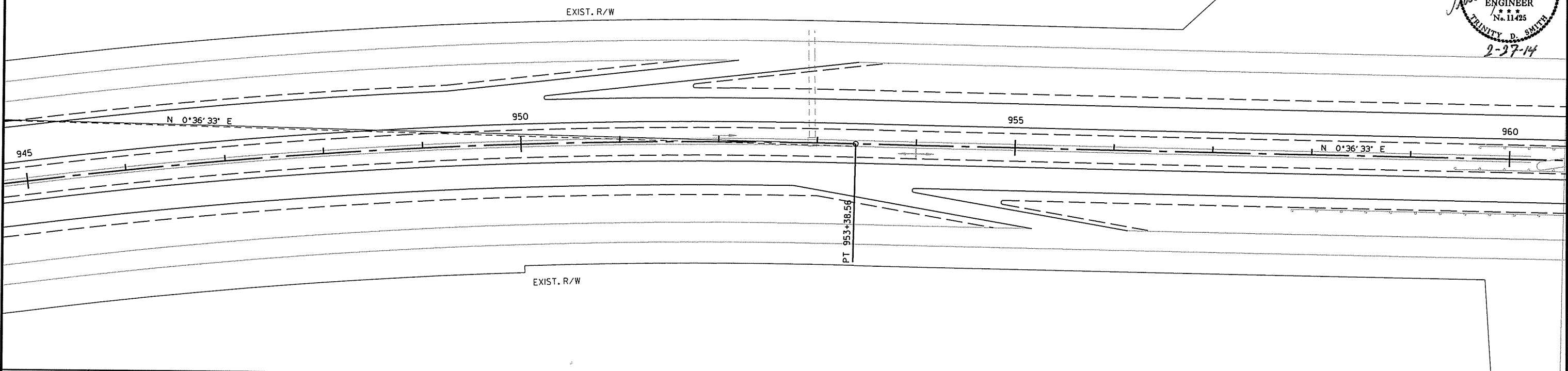
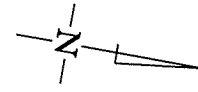
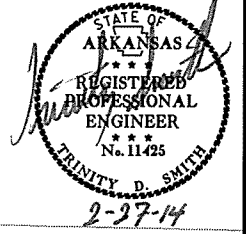
RB0110.DGN



STA. 952+92 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. AID DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
JOB NO. BB0110							25	38

2 PLAN SHEETS



STA. 968+01 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 4'-8" X 4'-0"  
 RETAIN

PLAN SHEETS

2/12/2014

RB0110.DGN

HWY. 42

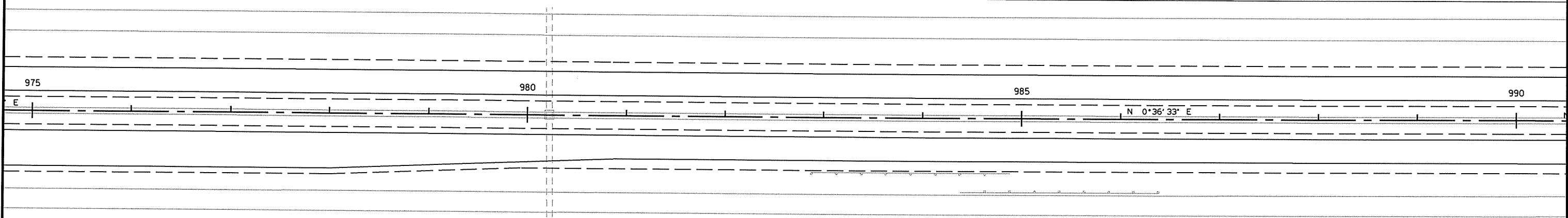
STA. 980+23 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 4'-0" X 4'-0"  
 RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
							JOB NO. BB0110	26 38

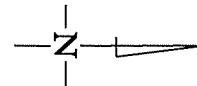
② PLAN SHEETS



EXIST. R/W

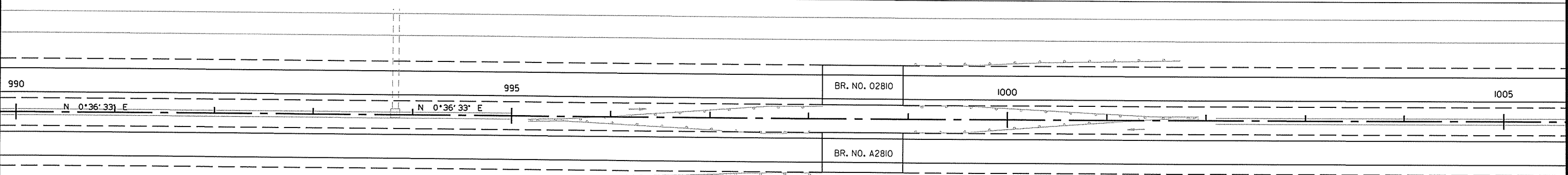


EXIST. R/W



EXIST. R/W

STA. 998+14 BR. END  
 EXISTING 8' BRIDGE 02810  
 39'-0" CLEAR ROADWAY  
 STA. 998+95 BR. END  
 REMOVE ACHM OVERLAY  
 PLACE POLYMER OVERLAY



EXIST. R/W

STA. 993+83 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-0" X 4'-0"  
 RETAIN

STA. 998+14 BR. END  
 EXISTING 8' BRIDGE A2810  
 39'-0" CLEAR ROADWAY  
 STA. 998+95 BR. END  
 REMOVE ACHM OVERLAY  
 PLACE POLYMER OVERLAY

PLAN SHEETS

2/12/2014

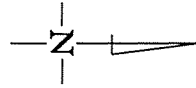
RB0110.DGN

STA. 1006+49 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 3'-6" X 4'-0"  
 RETAIN

DATE REVISED	DATE FILED	DATE REVISED	DATE FILED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
							JOB NO. BB0110	27 38

② PLAN SHEETS

STA. 1017+24 - IN PLACE  
 TYPE RM MODIFIED DROP INLET IN MEDIAN  
 4'-0" X 4'-0"  
 RETAIN



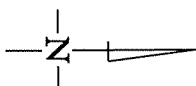
EXIST. R/W

1005 1010 1015 1020

N 0°36'33" E

EXIST. R/W

EXIST. R/W



1020 1025 1030 1035

N 0°36'33" E

N 0°36'33" E

PC 1028+22.37

2/12/2014

RBB0110.DGN

EXIST. R/W

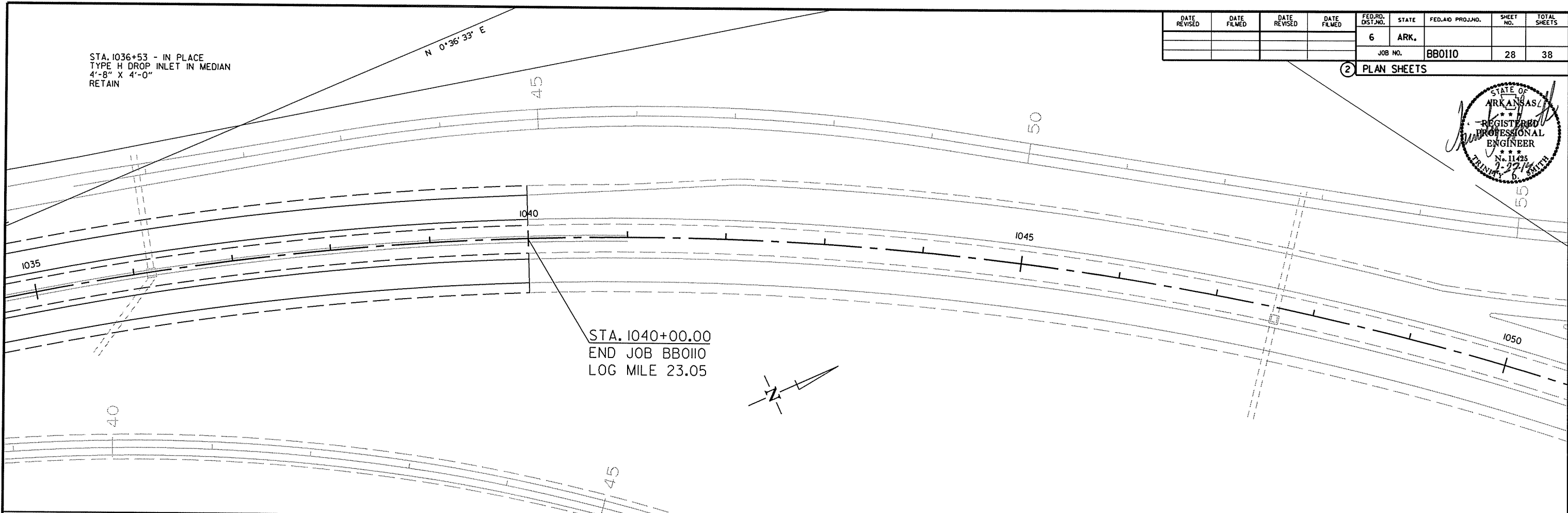
PLAN SHEETS

STA. 1036+53 - IN PLACE  
 TYPE H DROP INLET IN MEDIAN  
 4'-8" X 4'-0"  
 RETAIN

N 0°36'33" E

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						JOB NO.	BB0110	28

② PLAN SHEETS



2/12/2014

RB0110.DGN

PLAN SHEETS

DATE REVISION	DATE FILED	DATE REVISED	DATE FILED	FED. PROJ. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
							JOB NO.	
							BB0110	29
							02809, A2809 LAYOUT	5512

Note: For Riv Data, Guard Rail and Approach Slabs & Gutters - see Roadway Plans

GENERAL NOTES

BENCH MARK: "O" CUT ON DROP INLET 2' LT STA. 912+62. ELEV. 219.91.

ALL PILING SHALL BE 16" OCTAGONAL PRECAST CONCRETE AND SHALL BE DRIVEN WITH AN APPROVED AIR, STEAM, OR DIESEL HAMMER TO A MINIMUM BEARING CAPACITY OF 44 TONS PER PILE AND TO A MINIMUM PENETRATION OF 20" BELOW THE GROUND LINE. LOADINGS OF PILING SHOWN ARE BASED ON PILING DRIVEN IN THE EXISTING BRIDGE NO. 2809A.

ALL CONCRETE IN THE SUPERSTRUCTURE SHALL BE CLASS S(4). ALL CONCRETE IN THE SUBSTRUCTURE SHALL BE CLASS S AND SHALL BE POURED IN THE DRY. ALL EXPOSED CORNERS SHALL BE CHAMFERED 3/4" UNLESS OTHERWISE NOTED.

THE CONTRACTOR SHALL MAKE CHECK MEASUREMENTS OF THE EXISTING BRIDGE AND MAKE ADJUSTMENTS NECESSARY TO FIT THE NEW WORK TO THE EXISTING STRUCTURE.

PLANS OF THE EXISTING STRUCTURE WILL BE MADE AVAILABLE TO THE CONTRACTOR UPON REQUEST.

FOR PLANS OF EXISTING STRUCTURE, SEE DWG. NOS. 5385, 5386, 8017 & 8870.

FOR DETAILS OF WIDENING BENTS, SEE DWG. NO. 19219 & 19220.

FOR DETAILS OF WIDENING SPANS, SEE DWG. NOS. 19221 & 19222.

SPECIFICATIONS: ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 1972 AND APPLICABLE SPECIAL PROVISIONS.

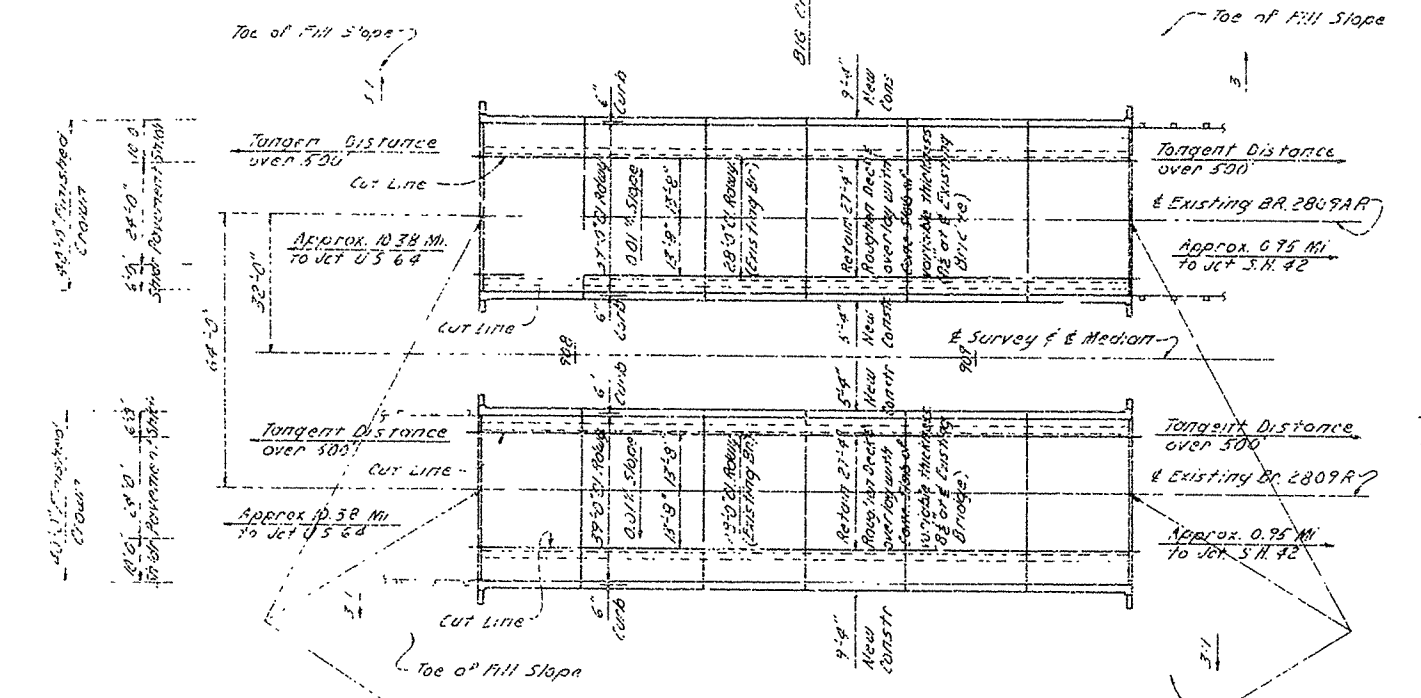
DESIGN SPECIFICATIONS: AASHTO 1973.

LIVE LOADING: HS 20 AND SPECIAL INTERSTATE LOADING.

UNIT STRESSES:

SUPERSTRUCTURE: CLASS S(4) CONCRETE (f <sub>c</sub> =10)	1,400 PSI
REINFORCING STEEL (A615, GRADE 60)	24,000 PSI
SUBSTRUCTURE: CLASS S CONCRETE (f <sub>c</sub> =10)	1,200 PSI
REINFORCING STEEL (A615, GRADE 40)	20,000 PSI

THE EXISTING PLATE GUARD RAIL IS TO BE SALVAGED AND SHALL REMAIN THE PROPERTY OF THE STATE.

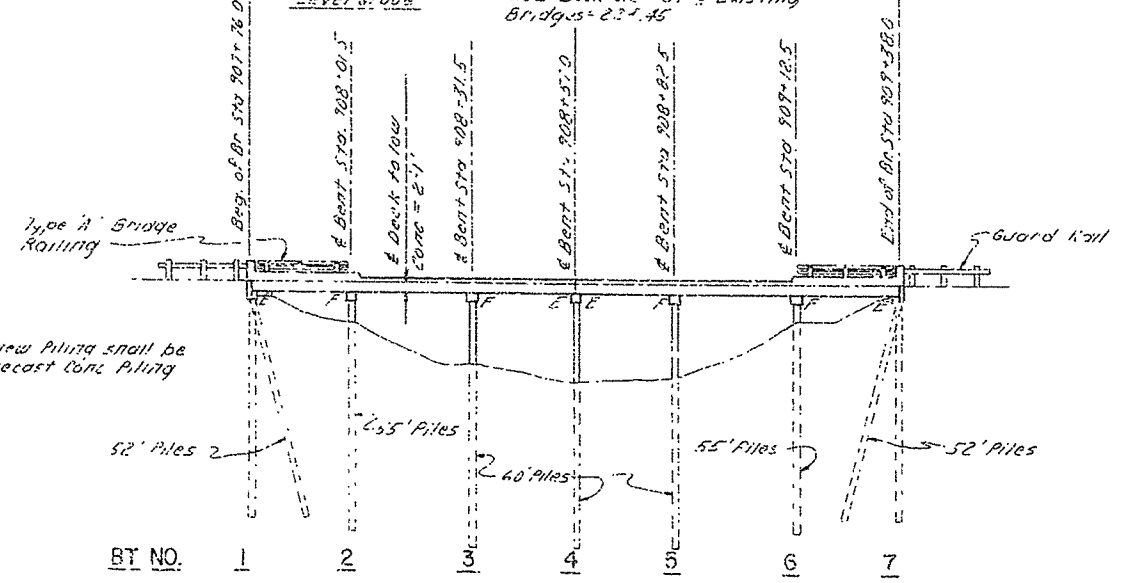


PLAN

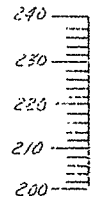
Total Length of Bridge = 162'-0"

91'-0" Continuous R.C. Slab Unit (Spans: 25'-6", 30'-0" & 25'-6")

New Deck E. at Existing Bridges = 2.2' - 4.5'



ELEVATION



FOR INFORMATION ONLY

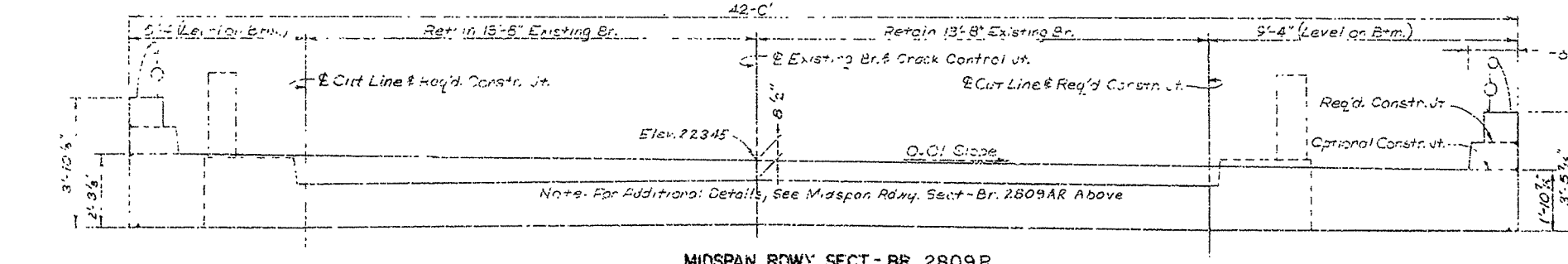
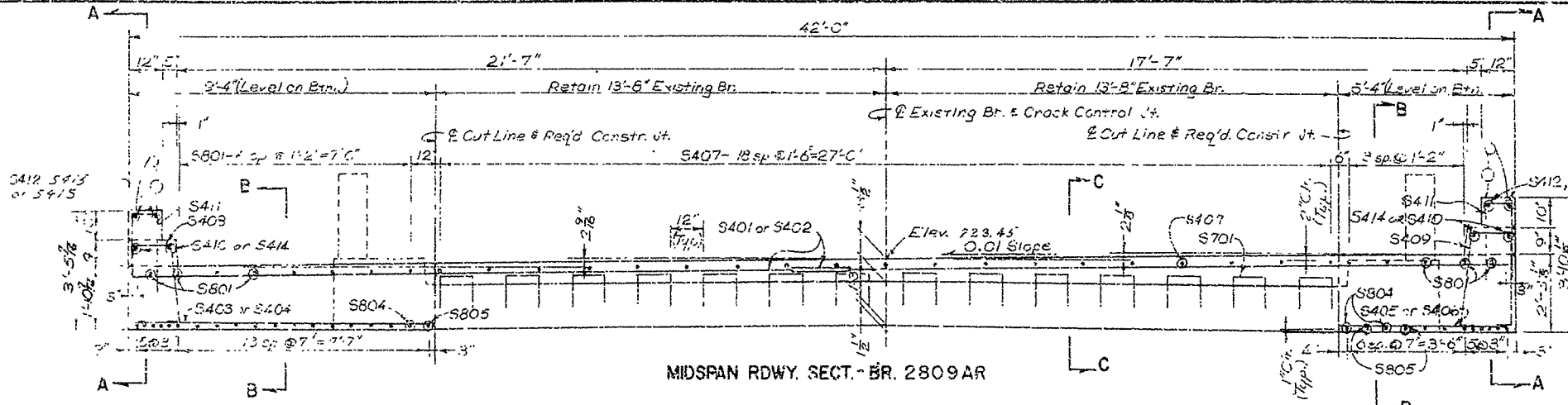
LAYOUT OF BRIDGES OVER  
BIG CREEK  
CLARKEDALE-MISS. CO. LINE RECONST.  
CRITTENDEN COUNTY  
ROUTE 55 SEC. 11  
ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.  
DRAWN BY: J.P.S. DATE: 2-14-75  
CHECKED BY: J.C.K. DATE: 2-24-75  
DESIGNED BY: J.C.K. DATE: 2-7-75  
SCALE: 1" = 20'-0"

*Paul Pinkerton*  
BRIDGE ENGINEER

BRIDGE NO. 02809, A2809 DRAWING NO. 55512

DATE REVISION	DATE FILLED	DATE REVISION	DATE FILLED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	BB0110	30	32	
				① 02809, A2809 SPAN DETAILS		55513		



**GENERAL NOTES**

STRIP, CLEAN AND RETAIN EXISTING TOP AND BOTTOM TRANSVERSE REINFORCING EXTENDING FROM THE CUT LINES INTO THE NEW SLAB A MINIMUM OF 2'-0".

ALL NEW CONCRETE IN THE SUPERSTRUCTURE WILL BE CLASS S401. ALL EXPOSED CORNERS WILL BE CHAMFERED 1/4" UNLESS OTHERWISE NOTED.

NEW REINFORCING STEEL SHALL BE ASTM A 615, GRADE 60. SHOP LISTS AND BENDING DIAGRAMS MUST BE SUBMITTED AND APPROVAL SECURED BEFORE FABRICATION IS BEGUN.

WIRE SUPPORTS FOR REINFORCING BARS WILL NOT BE PAID FOR DIRECTLY, BUT WILL BE CONSIDERED SURSARARY TO THE ITEM REINFORCING STEEL. SHOP LISTS AND DIAGRAMS OF WIRE SUPPORTS MUST BE SUBMITTED FOR APPROVAL BEFORE FABRICATION IS BEGUN.

BEAM BOLSTERS AND HIGH CHAIRS SHALL HAVE A MAXIMUM LONGITUDINAL AND TRANSVERSE SPACING OF 4'-0".

DEAD LOAD DEFLECTION FOR THE THREE SPAN UNIT IS NEGLIGIBLE.

FOR ADDITIONAL GENERAL NOTES, SEE BRIDGE LAYOUT DRAWING NO. 19218 FOR DETAILS OF TYPE A BRIDGE RAILING, SEE DWG. NO. 14892A.

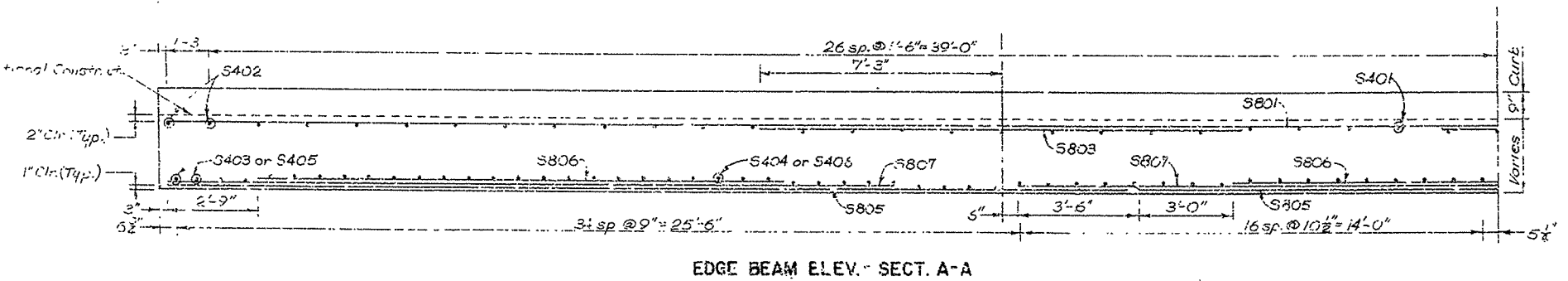
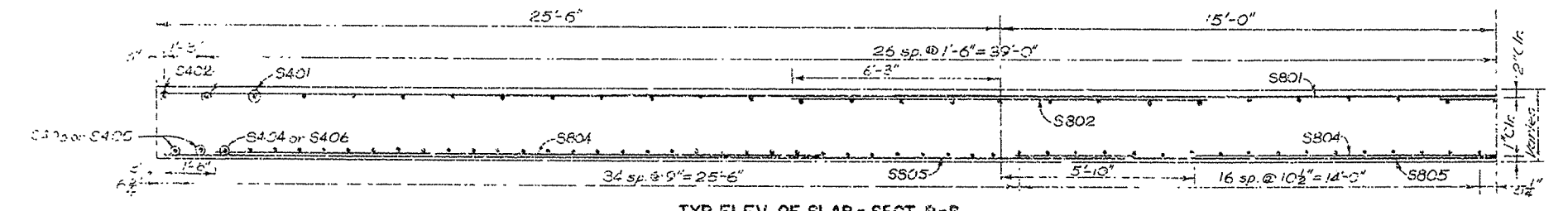
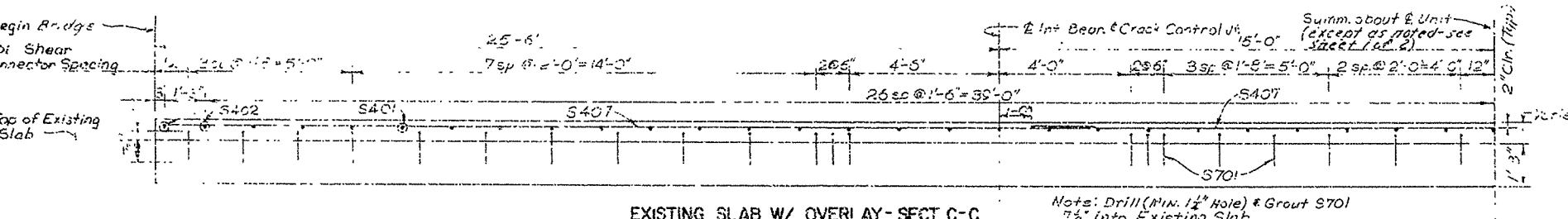
SPECIFICATIONS: ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 1972, AND APPLICABLE SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO 1973

LIVE LOAD: 0.1018 WHEELS/FT. OF WIDTH PLUS 50% IMPACT.

HS20

UNIT STRESSES: CLASS S401 CONCRETE (240) 1,400 PSI  
REINFORCING STEEL (A615, GRADE 60) 24,000 PSI



**FOR INFORMATION ONLY**

SHEET 2 OF 2

DETAILS OF 8'-0" CONTINUOUS R.C. SLAB UNIT BRIDGE OVER BIG CREEK CLARKEDALE- MISS. CO. LINE RECONST. CRITTENDEN COUNTY ROUTE 1-55 SEC. 11 ARKANSAS STATE HIGHWAY COMMISSION LITTLE ROCK, ARK.

SCALE: 1/4" = 1'-0" or as noted

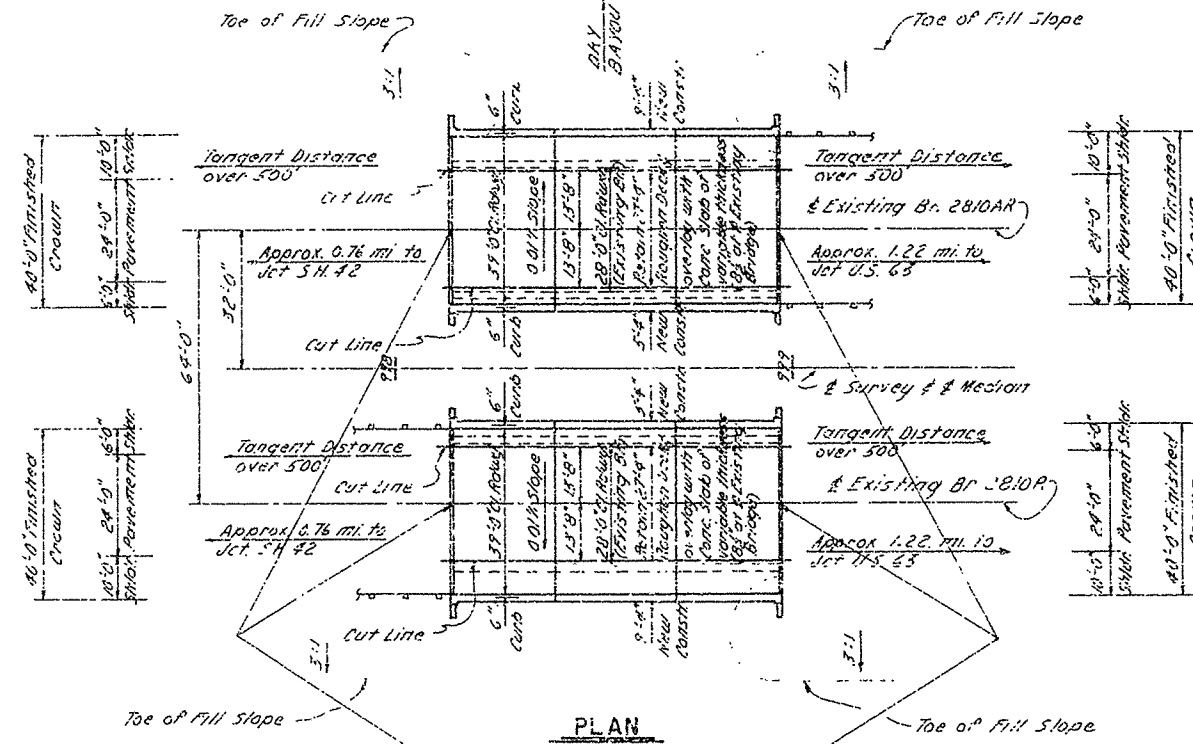
BRIDGE NO. 02809, A2809 DRAWING NO. 55513

*Res. D. Pemberton*  
BRIDGE ENGINEER

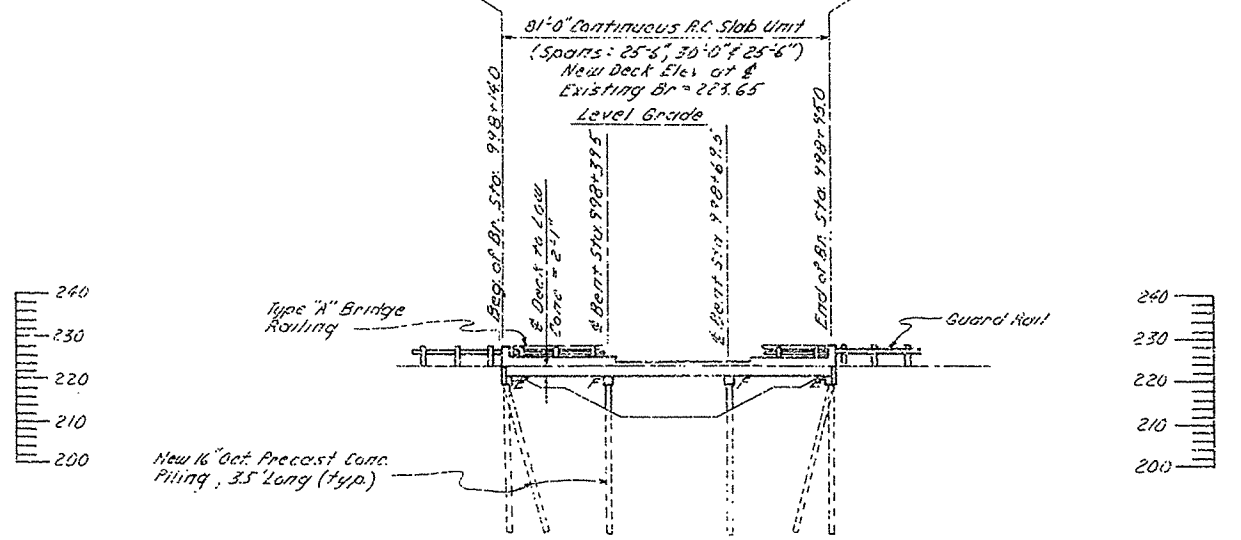
DATE REVISION	DATE REVISION	DATE REVISION	DATE REVISION	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	ROUTE NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.				
						BB0110	31	38	
								02810, A2810 LAYOUT	55514



Note: For R/W Data, Guard Rail and Approach Slabs & Gutters - see Rekey Plans



PLAN



ELEVATION

BT. NO. 1 2 3 4

GENERAL NOTES

BENCH MARK: "C" CUT ON SOUTH END OF BRIDGE END 13' LT. STA. 998+44, ELEV. 225.23

ALL PILING SHALL BE 16" OCTAGONAL PRECAST CONCRETE AND SHALL BE DRIVEN WITH AN APPROVED AIR, STEAM, OR DIESEL HAMMER TO A MINIMUM BEARING CAPACITY OF 44 TONS PER PILE AND TO A MINIMUM PENETRATION OF 20' BELOW THE GROUND LINE. LENGTHS OF PILING SHOWN ARE BASED ON PILING DRIVEN IN THE EXISTING BRIDGE NO. 2810A.

ALL CONCRETE IN THE SUPERSTRUCTURE SHALL BE CLASS S(AE). ALL CONCRETE IN THE SUBSTRUCTURE SHALL BE CLASS S AND SHALL BE POURED IN THE DRY. ALL EXPOSED CORNERS SHALL BE CHAMFERED 3/4" UNLESS OTHERWISE NOTED.

THE CONTRACTOR SHALL MAKE CHECK MEASUREMENTS OF THE EXISTING BRIDGE AND MAKE ADJUSTMENTS NECESSARY TO FIT THE NEW WORK TO THE EXISTING STRUCTURE.

PLANS OF THE EXISTING STRUCTURE WILL BE MADE AVAILABLE TO THE CONTRACTOR UPON REQUEST.

FOR PLANS OF EXISTING STRUCTURE, SEE DWG. NO. 5365, 5336, 8013 & 8871

FOR DETAILS OF WIDENING BENTS, SEE DWG. NO. 19219 & 19220  
FOR DETAILS OF WIDENING SPANS, SEE DWG. NO. 19224 & 19225

SPECIFICATIONS: KANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 1972 AND APPLICABLE SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO 1973  
LIVE LOADING: HS20 AND SPECIAL INTERSTATE LOADING

UNIT STRESSES:  
SUPERSTRUCTURE: CLASS S(AE) CONCRETE (f<sub>c</sub>=10) 1,400 PSI  
REINFORCING STEEL (A615, GRADE 60) 24,000 PSI

SUBSTRUCTURE: CLASS S CONCRETE (f<sub>c</sub>=10) 1,200 PSI  
REINFORCING STEEL (A615, GRADE 40) 20,000 PSI

THE EXISTING PLATE GUARD RAIL IS TO BE SALVAGED AND SHALL REMAIN THE PROPERTY OF THE STATE.

FOR INFORMATION ONLY

LAYOUT OF BRIDGES OVER  
DRY BAYOU  
CLARKEDALE - MISS. CO. LINE RECONSTR.  
CRITTENDEN COUNTY  
ROUTE 55 SEC. 11  
ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.  
DRAWN BY: J.P.S. DATE: 2-18-75  
CHECKED BY: J.C.K. DATE: 2-26-75 SCALE: 1"=20'-0"  
DESIGNED BY: J.S.K. DATE: 2-7-75  
BRIDGE NO. 02810, A2810 DRAWING NO. 55514

*Aracel P. Johnston*  
BRIDGE ENGINEER

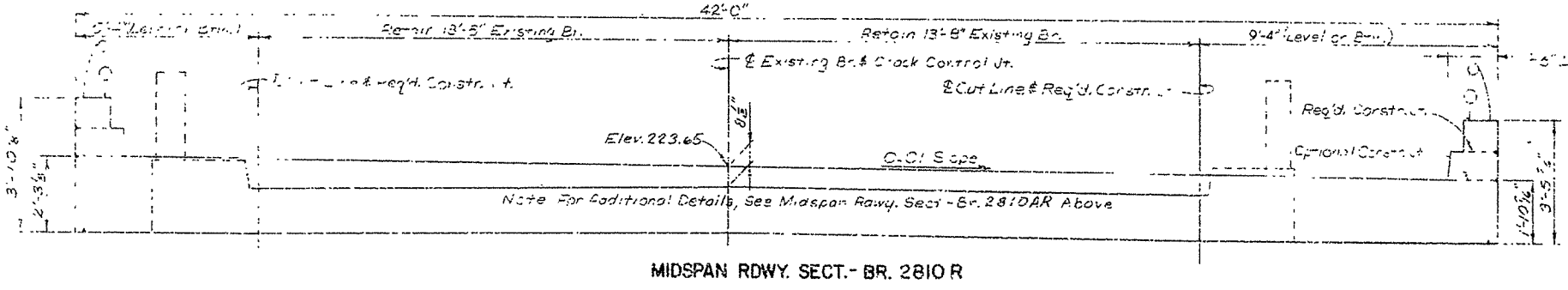
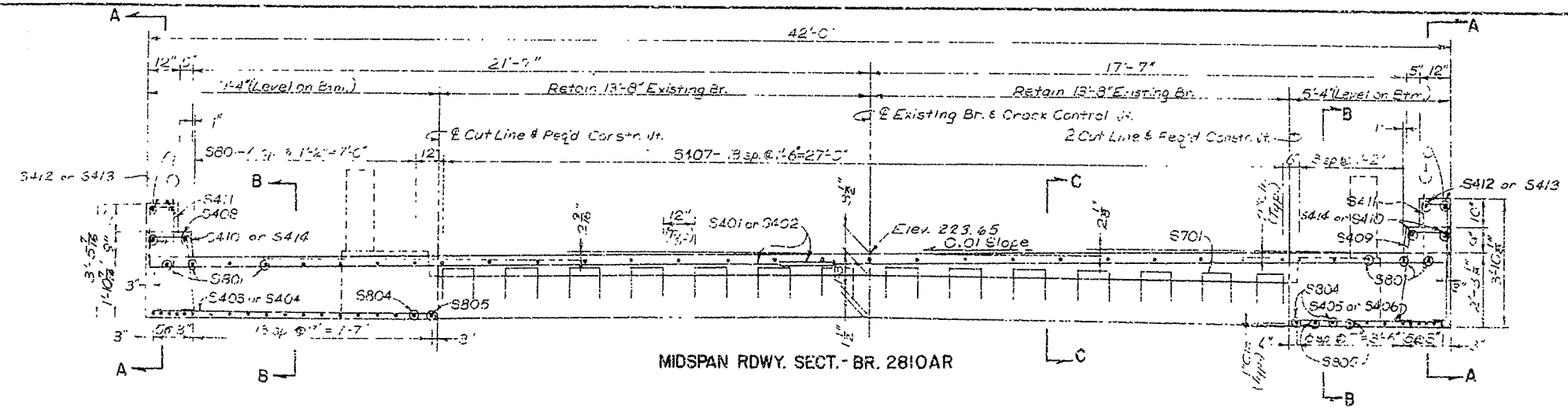
997

998

999

1000

DATE REVISED	DATE FILED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.		32	38
				JOB NO.	BB0110	
				① 02810, A2810 SPAN DETAILS	55515	



**GENERAL NOTES**

STRIP, CLEAN AND RETAIN EXISTING TOP AND BOTTOM TRANSVERSE REINFORCING EXTENDING FROM THE CUT LINES INTO THE NEW SLAB A MINIMUM OF 2'-0".

ALL NEW CONCRETE IN THE SUPERSTRUCTURE WILL BE CLASS S(A)E. ALL EXPOSED CORNERS WILL BE CHAMFERED 3/4" UNLESS OTHERWISE NOTED.

NEW REINFORCING STEEL SHALL BE ASTM A 615, GRADE 60. SHOP LISTS AND BENDING DIAGRAMS MUST BE SUBMITTED AND APPROVAL SECURED BEFORE FABRICATION IS BEGUN.

WIRE SUPPORTS FOR REINFORCING BARS WILL NOT BE PAID FOR DIRECTLY, BUT WILL BE CONSIDERED SUBSIDIARY TO THE IRON REINFORCING STEEL. SHOP LISTS AND DIAGRAMS OF WIRE SUPPORTS MUST BE SUBMITTED FOR APPROVAL BEFORE FABRICATION IS BEGUN.

BEAM BOLSTERS AND HIGH CHAIRS SHALL HAVE A MAXIMUM LONGITUDINAL AND TRANSVERSE SPACING OF 4'-0".

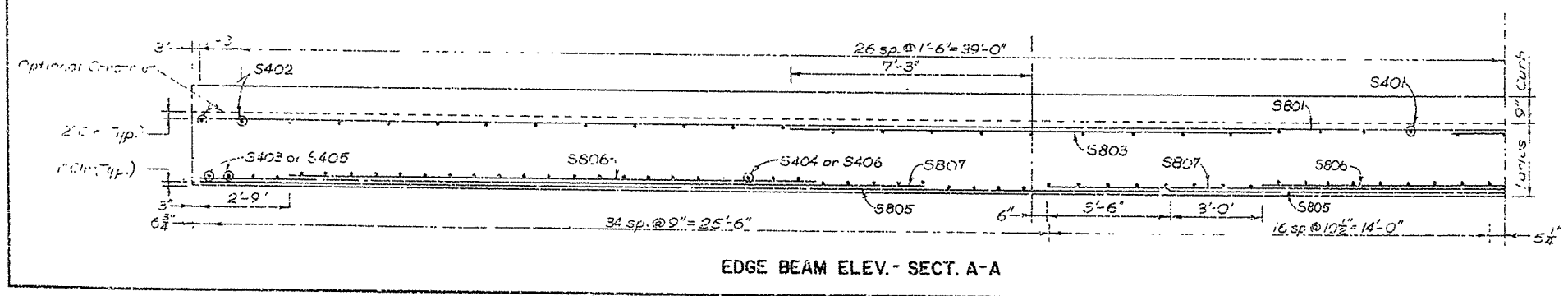
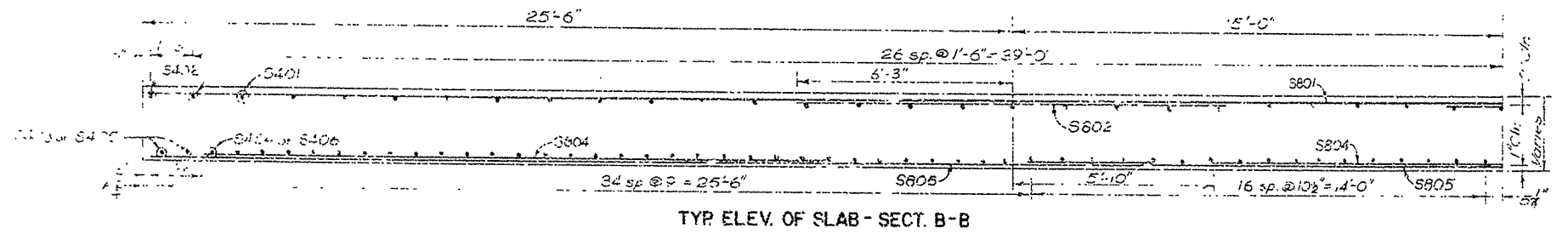
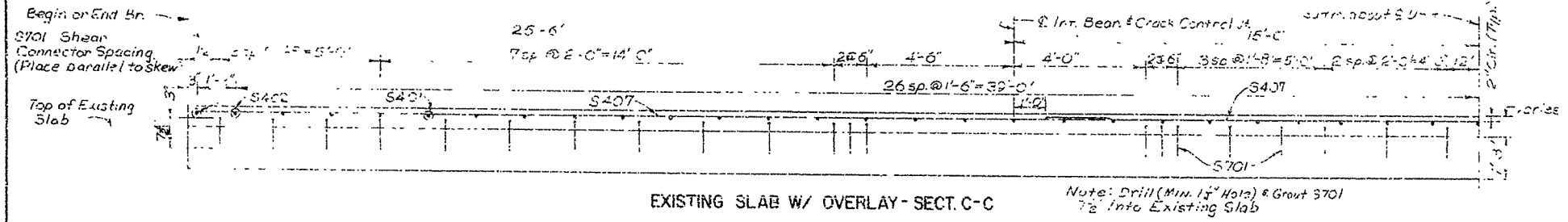
DEAD LOAD DEFLECTION FOR THE THREE SPAN UNIT IS NEGLIGIBLE.

FOR ADDITIONAL GENERAL NOTES, SEE BRIDGE LAYOUT DRAWING NO. 19223 FOR DETAILS OF TYPE A BRIDGE RAILING, SEE DWG. NO. 14992A.

SPECIFICATIONS: ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 1972, AND APPLICABLE SPECIAL PROVISIONS.

DESIGN SPECIFICATIONS: AASHTO 1973  
LIVE LOAD: 8,000 LBS WHEELS/FT. OF WIDTH PLUS 30% IMPACT.  
HS20

UNIT STRESSES: CLASS S(A)E CONCRETE (N40) 1,400 PSI  
REINFORCING STEEL (A615, GRADE 60) 24,000 PSI



**FOR INFORMATION ONLY**

SHEET 2 OF 2

DETAILS OF 81'-0" CONTINUOUS R C SLAB UNIT BRIDGE OVER DRY BAYOU CLARKEDALE-MISS. CO. LINE RECONST. CRITTENDEN COUNTY ROUTE 1-55 SEC. 11 ARKANSAS STATE HIGHWAY COMMISSION LITTLE ROCK, AR.

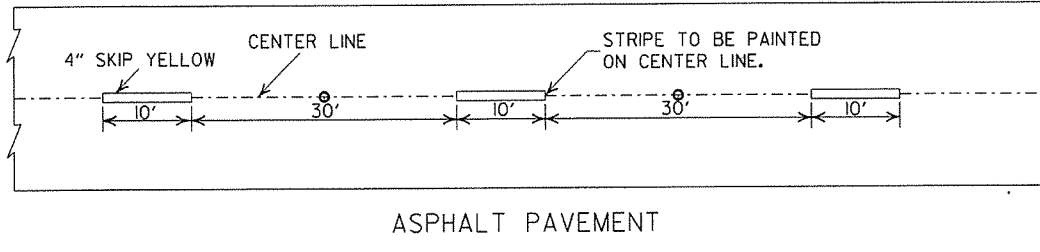
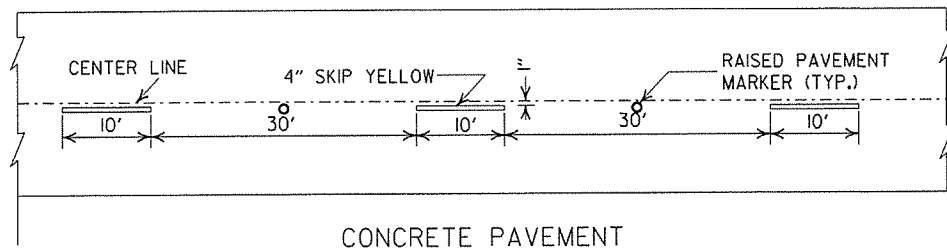
DRAWN BY: J.P.S. DATE: 2-21-75  
CHECKED BY: J.C.K. DATE: 2-26-75  
DESIGNED BY: J.C.K. DATE: 2-75

BRIDGE NO. 02810, A2810 DRAWING NO. 55515

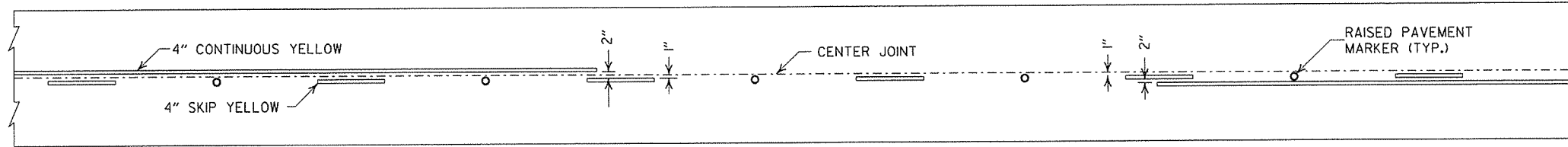


NOTES:

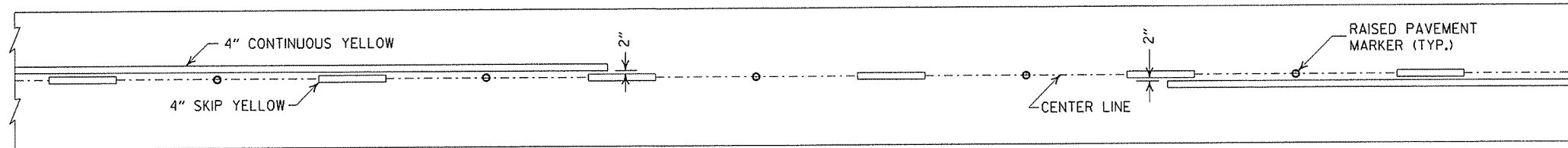
1. ALL LINES SHALL HAVE A WIDTH OF 4 INCHES.
2. THE THICKNESS AND RATE OF PAINT APPLICATION SHALL BE AS SPECIFIED IN SECTION 718 OF THE STANDARD SPECIFICATIONS.
3. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
4. RAISED PAVEMENT MARKERS SHALL BE CENTERED BETWEEN SKIP LINES ON 40 FEET SPACING UNLESS OTHERWISE SHOWN ON THE PLANS.



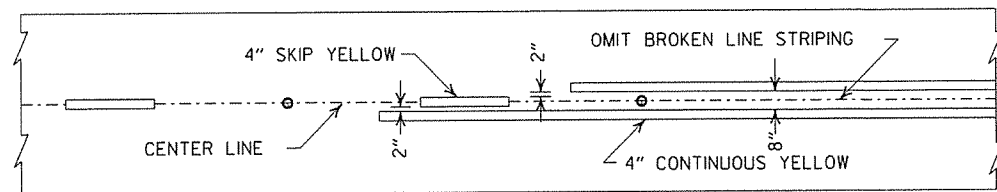
BROKEN LINE STRIPING



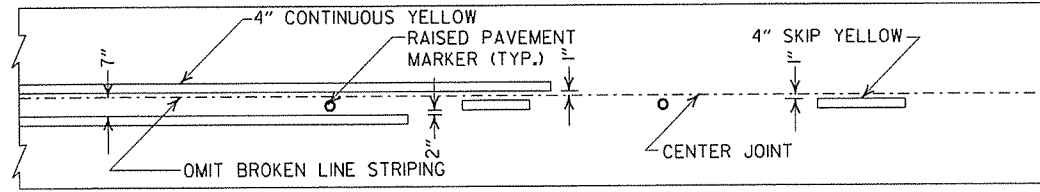
SOLID LINE STRIPING ON CONCRETE PAVEMENT



SOLID LINE STRIPING ON ASPHALT PAVEMENT

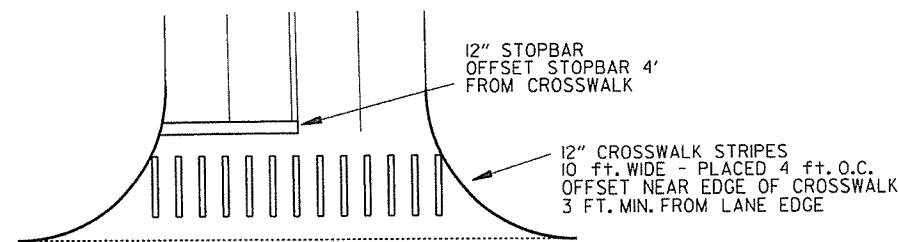


ASPHALT PAVEMENT



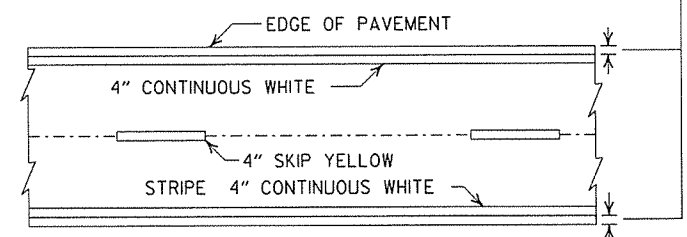
CONCRETE PAVEMENT

STRIPING AT ADJACENT NO PASSING LANES

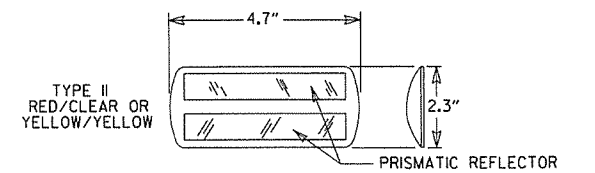


CROSSWALK AND STOPBAR DETAILS

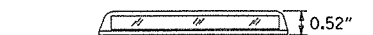
2" FOR ASPHALT OR CONCRETE PAVEMENT  
6" FOR BITUMINOUS SURFACE TREATMENT



PAVEMENT EDGE LINE MARKING



NOTE: THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

GENERAL NOTES:

THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND RAISED PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

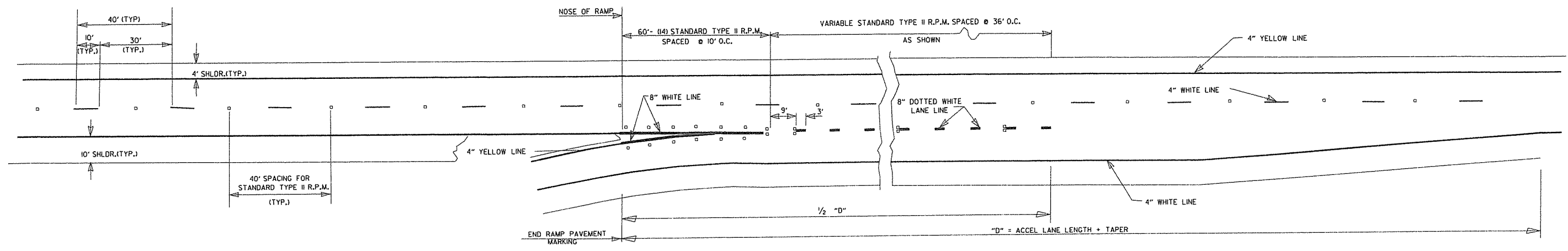
NOTE: DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.

DATE	REVISION	FILMED
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REVISED GENERAL NOTES & REMOVED FLOWABLE PVMT MRKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTLS.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80

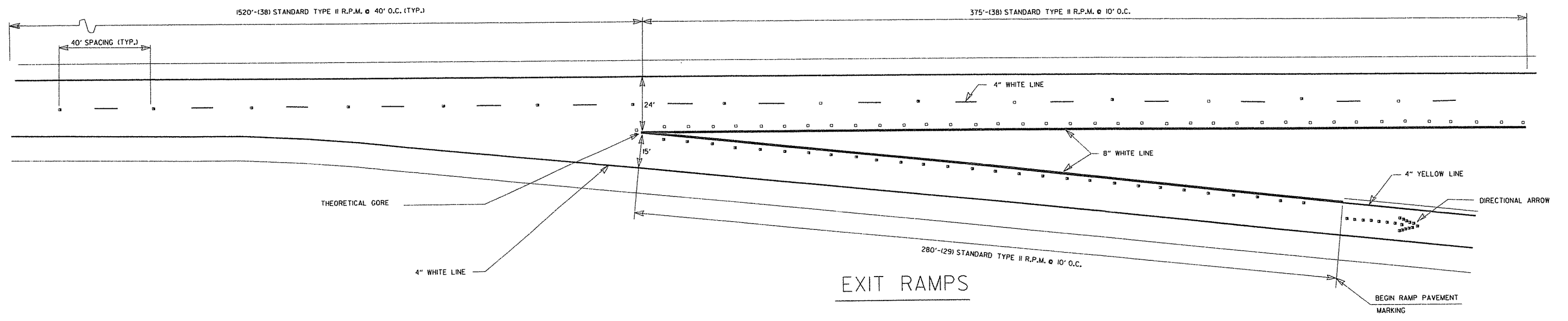
ARKANSAS STATE HIGHWAY COMMISSION	
PAVEMENT MARKING DETAILS	
STANDARD DRAWING PM-1	

ENTRANCE RAMP  
8" WHITE = 228 LIN. FT.  
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH

EXIT RAMP  
4" WHITE = 280 LIN. FT.  
8" WHITE = 655 LIN. FT.  
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH  
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 48 EACH  
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH



ENTRANCE RAMPS

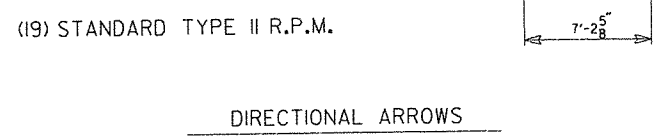
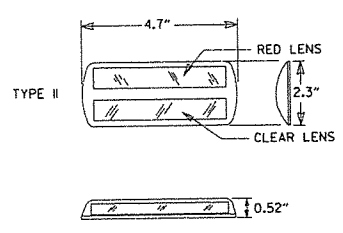


EXIT RAMPS

GENERAL NOTES:  
THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:  
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE AHTD QUALIFIED PRODUCTS LIST.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

DIRECTIONAL ARROWS

NOTE:  
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.

DATE	REVISION	FILMED
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
7-26-12	REVISED RPM NOTATION	
12-15-11	REVISED RPMs ACCORDING TO LATEST POLICY	
11-17-10	REMOVED PLOWABLE PAVEMENT MARKERS	
6-3-10	REVISED PER 2009 MUTCD	
11-18-04	REVISED NOTES	
8-22-02	ADDED & REVISED NOTES; REV. ENTRANCE & EXIT RAMPS	
5-18-00	REMOVED HASHMARKS	
7-02-98	CHANGED TYPES TO ROMAN NUMERALS	
4-26-96	ADDED DIMENSIONS & QUANTITIES; REVISED LANE WIDTH ON EXIT RAMP	
2-2-95	PLACED IN USE	2-2-95

ARKANSAS STATE HIGHWAY COMMISSION  
PERMANENT PAVEMENT MARKING  
ON ACCESS CONTROLLED ROADWAYS  
STANDARD DRAWING PM-2


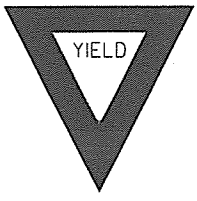
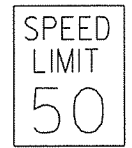




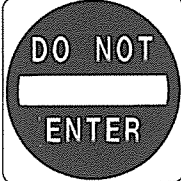

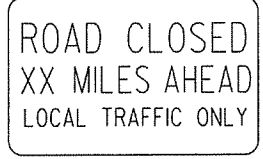
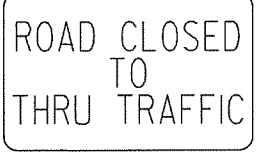
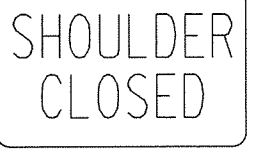
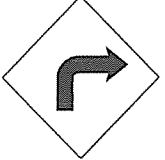
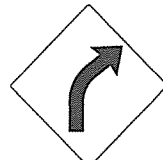
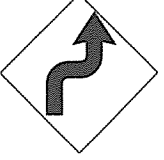

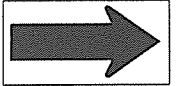
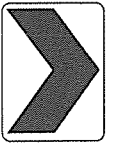
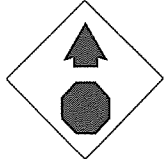
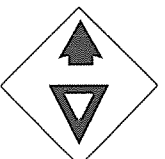
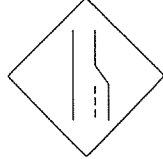



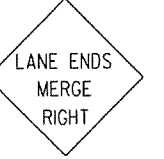


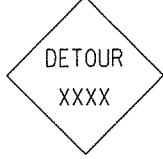





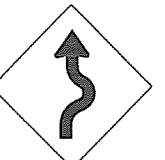


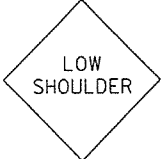
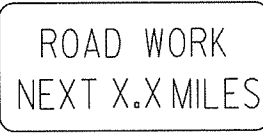
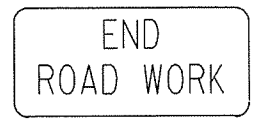
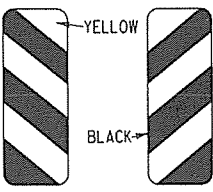
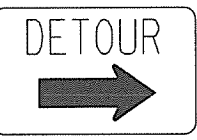


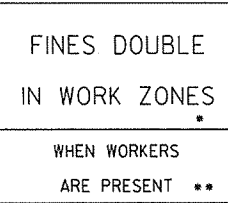
500 FT	1/2 MILE
1000 FT	3/4 MILE
1500 FT	1 MILE AHEAD

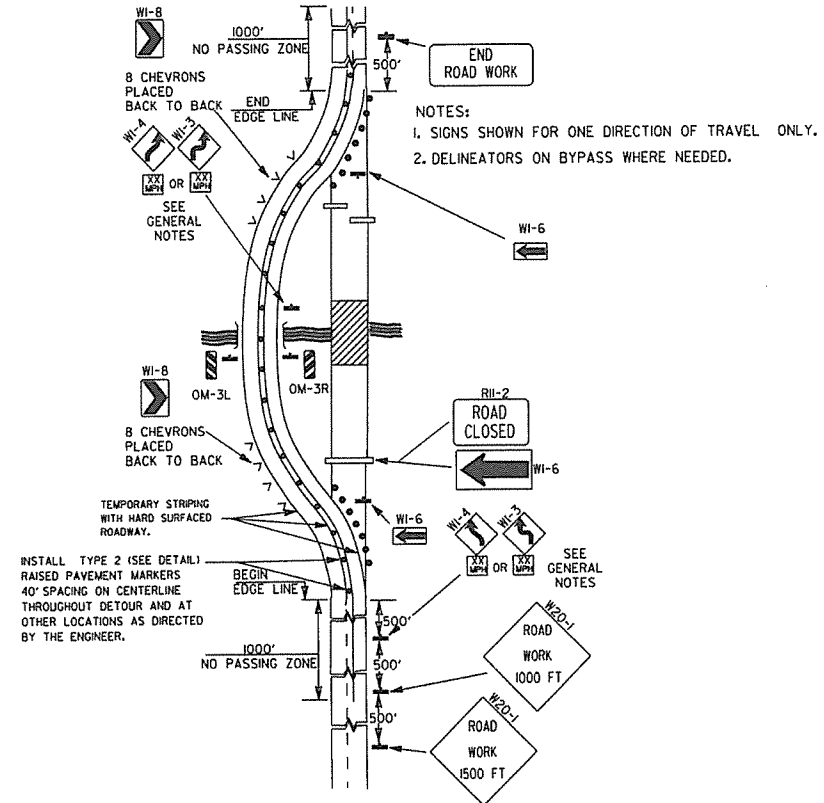
GENERAL NOTES:

- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SO. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN, WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

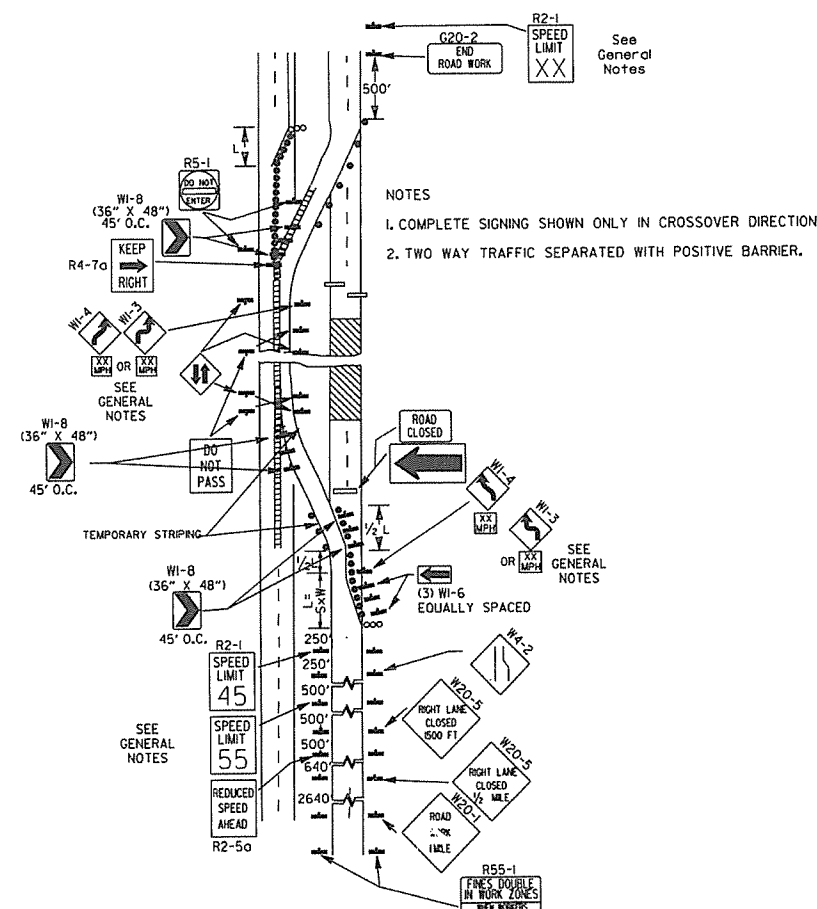
NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF NCHRP-350 OR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

12-15-11	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED

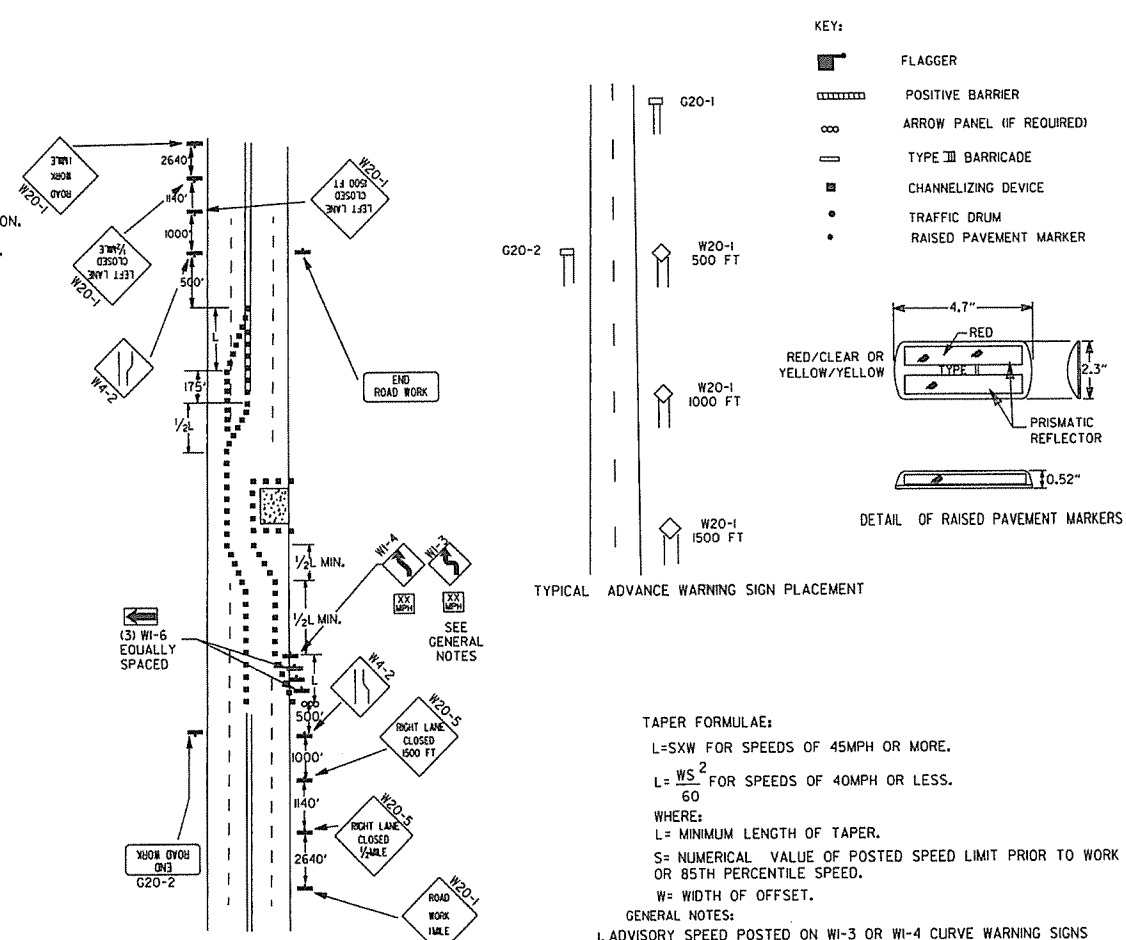
<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5A</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R2-5C</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>RSP-1</p>  <p>48"x30"</p>	<p>W1-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>W1-3</p>  <p>STD. 48"x48"</p>	<p>W1-4</p>  <p>STD. 48"x48"</p>	<p>W1-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>W1-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>W1-4b</p>  <p>STD. 48"x48"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>
						<p>R56-1</p>  <p>STD. 18"x18"</p>
						<p>R55-1</p>  <p>36"x60"</p> <p>WHEN WORKERS ARE PRESENT **</p> <p>• USE 6" C LETTERS •• USE 4" D LETTERS</p>



(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



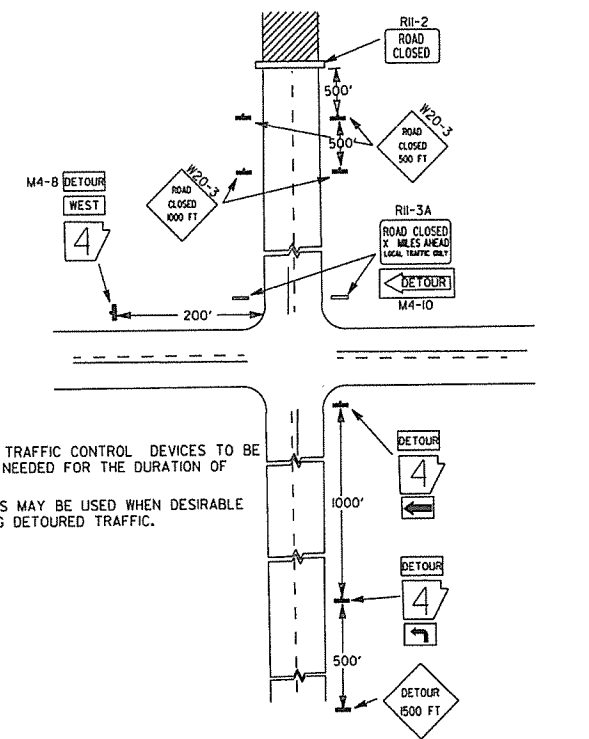
(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

TAPER FORMULAE:  
 L = SXW FOR SPEEDS OF 45MPH OR MORE.  
 L =  $\frac{WS^2}{60}$  FOR SPEEDS OF 40MPH OR LESS.  
 WHERE:  
 L = MINIMUM LENGTH OF TAPER.  
 S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.  
 W = WIDTH OF OFFSET.

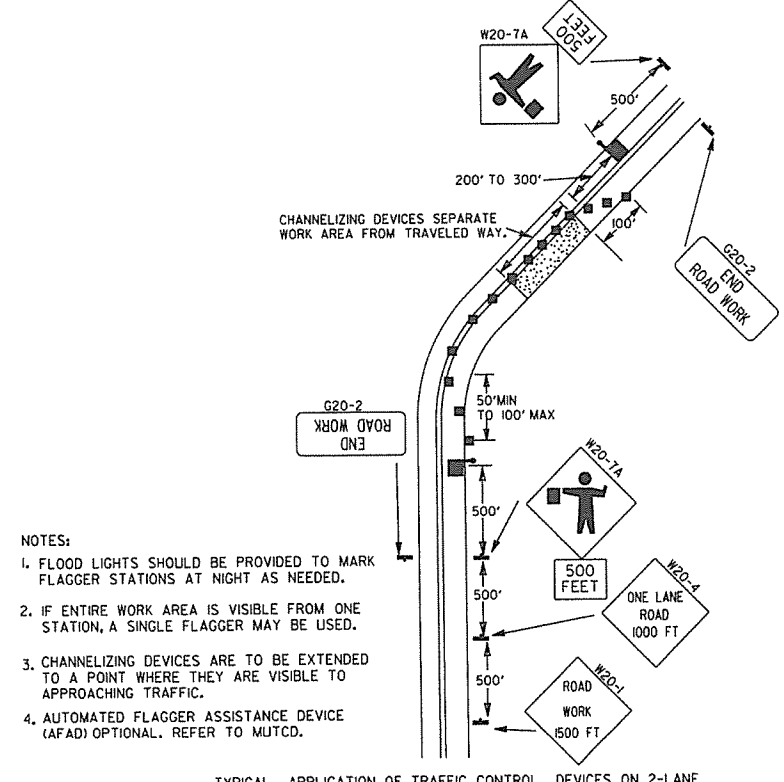
- GENERAL NOTES:
- ADVISORY SPEED POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS TO BE DETERMINED AT SITE. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
  - WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-(K55) SHALL BE OMITTED AND THE R2-5A SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-145MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(KXX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
  - WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-(K45) SHALL BE OMITTED. ADDITIONAL R2-155MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(KXX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
  - THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
  - WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
  - PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
  - TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.

DATE	REVISION	FILMED
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-11-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VL MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

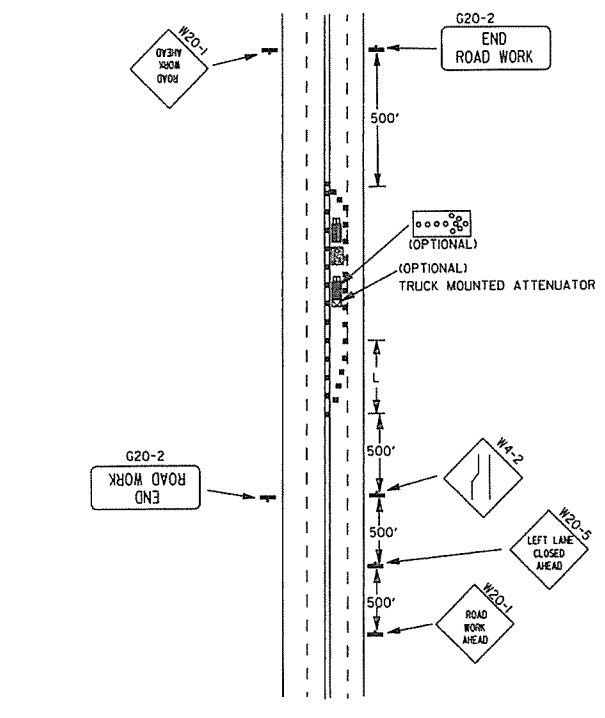
ARKANSAS STATE HIGHWAY COMMISSION  
 STANDARD TRAFFIC CONTROLS  
 FOR HIGHWAY CONSTRUCTION  
 STANDARD DRAWING TC-2



(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.

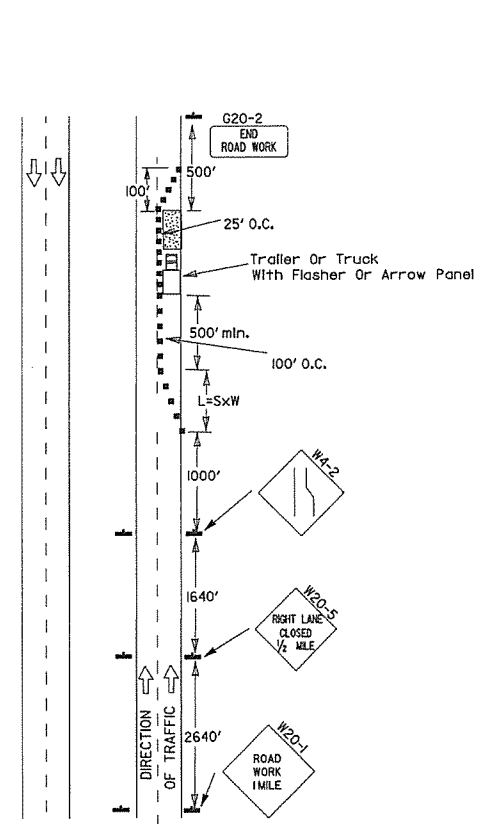


(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.

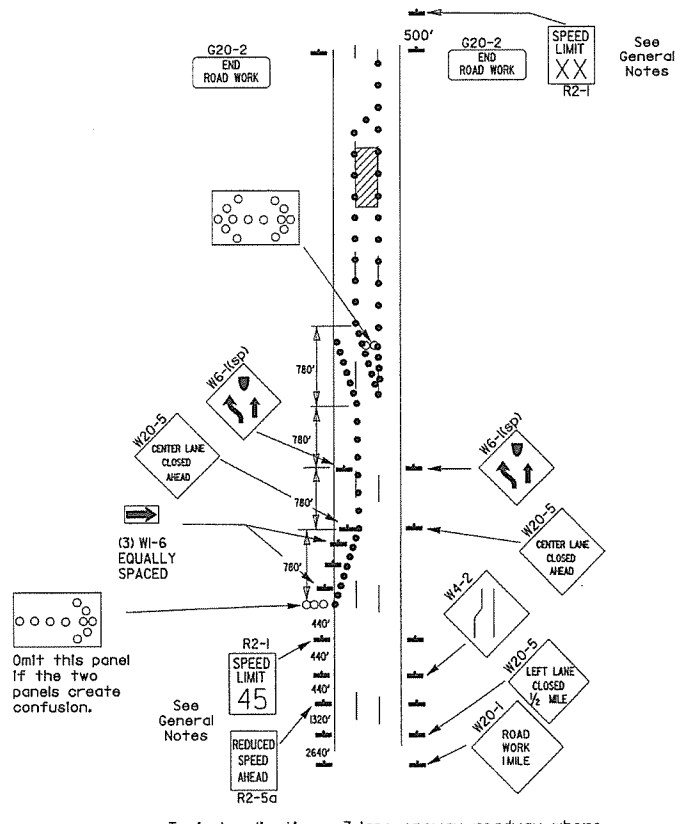


(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

Channelizing devices

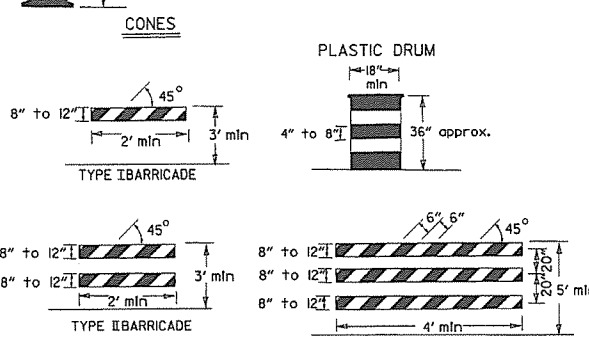


(A) Typical application - daytime maintenance operations of short duration on a 4-lane divided roadway where half of the roadway is closed.

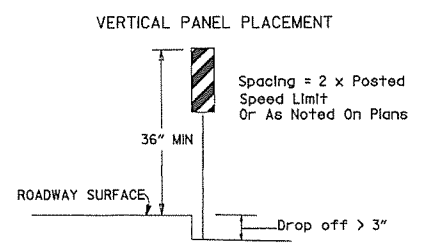


(B) Typical application - 3-lane oneway roadway where center lane is closed.

When cones are used on freeways and multi-lane highways, they shall be 28" min. During hours of darkness, 28" cones shall be reflectorized in accordance with the M.U.T.C.D.



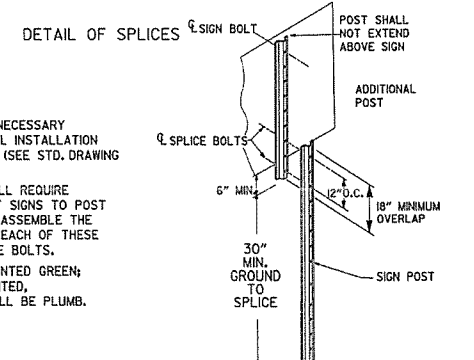
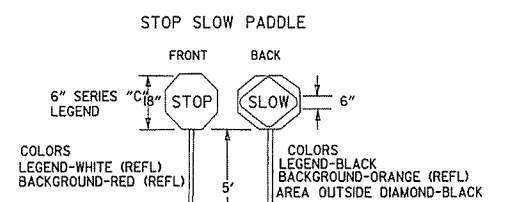
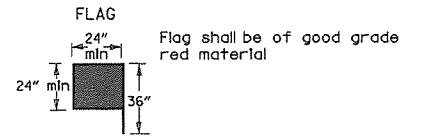
NOTE: For all road closures, the Type III barricades shall be of sufficient length to extend across entire roadway.



TRAFFIC CONTROL DEVICES FOR VERTICAL PAVEMENT DIFFERENTIALS

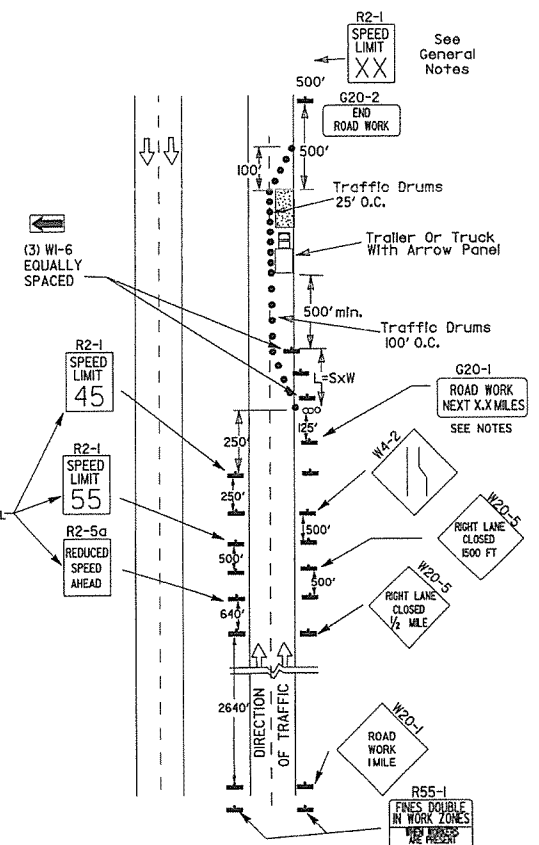
VERTICAL DIFFERENTIAL	LOCATIONS	TRAFFIC CONTROL
1" to 3"	Centerline, lane lines	W8-11
1" to 3"	Edge of shoulder	W8-9
Greater than 3"	Lane lines	Standard lane closure required
Greater than 3"	Edge of traveled lane	*RSP-1 and vertical panels, drums or concrete barrier
Greater than 3"	Edge of shoulder	*Vertical panels, drums or concrete barrier

When the shoulder area is used as part of the traveled lane and there is insufficient width to place drums on the remaining shoulder width, then vertical panels shall be used.

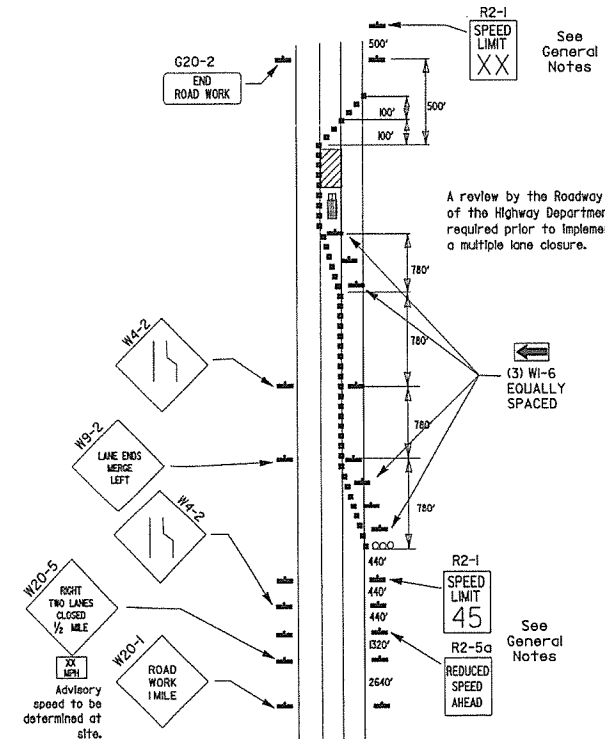


- KEY:
- Arrow Panel (if Required)
  - Channelizing Device
  - Traffic drum

- GENERAL NOTES:
- A speed limit reduction may be implemented ONLY when designated in the plan or when recommended by the Roadway Design Division.
  - When the existing speed limit is 55mph and the plans require a speed limit of 45mph, the R2-1(55) shall be omitted and the R2-5A shall be installed at that location. Additional R2-145mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
  - When the existing speed limit is 65mph and the plans require a speed limit of 55mph, the R2-1(45) shall be omitted. Additional R2-155mph speed limit signs shall be installed at a maximum of 1/2 mile intervals. At the end of the work area a R2-1(XX) shall be installed to match original speed limit.
  - The maximum spacing between channelizing devices in a taper should be approximately equal in feet to the speed limit. Beyond the taper, maximum spacing shall be two times the speed limit or as directed by the Engineer.
  - Warning lights and/or flags may be mounted to signs or channelizing devices at night as needed.
  - Pavement markings no longer applicable which might create confusion in the minds of vehicle operators shall be removed or obliterated as soon as practicable.
  - The G20-1 sign will be required on jobs of over two miles in length. When the lane closure is not at the beginning of the project, the G20-1 sign shall be erected 125' in advance of the job limit. Additional W20-1(1 MILE) signs are not required in advance of lane closures that begin inside the project limits.
  - Flaggers shall use STOP/SLOW paddles for controlling traffic through work zones. Flags may be used only for emergency situations.
  - All plastic drums and cones shall meet the requirements of NCHRP-350 or Manual For Assessing Safety Hardware (MASH).
  - Trailer mounted devices such as arrow panels and portable changeable message signs shall be delineated by affixing conspicuity material in a continuous line on the face of the trailer. When placed on or adjacent to the shoulder and not behind a positive barrier, these devices shall be delineated by placing five (5) traffic drums, equally spaced along the traffic side of the device.

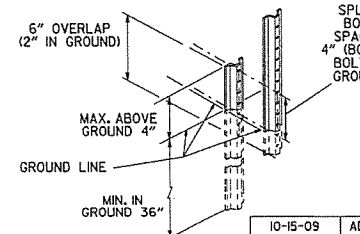


(C) Typical application - construction operations of intermediate to long term duration on a 4-lane divided roadway where half of the roadway is closed.

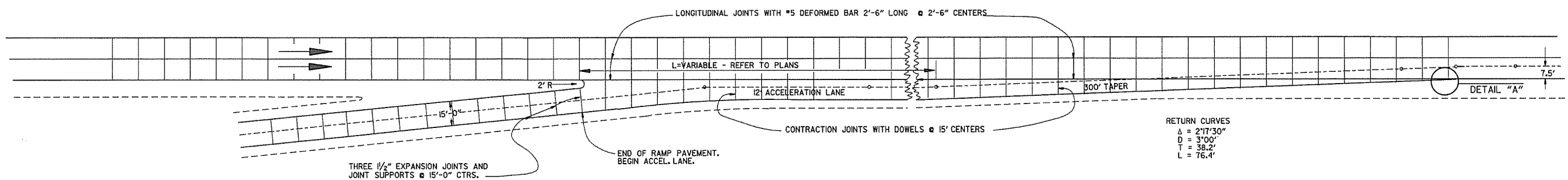


(D) Typical application - closing multiple lanes of a multilane highway.

NOTES: USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2). NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS. SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.

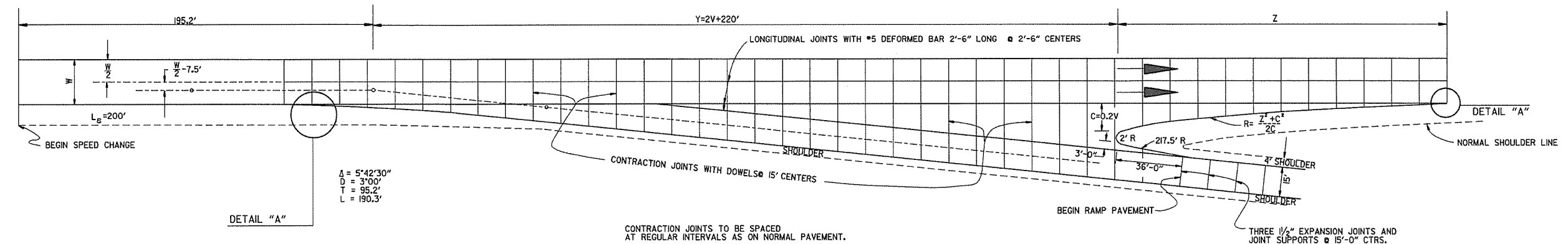


DATE	REVISION	FILMED
10-15-09	ADDED REFERENCE TO MASH	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	



ENTRANCE RAMP

NOTE: JOINT SPACING ON THE MAIN LANES SHALL BE ADJUSTED AS NECESSARY TO CONFORM TO THESE JOINT LAYOUTS. THE MAIN LANE JOINT SPACING MAY BE REDUCED TO A 12' MINIMUM.

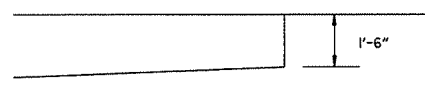


EXIT RAMP

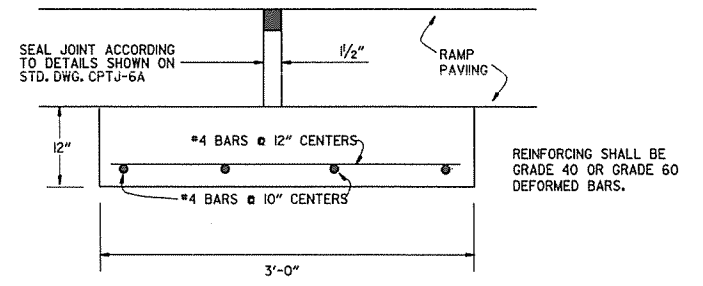
CONTRACTION JOINTS TO BE SPACED AT REGULAR INTERVALS AS ON NORMAL PAVEMENT.

EXIT RAMP

DESIGN SPEED V	Y	NOSE OFFSET C	LENGTH NOSE TAPER Z	RETURN RADIUS R	ADDITIONAL SURFACING SQ. YDS.
40	300.0	8.0	96.0	580.0	602.43
50	320.0	10.0	120.0	725.0	687.29
60	340.0	12.0	168.0	1182.0	790.55
70	360.0	14.0	210.0	1582.0	902.27



DETAIL "A"



DETAIL OF EXPANSION JOINT & JOINT SUPPORT

NOTE: THE EXPANSION JOINTS SHALL BE MEASURED AND PAID FOR AS P.C.C. PAVEMENT (RAMP THICKNESS) WHEN RAMP PAVING IS ASPHALT. EXPANSION JOINT IS NOT REQUIRED. THE JOINT SUPPORT MAY BE CONSTRUCTED WITH CLASS "A", "S", OR PAVING CONCRETE. PAYMENT FOR THE JOINT SUPPORT SHALL BE FOR THE CONTRACT UNIT PRICE BID FOR THE CLASS OF CONCRETE USED. ALL OTHER WORK AND MATERIALS REQUIRED FOR THE CONSTRUCTION OF THE JOINT SUPPORT SHALL BE INCLUDED IN THE PRICE BID FOR THE ABOVE ITEMS.

DATE	REVISION	DATE FILM'D
8-22-02	DELETED NOTE	
11-16-01	CORRECTED SPELLING ON ENTRANCE RAMP NOTE	
5-13-99	ADDED, EDITED AND DELETED NOTES	
11-03-94	ADDED NOTE RE: REINF. BARS	
10-1-92	ADDED DETAIL A & OTHER MINOR CHANGES	10-1-92
1-25-90	REVISED EXPANSION JOINT	1-25-90
7-15-88	CONFORM'D TO 1988 SPECIFICATIONS	65C-7-15-88
3-2-81	ISSUED	511-12-2-72

ARKANSAS STATE HIGHWAY COMMISSION

DETAILS OF STANDARD TURNOUT

FOR

ENTRANCE & EXIT RAMPS (NON-REINFORCED)

STANDARD DRAWING TR-1A