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5/19/2022
Robert.Dorrington
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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	1	34
STATEWIDE CABLE MEDIAN BARRIER IMPVTS. (2022) (S)						

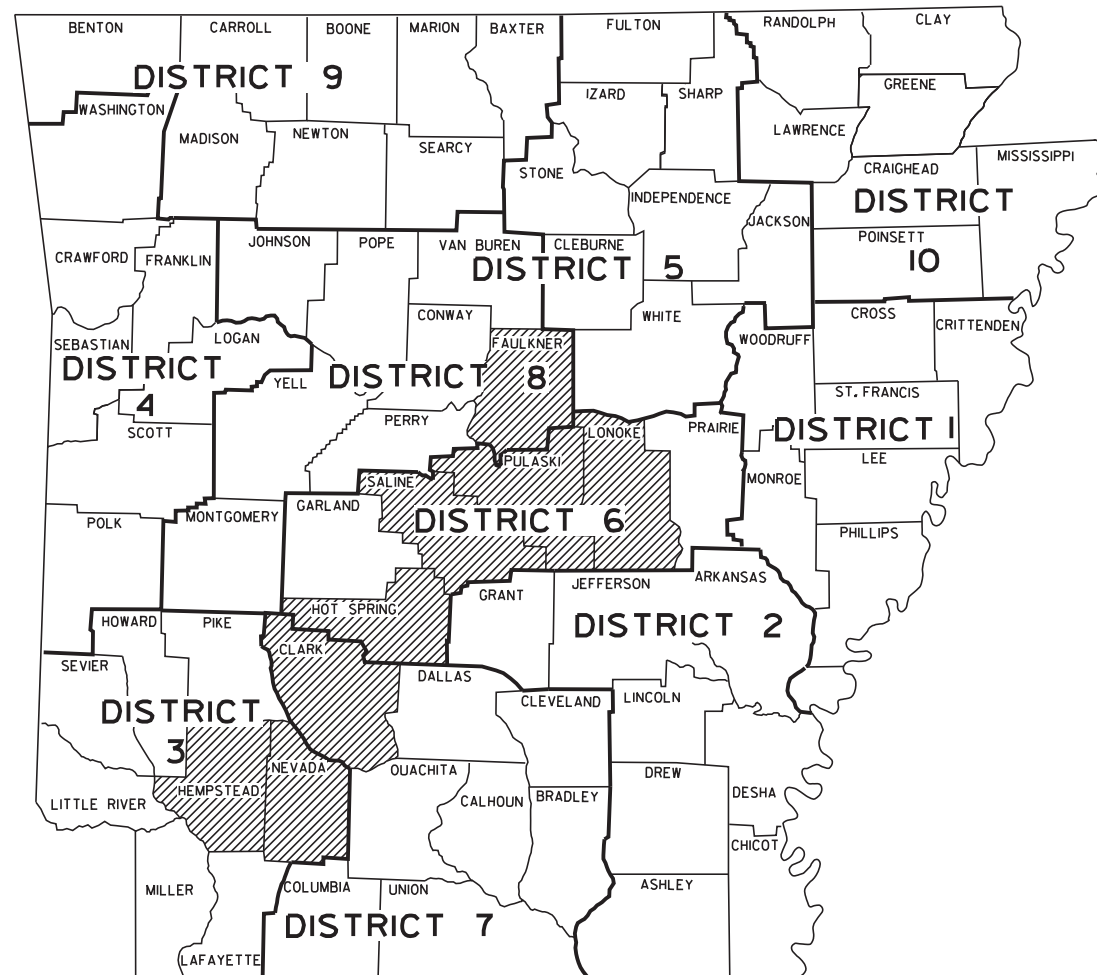
ARKANSAS DEPARTMENT OF TRANSPORTATION
CONSTRUCTION PLANS FOR STATE HIGHWAY

STATEWIDE CABLE MEDIAN
BARRIER IMPVTS. (2022) (S)

VARIOUS COUNTIES

JOB 012388

FED. AID PROJ. HSIP-0076(258)



ARK. HWY. DIST. NOS. 3, 6, 7 & 8



C:\BMS\BRIDGE\FARMER - PW\ROBERT - DARRINGTON\BRI\DOEFARMER.COM\DWG\156\R012388_01_GN_02_IND.DGN 11/2/2022 3:17:46 PM ROBERT.DARRINGTON

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
06-14-22		6	ARK.	012388	2	34
11-02-22						

INDEX OF SHEETS, STANDARD DRAWINGS, GOVERNING SPECS, AND GENERAL NOTES

INDEX OF SHEETS

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2	INDEX OF SHEETS, GOVERNING SPECIFICATIONS, AND GENERAL NOTES
3-4	TYPICAL SECTIONS OF IMPROVEMENT
5-14	SPECIAL DETAILS
15-19	MAINTENANCE OF TRAFFIC DETAILS
20-25	QUANTITIES
26	SUMMARY OF QUANTITIES AND REVISIONS
27	HEMPSTEAD COUNTY PLAN
28	NEVADA COUNTY PLAN
29	CLARK COUNTY PLAN
30	HOT SPRING COUNTY PLAN
31	SALINE COUNTY PLAN
32	PULASKI COUNTY PLAN
33	FAULKNER COUNTY PLAN
34	LONOKE COUNTY PLAN



GENERAL NOTES

- ALL PIPE LINES, POWER, TELEPHONE, AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO INSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATIONS.
- ALL FLEXIBLE BASE AND ASPHALTIC PAVEMENT MATERIALS REMOVED SHALL BE PAID FOR UNDER THE ITEM NO. 210 - UNCLASSIFIED EXCAVATION.
- THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE OF THE PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

ROADWAY STANDARD DRAWINGS

DRWG.NO.	TITLE	DATE
CDP-1	CONCRETE DITCH PAVING	12-08-16
GR-6	GUARDRAIL DETAILS	05-19-22
GR-7	GUARDRAIL DETAILS	11-07-19
GR-8	GUARDRAIL DETAILS	11-07-19
GR-9	GUARDRAIL DETAILS	11-07-19
GR-10	GUARDRAIL DETAILS	11-07-19
GR-11	GUARDRAIL DETAILS	11-07-19
GR-12	GUARDRAIL DETAILS	05-14-20
TC-1	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	11-07-19
TC-2	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	05-20-21
TC-3	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-12-21
TEC-1	TEMPORARY EROSION CONTROL DEVICES	11-16-17
TEC-2	TEMPORARY EROSION CONTROL DEVICES	06-02-94
TEC-3	TEMPORARY EROSION CONTROL DEVICES	11-03-94
TEC-4	TEMPORARY EROSION CONTROL DEVICES	07-26-12

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-3	CONTRACTOR'S LICENSE
100-4	DEPARTMENT NAME CHANGE
102-2	ISSUANCE OF PROPOSALS
105-4	MAINTENANCE DURING CONSTRUCTION
107-2	RESTRAINING CONDITIONS
108-1	LIQUIDATED DAMAGES
108-2	WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER
110-1	PROTECTION OF WATER QUALITY AND WETLANDS
210-1	UNCLASSIFIED EXCAVATION
303-1	AGGREGATE BASE COURSE
306-1	QUALITY CONTROL AND ACCEPTANCE
400-4	DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES
400-5	PERCENT AIR VOIDS FOR ACHM MIX DESIGNS
400-6	LIQUID ANTI-STRIP ADDITIVE
404-3	DESIGN OF ASPHALT MIXTURES
410-1	CONSTRUCTION REQUIREMENTS AND ACCEPTANCE OF ASPHALT CONCRETE PLANT MIX COURSES
410-2	DEVICES FOR MEASURING DENSITY FOR ROLLING PATTERNS
410-4	EVALUATION OF ACHM SUBLOT REPLACEMENT MATERIAL
600-2	INCIDENTAL CONSTRUCTION
603-1	LANE CLOSURE NOTIFICATION
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
604-3	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES (MASH)
605-1	CONCRETE DITCH PAVING
617-1	GUARDRAIL TERMINAL (TYPE 2)
620-1	MULCH COVER
621-1	FILTER SOCKS
802-4	CEMENT
JOB 012388	ASSESSMENT OF WORKING DAYS-MAINTENANCE OF TRAFFIC
JOB 012388	BIDDING REQUIREMENTS AND CONDITIONS
JOB 012388	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB 012388	CARGO PREFERENCE ACT REQUIREMENTS
JOB 012388	CONCRETE DITCH PAVING
JOB 012388	COORDINATION OF WORK
JOB 012388	DISADVANTAGED BUSINESS ENTERPRISE BIDDER'S RESPONSIBILITIES
JOB 012388	ESTABLISHING CONTRACT TIME-WORKING DAY CONTRACT
JOB 012388	GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION
JOB 012388	LIQUIDATED DAMAGES PROCEDURE FOR BID LETTINGS
JOB 012388	LONGITUDINAL JOINT DENSITIES FOR ACHM SURFACE COURSES
JOB 012388	MAINTENANCE OF TRAFFIC
JOB 012388	MANDATORY ELECTRONIC CONTRACT
JOB 012388	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB 012388	PARTNERING REQUIREMENTS
JOB 012388	PRICE ADJUSTMENT FOR ASPHALT BINDER
JOB 012388	PRICE ADJUSTMENT FOR FUEL
JOB 012388	PROHIBITION OF CERTAIN TELECOMMUNICATIONS AND VIDEO SURVEILLANCE SERVICES OR EQUIPMENT
JOB 012388	REMOVAL AND DISPOSAL OF IMPACT ATTENUATION BARRIERS
JOB 012388	REMOVAL AND DISPOSAL OF WIRE ROPE SAFETY FENCE
JOB 012388	STORMWATER POLLUTION PREVENTION PLAN - DISTRICT 3
JOB 012388	STORMWATER POLLUTION PREVENTION PLAN - DISTRICT 6
JOB 012388	STORMWATER POLLUTION PREVENTION PLAN - DISTRICT 7
JOB 012388	STORMWATER POLLUTION PREVENTION PLAN - DISTRICT 8
JOB 012388	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB 012388	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB 012388	UTILITY ADJUSTMENTS
JOB 012388	VALUE ENGINEERING
JOB 012388	WARM MIX ASPHALT
JOB 012388	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS - DISTRICT 3
JOB 012388	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS - DISTRICT 6
JOB 012388	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS - DISTRICT 7
JOB 012388	WIRE ROPE SAFETY FENCE MAINTENANCE MATERIALS - DISTRICT 8
JOB 012388	WIRE ROPE SAFETY FENCE (POST REPAIR)
JOB 012388	WIRE ROPE SAFETY FENCE (WRSF) SPECIFICATIONS
JOB 012388	WRSF TRAINING WORKSHOP

INDEX OF SHEETS, STANDARD DRAWINGS, GOVERNING SPECIFICATIONS, AND GENERAL NOTES

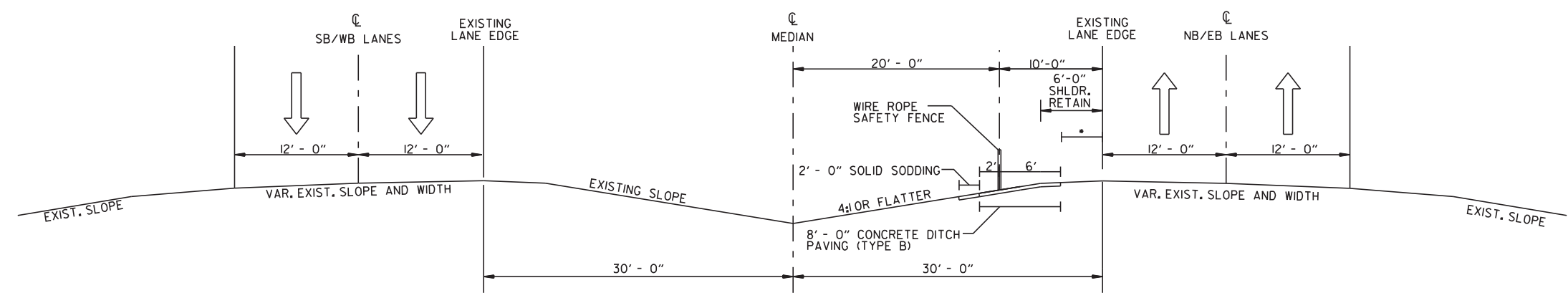
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ROBERT, DARRINGTON

10/6/2022

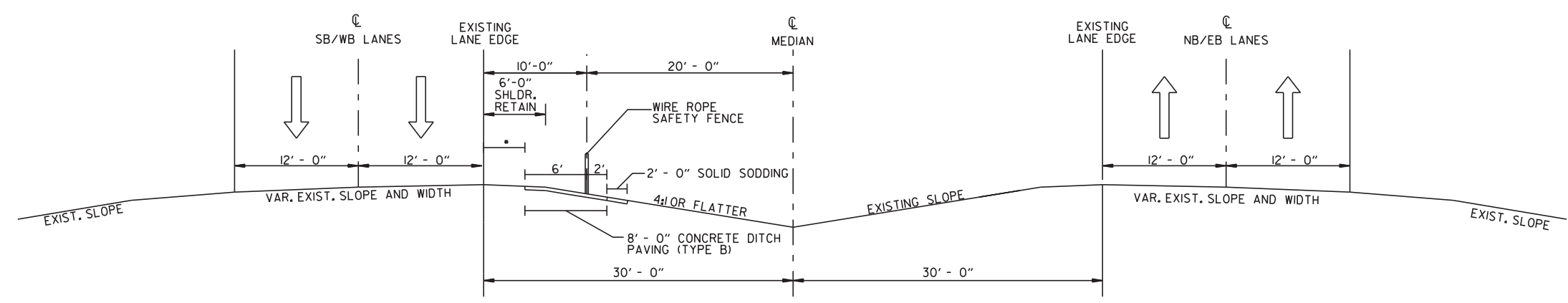
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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
09-22-22		6	ARK.	012388	3	34
TYPICAL SECTIONS OF IMPROVEMENT						



TYPICAL SECTION OF IMPROVEMENT
FOR WIRE ROPE SAFETY FENCE RIGHT OF CENTERLINE
SITES 1-10, 13-15, 25-28

• 4'-0" EXISTING PAVED SHOULDER TO REMAIN

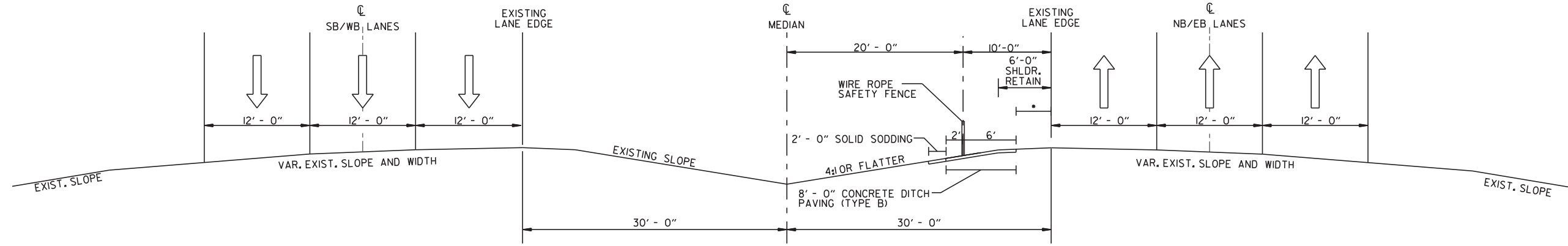


TYPICAL SECTION OF IMPROVEMENT
FOR WIRE ROPE SAFETY FENCE LEFT OF CENTERLINE
SITES 1-10, 13-15, 25-28

TYPICAL SECTIONS OF IMPROVEMENT

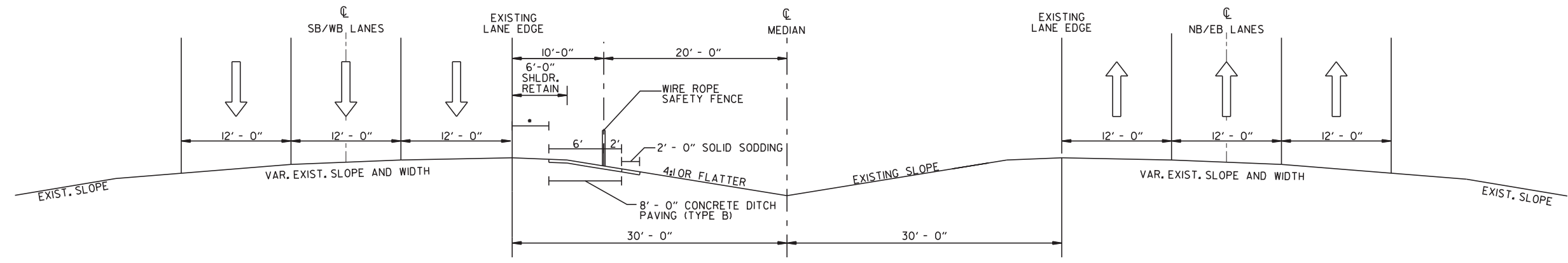
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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
09-22-22		6	ARK.	012388	4	34
TYPICAL SECTIONS OF IMPROVEMENT						



TYPICAL SECTION OF IMPROVEMENT
FOR WIRE ROPE SAFETY FENCE RIGHT OF CENTERLINE
SITES 11, 12, 16-24

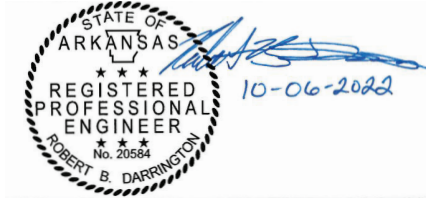
• 4'-0" EXISTING PAVED SHOULDER TO REMAIN



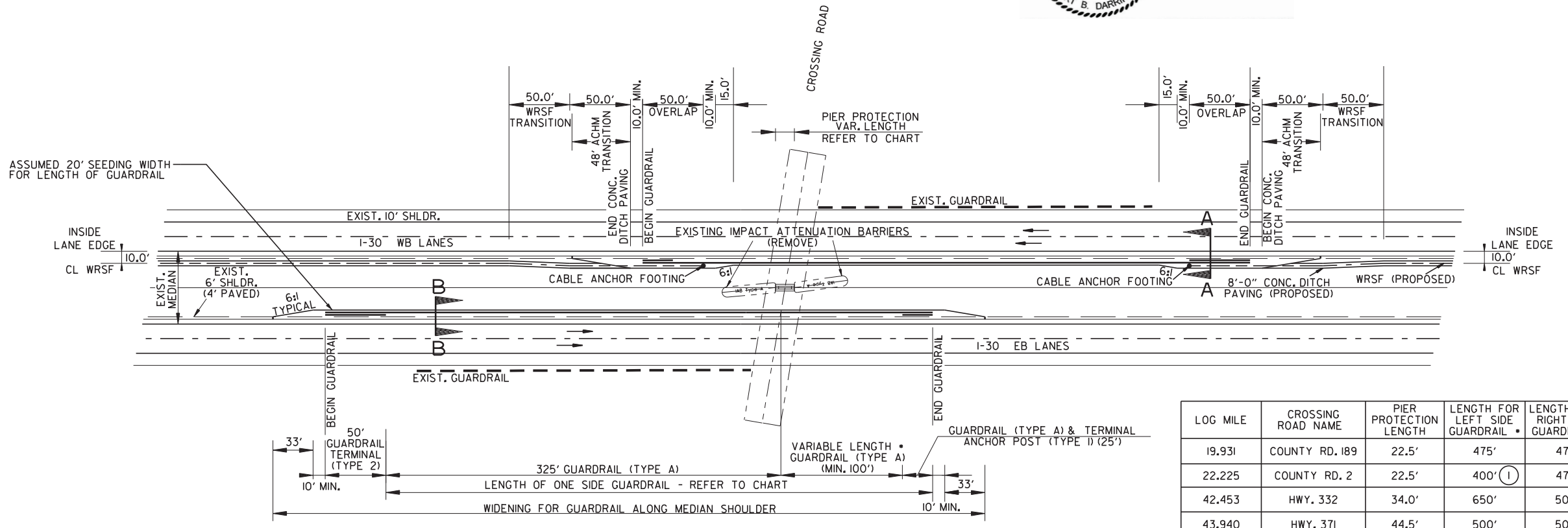
TYPICAL SECTION OF IMPROVEMENT
FOR WIRE ROPE SAFETY FENCE LEFT OF CENTERLINE
SITES 11, 12, 16-24

TYPICAL SECTIONS OF IMPROVEMENT

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
09-22-22		6	ARK.	012388	5	34
SPECIAL DETAILS						



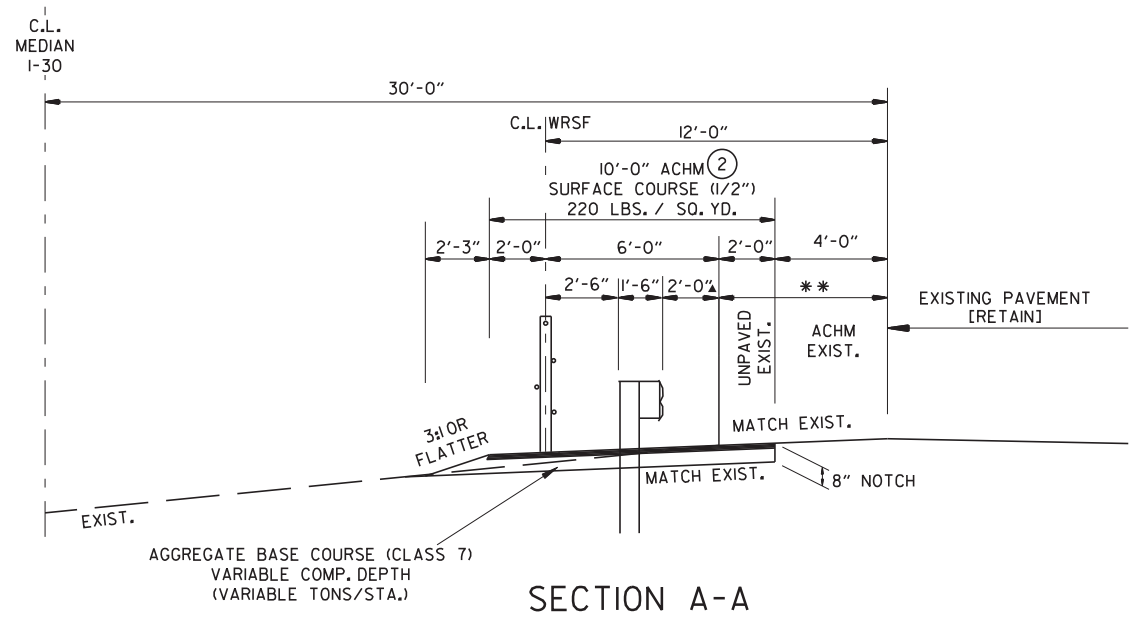
LOG MILE	CROSSING ROAD NAME	PIER PROTECTION LENGTH	LENGTH FOR LEFT SIDE GUARDRAIL	LENGTH FOR RIGHT SIDE GUARDRAIL
19.931	COUNTY RD. 189	22.5'	475'	475'
22.225	COUNTY RD. 2	22.5'	400' (1)	475'
42.453	HWY. 332	34.0'	650'	500'
43.940	HWY. 371	44.5'	500'	500'
142.287	HWY. 365	65.5'	525'	375' (1)

* GUARDRAIL LENGTH IS ROUNDED UP BASED ON GUARDRAIL STOCK LENGTH OF 25.0'

(1) GUARDRAIL LENGTH LESS THAN DESIRABLE DUE TO PROXIMITY OF EXISTING MEDIAN CROSSING AND EXISTING CONCRETE MEDIAN BARRIER.

DETAIL AT OVERPASSES

NOTE: REFER TO QUANTITY SHEETS FOR PLACEMENT OF WIRE ROPE SAFETY FENCE.

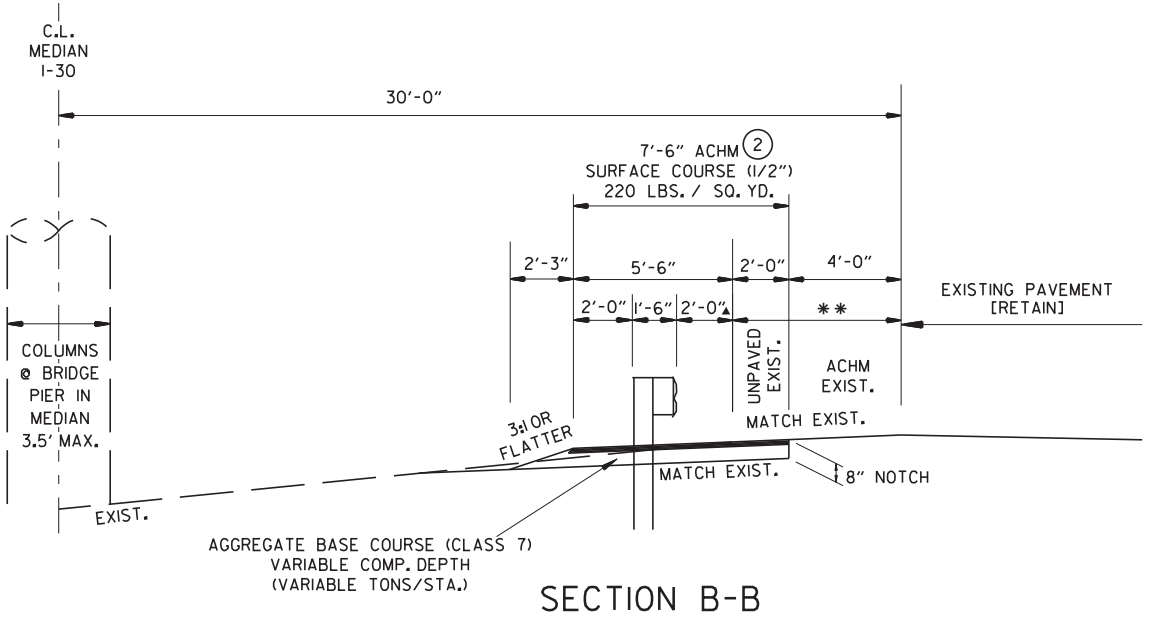


SECTION A-A

** 10'-1" ACHM EXIST. AT SITES 12 & 18

▲ 3'-0" AT SITES 12 & 18

(2) TRENCHING & SHOULDER PREPARATION (8" DEPTH)



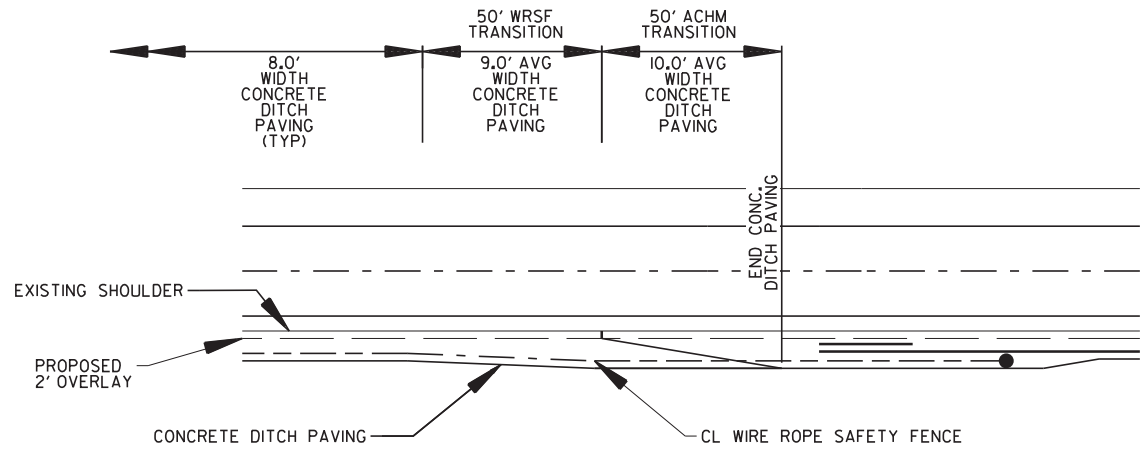
SECTION B-B

DETAILS OF SHOULDER WIDENING FOR GUARDRAIL AND OVERLAPS WITH ENDS OF WIRE ROPE SAFETY FENCE

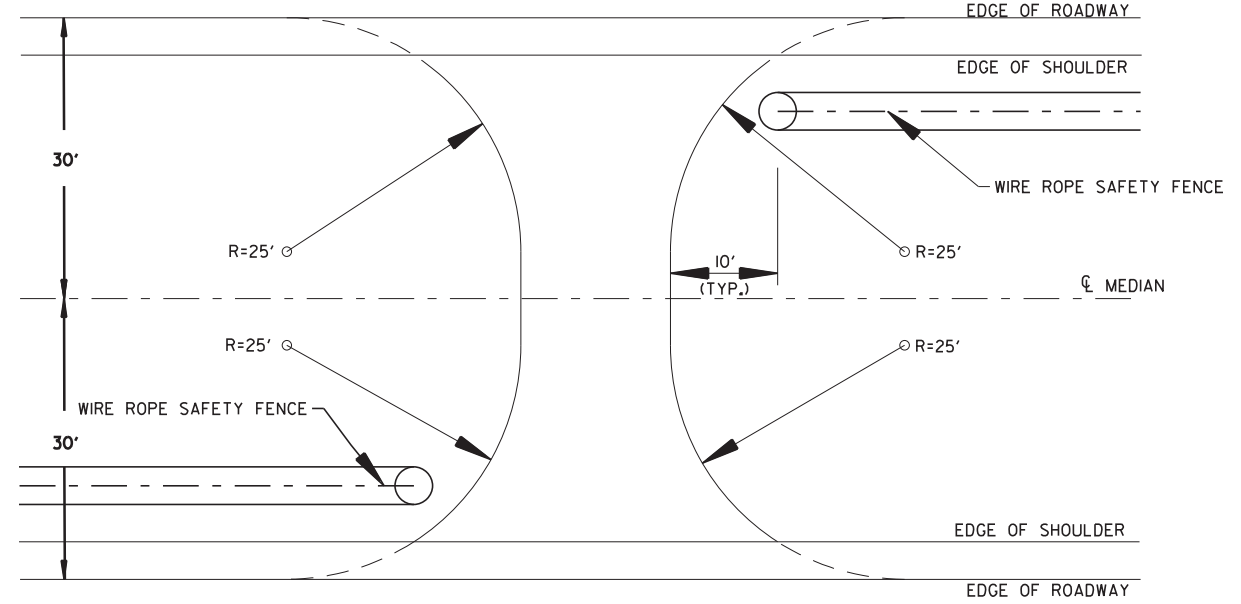
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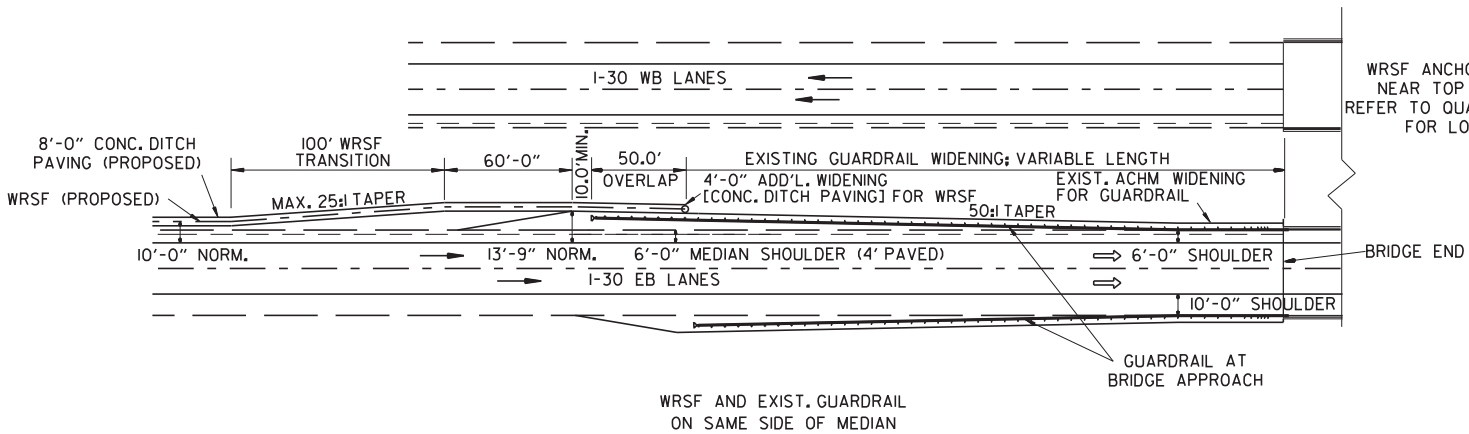
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
09-22-22		6	ARK.	012388	6	34
SPECIAL DETAILS						



DETAIL OF CONCRETE DITCH PAVING AT GUARDRAIL WIDENING

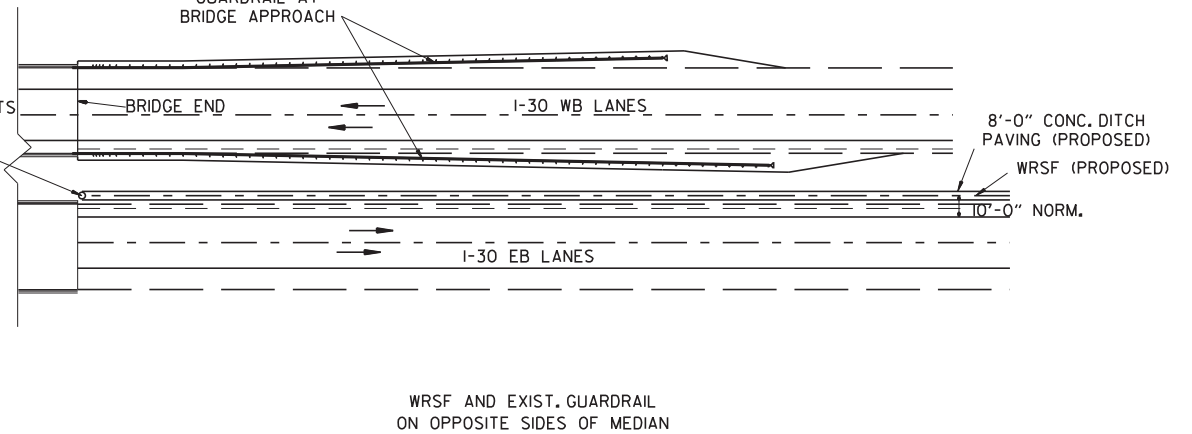


DETAIL OF EXISTING MEDIAN CROSSING



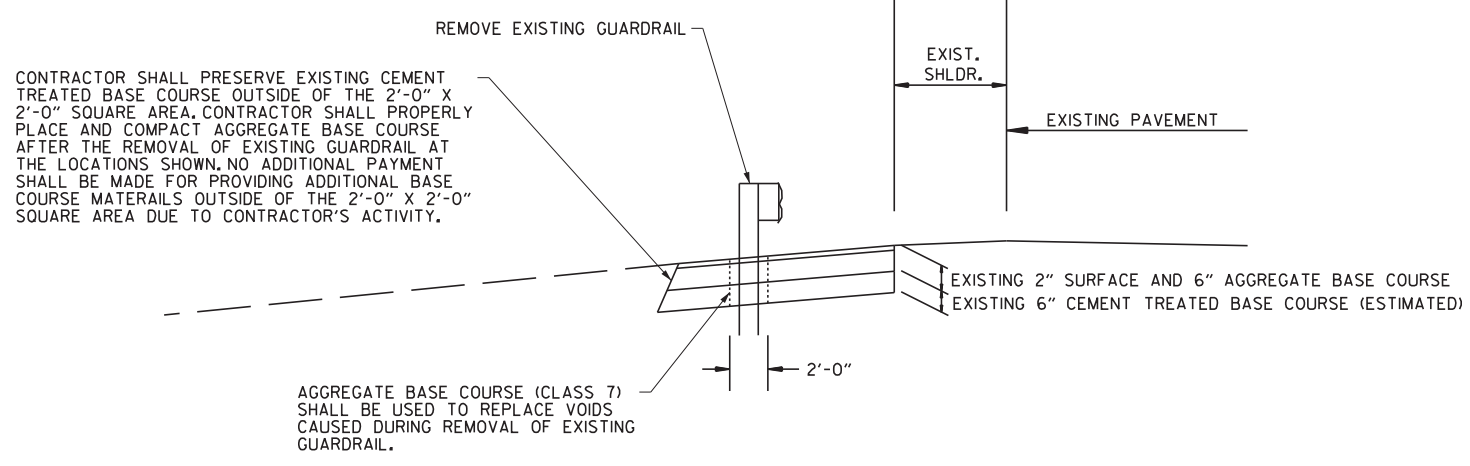
DETAIL OF WIRE ROPE SAFETY FENCE AT EXISTING BRIDGE ENDS

REFER TO QUANTITY SHEETS FOR RELATIVE PLACEMENT OF GUARDRAIL AND WIRE ROPE SAFETY FENCE AT EACH BRIDGE END

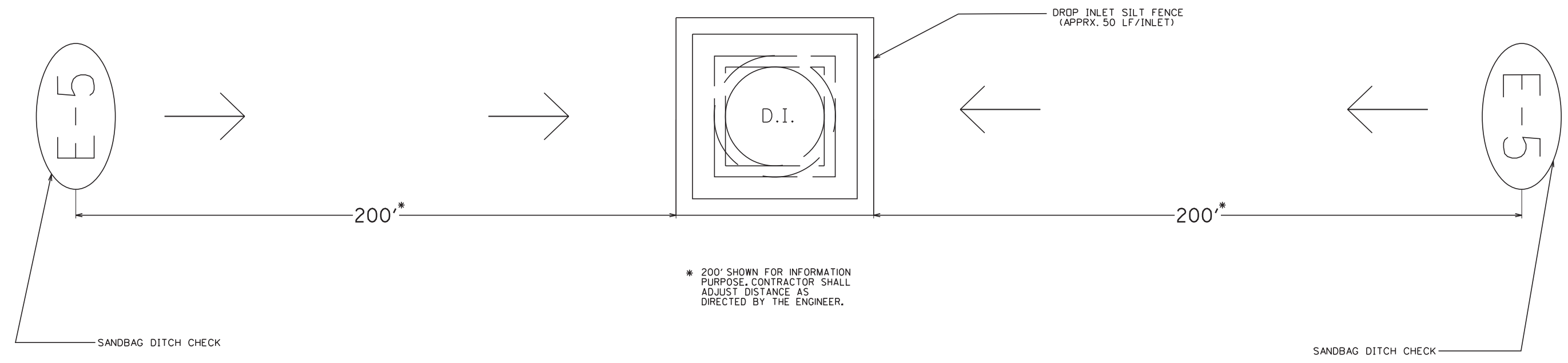


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Robert.Darrington
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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	7	34
SPECIAL DETAILS						



DETAIL OF GUARDRAIL REMOVAL
N.T.S.



TEMPORARY EROSION CONTROL DETAIL AT
MEDIAN INLET

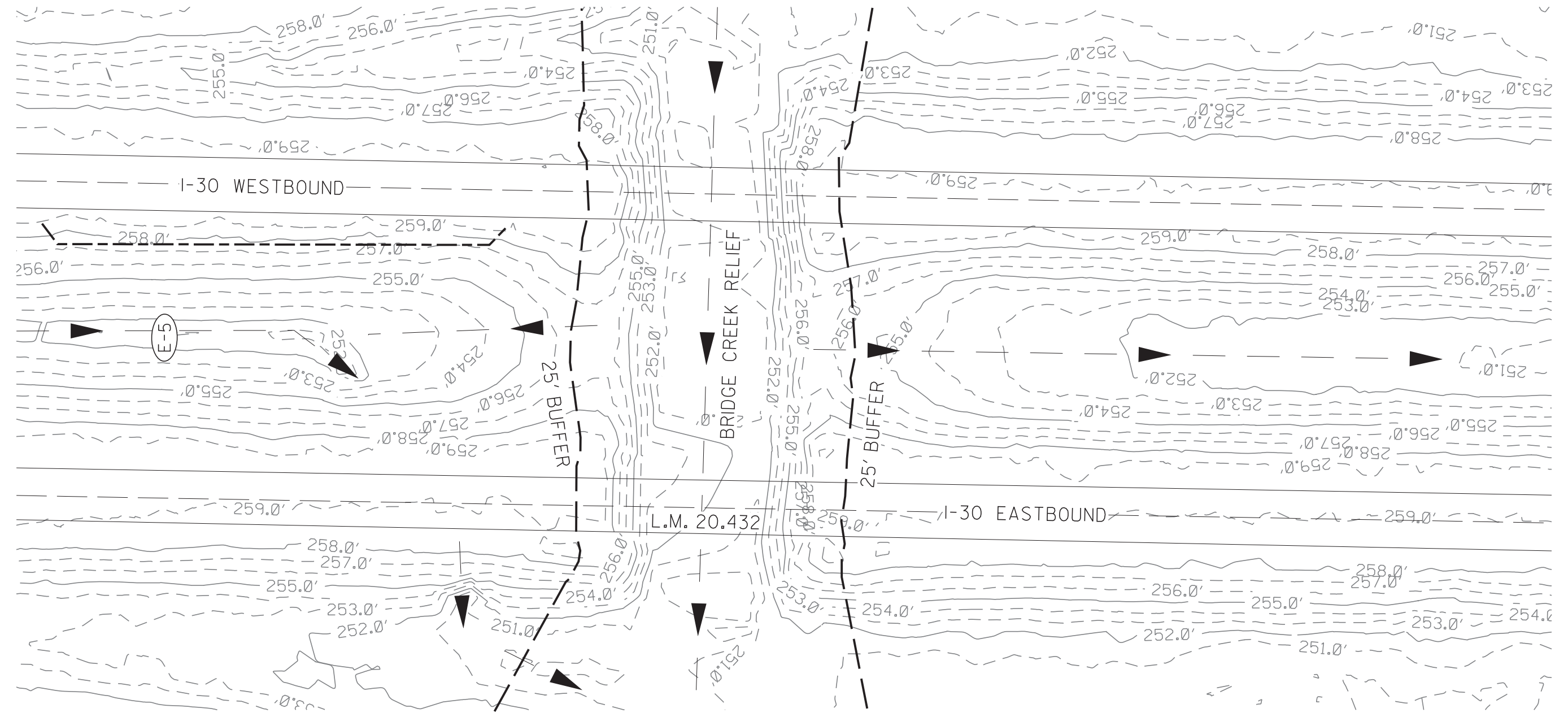
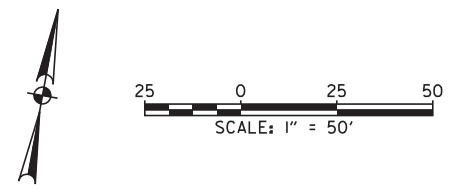
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Robert.Darrington

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	8	34
SPECIAL DETAILS						



SITE I
HEMPSTEAD COUNTY SPECIAL DETAILS

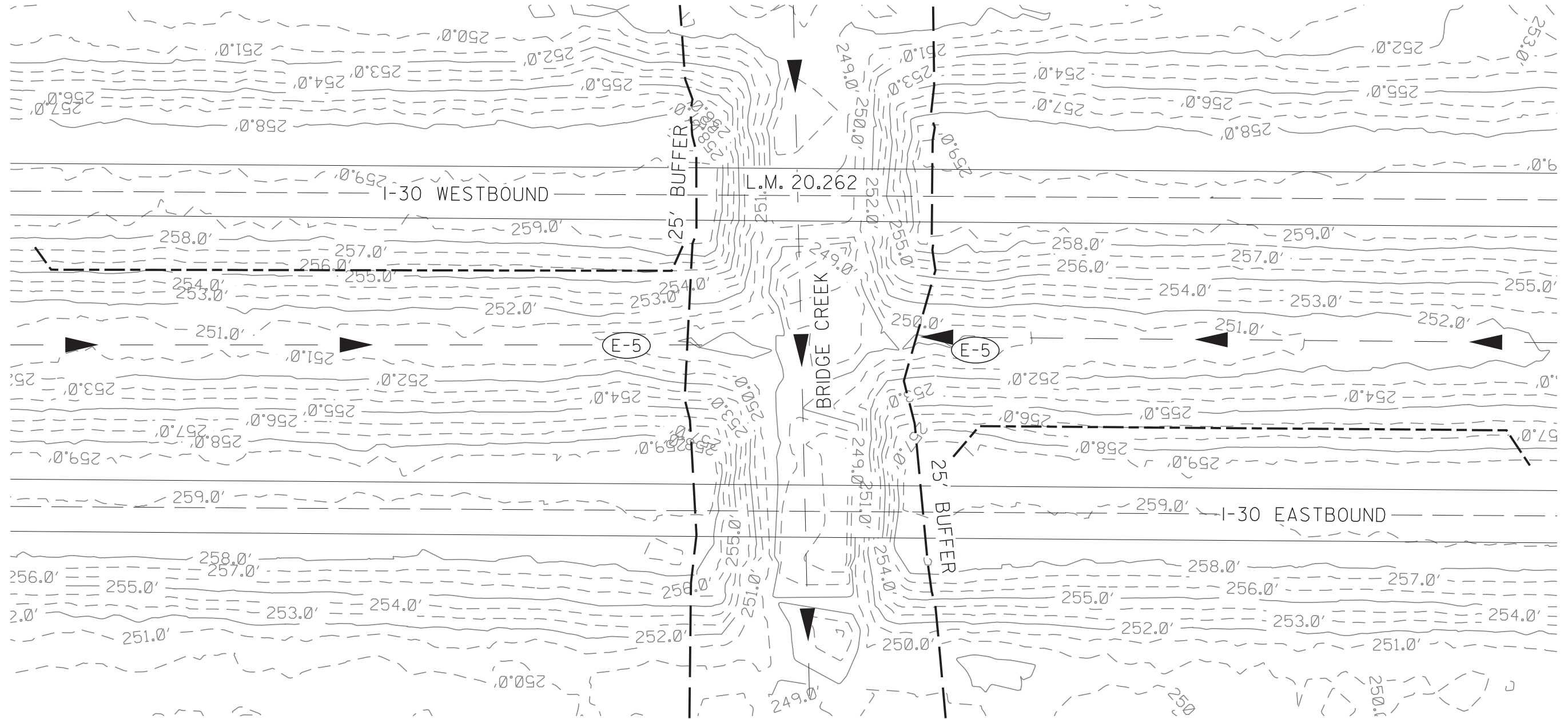
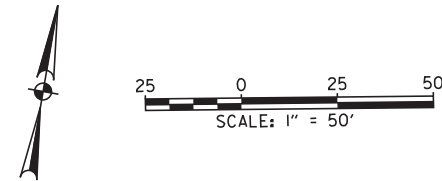
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5/19/2022

Robert.L.Darrington

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	9	34
SPECIAL DETAILS						



SITE 1
HEMPSTEAD COUNTY SPECIAL DETAILS

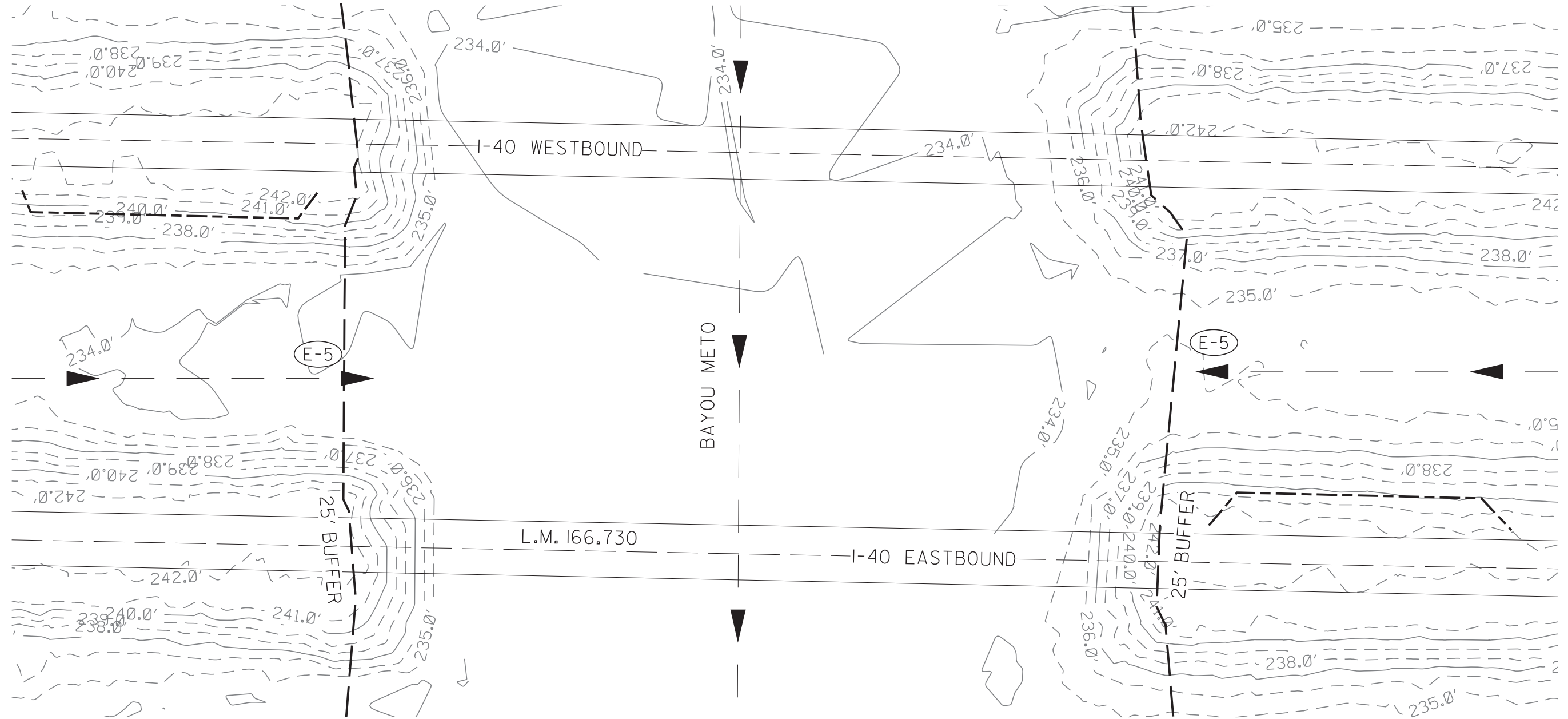
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5/19/2022

Robert.Darrington

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	02388	10	34
SPECIAL DETAILS						



SAND BAG DITCH CHECKS	(E-5)
FILTER SOCK	-----

SITE 26
LONOKE COUNTY SPECIAL DETAILS

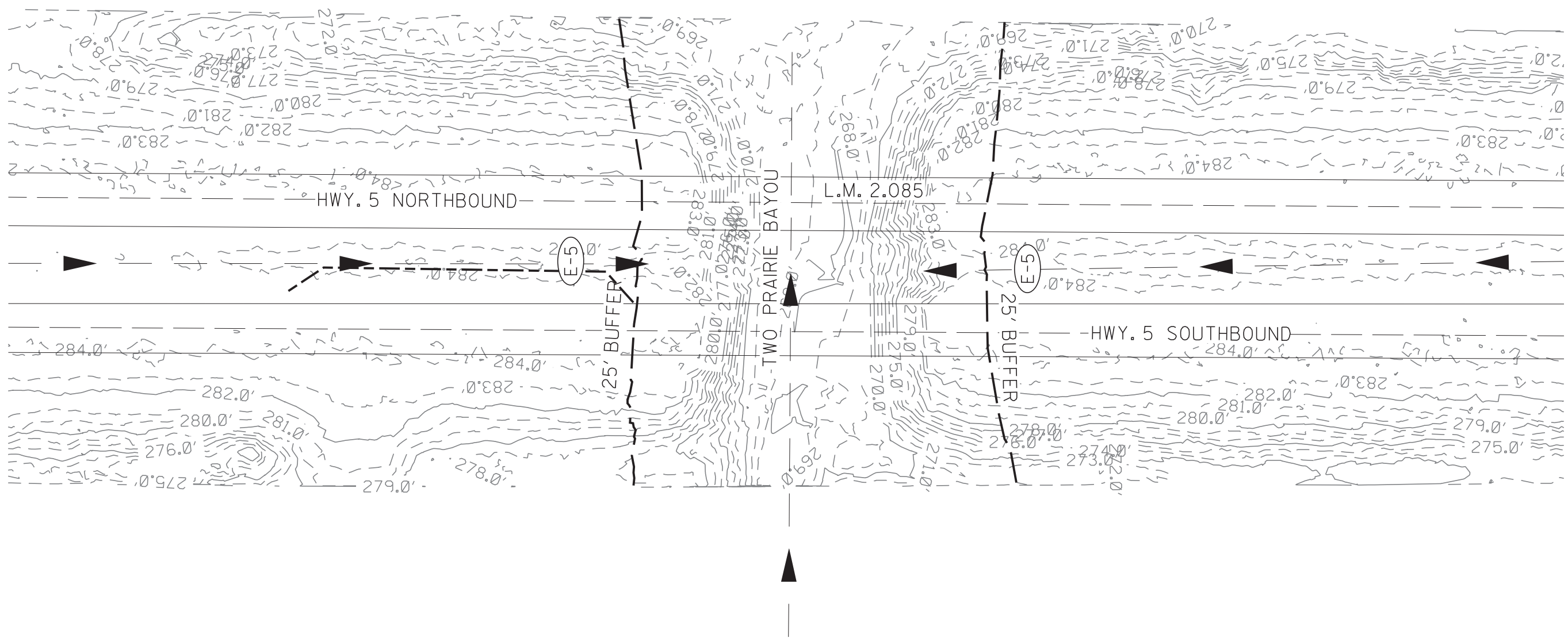
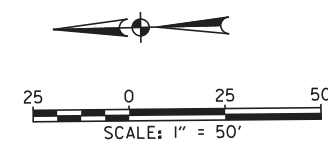
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Robert.Darrington

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	II	34
SPECIAL DETAILS						



SAND BAG DITCH CHECKS	
FILTER SOCK	

SITE 28
LONOKE COUNTY SPECIAL DETAILS

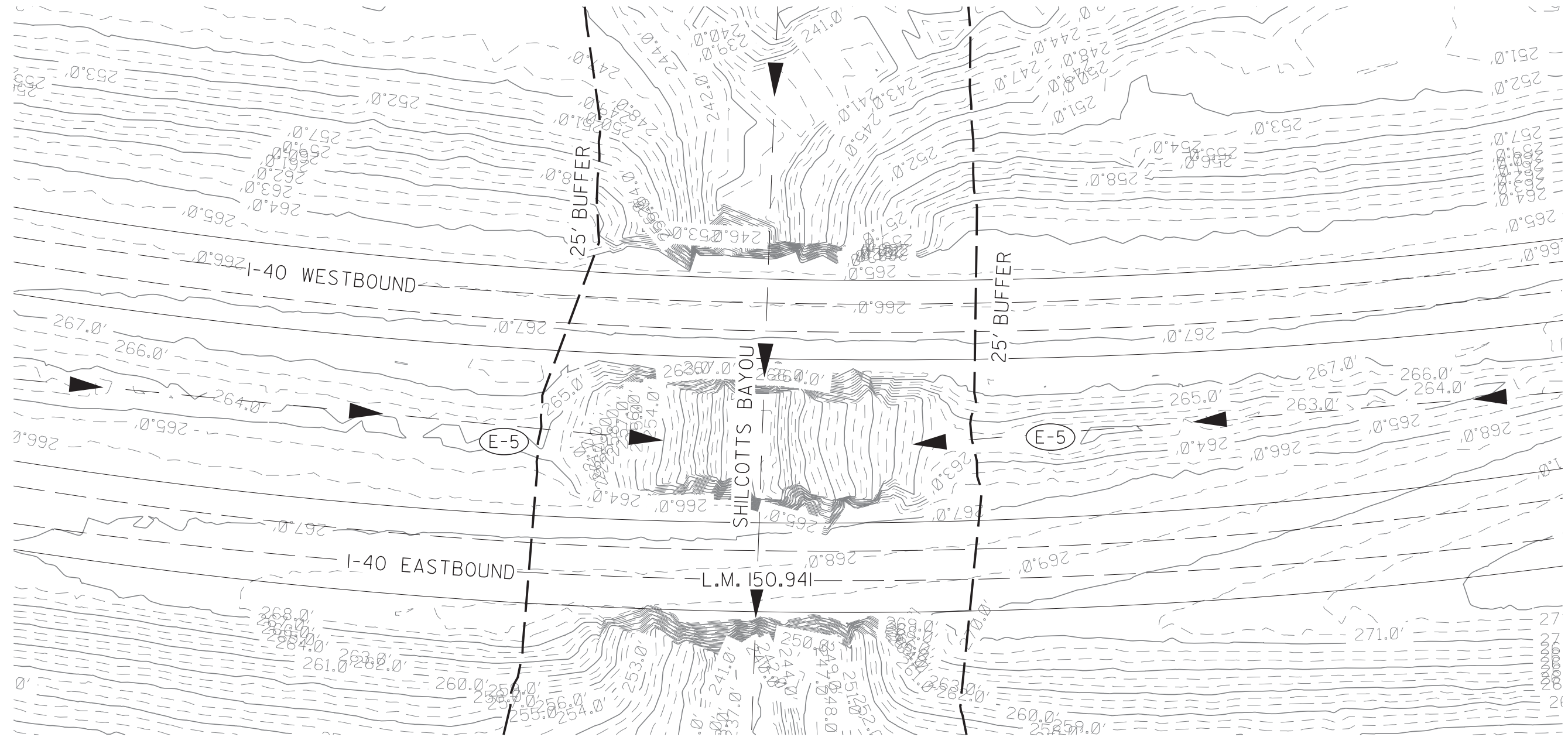
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5/19/2022

Robert.Darrington

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	02388	12	34
SPECIAL DETAILS						



SAND BAG DITCH CHECKS (E-5)

FILTER SOCK (---)

SITE 24
PULASKI COUNTY SPECIAL DETAILS

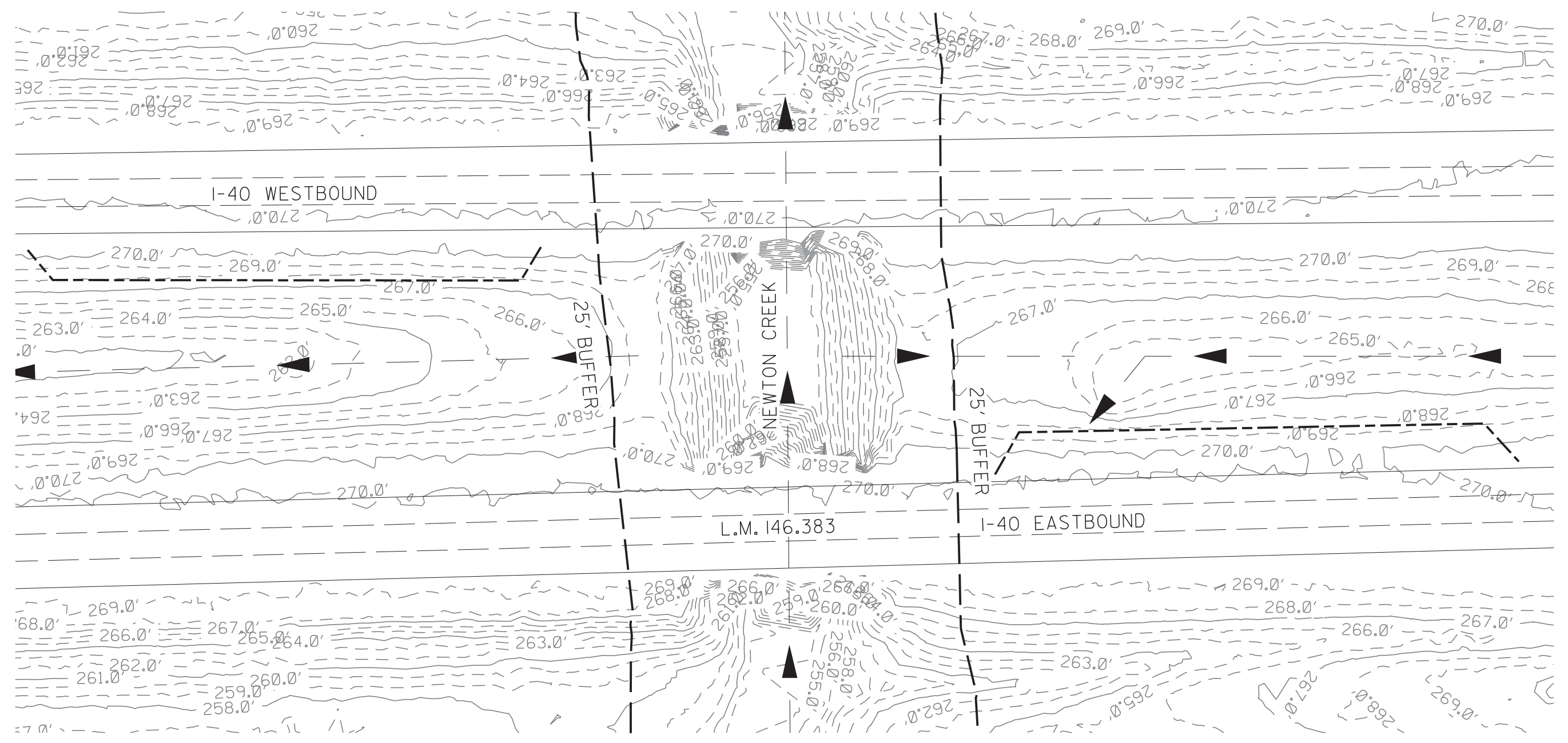
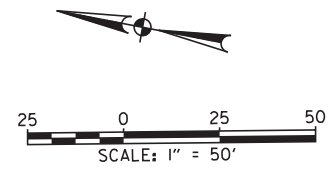
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Robert.Darrington

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	13	34
SPECIAL DETAILS						



SAND BAG DITCH CHECKS	(E-5)
FILTER SOCK	-----

SITE 20
PULASKI COUNTY SPECIAL DETAILS

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FILTER SOCK LOCATIONS

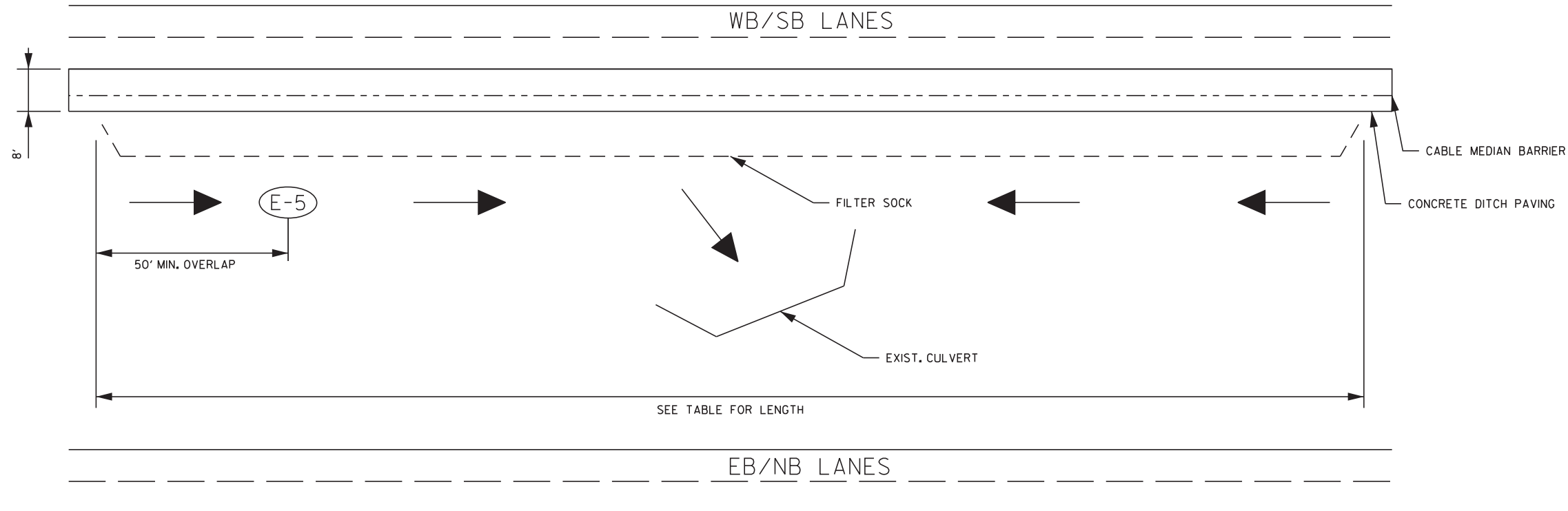
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18.907	18.957	1	HEMPSTEAD	EXIST. CULVERT	250'
19.836	20.039	1	HEMPSTEAD	CONCRETE DITCH PAVING	1070'
19.839	20.039	1	HEMPSTEAD	CONCRETE DITCH PAVING	1055'
20.180	20.246	1	HEMPSTEAD	EXIST. CULVERT	350'
20.365	20.413	1	HEMPSTEAD	BRIDGE END	250'
20.433	20.418	1	HEMPSTEAD	BRIDGE END	250'
20.530	20.568	1	HEMPSTEAD	EXIST. CULVERT	200'
20.871	20.919	1	HEMPSTEAD	EXIST. CULVERT	250'
22.099	22.194	1	HEMPSTEAD	EXIST. CULVERT	500'
22.099	22.194	1	HEMPSTEAD	EXIST. CULVERT	500'
22.714	22.771	1	HEMPSTEAD	EXIST. CULVERT	300'
22.885	22.330	2	HEMPSTEAD	BRIDGE END	250'
29.797	29.845	2	HEMPSTEAD	EXIST. CULVERT	250'
29.987	30.035	2	HEMPSTEAD	EXIST. CULVERT	250'
30.082	30.130	2	HEMPSTEAD	EXIST. CULVERT	250'
30.365	30.413	2	HEMPSTEAD	EXIST. CULVERT	250'
30.740	30.788	2	HEMPSTEAD	BRIDGE END	250'
30.898	30.946	2	HEMPSTEAD	BRIDGE END	250'
31.048	31.096	2	HEMPSTEAD	EXIST. CULVERT	250'
42.320	42.368	3	NEVADA	EXIST. CULVERT	250'
44.039	44.134	4	NEVADA	EXIST. CULVERT	500'
55.868	55.944	5	CLARK	EXIST. CULVERT	400'
56.674	56.722	6	CLARK	EXIST. CULVERT	250'
57.810	57.905	7	CLARK	EXIST. CULVERT	500'
101.402	101.497	8	HOT SPRING	EXIST. CULVERT	500'
105.279	105.306	9	HOT SPRING	BRIDGE END	150'
105.325	105.373	9	HOT SPRING	EXIST. CULVERT	250'
106.017	106.112	10	SALINE	EXIST. CULVERT	500'
137.284	137.332	13	PULASKI	BRIDGE END	250'
137.478	137.525	13	PULASKI	EXIST. CULVERT	400'
123.812	123.755	15	FAULKNER	EXIST. CULVERT	300'
132.764	132.812	16	FAULKNER	EXIST. CULVERT	250'
132.007	132.959	16	FAULKNER	EXIST. CULVERT	250'
133.097	133.145	16	FAULKNER	EXIST. CULVERT	250'
142.649	142.744	18	PULASKI	EXIST. CULVERT	500'
143.154	143.201	18	PULASKI	EXIST. CULVERT	250'
146.042	146.137	20	PULASKI	EXIST. CULVERT	500'
146.249	146.297	20	PULASKI	EXIST. CULVERT	250'
146.322	146.37	20	PULASKI	BRIDGE END	250'
146.388	146.436	20	PULASKI	EXIST. CULVERT	250'
146.581	146.629	20	PULASKI	EXIST. CULVERT	250'
146.770	146.818	20	PULASKI	EXIST. CULVERT	250'
146.873	146.921	20	PULASKI	EXIST. CULVERT	250'
147.046	147.094	20	PULASKI	EXIST. CULVERT	250'
147.153	147.248	20	PULASKI	EXIST. CULVERT	500'

FILTER SOCK LOCATIONS (CONT.)

BEGIN LM	END LM	SITE	COUNTY	DESCRIPTION	LENGTH
147.453	147.501	21	PULASKI	EXIST. CULVERT	250'
147.616	147.664	21	PULASKI	EXIST. CULVERT	250'
147.847	147.895	21	PULASKI	EXIST. CULVERT	250'
149.506	149.554	23	PULASKI	EXIST. CULVERT	250'
149.781	149.876	23	PULASKI	CONCRETE DITCH PAVING	500'
155.897	155.944	25	PULASKI	BRIDGE END	250'
166.534	166.582	26	LONOKE	EXIST. CULVERT	250'
166.636	166.684	26	LONOKE	BRIDGE END	250'
166.755	166.803	26	LONOKE	BRIDGE END	250'
166.819	166.867	26	LONOKE	EXIST. CULVERT	250'
170.900	170.948	27	LONOKE	EXIST. CULVERT	250'
2.057	2.076	28	LONOKE	BRIDGE END	100'



DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
09-22-22		6	ARK.	012388	14	34
SPECIAL DETAILS						



FILTER SOCK DETAIL
N.T.S.

FILTER SOCK SPECIAL DETAIL

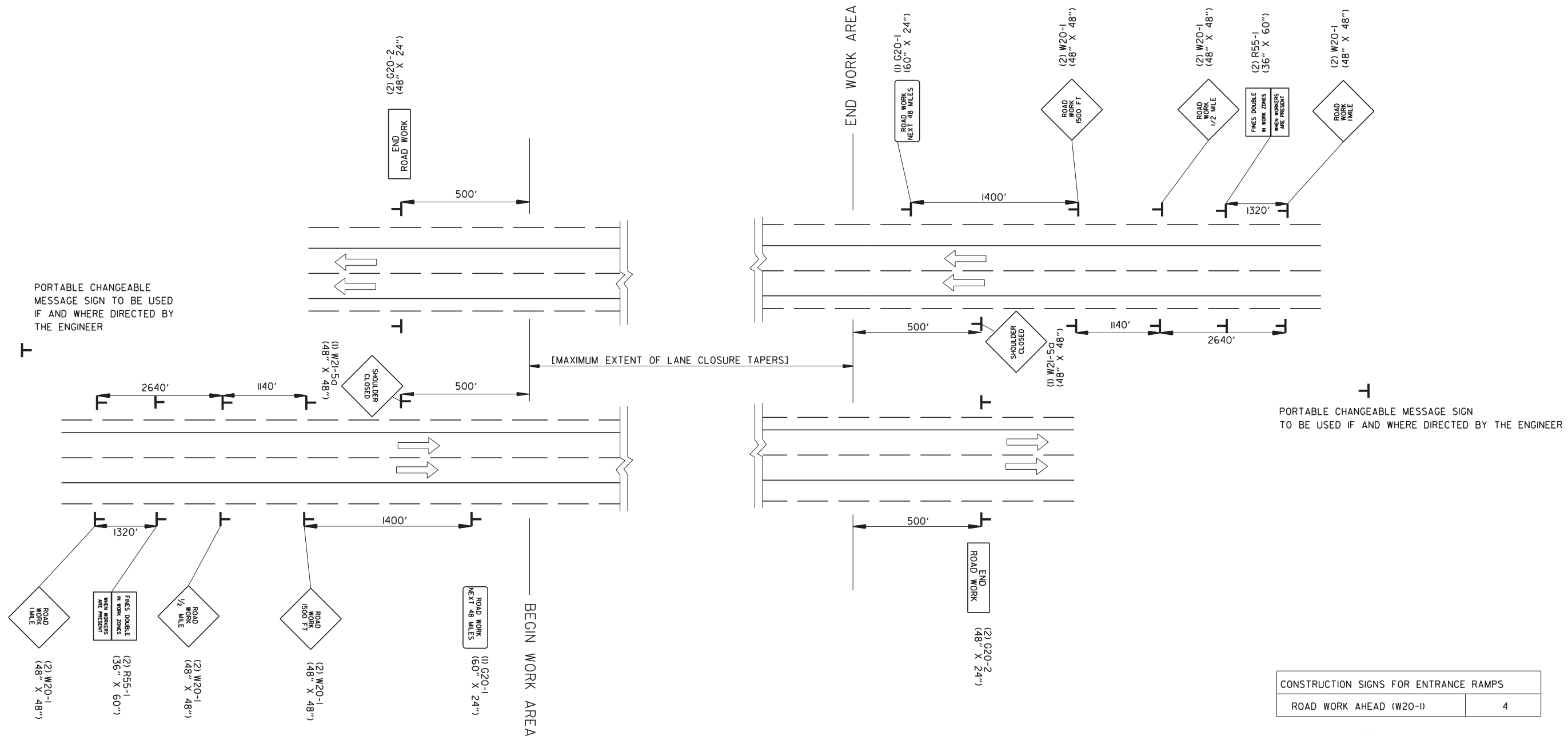
SAND BAG DITCH CHECK (E-5)

9:42:55 AM
 5/19/2022
 Robert.Darrington
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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	15	34
MAINTENANCE OF TRAFFIC DETAILS						

NOTE :
 W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS
 TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS
 AS WORKING AREA SHIFTS.



PORTABLE CHANGEABLE MESSAGE SIGN TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER

PORTABLE CHANGEABLE MESSAGE SIGN TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER

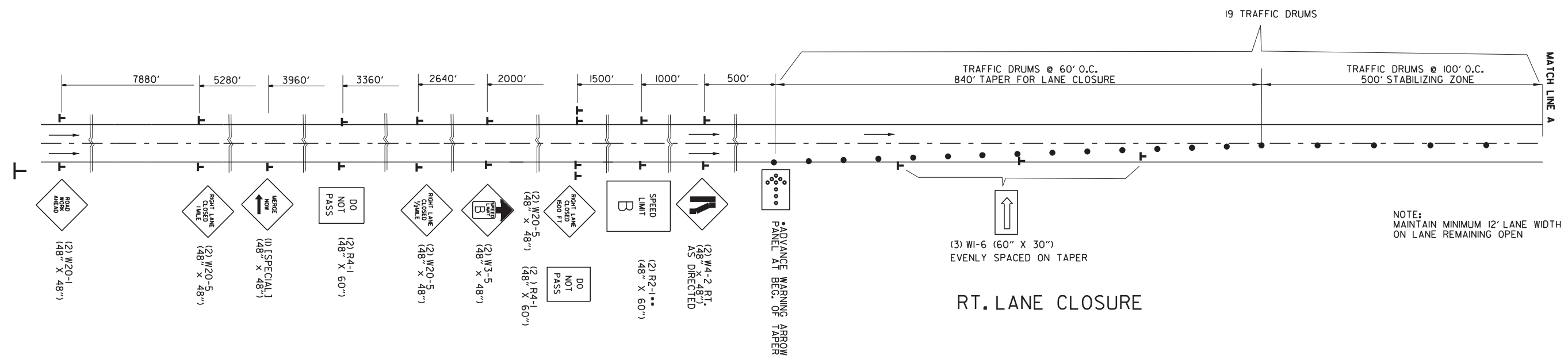
NOTE :
 W20-1 (VARIOUS DISTANCE) ADVANCE SIGNS
 TO BE REPLACED AS NEEDED BY EQUIVALENT W20-5 SIGNS
 AS WORKING AREA SHIFTS.

ADVANCE SIGNS AT BEGINNING AND END OF JOB
 ALL STAGES

MAINTENANCE OF TRAFFIC DETAILS
 ADVANCE SIGNS AT SITE ENDS

9:42:29 AM 5/19/2022 Robert.Darrington c:\bms\bridge\farmer-pw\robert.darrington\brldge\farmer.com\dms\0164\02388_M01_02.dgn

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	16	34
MAINTENANCE OF TRAFFIC DETAILS						

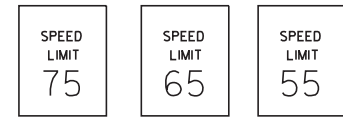


•PORTABLE CHANGEABLE MESSAGE SIGN TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER

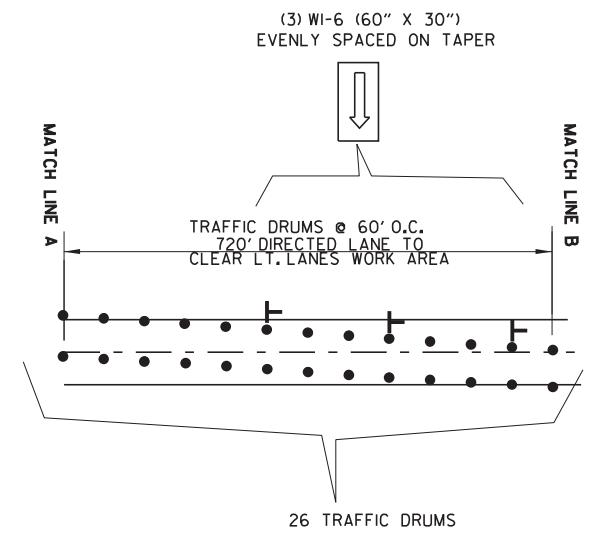
•TO BE PLACED AT MINIMUM OFFSET OF 12' FROM EDGE OF THROUGH LANE OF TRAFFIC, OR FURTHER IF PRACTICAL.

SPEED LIMIT SIGNS ARE ALSO PROVIDED FOR PLACEMENT PAST ENTRANCE RAMP WITHIN THE WORK ZONE.

•• SPEED LIMIT SIGNS SHALL MATCH PERMANENT SPEED LIMIT.



NOTE:
 FOR SITES 1-10 AND 15, USE R2-(175) FOR 75 MPH TRAVEL SPEED.
 FOR SITES 13, 26 AND 27, USE R2-(165) FOR 65 MPH TRAVEL SPEED.
 FOR SITE 28, USE R2-(155) FOR 55 MPH TRAVEL SPEED.

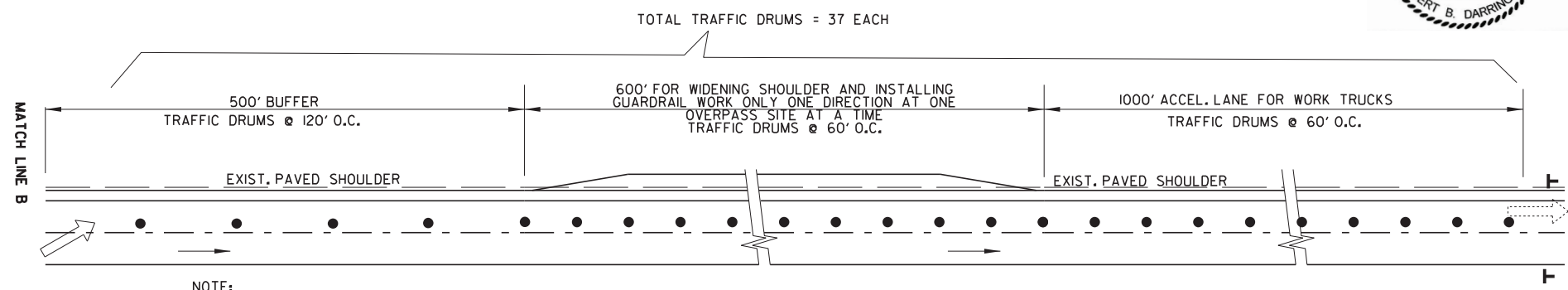


DIVERSION FOR LT. LANE WORK ZONE

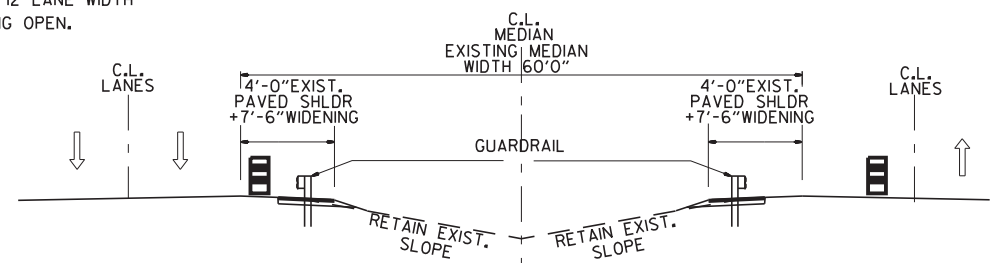
MAINTENANCE OF TRAFFIC DETAILS LANE CLOSURE

C:\BMS\BRIDGEFARMER-PM\ROBERT_DARRINGTON\BRI\DEFARMER.COM\DWG\10164\1012388_NOT_03.DWG 10/6/2022 11:31:52 AM ROBERT_DARRINGTON

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
09-22-22		6	ARK.	012388	17	34
MAINTENANCE OF TRAFFIC DETAILS						



NOTE:
MAINTAIN MINIMUM 12' LANE WIDTH
ON LANE REMAINING OPEN.



MOVABLE WORK ZONE FOR GUARDRAIL INSTALLATION

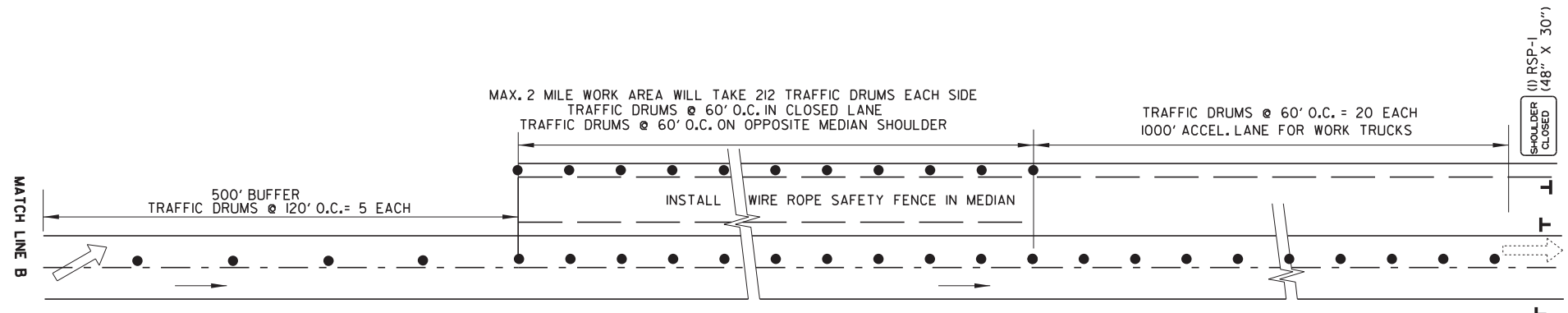


(2) R2-1
(48" X 60")

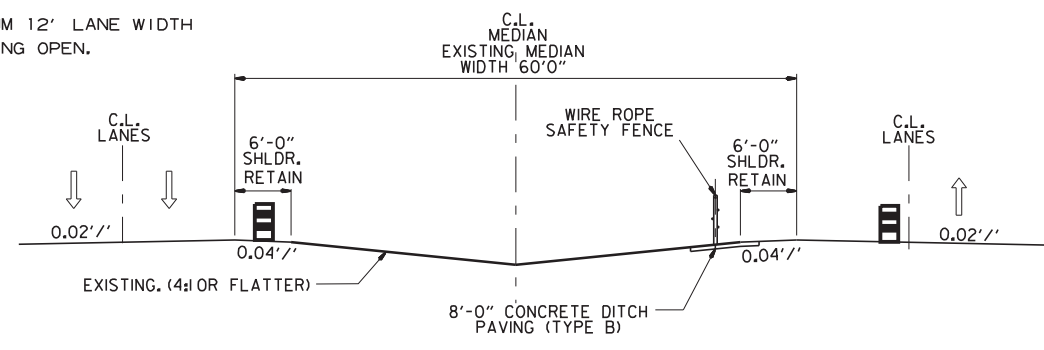
NOTE: REFER TO SP-MAINTENANCE OF TRAFFIC FOR LANE CLOSURE LIMITATIONS AND RESTRICTIONS. QUANTITY OF TRAFFIC DRUMS PROVIDED IN THE CONTRACT IS THE MAXIMUM NUMBER REQUIRED FOR ONE LANE CLOSURE.

→ WORK VEHICLE ENTRY LOCATION

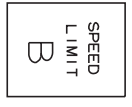
→ WORK VEHICLE EXIT LOCATION



NOTE:
MAINTAIN MINIMUM 12' LANE WIDTH
ON LANE REMAINING OPEN.

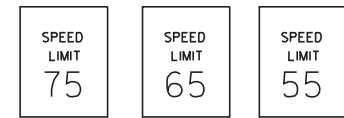


MOVABLE WORK ZONE FOR WRSF INSTALLATION



(2) R2-1
(48" X 60")

*SPEED LIMIT SIGNS SHALL MATCH PERMANENT SPEED LIMIT.



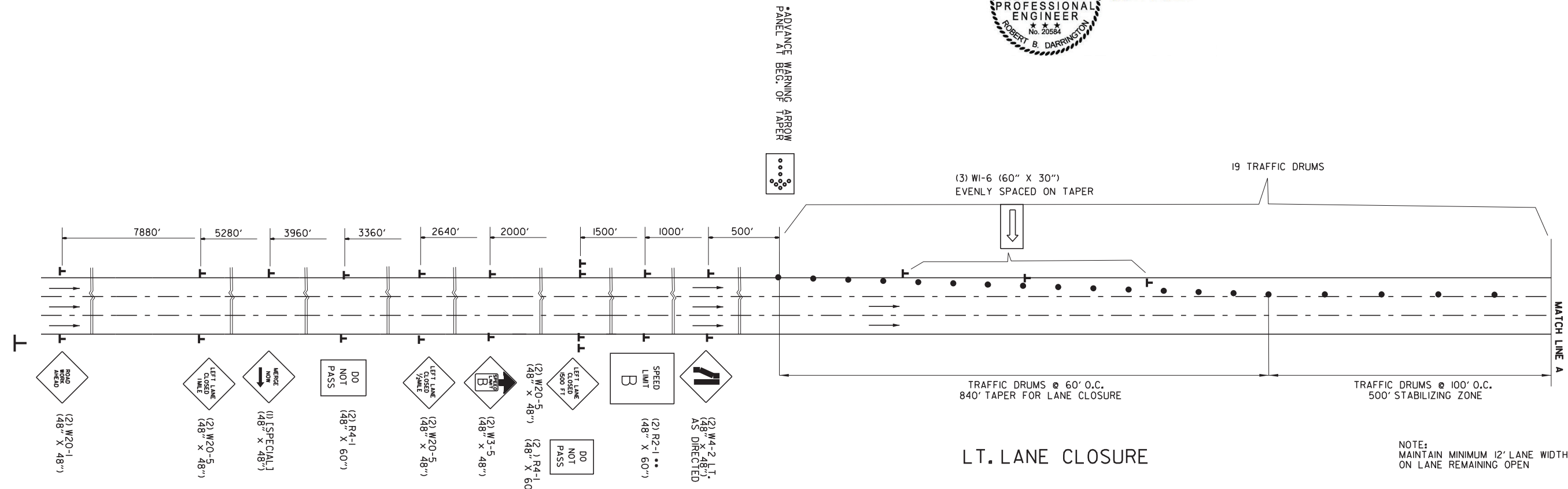
NOTE:
FOR SITES 1-10 AND 15, USE R2-1(75) FOR 75 MPH TRAVEL SPEED.
FOR SITES 13, 26 AND 27, USE R2-1(65) FOR 65 MPH TRAVEL SPEED.
FOR SITE 28, USE R2-1(55) FOR 55 MPH TRAVEL SPEED.

NOTE: CONTRACTOR MUST UTILIZE ENTRY/EXIT LOCATION AS SHOWN ON THE PLANS.

MAINTENANCE OF TRAFFIC DETAILS FOR TWO LANE ROAD WORK AREAS

5/19/2022 9:43:37 AM Robert.Darrington c:\bms\bridge\former-pw\robert.darrington\bridge\former.com\dms\0164\02388_M01_04.dgn

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	18	34
MAINTENANCE OF TRAFFIC DETAILS						

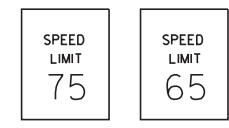


•PORTABLE CHANGEABLE MESSAGE SIGN TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER

•TO BE PLACED AT MINIMUM OFFSET OF 12' FROM EDGE OF THROUGH LANE OF TRAFFIC, OR FURTHER IF PRACTICAL.

SPEED LIMIT SIGNS ARE ALSO PROVIDED FOR PLACEMENT PAST ENTRANCE RAMP WITHIN THE WORK ZONE.

•• SPEED LIMIT SIGNS SHALL MATCH PERMANENT SPEED LIMIT.



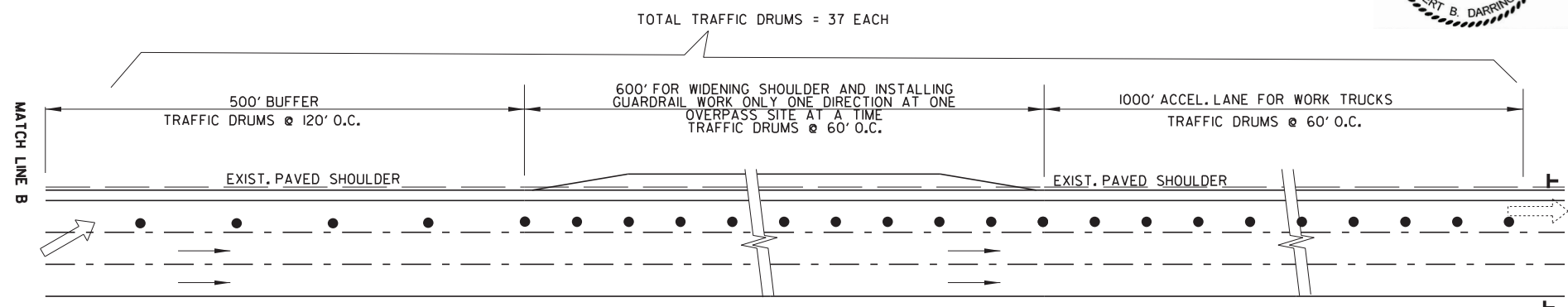
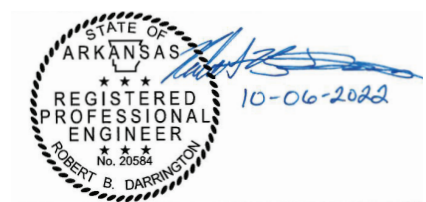
NOTE: FOR SITES 16-20, USE R2-(K75) FOR 75 MPH TRAVEL SPEED. FOR SITES 11-12 AND 21-24, USE R2-(K65) FOR 65 MPH TRAVEL SPEED.

NOTE: MAINTAIN MINIMUM 12' LANE WIDTH ON LANE REMAINING OPEN

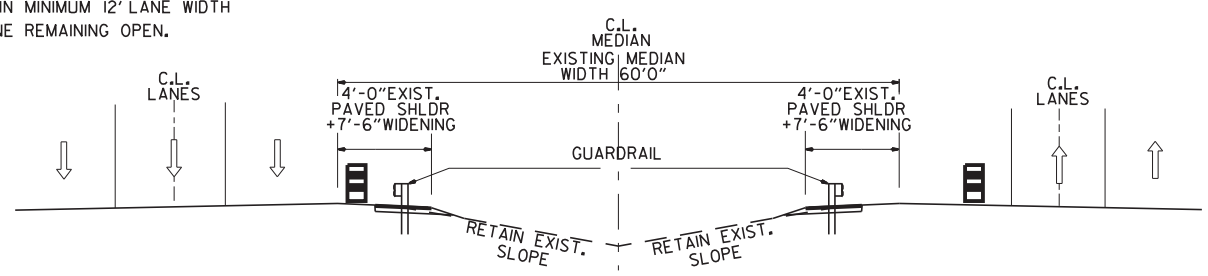
MAINTENANCE OF TRAFFIC DETAILS LEFT LANE CLOSURE

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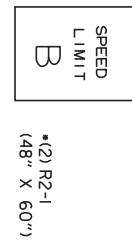
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
09-22-22		6	ARK.	012388	19	34
MAINTENANCE OF TRAFFIC DETAILS						



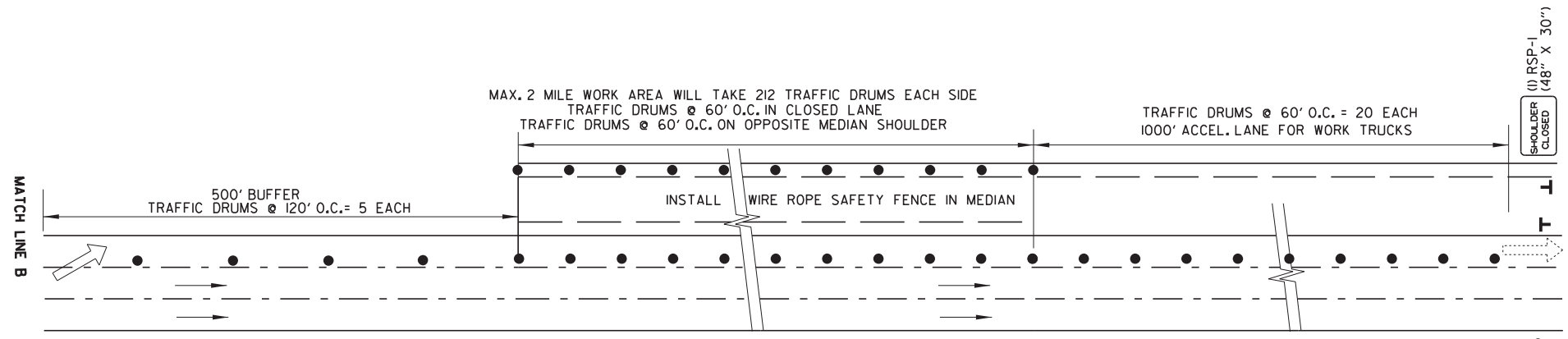
NOTE: MAINTAIN MINIMUM 12' LANE WIDTH ON LANE REMAINING OPEN.



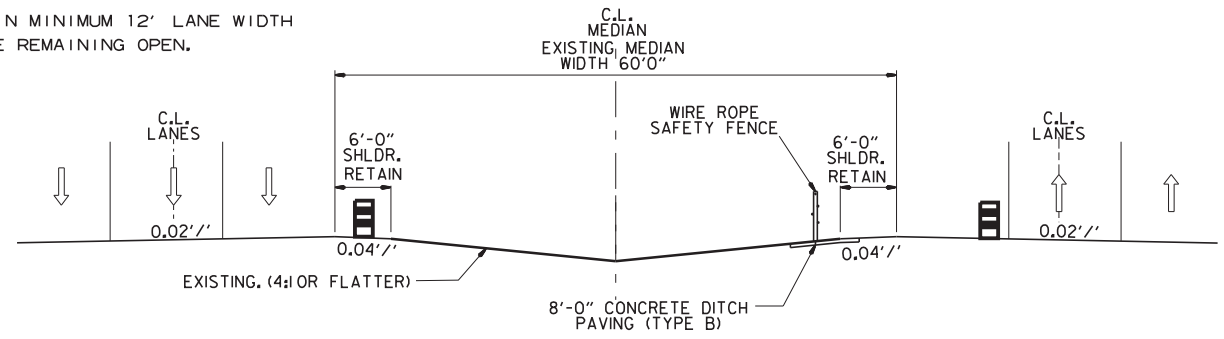
MOVABLE WORK ZONE FOR GUARDRAIL INSTALLATION



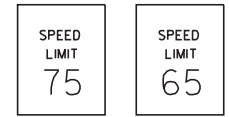
NOTE: REFER TO SP-MAINTENANCE OF TRAFFIC FOR LANE CLOSURE LIMITATIONS AND RESTRICTIONS. QUANTITY OF TRAFFIC DRUMS PROVIDED IN THE CONTRACT IS THE MAXIMUM NUMBER REQUIRED FOR ONE LANE CLOSURE.



NOTE: MAINTAIN MINIMUM 12' LANE WIDTH ON LANE REMAINING OPEN.

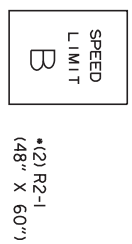


MOVABLE WORK ZONE FOR WRSF INSTALLATION



*SPEED LIMIT SIGNS SHALL MATCH PERMANENT SPEED LIMIT.

NOTE: FOR SITES 16-20, USE R2-(75) FOR 75 MPH TRAVEL SPEED. FOR SITES 11-12 AND 21-24, USE R2-(65) FOR 65 MPH TRAVEL SPEED.



NOTE: CONTRACTOR MUST UTILIZE ENTRY/EXIT LOCATION AS SHOWN ON THE PLANS.

MAINTENANCE OF TRAFFIC DETAILS FOR THREE LANE ROAD WORK AREAS

C:\BMS\BRIDGE\FARMER - PW\ROBERT_DARRINGTON\BRI\DEFARMER.COM\DW\101621\012388_QUANTITIES_01.DGN 10/6/2022 11:32:03 AM ROBERT_DARRINGTON

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
06-14-22	09-22-22	6	ARK.	012388	20	34
QUANTITIES						



ADVANCE WARNING SIGNS AND DEVICES

SIGN NUMBER	DESCRIPTION	SIGN SIZE	STAGE 1	MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED		TRAFFIC DRUMS	• ADVANCE WARNING ARROW PANEL	• PORTABLE CHANGEABLE MESSAGE SIGN
					EACH	NO.			
W20-1	ROAD WORK 1500 FT.	48"x48"	4	4	4	64.0			
W20-1	ROAD WORK AHEAD	48"x48"	6	6	6	96.0			
G20-2	END ROAD WORK	48"x24"	4	4	4	32.0			
G20-1	ROAD WORK NEXT xx MILES	60"x24"	2	2	2	20.0			
W20-1	ROAD WORK 1/2 MILE	48"x48"	4	4	4	64.0			
W20-1	ROAD WORK 1 MILE	48"x48"	4	4	4	64.0			
W1-6	LARGE ARROW	60"x30"	6	6	6	75.0			
R2-1	SPEED LIMIT 55MPH	48"x60"	8	8	8	160.0			
R2-1	SPEED LIMIT 65MPH	48"x60"	8	8	8	160.0			
R2-1	SPEED LIMIT 75MPH	48"x60"	8	8	8	160.0			
R4-1	DO NOT PASS	24"x30"	4	4	4	20.0			
R55-1	FINES DOUBLES IN WORK ZONES WHEN WORKERS ARE PRESENT	36"x60"	4	4	4	60.0			
W3-5	SPEED LIMIT CHANGE	48"x48"	2	2	2	32.0			
W4-2 RT	RIGHT LANE ENDS	48"x48"	2	2	2	32.0			
W4-2 LT	LEFT LANE ENDS	48"x48"	2	2	2	32.0			
W20-5	RIGHT LANE CLOSED 1500 FT	48"x48"	2	2	2	32.0			
W20-5	RIGHT LANE CLOSED 1/2 MILE	48"x48"	2	2	2	32.0			
W20-5	RIGHT LANE CLOSED 1MILE	48"x48"	2	2	2	32.0			
W21-5a	RIGHT SHOULDER CLOSED	36"x36"	2	2	2	18.0			
W20-5	RIGHT LANE CLOSED 1500 FT	48"x48"	2	2	2	32.0			
W20-5	RIGHT LANE CLOSED 1/2 MILE	48"x48"	2	2	2	32.0			
W20-5	RIGHT LANE CLOSED 1MILE	48"x48"	2	2	2	32.0			
W20-5A	LEFT SHOULDER CLOSED	36"x36"	2	2	2	18.0			
SPECIAL	MERGE NOW LT	48"x48"	1	1	1	16.0			
SPECIAL	MERGE NOW RT	48"x48"	1	1	1	16.0			
	TRAFFIC DRUMS		494	494			494		
	ADVANCE WARNING ARROW PANEL		1	1			196		
	PORTABLE CHANGEABLE MESSAGE SIGN		3	3				84	
TOTALS:						1331.0	494	196	84

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

• QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.
TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

GUARDRAIL

LOG MILE	LOG MILE	SITE NUMBER	COUNTY	LOCATION	GUARDRAIL (TYPE A)	THRE BEAM GUARDRAIL TERMINAL	GUARDRAIL TERMINAL (TYPE 2)	TERMINAL ANCHOR POST (TYPE 1)
					LIN. FT.			
						EACH		
18,926	18,940	SITE 1	HEMPSTEAD	LT OF C.L. I-30, SEC. 12	50			
19,857	19,957	SITE 1		RT OF C.L. I-30, SEC. 12, COUNTY RD 189 OVERPASS	450			
19,904	20,004	SITE 1		LT OF C.L. I-30, SEC. 12, COUNTY RD 189 OVERPASS	450			
22,152	22,252	SITE 1		RT OF C.L. I-30, SEC. 12, COUNTY RD 2 OVERPASS	450			
22,195	22,279	SITE 1		LT OF C.L. I-30, SEC. 12, COUNTY RD 2 OVERPASS	375			
HEMPSTEAD COUNTY SUBTOTALS:					1775		4	5
42,376	42,481	SITE 3	NEVADA	RT OF C.L. I-30, SEC. 13, HWY. 332 OVERPASS	475			
42,399	42,532	SITE 3		LT OF C.L. I-30, SEC. 13, HWY. 332 OVERPASS	625			
43,864	43,969	SITE 4		RT OF C.L. I-30, SEC. 13, HWY. 371 OVERPASS	475			
43,889	43,994	SITE 4		LT OF C.L. I-30, SEC. 13, HWY. 371 OVERPASS	475			
44,095	44,120	SITE 4		LT OF C.L. I-30, SEC. 13, HWY. 371 OVERPASS	50			
NEVADA COUNTY SUBTOTALS:					2100		5	5
55,886	55,907	SITE 5	CLARK	RT OF C.L. I-30, SEC. 14	50			
56,160	56,185	SITE 5		LT OF C.L. I-30, SEC. 14	50			
56,820	56,845	SITE 6		LT OF C.L. I-30, SEC. 14	50			
57,824	57,848	SITE 7		RT OF C.L. I-30, SEC. 14	50			
CLARK COUNTY SUBTOTALS:					200		4	4
101,416	101,439	SITE 8	HOT SPRING	RT OF C.L. I-30, SEC. 21	50			
HOT SPRING COUNTY SUBTOTALS:					50			
136,340	136,449	SITE 12	PULASKI	RT OF C.L. I-30, SEC. 23	525			
136,335	136,488	SITE 12		LT OF C.L. I-30, SEC. 23	725			
142,237	142,317	SITE 18		RT OF C.L. I-40, SEC. 33, COUNTY RD. 365 OVERPASS	350			
142,254	142,363	SITE 18		LT OF C.L. I-40, SEC. 33, COUNTY RD. 365 OVERPASS	500			
143,172	143,196	SITE 18		LT OF C.L. I-40, SEC. 33, COUNTY RD. 365 OVERPASS	50			
147,217	147,238	SITE 20		LT OF C.L. I-40, SEC. 33	50			
PULASKI COUNTY SUBTOTALS:					2200	1	5	6
120,669	120,692	SITE 15	FAULKNER	RT OF C.L. I-40, SEC. 32	50			
123,725	123,740	SITE 15		LT OF C.L. I-40, SEC. 32	50			
FAULKNER COUNTY SUBTOTALS:					100		2	2
TOTALS:					6425	1	21	23

EARTHWORK

LOG MILE	LOG MILE	SITE NUMBER	COUNTY	LOCATION / DESCRIPTION	UNCLASSIFIED EXCAVATION CU. YD.	COMPACTED EMBANKMENT CU. YD.
18,913	18,940	SITE 1	HEMPSTEAD	GUARDRAIL WIDENING, LT OF C.L. I-30, SEC. 12	50	25
19,849	19,965	SITE 1		GUARDRAIL WIDENING, RT OF C.L. I-30, SEC. 12, COUNTY RD 189 OVERPASS	165	60
19,893	20,015	SITE 1		GUARDRAIL WIDENING, LT OF C.L. I-30, SEC. 12, COUNTY RD 189 OVERPASS	180	75
22,146	22,263	SITE 1		GUARDRAIL WIDENING, RT OF C.L. I-30, SEC. 12, COUNTY RD 2 OVERPASS	170	50
22,184	22,281	SITE 1		GUARDRAIL WIDENING, LT OF C.L. I-30, SEC. 12, COUNTY RD 2 OVERPASS	145	50
HEMPSTEAD COUNTY SUBTOTALS:					710	260
42,368	42,489	SITE 3	NEVADA	GUARDRAIL WIDENING, RT OF C.L. I-30, SEC. 13, HWY. 332 OVERPASS	165	55
42,388	42,541	SITE 3		GUARDRAIL WIDENING, LT OF C.L. I-30, SEC. 13, HWY. 332 OVERPASS	200	100
43,861	43,972	SITE 4		GUARDRAIL WIDENING, RT OF C.L. I-30, SEC. 13, HWY. 371 OVERPASS	165	60
43,883	44,005	SITE 4		GUARDRAIL WIDENING, LT OF C.L. I-30, SEC. 13, HWY. 371 OVERPASS	180	100
44,095	44,120	SITE 4		GUARDRAIL WIDENING, LT OF C.L. I-30, SEC. 13	50	
NEVADA COUNTY SUBTOTALS:					760	315
55,878	55,918	SITE 5	CLARK	GUARDRAIL WIDENING, RT OF C.L. I-30, SEC. 14	50	35
56,149	56,193	SITE 5		GUARDRAIL WIDENING, LT OF C.L. I-30, SEC. 14	50	35
56,809	56,853	SITE 6		GUARDRAIL WIDENING, LT OF C.L. I-30, SEC. 14	50	35
57,816	57,859	SITE 7		GUARDRAIL WIDENING, RT OF C.L. I-30, SEC. 14	50	30
CLARK COUNTY SUBTOTALS:					200	135
101,408	101,450	SITE 8	HOT SPRING	GUARDRAIL WIDENING, LT OF C.L. I-30, SEC. 21	50	25
HOT SPRING COUNTY SUBTOTALS:					50	25
136,340	136,460	SITE 12	PULASKI	GUARDRAIL WIDENING, RT OF C.L. I-30, SEC. 23	160	90
136,327	136,459	SITE 12		GUARDRAIL WIDENING, LT OF C.L. I-30, SEC. 23	245	95
142,236	142,318	SITE 18		GUARDRAIL WIDENING, RT OF C.L. I-40, SEC. 33, HWY. 365 OVERPASS	130	45
142,253	142,367	SITE 18		GUARDRAIL WIDENING, LT OF C.L. I-40, SEC. 33, HWY. 365 OVERPASS	180	75
143,163	143,204	SITE 18		GUARDRAIL WIDENING, LT OF C.L. I-40, SEC. 33	50	30
147,206	147,246	SITE 20		GUARDRAIL WIDENING, LT OF C.L. I-40, SEC. 33	50	40
PULASKI COUNTY SUBTOTALS:					815	375
120,661	120,703	SITE 15	FAULKNER	GUARDRAIL WIDENING, RT OF C.L. I-40, SEC. 32	50	35
123,725	123,748	SITE 15		GUARDRAIL WIDENING, LT OF C.L. I-40, SEC. 32	50	25
FAULKNER COUNTY SUBTOTALS:					100	60
TOTALS:					2635	1170

NOTE: EARTHWORK QUANTITIES SHALL BE PAID AS PLAN QUANTITY.

REMOVAL AND DISPOSAL OF ITEMS

LOG MILE	LOG MILE	SITE NUMBER	COUNTY	LOCATION	GUARDRAIL	IMPACT ATTENUATION BARRIER	GUARDRAIL TERMINAL (TYPE 2)	TERMINAL ANCHOR POST	WRSF
					LIN. FT.	EACH	EACH	EACH	LIN. FT.
18,935	18,940	SITE 1	HEMPSTEAD	LT OF C.L. I-30, SEC. 12					
19,921	19,942	SITE 1		I-30 MEDIAN, SEC. 12, COUNTY RD 189.		2			
22,221	22,233	SITE 1		I-30 MEDIAN, SEC. 12, COUNTY RD 2.		2			
HEMPSTEAD COUNTY SUBTOTALS:						4		1	
136,340	136,350	SITE 12	PULASKI	C.L. I-30, SEC. 23					
136,340	136,412	SITE 12		RT OF C.L. I-30, SEC. 23					380
136,400	136,444	SITE 12		RT OF C.L. I-30, SEC. 23	155				
136,433	136,477	SITE 12		LT OF C.L. I-30, SEC. 23	155				
PULASKI COUNTY SUBTOTALS:					310	1	2	2	380
TOTALS:					310	5	2	3	380

WIRE ROPE SAFETY FENCE (POST REPAIR)

LOG MILE	LOG MILE	WRSF (POST REPAIR)
		EACH
• ENTIRE PROJECT		50
TOTAL:		50

• QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.
TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

QUANTITIES

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	21	34
QUANTITIES						

EROSION CONTROL

LOG MILE	LOG MILE	SITE NUMBER	COUNTY	LOCATION	PERMANENT EROSION CONTROL					TEMPORARY EROSION CONTROL								
					SEEDING	LIME	MULCH COVER	WATER	SECOND SEEDING APPLICATION	TEMPORARY SEEDING	MULCH COVER	WATER	FILTER SOCK (18")	SAND BAG DITCH CHECKS	DROP INLET SILT FENCE	SILT FENCE	*SEDIMENT REMOVAL & DISPOSAL	
					ACRE	TON	ACRE	M.GAL.	ACRE	ACRE	ACRE	M.GAL.	(E-3) LIN. FT.	(E-5) BAG	(E-7) LIN. FT.	(E-11) LIN. FT.	CU. YD.	
18.765	22.765	SITE 1	HEMPSTEAD	C.L. I-30, SEC. 12, EAST OF FULTON									4950	836	150	105	94	
20.246		SITE 1		I-30, SEC. 12, MEDIAN														4
18.926	18.940	SITE 1		LT OF C.L. I-30, SEC. 12	0.03	0.06	0.03	3.1	0.03	0.03	0.03	0.03	0.6					
19.857	19.957	SITE 1		RT OF C.L. I-30, SEC. 12, COUNTY RD 189 OVERPASS	0.25	0.50	0.25	25.5	0.25	0.25	0.25	0.25	5.1					
19.904	20.004	SITE 1		LT OF C.L. I-30, SEC. 12, COUNTY RD 189 OVERPASS	0.25	0.50	0.25	25.5	0.25	0.25	0.25	0.25	5.1					
22.152	22.252	SITE 1		RT OF C.L. I-30, SEC. 12, COUNTY RD 2 OVERPASS	0.25	0.50	0.25	25.5	0.25	0.25	0.25	0.25	5.1					
22.195	22.279	SITE 1		LT OF C.L. I-30, SEC. 12, COUNTY RD 2 OVERPASS	0.24	0.48	0.24	24.5	0.24	0.24	0.24	0.24	4.9					
29.285		SITE 2		I-30, SEC. 12, MEDIAN													115	5
30.898		SITE 2		I-30, SEC. 12, MEDIAN													105	4
29.285	31.329	SITE 2		C.L. I-30, SEC. 12, NORTH OF HOPE										2000	462			41
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.					0.26	0.52	0.26	26.5	0.26	0.26	0.26	5.3	1740	330	38	81	37	
HEMPSTEAD COUNTY SUBTOTALS:					1.28	2.56	1.28	130.6	1.28	1.28	1.28	26.1	8690	1628	188	406	185	
42.376	42.481	SITE 3	NEVADA	RT OF C.L. I-30, SEC. 13, HWY. 332 OVERPASS	0.26	0.52	0.26	26.5	0.26	0.26	0.26	5.3						
42.399	42.532	SITE 3		LT OF C.L. I-30, SEC. 13, HWY. 332 OVERPASS	0.32	0.64	0.32	32.6	0.32	0.32	0.32	6.5						
42.326	42.540	SITE 3		C.L. I-30, SEC. 13, HWY. 332 OVERPASS									250	88			7	
43.747	44.105	SITE 4		C.L. I-30, SEC. 13, HWY. 371 OVERPASS									500	88			9	
43.864	43.969	SITE 4		RT OF C.L. I-30, SEC. 13, HWY. 371 OVERPASS	0.26	0.52	0.26	26.5	0.26	0.26	0.26	5.3						
43.389	43.994	SITE 4		LT OF C.L. I-30, SEC. 13, HWY. 371 OVERPASS	0.26	0.52	0.26	26.5	0.26	0.26	0.26	5.3						
44.095	44.120	SITE 4	LT OF C.L. I-30, SEC. 13	0.06	0.12	0.06	6.1	0.06	0.06	0.06	1.2							
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.					0.29	0.58	0.29	29.6	0.29	0.29	0.29	5.9	190	44			4	
NEVADA COUNTY SUBTOTALS:					1.45	2.90	1.45	147.9	1.45	1.45	1.45	29.6	940	220			20	
55.886	55.907	SITE 5	CLARK	RT OF C.L. I-30, SEC. 14	0.06	0.12	0.06	6.1	0.06	0.06	0.06	1.2						
56.160	56.185	SITE 5		LT OF C.L. I-30, SEC. 14	0.06	0.12	0.06	6.1	0.06	0.06	0.06	1.2						
55.900	56.170	SITE 5		C.L. I-30, SEC. 14, EAST OF HWY. 51									400	88			8	
56.680	56.830	SITE 6		C.L. I-30, SEC. 14, EAST OF HWY. 51									250	88			7	
56.820	56.845	SITE 6		LT OF C.L. I-30, SEC. 14	0.06	0.12	0.06	6.1	0.06	0.06	0.06	1.2						
57.824	57.848	SITE 7		RT OF C.L. I-30, SEC. 14	0.06	0.12	0.06	6.1	0.06	0.06	0.06	1.2						
57.838	58.011	SITE 7	C.L. I-30, SEC. 14, WEST OF HWY. 182									500	66			8		
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.					0.06	0.12	0.06	6.1	0.06	0.06	0.06	1.2	290	66			6	
CLARK COUNTY SUBTOTALS:					0.30	0.60	0.30	30.6	0.30	0.30	0.30	6.1	1440	308			29	
101.416	101.439	SITE 8	HOT SPRING	RT OF C.L. I-30, SEC. 21	0.06	0.12	0.06	6.1	0.06	0.06	0.06	1.2						
101.430	101.720	SITE 8		C.L. I-30, SEC. 21, WEST OF COUNTY RD 74									500	88			9	
105.343	105.460	SITE 9		C.L. I-30, SEC. 21, EAST OF COUNTY RD 70									400	66			7	
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.					0.02	0.04	0.02	2.0	0.02	0.02	0.02	0.4	230	44			5	
HOT SPRING COUNTY SUBTOTALS:					0.08	0.16	0.08	8.2	0.08	0.08	0.08	1.6	1130	198			21	
105.880	106.105	SITE 10	SALINE	C.L. I-30, SEC. 22, SOUTH OF OLD MILITARY RD									500	88	50		11	
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.													130	22	13		3	
SALINE COUNTY SUBTOTALS:													630	110	63		14	
131.923	132.109	SITE 11	PULASKI	C.L. I-30, SEC. 23, EAST OF N CHICOT RD OVERPASS														
136.340	136.449	SITE 12		RT OF C.L. I-30, SEC. 23	0.26	0.52	0.26	26.5	0.26	0.26	0.26	5.3			66	100		7
136.354	136.488	SITE 12		LT OF C.L. I-30, SEC. 23	0.32	0.64	0.32	32.6	0.32	0.32	0.32	6.5						
136.340	136.801	SITE 12		C.L. I-30, SEC. 23, WEST OF HWY. 367														6
137.312	137.519	SITE 13		C.L. I-30, SEC. 23, EAST OF HWY. 367										500	66	100		12
0.733	0.908	SITE 14		C.L. I-530, SEC. 1, SOUTH OF I-30											66	50		5
142.237	142.317	SITE 18		RT OF C.L. I-40, SEC. 33, COUNTY RD 365 OVERPASS	0.21	0.42	0.21	21.4	0.21	0.21	0.21	4.3						
142.254	142.363	SITE 18		LT OF C.L. I-40, SEC. 33, COUNTY RD 365 OVERPASS	0.28	0.56	0.28	28.6	0.28	0.28	0.28	5.7						
142.236	143.365	SITE 18		C.L. I-40, SEC. 33, HWY. 365 OVERPASS TO WEST WINIFREE CREEK										1000	308	100		28
143.172	143.196	SITE 18		LT OF C.L. I-40, SEC. 33	0.06	0.12	0.06	6.1	0.06	0.06	0.06	1.2						
143.952	144.452	SITE 19		C.L. I-40, SEC. 33, EAST OF COUNTY RD 54 OVERPASS											154	150		13
146.370		SITE 20		I-40, SEC. 33 MEDIAN													100	
146.388		SITE 20		I-40, SEC. 33 MEDIAN													100	
145.861	147.227	SITE 20		C.L. I-40, SEC. 33, NORTH OF WHITE OAK CROSSING TO WEST OF I-430										2750	396	100		50
147.217	147.238	SITE 20		LT OF C.L. I-40, SEC. 33	0.06	0.12	0.06	6.1	0.06	0.06	0.06	1.2						
147.398	148.451	SITE 21		C.L. I-40, SEC. 33, WEST OF I-430 TO EAST OF COUNTY RD 51										1000	308	150		30
148.905	149.124	SITE 22		C.L. I-40, SEC. 33, EAST OF YOUNG RD											88	250		14
149.395	149.890	SITE 23		C.L. I-40, SEC. 33, ARIENE LAMAN DR OVERPASS TO WEST OF MILITARY DR										750	132			14
150.475	150.924	SITE 24		C.L. I-40, SEC. 33 EAST PF MILITARY DR											110	150		11
155.606	155.891	SITE 25	C.L. I-40, SEC. 33, SPRING HILL DR OVERPASS										250	66	50		8	
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.					0.30	0.60	0.30	30.6	0.30	0.30	0.30	6.1	1570	484	300	50	51	
PULASKI COUNTY SUBTOTALS:					1.49	2.98	1.49	152.0	1.49	1.49	1.49	30.4	7820	2376	1500	250	253	
166.470	167.017	SITE 26	LONOKE	C.L. I-40, SEC. 41, WEST OF HWY. 15 OVERPASS									1000	198			19	
170.746	170.979	SITE 27		C.L. I-40, SEC. 41, WEST OF HWY. 571 OVERPASS										250	88		7	
1.125	2.654	SITE 28		C.L. HWY. 5, SEC. 12, SOUTH OF HWY. 89										100	704	1400		85
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.													340	264	350		29	
LONOKE COUNTY SUBTOTALS:													1690	1254	1750		140	
120.669	120.692	SITE 15	FAULKNER	RT OF C.L. I-40, SEC. 32	0.06	0.12	0.06	6.1	0.06	0.06	0.06	1.2						
123.725	123.740	SITE 15		LT OF C.L. I-40, SEC. 32	0.06	0.12	0.06	6.1	0.06	0.06	0.06	1.2						
120.683	123.725	SITE 15		C.L. I-40, SEC. 32, EAST OF CADRON CREEK TO EAST OF HWY. 25 OVERPASS										300	682	100		38
132.693	133.365	SITE 16		C.L. I-40, SEC. 32 WEST OF LAKE CONWAY										750	220			18
133.789	134.101	SITE 17		C.L. I-40, SEC. 32 WEST OF LAKE CONWAY											132	50		8
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.					0.03	0.06	0.03	3.1	0.03	0.03	0.03	0.6	270	264	38		17	
FAULKNER COUNTY SUBTOTALS:					0.15	0.30	0.15	15.3	0.15	0.15	0.15	3.1	1320	1298	188		81	
TOTALS:					4.75	9.50	4.75	484.60	4.75	4.75	4.75	96.90	23660	7392	3689	656	743	

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DATE REVISED	DATE REVISION	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
09-22-22		6	ARK.	012388	22	34
QUANTITIES						

CONCRETE DITCH PAVING (BOX 2 OF 3)

CONCRETE DITCH PAVING (BOX 1 OF 3)

LOG MILE	LOG MILE	SITE NUMBER	COUNTY	LOCATION	LENGTH LIN. FT.	"W" FEET	CONC. DITCH PAVING			SOLID SODDING SQ. YD.	WATER M. GAL.
							(TYPE B)	SO. YD.	M. GAL.		
18.790	18.800	SITE 1	HEMPSTEAD	LT OF C.L. I-30, SEC. 12	52.80	4.00	23.47	11.73	0.15		
18.800	18.806	SITE 1		LT OF C.L. I-30, SEC. 12	31.68	10.00	35.20	7.04	0.09		
18.806	18.816	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	9.00	52.80	11.73	0.15		
18.816	18.905	SITE 1		LT OF C.L. I-30, SEC. 12	469.92	8.00	417.71	104.43	1.32		
18.905	18.915	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	9.00	52.80	11.73	0.15		
18.915	18.924	SITE 1		LT OF C.L. I-30, SEC. 12	47.52	10.00	52.80	10.56	0.13		
18.924	18.986	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	4.00	23.47	11.73	0.15		
18.986	18.995	SITE 1		LT OF C.L. I-30, SEC. 12	47.52	10.00	52.80	10.56	0.13		
18.995	19.005	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	9.00	52.80	11.73	0.15		
19.005	19.883	SITE 1		LT OF C.L. I-30, SEC. 12	4635.84	8.00	4120.75	1030.19	12.98		
19.883	19.893	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	9.00	52.80	11.73	0.15		
19.893	19.902	SITE 1		RT OF C.L. I-30, SEC. 12	47.52	10.00	52.80	10.56	0.13		
20.006	20.015	SITE 1		LT OF C.L. I-30, SEC. 12	47.52	10.00	52.80	10.56	0.13		
20.015	20.025	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	9.00	52.80	11.73	0.15		
20.025	20.246	SITE 1		LT OF C.L. I-30, SEC. 12	1166.88	8.00	1037.23	259.31	3.27		
20.246	20.315	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	4.00	23.47	11.73	0.15		
20.315	20.325	SITE 1		LT OF C.L. I-30, SEC. 12	47.52	10.00	52.80	10.56	0.13		
20.325	20.334	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	9.00	52.80	11.73	0.15		
20.334	20.344	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	9.00	52.80	11.73	0.15		
20.344	20.413	SITE 1		LT OF C.L. I-30, SEC. 12	364.32	8.00	323.84	80.96	1.02		
20.413	20.525	SITE 1		RT OF C.L. I-30, SEC. 12	485.76	8.00	431.79	107.95	1.36		
20.525	22.174	SITE 1		LT OF C.L. I-30, SEC. 12	8664.48	8.00	7701.76	1925.44	24.26		
22.174	22.184	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	9.00	52.80	11.73	0.15		
22.184	22.193	SITE 1		LT OF C.L. I-30, SEC. 12	47.52	10.00	52.80	10.56	0.13		
22.193	22.292	SITE 1		LT OF C.L. I-30, SEC. 12	2339.04	8.00	2079.15	519.79	6.55		
22.292	22.735	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	9.00	52.80	11.73	0.15		
22.735	22.745	SITE 1		LT OF C.L. I-30, SEC. 12	26.40	10.00	29.33	5.87	0.07		
22.745	22.750	SITE 1		LT OF C.L. I-30, SEC. 12	52.80	4.00	23.47	11.73	0.15		
22.750	22.760	SITE 1		RT OF C.L. I-30, SEC. 12	7011.84	8.00	6232.75	1558.19	19.63		
22.760	30.613	SITE 2		RT OF C.L. I-30, SEC. 12	100.32	9.00	100.32	22.29	0.28		
30.613	30.632	SITE 2		RT OF C.L. I-30, SEC. 12	63.36	10.00	70.40	14.08	0.18		
30.632	30.644	SITE 2		RT OF C.L. I-30, SEC. 12	52.80	4.00	23.47	11.73	0.15		
30.644	30.654	SITE 2		RT OF C.L. I-30, SEC. 12	200.64	8.00	178.35	44.59	0.56		
30.654	30.778	SITE 2		RT OF C.L. I-30, SEC. 12	100.32	9.00	100.32	22.29	0.28		
30.778	30.797	SITE 2		RT OF C.L. I-30, SEC. 12	58.08	10.00	64.53	12.91	0.16		
30.797	30.808	SITE 2	RT OF C.L. I-30, SEC. 12	52.80	4.00	23.47	11.73	0.15			
30.808	30.818	SITE 2	RT OF C.L. I-30, SEC. 12	2275.68	8.00	2022.83	505.71	6.37			
30.818	31.329	SITE 2				25826.28	6448.62	81.25			
HEMPSTEAD COUNTY SUBTOTALS:											
42.326	42.379	SITE 3	NEVADA	LT OF C.L. I-30, SEC. 13	279.84	8.00	248.75	62.19	0.78		
42.379	42.388	SITE 3		LT OF C.L. I-30, SEC. 13	47.52	9.00	47.52	10.56	0.13		
42.388	42.397	SITE 3		LT OF C.L. I-30, SEC. 13	47.52	10.00	52.80	10.56	0.13		
42.397	43.869	SITE 4		LT OF C.L. I-30, SEC. 13	644.16	8.00	572.59	143.15	1.80		
43.869	43.878	SITE 4		LT OF C.L. I-30, SEC. 13	47.52	9.00	47.52	10.56	0.13		
43.878	43.887	SITE 4		LT OF C.L. I-30, SEC. 13	47.52	10.00	52.80	10.56	0.13		
43.887	44.005	SITE 4		LT OF C.L. I-30, SEC. 13	47.52	10.00	52.80	10.56	0.13		
44.005	44.015	SITE 4		LT OF C.L. I-30, SEC. 13	52.80	9.00	52.80	11.73	0.15		
44.015	44.075	SITE 4		LT OF C.L. I-30, SEC. 13	316.80	8.00	281.60	70.40	0.89		
44.075	44.084	SITE 4		LT OF C.L. I-30, SEC. 13	47.52	9.00	47.52	10.56	0.13		
44.084	44.093	SITE 4	LT OF C.L. I-30, SEC. 13	47.52	10.00	52.80	10.56	0.13			
NEVADA COUNTY SUBTOTALS:											
55.909	55.918	SITE 5	CLARK	RT OF C.L. I-30, SEC. 14	47.52	10.00	52.80	10.56	0.13		
55.918	55.928	SITE 5		RT OF C.L. I-30, SEC. 14	52.80	9.00	52.80	11.73	0.15		
55.928	55.942	SITE 5		RT OF C.L. I-30, SEC. 14	73.92	8.00	65.71	16.43	0.21		
55.942	56.139	SITE 5		LT OF C.L. I-30, SEC. 14	997.92	8.00	887.04	221.76	2.79		
56.139	56.149	SITE 5		LT OF C.L. I-30, SEC. 14	52.80	9.00	52.80	11.73	0.15		
56.149	56.158	SITE 5		LT OF C.L. I-30, SEC. 14	47.52	10.00	52.80	10.56	0.13		
56.158	56.799	SITE 6		LT OF C.L. I-30, SEC. 14	628.32	8.00	558.51	139.63	1.76		
56.799	56.809	SITE 6		LT OF C.L. I-30, SEC. 14	52.80	9.00	52.80	11.73	0.15		
56.809	56.818	SITE 6		LT OF C.L. I-30, SEC. 14	47.52	10.00	52.80	10.56	0.13		
56.818	57.859	SITE 7		RT OF C.L. I-30, SEC. 14	47.52	10.00	52.80	10.56	0.13		
57.859	57.869	SITE 7	RT OF C.L. I-30, SEC. 14	52.80	9.00	52.80	11.73	0.15			
57.869	58.011	SITE 7	RT OF C.L. I-30, SEC. 14	749.76	8.00	666.45	166.61	2.10			
CLARK COUNTY SUBTOTALS:											
101.441	101.450	SITE 8	HOT SPRING	RT OF C.L. I-30, SEC. 21	47.52	10.00	52.80	10.56	0.13		
101.450	101.460	SITE 8		RT OF C.L. I-30, SEC. 21	52.80	9.00	52.80	11.73	0.15		
101.460	101.691	SITE 8		RT OF C.L. I-30, SEC. 21	1219.68	8.00	1084.16	271.04	3.42		
101.691	101.701	SITE 8		RT OF C.L. I-30, SEC. 21	52.80	9.00	52.80	11.73	0.15		
101.701	101.710	SITE 8		RT OF C.L. I-30, SEC. 21	47.52	10.00	52.80	10.56	0.13		
101.710	101.720	SITE 8		RT OF C.L. I-30, SEC. 21	52.80	4.00	23.47	11.73	0.15		
101.720	105.279	SITE 9	RT OF C.L. I-30, SEC. 21	955.68	8.00	849.49	212.37	2.68			
HOT SPRING COUNTY SUBTOTALS:											
105.932	106.150	SITE 10	SALINE	LT OF C.L. I-30, SEC. 22	1151.04	8.00	1023.15	255.79	3.22		
SALINE COUNTY SUBTOTALS:											

LOG MILE	LOG MILE	SITE NUMBER	COUNTY	LOCATION	LENGTH LIN. FT.	"W" FEET	CONC. DITCH PAVING			SOLID SODDING SQ. YD.	WATER M. GAL.
							(TYPE B)	SO. YD.	M. GAL.		
131.923	131.933	SITE 11	PULASKI	RT OF C.L. I-30, SEC. 23	52.80	4.00	23.47	11.73	0.15		
131.933	131.940	SITE 11		RT OF C.L. I-30, SEC. 23	36.96	10.00	41.07	8.21	0.10		
131.940	131.950	SITE 11		RT OF C.L. I-30, SEC. 23	52.80	9.00	52.80	11.73	0.15		
131.950	132.075	SITE 11		RT OF C.L. I-30, SEC. 23	660.00	8.00	586.67	146.67	1.85		
132.075	132.094	SITE 11		RT OF C.L. I-30, SEC. 23	100.32	9.00	100.32	22.29	0.28		
132.094	132.099	SITE 11		RT OF C.L. I-30, SEC. 23	26.40	10.00	29.33	5.87	0.07		
132.099	132.109	SITE 11		RT OF C.L. I-30, SEC. 23	52.80	4.00	23.47	11.73	0.15		
132.109	136.451	SITE 12		RT OF C.L. I-30, SEC. 23	47.52	10.00	52.80	10.56	0.13		
136.451	136.460	SITE 12		RT OF C.L. I-30, SEC. 23	52.80	9.00	52.80	11.73	0.15		
136.460	136.470	SITE 12		RT OF C.L. I-30, SEC. 23	992.64	8.00	882.35	220.59	2.78		
136.470	136.658	SITE 12		RT OF C.L. I-30, SEC. 23	52.80	9.00	52.80	11.73	0.15		
136.658	136.668	SITE 12		RT OF C.L. I-30, SEC. 23	47.52	10.00	52.80	10.56	0.13		
136.668	136.677	SITE 12		RT OF C.L. I-30, SEC. 23	52.80	4.00	23.47	11.73	0.15		
136.677	136.687	SITE 12		RT OF C.L. I-30, SEC. 23	52.80	4.00	23.47	11.73	0.15		
136.687	136.707	SITE 12		RT OF C.L. I-30, SEC. 23	21.12	10.00	23.47	4.69	0.06		
136.707	136.711	SITE 12		RT OF C.L. I-30, SEC. 23	52.80	9.00	52.80	11.73	0.15		
136.711	136.721	SITE 12		RT OF C.L. I-30, SEC. 23	237.60	8.00	211.20	52.80	0.67		
136.721	136.766	SITE 12		RT OF C.L. I-30, SEC. 23	100.32	9.00	100.32	22.29	0.28		
136.766	136.785	SITE 12		RT OF C.L. I-30, SEC. 23	31.68	10.00	35.20	7.04	0.09		
136.785	136.791	SITE 12		RT OF C.L. I-30, SEC. 23	52.80	4.00	23.47	11.73	0.15		
136.791	137.519	SITE 13		RT OF C.L. I-30, SEC. 23	1240.80	8.00	1102.93	275.73	3.47		
137.519	0.733	SITE 14		RT OF C.L. I-530, SEC. 1	52.80	4.00	23.47	11.73	0.15		
0.733	0.743	SITE 14		RT OF C.L. I-530, SEC. 1	36.96	10.00	41.07	8.21	0.10		
0.743	0.750	SITE 14		RT OF C.L. I-530, SEC. 1	100.32	9.00	100.32	22.29	0.28		
0.750	0.769	SITE 14		RT OF C.L. I-530, SEC. 1	607.20	8.00	539.73	134.93	1.70		
0.769	0.884	SITE 14		RT OF C.L. I-530, SEC. 1	52.80	9.00	52.80	11.73	0.15		
0.884	0.894	SITE 14		RT OF C.L. I-530, SEC. 1	21.12	10.00	23.47	4.69	0.06		
0.894	0.898	SITE 14		RT OF C.L. I-530, SEC. 1	52.80	4.00	23.47	11.73	0.15		
0.898	0.908	SITE 14		RT OF C.L. I-530, SEC. 1	47.52	10.00	52.80	10.56	0.13		
0.908	142.365	SITE 18		LT OF C.L. I-40, SEC. 33	52.80	9.00	52.80	11.73	0.15		
142.365	142.374	SITE 18		LT OF C.L. I-40, SEC.							

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
06-14-22		6	ARK.	012388	23	34
09-22-22		QUANTITIES				



CONCRETE DITCH PAVING (BOX 3 OF 3)

LOG MILE	LOG MILE	SITE NUMBER	COUNTY	LOCATION	LENGTH LIN. FT.	"W" FEET	CONC. DITCH PAVING (TYPE B)	SOLID SODDING	WATER	
							SQ. YD.	SQ. YD.	M. GAL.	
1.125	1.446	SITE 28	LONOKE	RT OF C.L. HWY-5, SEC. 12	1694.88	8.00	1506.56	376.64	4.75	
1.470	1.690	SITE 28		LT OF C.L. HWY-5, SEC. 12	1161.60	8.00	1032.53	258.13	3.25	
1.706	1.982	SITE 28		RT OF C.L. HWY-5, SEC. 12	1457.28	8.00	1295.36	323.84	4.08	
1.995	2.076	SITE 28		RT OF C.L. HWY-5, SEC. 12	427.68	8.00	380.16	95.04	1.20	
2.184	2.243	SITE 28		LT OF C.L. HWY-5, SEC. 12	311.52	8.00	276.91	69.23	0.87	
2.256	2.475	SITE 28		RT OF C.L. HWY-5, SEC. 12	1156.32	8.00	1027.84	256.96	3.24	
2.499	2.654	SITE 28		LT OF C.L. HWY-5, SEC. 12	818.40	8.00	727.47	181.87	2.29	
166.470	166.684	SITE 26		LT OF C.L. I-40, SEC. 41	1129.92	8.00	1004.37	251.09	3.16	
166.755	167.017	SITE 26		RT OF C.L. I-40, SEC. 41	1383.36	8.00	1229.65	307.41	3.87	
170.746	170.998	SITE 27		LT OF C.L. I-40, SEC. 41	1330.56	8.00	1182.72	295.68	3.73	
170.998	171.008	SITE 27		LT OF C.L. I-40, SEC. 41	52.80	9.00	52.80	11.73	0.15	
171.008	171.017	SITE 27		LT OF C.L. I-40, SEC. 41	47.52	10.00	52.80	10.56	0.13	
171.017	171.027	SITE 27		LT OF C.L. I-40, SEC. 41	52.80	4.00	23.47	11.73	0.15	
LONOKE COUNTY SUBTOTALS:							9792.64	2449.91	30.87	
120.694	120.703	SITE 15		FAULKNER	RT OF C.L. I-40, SEC. 32	47.52	10.00	52.80	10.56	0.13
120.703	120.713	SITE 15	RT OF C.L. I-40, SEC. 32		52.80	9.00	52.80	11.73	0.15	
120.713	123.118	SITE 15	RT OF C.L. I-40, SEC. 32		12698.40	8.00	11287.47	2821.87	35.56	
123.126	123.704	SITE 15	LT OF C.L. I-40, SEC. 32		3051.84	8.00	2712.75	678.19	8.55	
123.704	123.714	SITE 15	LT OF C.L. I-40, SEC. 32		52.80	9.00	52.80	11.73	0.15	
123.714	123.723	SITE 15	LT OF C.L. I-40, SEC. 32		47.52	10.00	52.80	10.56	0.13	
132.693	133.269	SITE 16	RT OF C.L. I-40, SEC. 32		3041.28	8.00	2703.36	675.84	8.52	
133.269	133.279	SITE 16	RT OF C.L. I-40, SEC. 32		52.80	9.00	52.80	11.73	0.15	
133.279	133.289	SITE 16	RT OF C.L. I-40, SEC. 32		52.80	10.00	58.67	11.73	0.15	
133.289	133.299	SITE 16	RT OF C.L. I-40, SEC. 32		52.80	4.00	23.47	11.73	0.15	
133.304	133.314	SITE 16	RT OF C.L. I-40, SEC. 32		52.80	4.00	23.47	11.73	0.15	
133.314	133.324	SITE 16	RT OF C.L. I-40, SEC. 32		52.80	10.00	58.67	11.73	0.15	
133.324	133.334	SITE 16	RT OF C.L. I-40, SEC. 32		52.80	9.00	52.80	11.73	0.15	
133.334	133.365	SITE 16	LT OF C.L. I-40, SEC. 32		163.68	8.00	145.49	36.37	0.46	
133.789	134.103	SITE 17	LT OF C.L. I-40, SEC. 32		1657.92	8.00	1473.71	368.43	4.64	
FAULKNER COUNTY SUBTOTALS:							18803.86	4695.66	59.17	
TOTALS:							87671.06	21896.51	275.89	

BASIS OF ESTIMATE:
WATER.....12.6 GAL. / SQ. YD. OF SOLID SODDING.

• AT WRSF TRANSITION AREAS

TRENCHING AND SHOULDER PREPARATION

LOG MILE	LOG MILE	SITE NUMBER	COUNTY	LOCATION	TRENCHING AND SHOULDER PREPARATION STATION
18.915	18.940	SITE 1	HEMPSTEAD	WESTBOUND I-30, SEC. 12	1
19.849	19.965	SITE 1		EASTBOUND I-30, SEC. 12	6
19.893	20.015	SITE 1		WESTBOUND I-30, SEC. 12	6
22.144	22.263	SITE 1		EASTBOUND I-30, SEC. 12	6
22.184	22.287	SITE 1		WESTBOUND I-30, SEC. 12	5
HEMPSTEAD COUNTY SUBTOTAL:					24
42.368	42.489	SITE 3	NEVADA	EASTBOUND I-30, SEC. 13	6
42.388	42.540	SITE 3		WESTBOUND I-30, SEC. 13	8
43.856	43.977	SITE 4		EASTBOUND I-30, SEC. 13	6
43.878	44.005	SITE 4		WESTBOUND I-30, SEC. 13	7
44.084	44.128	SITE 4		WESTBOUND I-30, SEC. 13	2
NEVADA COUNTY SUBTOTAL:					29
55.878	55.918	SITE 5	CLARK	EASTBOUND I-30, SEC. 14	2
56.149	56.193	SITE 5		WESTBOUND I-30, SEC. 14	2
56.809	56.853	SITE 6		WESTBOUND I-30, SEC. 14	2
57.816	57.859	SITE 7		EASTBOUND I-30, SEC. 14	2
CLARK COUNTY SUBTOTAL:					8
101.408	101.450	SITE 8	HOT SPRING	EASTBOUND I-30, SEC. 21	2
HOT SPRING COUNTY SUBTOTAL:					2
136.327	136.496	SITE 12	PULASKI	WESTBOUND I-30, SEC. 23	9
136.340	136.460	SITE 12		EASTBOUND I-30, SEC. 23	6
142.229	142.325	SITE 18		EASTBOUND I-40, SEC. 33	5
142.246	142.374	SITE 18		WESTBOUND I-40, SEC. 33	7
143.161	143.204	SITE 18		WESTBOUND I-40, SEC. 33	2
147.206	147.246	SITE 20		WESTBOUND I-40, SEC. 33	2
PULASKI COUNTY SUBTOTAL:					31
120.661	120.703	SITE 15	FAULKNER	EASTBOUND I-40, SEC. 32	2
123.714	123.748	SITE 15		WESTBOUND I-40, SEC. 32	2
FAULKNER COUNTY SUBTOTAL:					4
TOTAL:					98

WIRE ROPE SAFETY FENCE

LOG MILE	LOG MILE	SITE NUMBER	COUNTY	LOCATION	WIRE ROPE SAFETY FENCE	WRSF ANCHOR*	WRSF MAINTENANCE MATERIALS	
					LIN. FT.	EACH	LUMP SUM	
18.790	18.936	SITE 1	HEMPSTEAD	LT OF C.L. I-30, SEC. 12	771	2		
18.976	19.914	SITE 1		LT OF C.L. I-30, SEC. 12	4953	2		
19.995	20.246	SITE 1		LT OF C.L. I-30, SEC. 12	1326	2		
20.315	20.413	SITE 1		LT OF C.L. I-30, SEC. 12	518	2		
20.433	20.525	SITE 1		RT OF C.L. I-30, SEC. 12	486	2		
20.533	22.203	SITE 1		LT OF C.L. I-30, SEC. 12	8818	2		
22.292	22.760	SITE 1		LT OF C.L. I-30, SEC. 12	2472	2		
29.285	30.654	SITE 2		RT OF C.L. I-30, SEC. 12	7229	2		
30.740	30.818	SITE 2		RT OF C.L. I-30, SEC. 12	412	2		
30.898	31.329	SITE 2		RT OF C.L. I-30, SEC. 12	2276	2		
HEMPSTEAD COUNTY SUBTOTALS:					29261	20		
42.326	42.408	SITE 3		NEVADA	LT OF C.L. I-30, SEC. 13	433	2	
43.747	43.898	SITE 4	LT OF C.L. I-30, SEC. 13		798	2		
43.985	44.105	SITE 4	LT OF C.L. I-30, SEC. 13		634	2		
NEVADA COUNTY SUBTOTALS:					1865	6		
55.900	55.942	SITE 5	CLARK	RT OF C.L. I-30, SEC. 14	222	2		
55.950	56.170	SITE 5		LT OF C.L. I-30, SEC. 14	1162	2		
56.680	56.830	SITE 6		LT OF C.L. I-30, SEC. 14	792	2		
57.838	58.011	SITE 7		RT OF C.L. I-30, SEC. 14	914	2		
CLARK COUNTY SUBTOTALS:					3090	8		
101.430	101.720	SITE 8	HOT SPRING	RT OF C.L. I-30, SEC. 21	1532	2		
105.279	105.460	SITE 9		RT OF C.L. I-30, SEC. 21	956	2		
HOT SPRING COUNTY SUBTOTALS:					2488	4		
105.932	106.160	SITE 10	SALINE	LT OF C.L. I-30, SEC. 22	1204	1		
SALINE COUNTY SUBTOTALS:					1204	1		
131.923	132.109	SITE 11	PULASKI	RT OF C.L. I-30, SEC. 23	983	2		
136.440	136.687	SITE 12		RT OF C.L. I-30, SEC. 23	1305	2		
136.697	136.801	SITE 12		RT OF C.L. I-30, SEC. 23	550	2		
137.284	137.519	SITE 13		RT OF C.L. I-30, SEC. 23	1241	2		
0.733	0.908	SITE 14		LT OF C.L. I-530, SEC. 1	924	2		
142.354	142.572	SITE 18		LT OF C.L. I-40, SEC. 33	1152	2		
142.609	143.181	SITE 18		LT OF C.L. I-40, SEC. 33	3021	2		
143.212	143.265	SITE 18		LT OF C.L. I-40, SEC. 33	280	2		
143.952	144.348	SITE 19		RT OF C.L. I-40, SEC. 33	2091	2		
144.386	144.452	SITE 19		RT OF C.L. I-40, SEC. 33	349	2		
145.861	146.370	SITE 20		LT OF C.L. I-40, SEC. 33	2688	2		
146.388	146.459	SITE 20		RT OF C.L. I-40, SEC. 33	375	2		
146.501	146.762	SITE 20		RT OF C.L. I-40, SEC. 33	1379	2		
146.770	147.227	SITE 20		LT OF C.L. I-40, SEC. 33	2413	2		
147.398	148.024	SITE 21		LT OF C.L. I-40, SEC. 33	3306	2		
148.066	148.348	SITE 21	LT OF C.L. I-40, SEC. 33	1489	2			
148.384	148.451	SITE 21	LT OF C.L. I-40, SEC. 33	354	2			
148.905	149.124	SITE 22	RT OF C.L. I-40, SEC. 33	1157	2			
149.405	149.890	SITE 23	RT OF C.L. I-40, SEC. 33	2561	2			
150.475	150.924	SITE 24	LT OF C.L. I-40, SEC. 33	2371	2			
155.600	155.944	SITE 25	LT OF C.L. I-40, SEC. 33	1817	2			
PULASKI COUNTY SUBTOTALS:					31806	42		
1.125	1.446	SITE 28	LONOKE	RT OF C.L. HWY-5, SEC. 12	1695	2		
1.470	1.690	SITE 28		LT OF C.L. HWY-5, SEC. 12	1162	2		
1.706	1.982	SITE 28		RT OF C.L. HWY-5, SEC. 12	1458	2		
1.995	2.076	SITE 28		RT OF C.L. HWY-5, SEC. 12	428	2		
2.184	2.243	SITE 28		LT OF C.L. HWY-5, SEC. 12	312	2		
2.256	2.475	SITE 28		RT OF C.L. HWY-5, SEC. 12	1157	2		
2.499	2.654	SITE 28		LT OF C.L. HWY-5, SEC. 12	819	2		
166.470	166.684	SITE 26		LT OF C.L. I-40, SEC. 41	1130	2		
166.755	167.017	SITE 26		RT OF C.L. I-40, SEC. 41	1384	2		
170.746	171.027	SITE 27		LT OF C.L. I-40, SEC. 41	1484	2		
LONOKE COUNTY SUBTOTALS:					11029	20		
120.683	123.118	SITE 15		FAULKNER	RT OF C.L. I-40, SEC. 32	12857	2	
123.126	123.735	SITE 15	LT OF C.L. I-40, SEC. 32		3216	2		
132.693	133.299	SITE 16	RT OF C.L. I-40, SEC. 32		3200	2		
133.304	133.365	SITE 16	LT OF C.L. I-40, SEC. 32		323	2		
133.789	134.115	SITE 17	RT OF C.L. I-40, SEC. 32		1722	2		
FAULKNER COUNTY SUBTOTALS:					21318	10		
TOTALS:					102061	111	1.00	

• THIS ITEM SHOWN FOR INFORMATION ONLY

QUANTITIES

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DATE REVISED	DATE REVISION	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
09-22-22		6	ARK.	012388	24	34
QUANTITIES						

BASE AND SURFACING (BOX 1 OF 2)

LOG MILE	LOG MILE	SITE NUMBER	COUNTY	LOCATION	LENGTH FEET	AGGREGATE BASE COURSE (CLASS 7)		ACHM SURFACE COURSE (1/2")			
						TON / STATION	TON	AVG. WID. FEET	SO.YD.	POUND / SO.YD.	PG 64-22 TON
ADDITIONAL FOR GUARDRAIL WIDENING											
18.915	18.924	SITE 1	HEMPSTEAD	LT OF C.L. 1-30, SEC.12, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52
18.924	18.940	SITE 1		LT OF C.L. 1-30, SEC.12, OVERLAP WIDENING	85.00	35.25	29.96	10.00	94.44	220.00	10.39
19.849	19.855	SITE 1		RT OF C.L. 1-30, SEC.12, SHLDR TRANSITION, COUNTY RD 189 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
19.855	19.959	SITE 1		RT OF C.L. 1-30, SEC.12, NORMAL WIDENING, COUNTY RD 189 OVERPASS	545.00	29.50	160.78	7.50	454.17	220.00	49.96
19.959	19.965	SITE 1		RT OF C.L. 1-30, SEC.12, SHLDR TRANSITION, COUNTY RD 189 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
19.893	19.902	SITE 1		LT OF C.L. 1-30, SEC.12, SHLDR TRANSITION, COUNTY RD 189 OVERPASS	48.00	25.50	12.24	6.00	32.00	220.00	3.52
19.902	19.916	SITE 1		LT OF C.L. 1-30, SEC.12, OVERLAP WIDENING, COUNTY RD 189 OVERPASS	70.00	35.25	24.68	10.00	77.78	220.00	8.56
19.916	19.919	SITE 1		LT OF C.L. 1-30, SEC. 12, TAPER TRANSITION, COUNTY RD 189 OVERPASS	15.00	32.50	4.88	8.75	14.58	220.00	1.60
19.919	19.990	SITE 1		LT OF C.L. 1-30, SEC.12, NORMAL WIDENING, COUNTY RD 189 OVERPASS	375.00	29.50	110.63	7.50	312.50	220.00	34.38
19.990	19.993	SITE 1		LT OF C.L. 1-30, SEC. 12, TAPER TRANSITION, COUNTY RD 189 OVERPASS	15.00	32.50	4.88	8.75	14.58	220.00	1.60
19.993	20.006	SITE 1		LT OF C.L. 1-30, SEC.12, OVERLAP WIDENING, COUNTY RD 189 OVERPASS	70.00	35.25	24.68	10.00	77.78	220.00	8.56
20.006	20.015	SITE 1		LT OF C.L. 1-30, SEC.12, SHLDR TRANSITION, COUNTY RD 189 OVERPASS	48.00	25.50	12.24	6.00	32.00	220.00	3.52
22.144	22.150	SITE 1		RT OF C.L. 1-30, SEC.12, SHLDR TRANSITION, COUNTY RD 2 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
22.150	22.254	SITE 1		RT OF C.L. 1-30, SEC.12, NORMAL WIDENING, COUNTY RD 2 OVERPASS	545.00	29.50	160.78	7.50	454.17	220.00	49.96
22.254	22.263	SITE 1		RT OF C.L. 1-30, SEC.12, SHLDR TRANSITION, COUNTY RD 2 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
22.184	22.193	SITE 1		LT OF C.L. 1-30, SEC.12, SHLDR TRANSITION, COUNTY RD 2 OVERPASS	48.00	25.50	12.24	6.00	32.00	220.00	3.52
22.193	22.205	SITE 1		LT OF C.L. 1-30, SEC.12, OVERLAP WIDENING, COUNTY RD 2 OVERPASS	70.00	35.25	24.68	10.00	77.78	220.00	8.56
22.205	22.208	SITE 1		LT OF C.L. 1-30, SEC. 12, TAPER TRANSITION, COUNTY RD 2 OVERPASS	15.00	32.50	4.88	8.75	14.58	220.00	1.60
22.208	22.281	SITE 1		LT OF C.L. 1-30, SEC.12, NORMAL WIDENING, COUNTY RD 2 OVERPASS	385.00	29.50	113.58	7.50	320.83	220.00	35.29
22.281	22.287	SITE 1		LT OF C.L. 1-30, SEC.12, SHLDR TRANSITION, COUNTY RD 2 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
ADDITIONAL FOR GUARDRAIL REMOVAL											
18.935	18.940	SITE 1	HEMPSTEAD	LT OF C.L. 1-30, SEC. 12				1.00			
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.						38.00			107.00		12.00
HEMPSTEAD COUNTY (DISTRICT 3) SUBTOTALS:							788.27		2235.29		246.14
ADDITIONAL FOR GUARDRAIL WIDENING											
42.368	42.374	SITE 3	NEVADA	RT OF C.L. 1-30, SEC.13, SHLDR TRANSITION, HWY. 332 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
42.374	42.483	SITE 3		RT OF C.L. 1-30, SEC.13, NORMAL WIDENING, HWY. 332 OVERPASS	570.00	29.50	168.15	7.50	475.00	220.00	52.25
42.483	42.489	SITE 3		RT OF C.L. 1-30, SEC.13, SHLDR TRANSITION, HWY. 332 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
42.388	42.397	SITE 3		LT OF C.L. 1-30, SEC.13, SHLDR TRANSITION, HWY. 332 OVERPASS	48.00	25.50	12.24	6.00	32.00	220.00	3.52
42.397	42.410	SITE 3		LT OF C.L. 1-30, SEC.13, OVERLAP WIDENING, HWY. 332 OVERPASS	70.00	35.25	24.68	10.00	77.78	220.00	8.56
42.410	42.413	SITE 3		LT OF C.L. 1-30, SEC.13, TAPER TRANSITION, HWY. 332 OVERPASS	15.00	32.50	4.88	8.75	14.58	220.00	1.60
42.413	42.534	SITE 3		LT OF C.L. 1-30, SEC.13, NORMAL WIDENING, HWY. 332 OVERPASS	635.00	29.50	187.33	7.50	529.17	220.00	58.21
42.534	42.540	SITE 3		LT OF C.L. 1-30, SEC.13, SHLDR TRANSITION, HWY. 332 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
43.856	43.862	SITE 4		RT OF C.L. 1-30, SEC.13, SHLDR TRANSITION, HWY. 371 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
43.862	43.971	SITE 4		RT OF C.L. 1-30, SEC.13, NORMAL WIDENING, HWY. 371 OVERPASS	570.00	29.50	168.15	7.50	475.00	220.00	52.25
43.971	43.977	SITE 4		RT OF C.L. 1-30, SEC.13, SHLDR TRANSITION, HWY. 371 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
43.878	43.887	SITE 4		LT OF C.L. 1-30, SEC.13, SHLDR TRANSITION, HWY. 371 OVERPASS	48.00	25.50	12.24	6.00	32.00	220.00	3.52
43.887	43.900	SITE 4		LT OF C.L. 1-30, SEC.13, OVERLAP WIDENING, HWY. 371 OVERPASS	70.00	35.25	24.68	10.00	77.78	220.00	8.56
43.900	43.903	SITE 4		LT OF C.L. 1-30, SEC.13, TAPER TRANSITION, HWY. 371 OVERPASS	15.00	32.50	4.88	8.75	14.58	220.00	1.60
43.903	43.980	SITE 4		LT OF C.L. 1-30, SEC.13, NORMAL WIDENING, HWY. 371 OVERPASS	400.00	29.50	118.00	7.50	333.33	220.00	36.67
43.980	43.983	SITE 4		LT OF C.L. 1-30, SEC.13, TAPER TRANSITION, HWY. 371 OVERPASS	15.00	32.50	4.88	8.75	14.58	220.00	1.60
43.983	43.996	SITE 4		LT OF C.L. 1-30, SEC.13, OVERLAP WIDENING, HWY. 371 OVERPASS	70.00	35.25	24.68	10.00	77.78	220.00	8.56
43.996	44.005	SITE 4		LT OF C.L. 1-30, SEC.13, SHLDR TRANSITION, HWY. 371 OVERPASS	48.00	25.50	12.24	6.00	32.00	220.00	3.52
44.084	44.093	SITE 4		LT OF C.L. 1-30, SEC.13, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52
44.093	44.107	SITE 4		LT OF C.L. 1-30, SEC.13, OVERLAP WIDENING	70.00	35.25	24.68	10.00	77.78	220.00	8.56
44.107	44.110	SITE 4	LT OF C.L. 1-30, SEC.13, TAPER TRANSITION	15.00	32.50	4.88	8.75	14.58	220.00	1.60	
44.110	44.122	SITE 4	LT OF C.L. 1-30, SEC.13, NORMAL WIDENING	60.00	29.50	17.70	7.50	50.00	220.00	5.50	
44.122	44.128	SITE 4	LT OF C.L. 1-30, SEC.13, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92	
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.						44.00			124.00		14.00
NEVADA COUNTY (DISTRICT 3) SUBTOTALS:							913.61		2588.46		285.12
ADDITIONAL FOR GUARDRAIL WIDENING											
55.878	55.884	SITE 5	CLARK	RT OF C.L. 1-30, SEC.14, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92
55.884	55.895	SITE 5		RT OF C.L. 1-30, SEC.14, NORMAL WIDENING	60.00	29.50	17.70	7.50	50.00	220.00	5.50
55.895	55.898	SITE 5		RT OF C.L. 1-30, SEC.14, TAPER TRANSITION	15.00	32.50	4.88	8.75	14.58	220.00	1.60
55.898	55.909	SITE 5		RT OF C.L. 1-30, SEC.14, OVERLAP WIDENING	70.00	35.25	24.68	10.00	77.78	220.00	8.56
55.909	55.918	SITE 5		RT OF C.L. 1-30, SEC.14, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52
56.149	56.158	SITE 5		LT OF C.L. 1-30, SEC.14, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52
56.158	56.172	SITE 5		LT OF C.L. 1-30, SEC.14, OVERLAP WIDENING	70.00	35.25	24.68	10.00	77.78	220.00	8.56
56.172	56.175	SITE 5		LT OF C.L. 1-30, SEC.14, TAPER TRANSITION	15.00	32.50	4.88	8.75	14.58	220.00	1.60
56.175	56.187	SITE 5		LT OF C.L. 1-30, SEC.14, NORMAL WIDENING	60.00	29.50	17.70	7.50	50.00	220.00	5.50
56.187	56.193	SITE 5		LT OF C.L. 1-30, SEC.14, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92
56.809	56.818	SITE 6		LT OF C.L. 1-30, SEC.14, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52
56.818	56.832	SITE 6		LT OF C.L. 1-30, SEC.14, OVERLAP WIDENING	70.00	35.25	24.68	10.00	77.78	220.00	8.56
56.832	56.835	SITE 6		LT OF C.L. 1-30, SEC.14, TAPER TRANSITION	15.00	32.50	4.88	8.75	14.58	220.00	1.60
56.835	56.847	SITE 6		LT OF C.L. 1-30, SEC.14, NORMAL WIDENING	60.00	29.50	17.70	7.50	50.00	220.00	5.50
56.847	56.853	SITE 6		LT OF C.L. 1-30, SEC.14, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92
57.816	57.822	SITE 7	RT OF C.L. 1-30, SEC.14, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92	
57.822	57.833	SITE 7	RT OF C.L. 1-30, SEC.14, NORMAL WIDENING	60.00	29.50	17.70	7.50	50.00	220.00	5.50	
57.833	57.836	SITE 7	RT OF C.L. 1-30, SEC.14, TAPER TRANSITION	15.00	32.50	4.88	8.75	14.58	220.00	1.60	
57.836	57.850	SITE 7	RT OF C.L. 1-30, SEC.14, OVERLAP WIDENING	70.00	35.25	24.68	10.00	77.78	220.00	8.56	
57.850	57.859	SITE 7	RT OF C.L. 1-30, SEC.14, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52	
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.						14.00			39.00		5.00
CLARK COUNTY (DISTRICT 7) SUBTOTALS:							280.72		806.12		89.40
ADDITIONAL FOR GUARDRAIL WIDENING											
101.408	101.414	SITE 8	HOT SPRING	RT OF C.L. 1-30, SEC.21, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92
101.414	101.425	SITE 8		RT OF C.L. 1-30, SEC.21, NORMAL WIDENING	60.00	29.50	17.70	7.50	50.00	220.00	5.50
101.425	101.428	SITE 8		RT OF C.L. 1-30, SEC.21, TAPER TRANSITION	15.00	32.50	4.88	8.75	14.58	220.00	1.60
101.428	101.441	SITE 8		RT OF C.L. 1-30, SEC.21, OVERLAP WIDENING	70.00	35.25	24.68	10.00	77.78	220.00	8.56
101.441	101.450	SITE 8		RT OF C.L. 1-30, SEC.21, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52
ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.						4.00			10.00		2.00
HOT SPRING COUNTY (DISTRICT 6) SUBTOTALS:							70.68		201.78		23.10



QUANTITIES

C:\BMS\BRIDGE\FARMER.COM\DWG\10162\10162-R01\2388_QUANTITIES_06.DGN 10/6/2022 11:32:25 AM ROBERT, DARRINGTON

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS	
06-14-22		6	ARK.	012388	25	34	
09-22-22		QUANTITIES					



BASE AND SURFACING (BOX 2 OF 2)

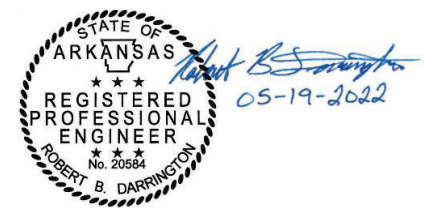
LOG MILE	LOG MILE	SITE NUMBER	COUNTY	LOCATION	LENGTH FEET	AGGREGATE BASE COURSE (CLASS 7)		ACHM SURFACE COURSE (1/2")			
						TON / STATION	TON	AVG. WID. FEET	SO.YD.	POUND / SO.YD.	PG 64-22 TON
ADDITIONAL FOR GUARDRAIL WIDENING											
136.340	136.435	SITE 12	PULASKI	RT OF C.L. I-30, SEC. 23, NORMAL WIDENING	495.00	29.50	146.03	7.50	412.50	220.00	45.38
136.435	136.438	SITE 12		RT OF C.L. I-30, SEC. 23, TAPER TRANSITION	15.00	32.50	4.88	8.75	14.58	220.00	1.60
136.438	136.451	SITE 12		RT OF C.L. I-30, SEC. 23, OVERLAP WIDENING	70.00	35.25	24.68	10.00	77.78	220.00	8.56
136.451	136.460	SITE 12		RT OF C.L. I-30, SEC. 23, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52
136.327	136.333	SITE 12		LT OF C.L. I-30, SEC. 23, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92
136.333	136.490	SITE 12		LT OF C.L. I-30, SEC. 23, NORMAL WIDENING	820.00	29.50	241.90	7.50	683.33	220.00	75.17
136.490	136.496	SITE 12		LT OF C.L. I-30, SEC. 23, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92
142.229	142.235	SITE 18		RT OF C.L. I-40, SEC. 33, SHLDR TRANSITION, COUNTY RD 365 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
142.235	142.319	SITE 18		RT OF C.L. I-40, SEC. 33, NORMAL WIDENING, COUNTY RD 365 OVERPASS	445.00	29.50	131.28	7.50	370.83	220.00	40.79
142.319	142.325	SITE 18		RT OF C.L. I-40, SEC. 33, SHLDR TRANSITION, COUNTY RD 365 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
142.246	142.252	SITE 18		LT OF C.L. I-40, SEC. 33, SHLDR TRANSITION, COUNTY RD 365 OVERPASS	33.00	21.75	7.18	4.75	17.42	220.00	1.92
142.252	142.349	SITE 18		LT OF C.L. I-40, SEC. 33, NORMAL WIDENING, COUNTY RD 365 OVERPASS	510.00	29.50	150.45	7.50	425.00	220.00	46.75
142.349	142.352	SITE 18		LT OF C.L. I-40, SEC. 33, TAPER TRANSITION, COUNTY RD 365 OVERPASS	15.00	32.50	4.88	8.75	14.58	220.00	1.60
142.352	142.365	SITE 18		LT OF C.L. I-40, SEC. 33, OVERLAP WIDENING, COUNTY RD 365 OVERPASS	70.00	35.25	24.68	10.00	77.78	220.00	8.56
142.365	142.374	SITE 18		LT OF C.L. I-40, SEC. 33, SHLDR TRANSITION, COUNTY RD 365 OVERPASS	48.00	25.50	12.24	6.00	32.00	220.00	3.52
143.161	143.170	SITE 18		LT OF C.L. I-40, SEC. 33, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52
143.170	143.183	SITE 18		LT OF C.L. I-40, SEC. 33, OVERLAP WIDENING	70.00	35.25	24.68	10.00	77.78	220.00	8.56
143.183	143.186	SITE 18		LT OF C.L. I-40, SEC. 33, TAPER TRANSITION	15.00	32.50	4.88	8.75	14.58	220.00	1.60
143.186	143.198	SITE 18		LT OF C.L. I-40, SEC. 33, NORMAL WIDENING	60.00	29.50	17.70	7.50	50.00	220.00	5.50
143.198	143.204	SITE 18		LT OF C.L. I-40, SEC. 33, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92
147.206	147.215	SITE 20	LT OF C.L. I-40, SEC. 33, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52	
147.215	147.226	SITE 20	LT OF C.L. I-40, SEC. 33, OVERLAP WIDENING	70.00	35.25	24.68	10.00	77.78	220.00	8.56	
147.226	147.229	SITE 20	LT OF C.L. I-40, SEC. 33, TAPER TRANSITION	15.00	32.50	4.88	8.75	14.58	220.00	1.60	
147.229	147.240	SITE 20	LT OF C.L. I-40, SEC. 33, NORMAL WIDENING	60.00	29.50	17.70	7.50	50.00	220.00	5.50	
147.240	147.246	SITE 20	LT OF C.L. I-40, SEC. 33, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92	
ADDITIONAL FOR GUARDRAIL REMOVAL											
136.400	136.444	SITE 18	PULASKI	RT OF C.L. I-30, SEC. 23			6.00				
136.433	136.477	SITE 18	PULASKI	LT OF C.L. I-30, SEC. 23			6.00				
* ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.						47.00		131.00		15.00	
PULASKI COUNTY (DISTRICT 6) SUBTOTALS:						981.52		2742.04		302.25	
ADDITIONAL FOR GUARDRAIL WIDENING											
120.661	120.667	SITE 15	FAULKNER	RT OF C.L. I-40, SEC. 32, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92
120.667	120.678	SITE 15		RT OF C.L. I-40, SEC. 32, NORMAL WIDENING	60.00	29.50	17.70	7.50	50.00	220.00	5.50
120.678	120.681	SITE 15		RT OF C.L. I-40, SEC. 32, TAPER TRANSITION	15.00	32.50	4.88	8.75	14.58	220.00	1.60
120.681	120.694	SITE 15		RT OF C.L. I-40, SEC. 32, OVERLAP WIDENING	70.00	35.25	24.68	10.00	77.78	220.00	8.56
120.694	120.703	SITE 15		RT OF C.L. I-40, SEC. 32, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52
123.714	123.723	SITE 15		LT OF C.L. I-40, SEC. 32, SHLDR TRANSITION	48.00	25.50	12.24	6.00	32.00	220.00	3.52
123.723	123.727	SITE 15		LT OF C.L. I-40, SEC. 32, OVERLAP WIDENING	70.00	35.25	24.68	10.00	77.78	220.00	8.56
123.727	123.730	SITE 15		LT OF C.L. I-40, SEC. 32, TAPER TRANSITION	15.00	32.50	4.88	8.75	14.58	220.00	1.60
123.730	123.742	SITE 15		LT OF C.L. I-40, SEC. 32, NORMAL WIDENING	60.00	29.50	17.70	7.50	50.00	220.00	5.50
123.742	123.748	SITE 15		LT OF C.L. I-40, SEC. 32, SHLDR TRANSITION	33.00	21.75	7.18	4.75	17.42	220.00	1.92
* ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.						7.00		20.00		3.00	
FAULKNER COUNTY (DISTRICT 8) SUBTOTALS:						140.36		403.56		45.20	
DISTRICT SUBTOTALS											
DISTRICT 3 SUBTOTALS								1701.88		4823.75	
DISTRICT 6 SUBTOTALS								1052.20		2943.82	
DISTRICT 7 SUBTOTALS								280.72		806.12	
DISTRICT 8 SUBTOTALS								140.36		403.56	
TOTALS:								3175.16		8977.25	

BASIS OF ESTIMATE:
 DISTRICT 3 ACHM SURFACE COURSE (1/2").....94.9% MIN. AGGR.....5.1% ASPHALT BINDER
 DISTRICT 6 ACHM SURFACE COURSE (1/2").....94.9% MIN. AGGR.....5.1% ASPHALT BINDER
 DISTRICT 7 ACHM SURFACE COURSE (1/2").....94.7% MIN. AGGR.....5.3% ASPHALT BINDER
 DISTRICT 8 ACHM SURFACE COURSE (1/2").....94.4% MIN. AGGR.....5.6% ASPHALT BINDER
 MAXIMUM NUMBER OF GYRATIONS = 115 FOR PG 64-22

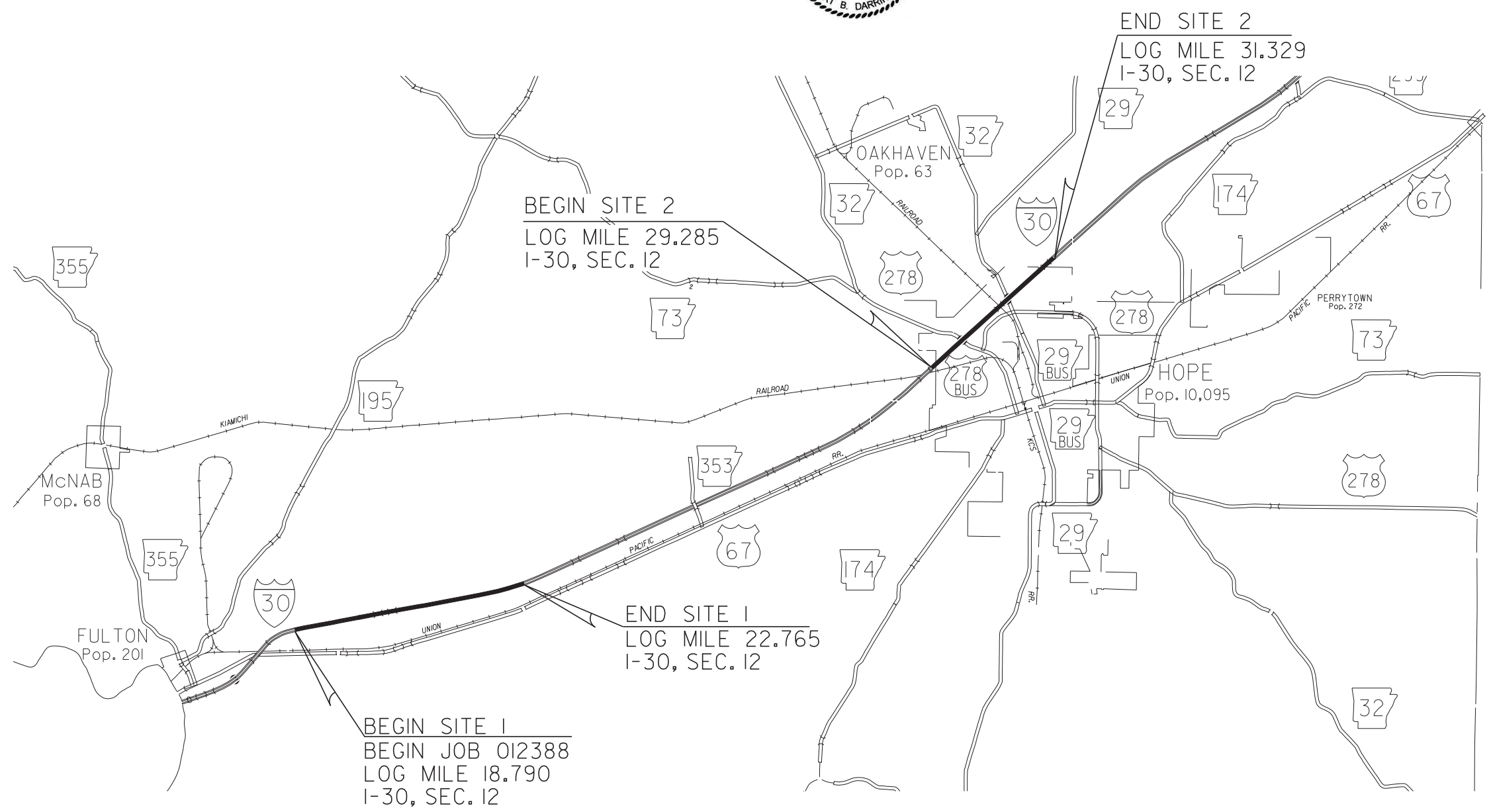
*QUANTITY ESTIMATED
 SEE SECTION 104.03 OF THE STD. SPECS.
 TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

QUANTITIES

9:21:13 AM
5/19/2022
Robert.Darrington
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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	27	34
HEMPSTEAD COUNTY PLAN						



SITES I AND 2
HEMPSTEAD COUNTY PLAN

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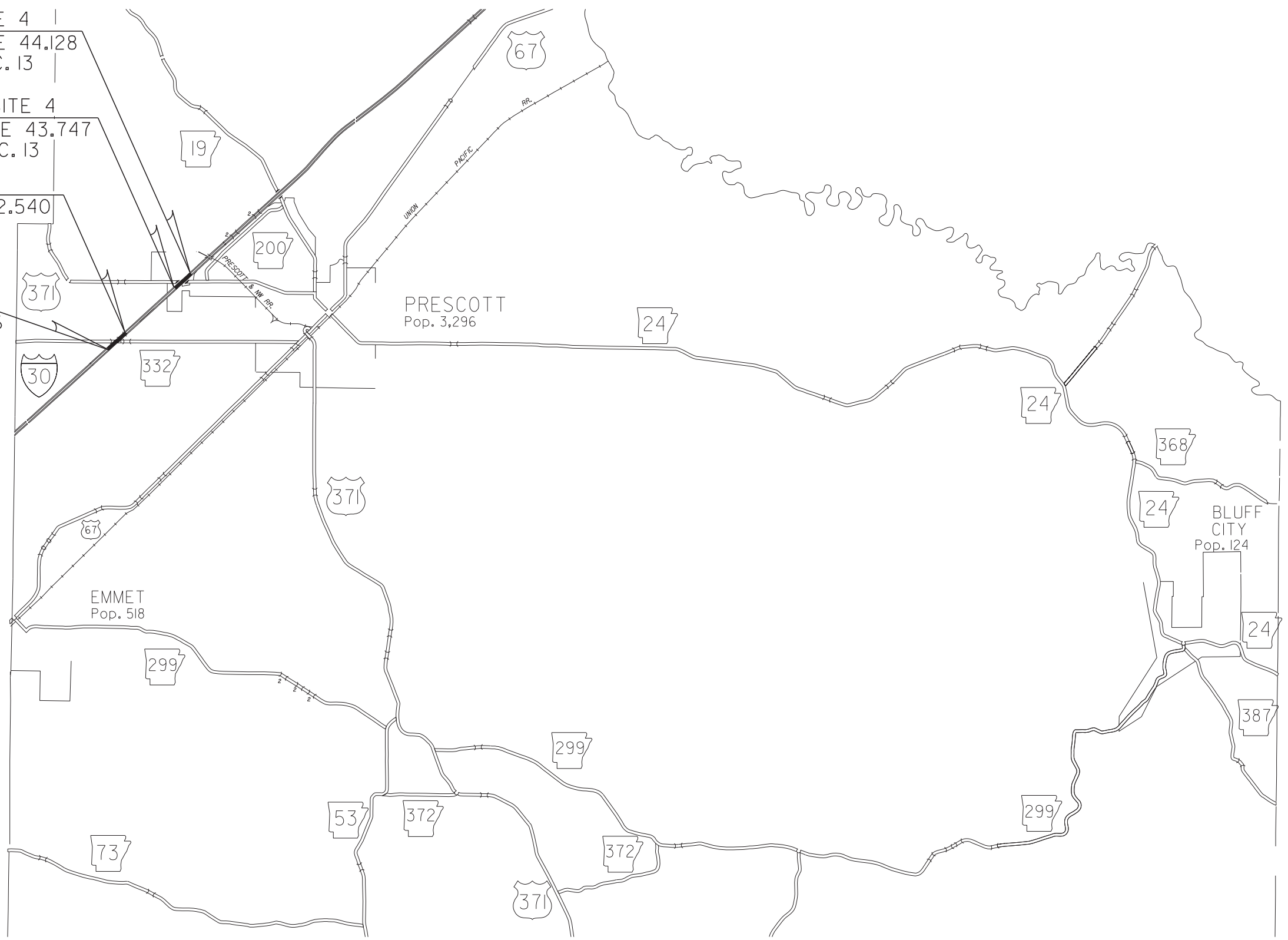


END SITE 4 |
 LOG MILE 44.128
 I-30, SEC. 13

BEGIN SITE 4
 LOG MILE 43.747
 I-30, SEC. 13

END SITE 3
 LOG MILE 42.540
 I-30, SEC. 13

BEGIN SITE 3
 LOG MILE 42.326
 I-30, SEC. 13



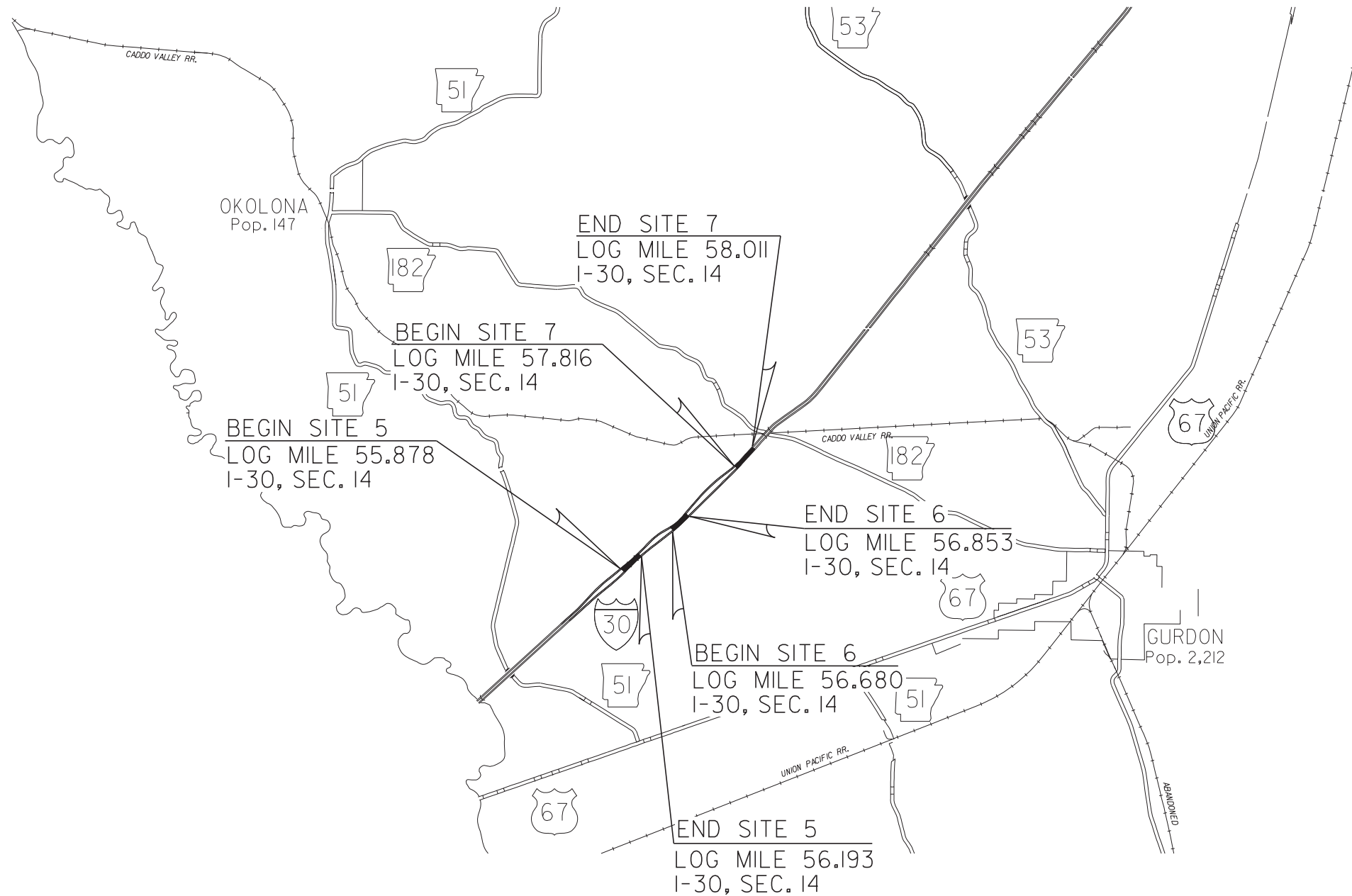
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	28	34
NEVADA COUNTY PLAN						

SITES 3 AND 4
 NEVADA COUNTY PLAN

9:22:23 AM
5/19/2022
Robert.Darrington
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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	29	34
CLARK COUNTY PLAN						

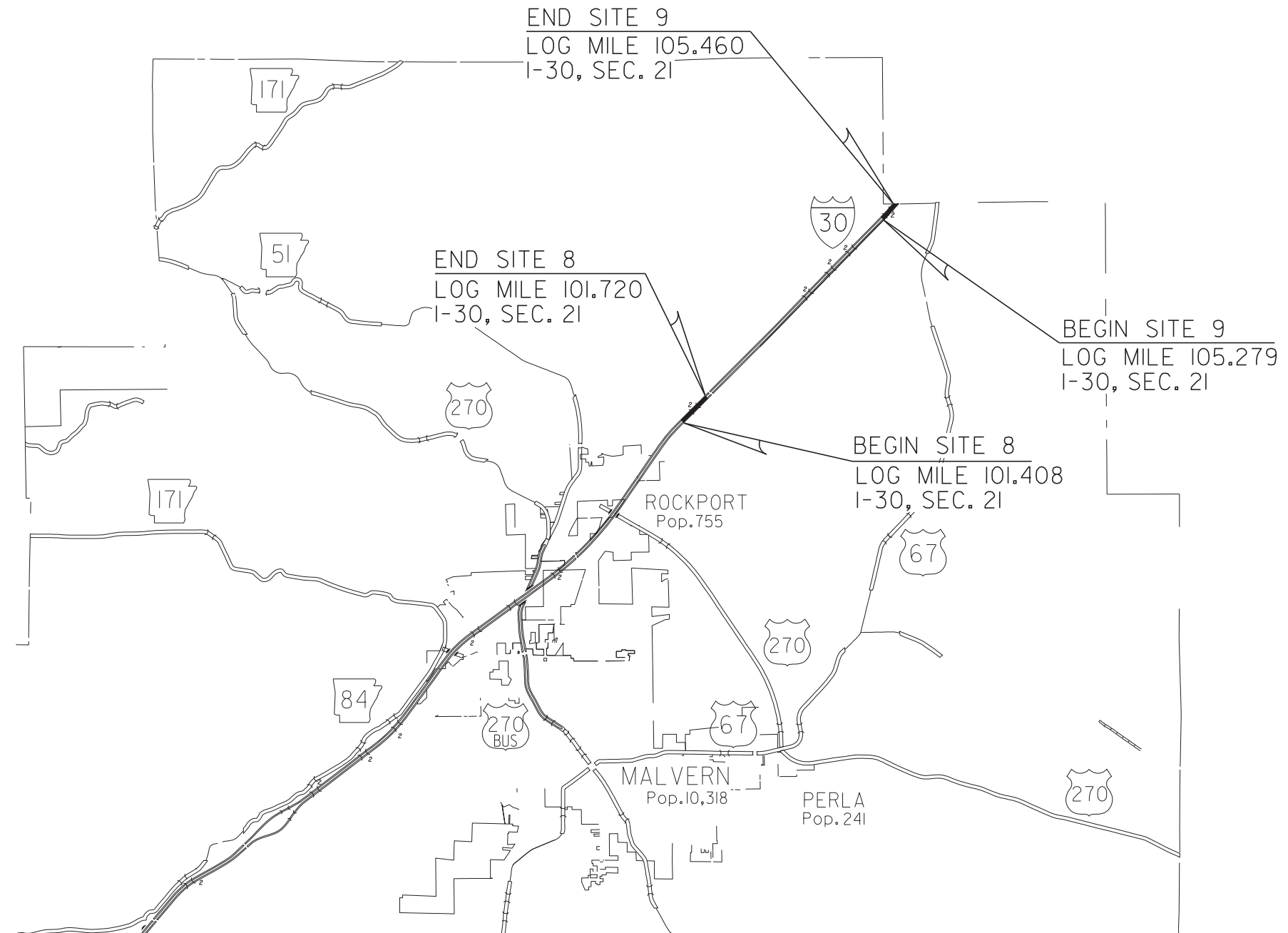


SITES 5-7
CLARK COUNTY PLAN

9:22:27 AM
5/19/2022
Robert.Darrington
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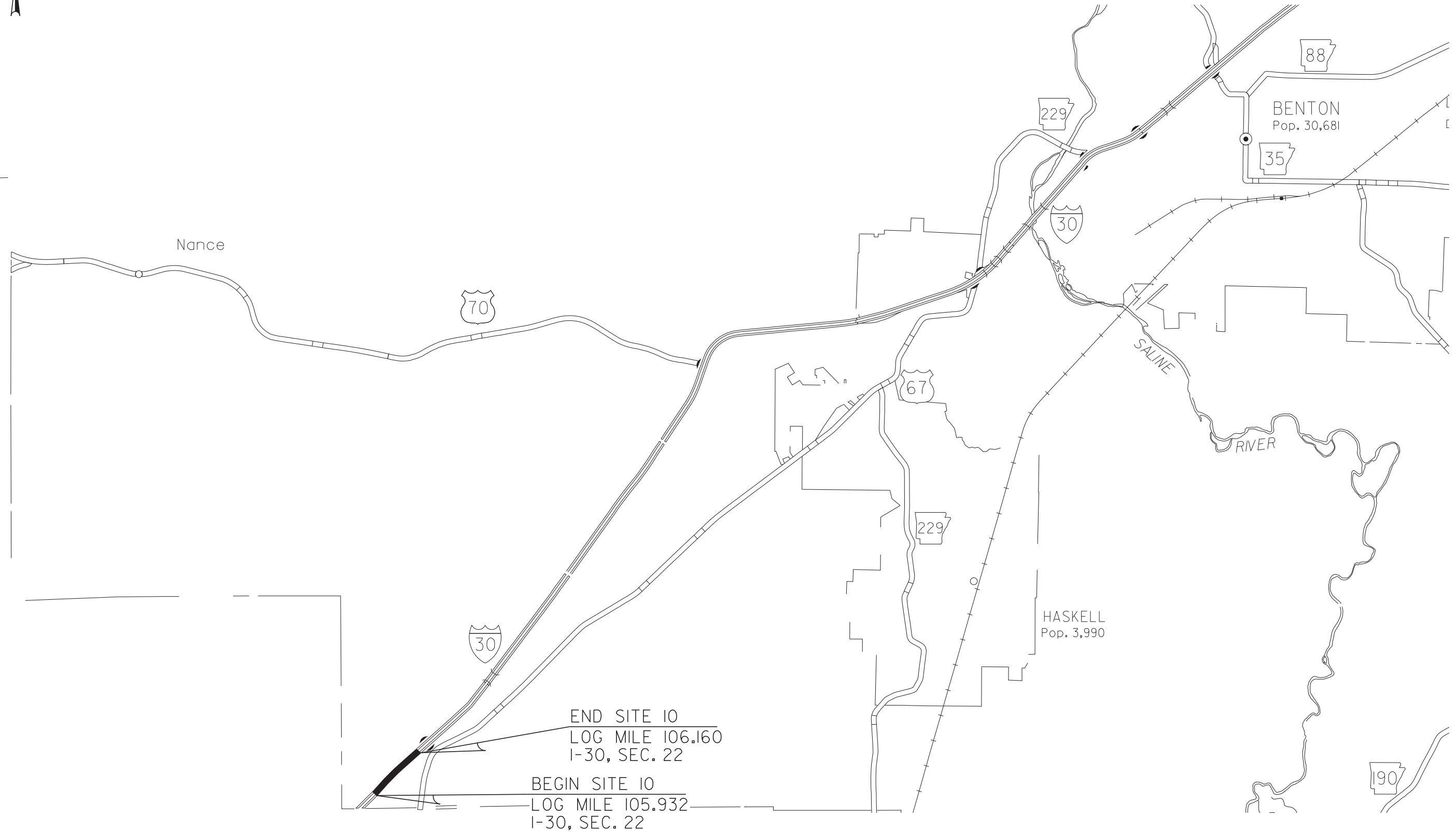
DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	30	34
HOT SPRING COUNTY PLAN						



SITES 8 AND 9 HOT SPRING COUNTY PLAN

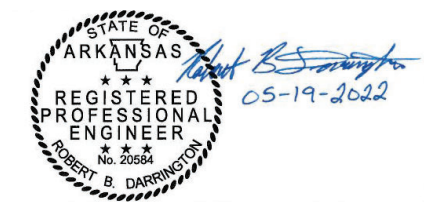
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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
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SALINE COUNTY PLAN						

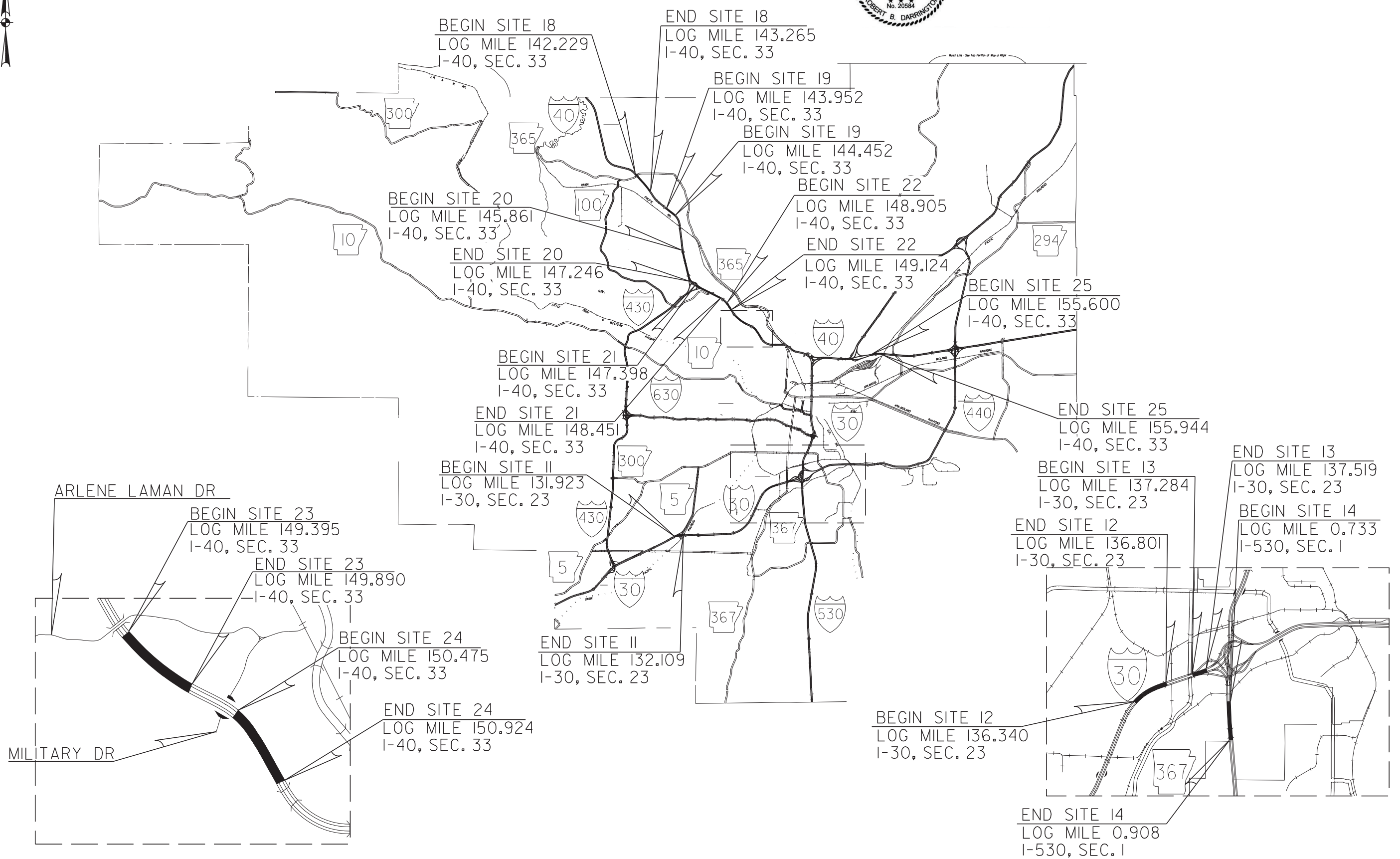


SITE 10
SALINE COUNTY PLAN

5/19/2022 9:23:36 AM Robert.Darrington c:\bms\bridge\farmer-pw\robert.darrington\bridge\farmer.com\dms\0161\02388_SpecialDetails_09.dgn



DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	32	34
PULASKI COUNTY PLAN						



SITES II-14, AND 18-25
PULASKI COUNTY PLAN

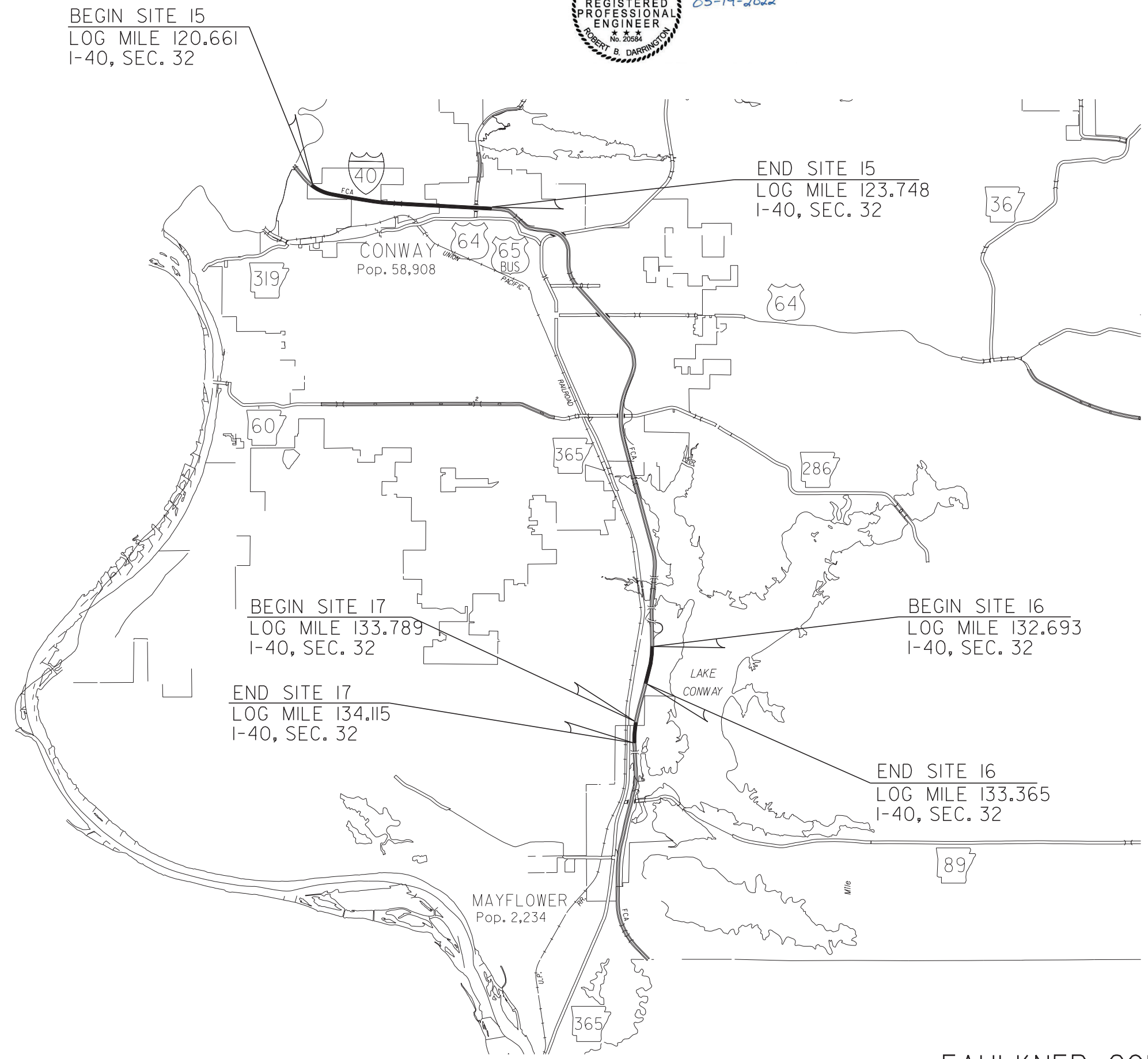
9:24:41 AM

5/19/2022

Robert.Darrington

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DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	33	34
FAULKNER COUNTY PLAN						



SITES 15-17
 FAULKNER COUNTY PLAN

9:24:45 AM

5/19/2022

Robert.L.Darrington

c:\bms\bridge\farmer-pw\robert.l.darrington\bridge\farmer.com\dms\0161\02388_SpecialDetails_IL.dgn

DATE REVISED	DATE REVISED	FED. RD. DIST. NO.	STATE	JOB NO.	SHEET NO.	TOTAL SHEETS
		6	ARK.	012388	34	34
LONOKE COUNTY PLAN						



END SITE 28
LOG MILE 2.654
HWY. 5, SEC. 12

BEGIN SITE 28
LOG MILE 1.125
HWY. 5, SEC. 12

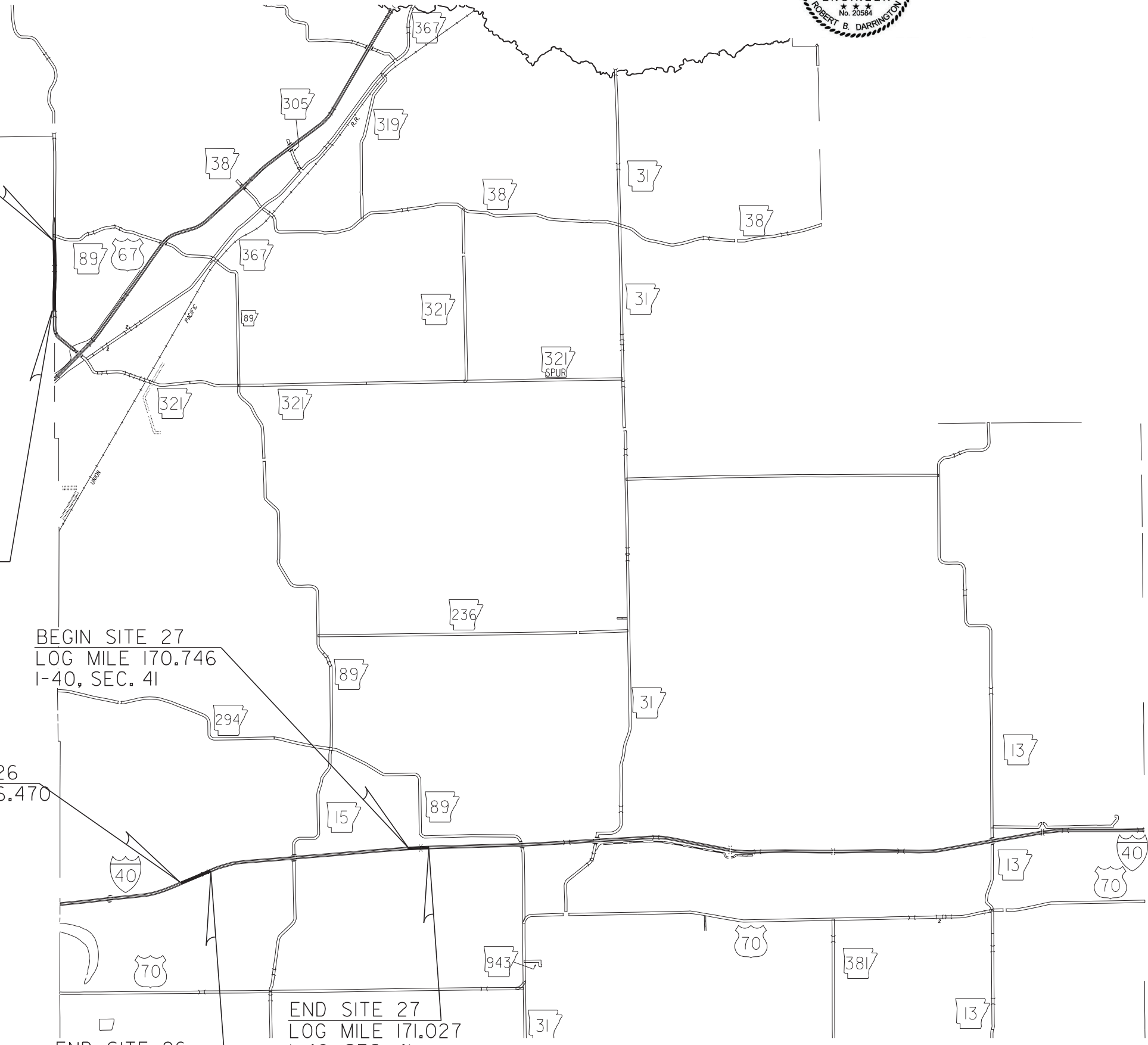
BEGIN SITE 27
LOG MILE 170.746
I-40, SEC. 41

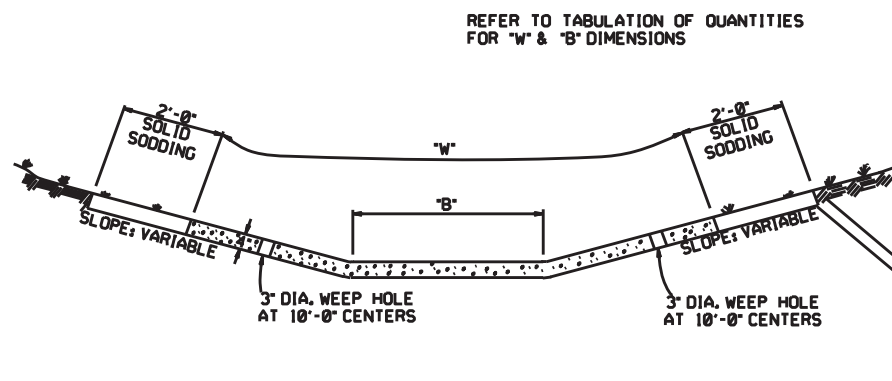
BEGIN SITE 26
LOG MILE 166.470
I-40, SEC. 41

END SITE 26
LOG MILE 167.017
I-40, SEC. 41

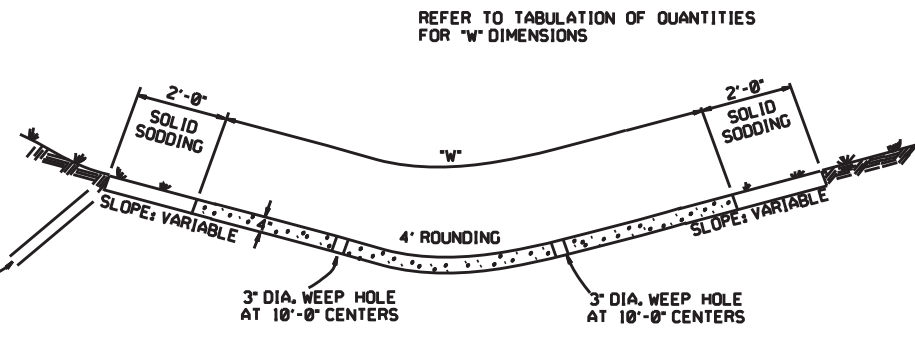
END SITE 27
LOG MILE 171.027
I-40, SEC. 41

SITES 26-28
LONOKE COUNTY PLAN





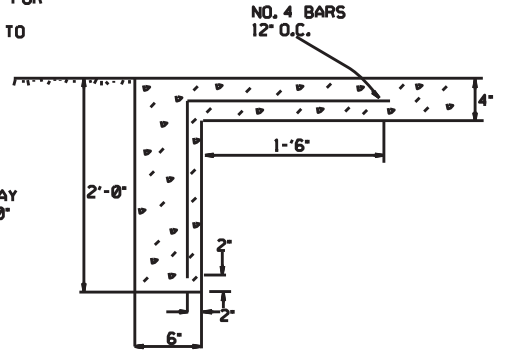
TYPE A



TYPE B

EXCAVATE TO NEAT LINES TO CONSTRUCT DITCH PAVING AND SOLID SODDING.

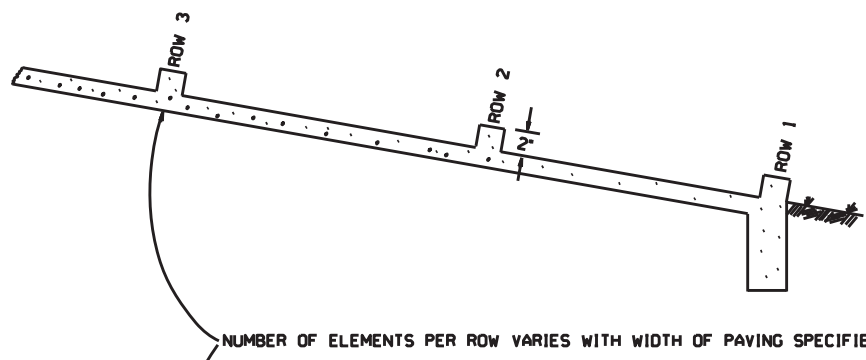
THE STEEL AND ADDITIONAL CONCRETE FOR THE WALLS SHALL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR "CONCRETE DITCH PAVING."



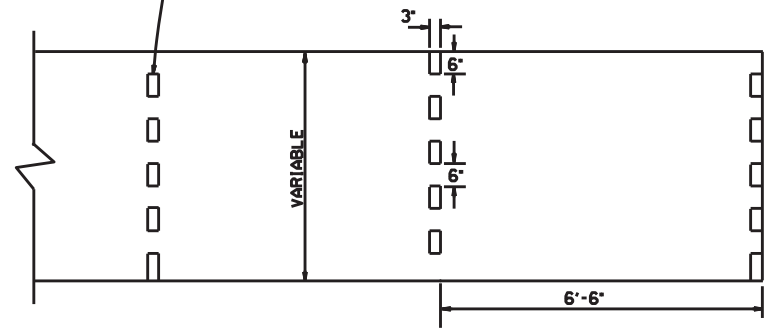
TOE WALL DETAIL FOR CONCRETE DITCH PAVING

GENERAL NOTES:

- THE FULL WIDTH OF EACH SECTION SHALL BE POURED MONOLITHICALLY.
- TOE WALLS TO BE CONSTRUCTED FULL WIDTH AT EACH END OF DITCH PAVING, AND POURED MONOLITHICALLY.
- SOLID SOD ALONG DITCH PAVING TO BE PLACED WITHIN 14 DAYS OF DITCH PAVING CONSTRUCTION.
- 1" WIDE TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE DITCH PAVING AT 45' INTERVALS. THE SPACE SHALL BE FILLED WITH APPROVED JOINT FILLER COMPLYING WITH AASHTO M213.



ENERGY DISSIPATORS TO BE USED FOR THE ENTIRE LENGTH OF DITCH WHEN SLOPE OF DITCH PAVING EXCEEDS 7%. THE DISSIPATORS WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID FOR CONCRETE DITCH PAVING.



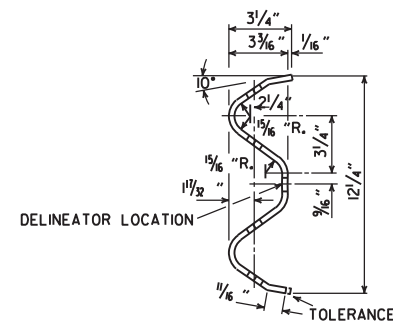
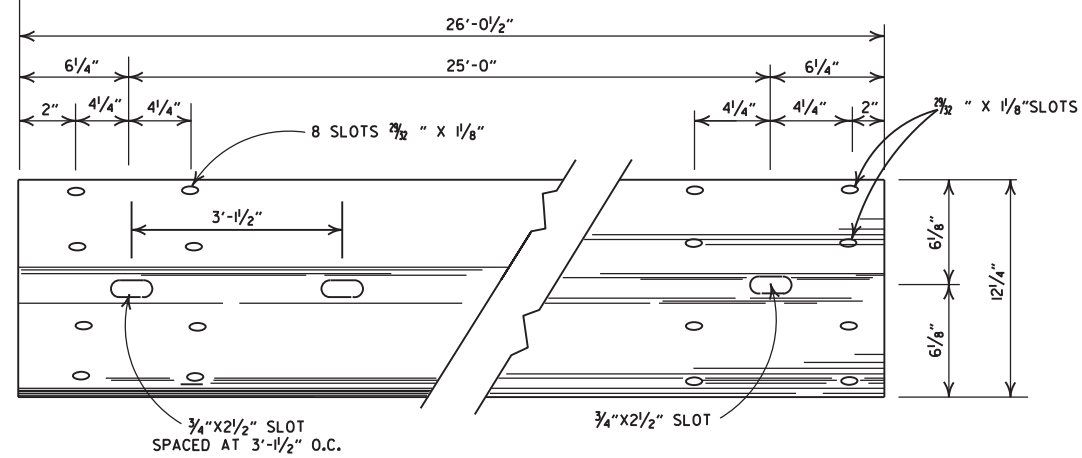
ENERGY DISSIPATORS (NO SCALE)

12-8-16	CORRECTED ENERGY DISSIPATOR DRAWING AND NOTE	
11-17-10	ADDED GENERAL NOTE	
6-2-94	ADDED GENERAL NOTE ABOUT SOLID SODDING	
11-30-8	ELIMINATED MIN. ROWS OF ELEMENTS	111-30-89
7-15-88	REVISED DISSIPATOR NOTE	853-7-15-88
4-3-87	REVISED ENERGY DISSIPATOR	871-4-3-87
1-9-87	MODIFIED NOTE ON ENERGY DISS.	832-1-9-87
11-3-86	ADDED NOTE TO ENERGY DISS.	849-12-1-86
11-1-84	ENERGY DISSIPATOR DETAILS ADDED	508-11-1-84
11-1-84	EXCAVATION DETAILS ADDED	
10-2-72	TYPES A & B REVISED AND REDRAWN	508-10-2-72
	DATE REVISION	DATE FILM'D

ARKANSAS STATE HIGHWAY COMMISSION

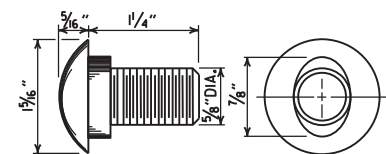
CONCRETE DITCH PAVING

STANDARD DRAWING CDP-1

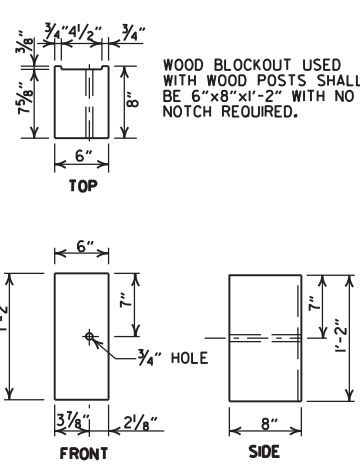
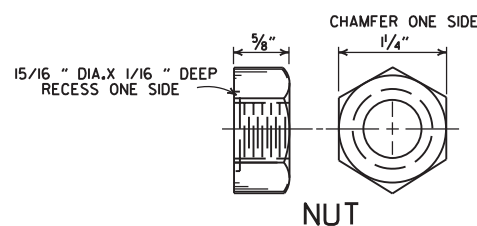
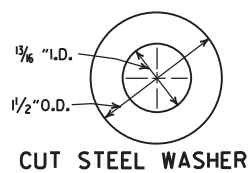


DETAILS OF W-BEAM GUARDRAIL

RAIL SECTION OF CLOSELY SIMILAR DIMENSIONS AND COMPARABLE STRENGTH MAY BE SUBSTITUTED IF APPROVED BY THE ENGINEER.

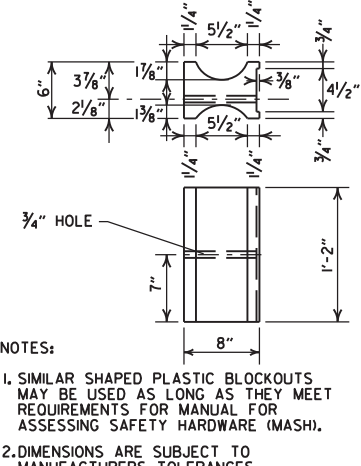


**SPLICE BOLT
POST BOLT - SAME EXCEPT LENGTH**



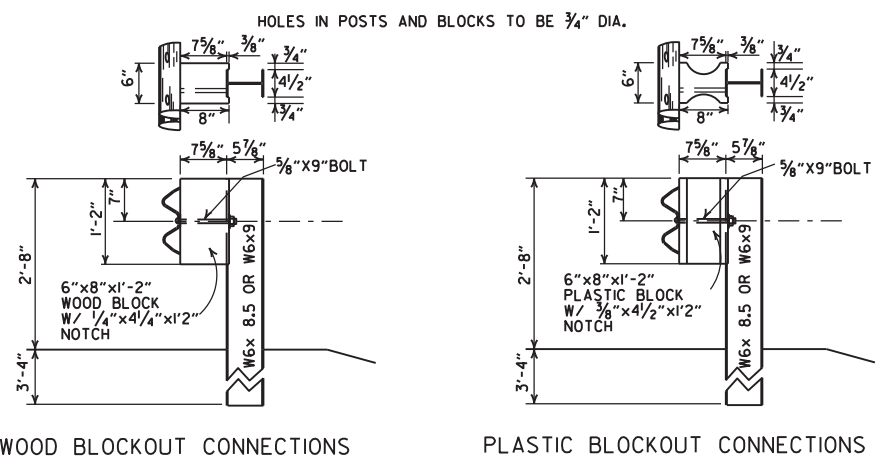
WOOD BLOCKOUT (W-BEAM)

WOOD BLOCKOUT USED WITH WOOD POSTS SHALL BE 6" X 8" X 1'-2" WITH NO NOTCH REQUIRED.

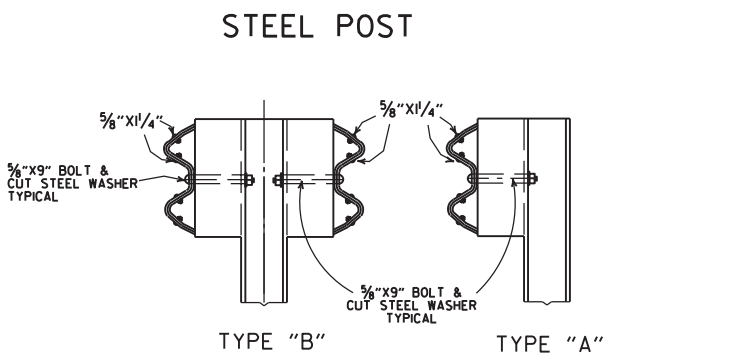
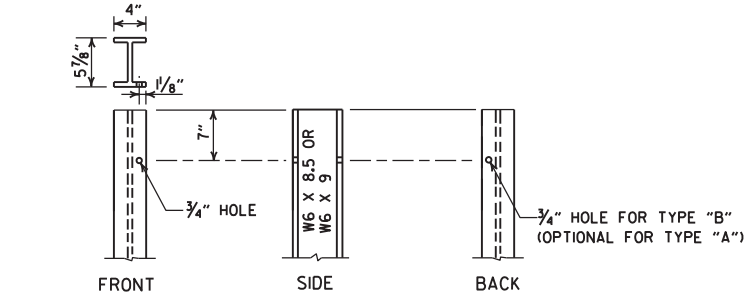


PLASTIC BLOCKOUT (W-BEAM)

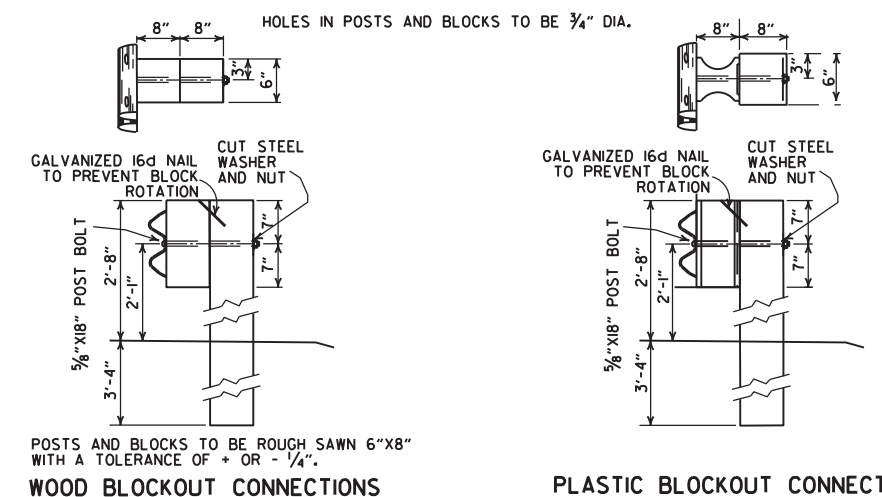
NOTES:
1. SIMILAR SHAPED PLASTIC BLOCKOUTS MAY BE USED AS LONG AS THEY MEET REQUIREMENTS FOR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
2. DIMENSIONS ARE SUBJECT TO MANUFACTURERS TOLERANCES.



DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)



DETAILS OF STEEL LINE POST CONNECTIONS (W-BEAM)



DETAILS OF WOOD LINE POST CONNECTIONS (W-BEAM)

-GENERAL NOTES-

ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT.

WHERE W-BEAM GUARDRAIL CONTINUES, THE INTERMEDIATE SECTIONS SHALL HAVE A POST SPACING OF 6'-3" UNLESS OTHERWISE NOTED.

W-BEAM GUARDRAIL REPRESENTING INTERMEDIATE SECTIONS WILL BE MEASURED ALONG THE ROADWAY FACE FROM CENTERLINE OF POST TO CENTERLINE OF POST.

USE W-BEAM GUARDRAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB. FOR EXTENSIONS OR MODIFICATION OF EXISTING GUARDRAIL, W-BEAM GUARDRAIL COMPONENTS OF THE SAME TYPE AS THOSE EXISTING SHALL BE USED.

ANY BACKFILLING UNDER OR AROUND POST SHALL BE DAMP SAND THOROUGHLY TAMPED IN PLACE.

WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7f (1400 f) OR NO. 1 1350 f SOUTHERN PINE.

CONTRACTOR SHALL HAVE THE OPTION OF USING WOOD BLOCKOUTS FOR W-BEAM GUARDRAIL OR PLASTIC BLOCKOUTS, AS LONG AS BLOCKOUT USED MEETS REQUIREMENTS FOR MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) FOR W-BEAM GUARDRAIL.

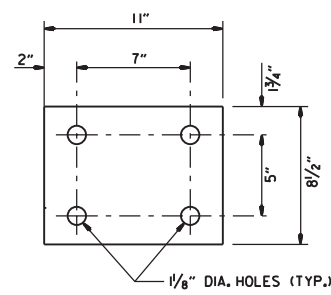
DELINEATORS SHALL BE MOUNTED AT 37.5' SPACING ON THE FRONT FACE OF THE GUARDRAIL. SPACING MAY BE REDUCED IN CURVES, AS DIRECTED BY THE ENGINEER. COLOR SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR DELINEATORS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID PER LIN. FT. FOR GUARDRAIL.

05-19-22	REVISED GENERAL NOTES, ADDED DELINEATOR LOCATION.	
11-07-19	RENUMBERED AND RENAMED	
11-16-17	REVISED GENERAL NOTES AND RAISED GUARDRAIL HEIGHT 3"	
07-14-10	RAISED HEIGHT OF GUARDRAIL 1"	
10-15-09	ADDED REFERENCE TO MASH	
04-10-03	REVISED GENERAL NOTES	
08-22-02	REVISED DIMENSION ON WOOD & PLASTIC BLOCKOUT CONNECTIONS & STEEL POST	
11-16-01	REVISED WOOD BLOCKOUT & DETAILS OF WOOD LINE POST CONNECTIONS	
03-30-00	REMOVED GUARDRAIL AT BRIDGE ENDS	
01-12-00	ADDED PLASTIC BLOCKOUT	
08-12-98	REV. BLOCKOUTS TO WOOD, DELETED CONC. POST & REV. GENERAL NOTE, DELETED DET. OF GUARDRAIL REPLACE. BEHIND CURB & DET. OF POST PLACE IN SOLID ROCK, & ADDED DETAILS OF STEEL LINE POST CONN. REMOVED BACK-UP PLATE, REVISED HOLES IN STEEL POLES	
04-03-97	REMOVED "LAP IN DIRECTION OF TRAFFIC" NOTE & PLACED ARROWS ON WASHERS	
10-18-96	REVISED WOOD POST NOTE	
06-02-94	ADDED ALT. STEEL POST SIZE	
08-05-93	REVISED STEEL POST SIZE	8-5-93
10-01-92	REDRAWN & REVISED	10-1-92
08-15-91	REVISED WASHER NOTE	8-15-91
08-02-90	REV. GEN. NOTE & DEPTH OF ANC. POST IN ROCK	8-2-90
07-15-88	REVISED SECTION 3 & GENERAL NOTES	
03-04-88	REV. ANCHOR POST, ELEV. NOTES & POST IN ROCK	780-3-4-88
10-30-87	REVISED WOOD LINE POST DETAIL	546-10-30-87
10-09-87	REDRAWN & REVISED	802-10-9-87
DATE	REVISION	FILMED

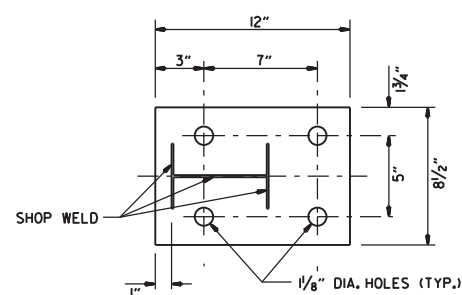
ARKANSAS STATE HIGHWAY COMMISSION

GUARDRAIL DETAILS

STANDARD DRAWING GR-6

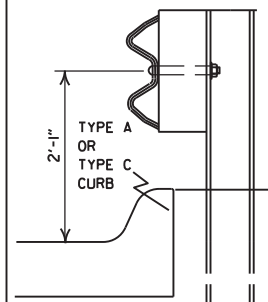


WASHER PLATE



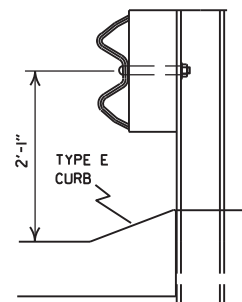
BASE PLATE

Note: Bolts, nuts, washers and plates shall be galvanized in accordance with Section 807 of the Standard Specifications.



FOR DESIGN SPEEDS OF 50 MPH OR LESS

ALIGN FACE OF GUARDRAIL WITH FACE OF CURB.

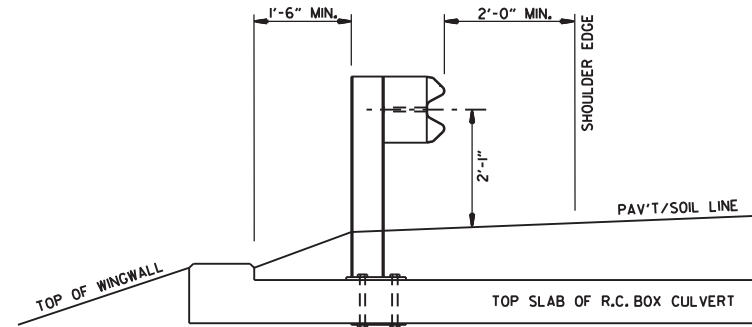


FOR DESIGN SPEEDS OF 55 MPH OR MORE

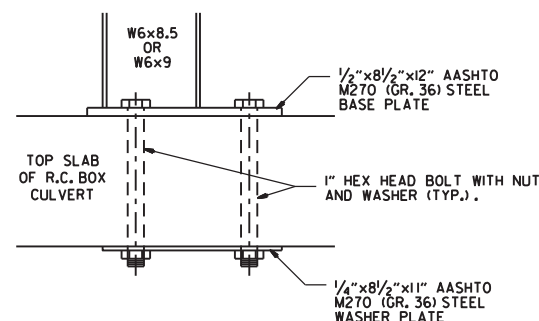
PLACE GUARDRAIL POSTS AGAINST BACK OF CURB.

DETAIL OF GUARDRAIL PLACEMENT BEHIND CURB (W-BEAM)

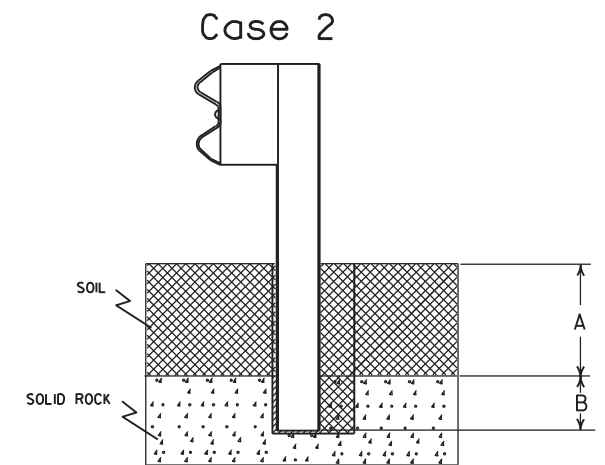
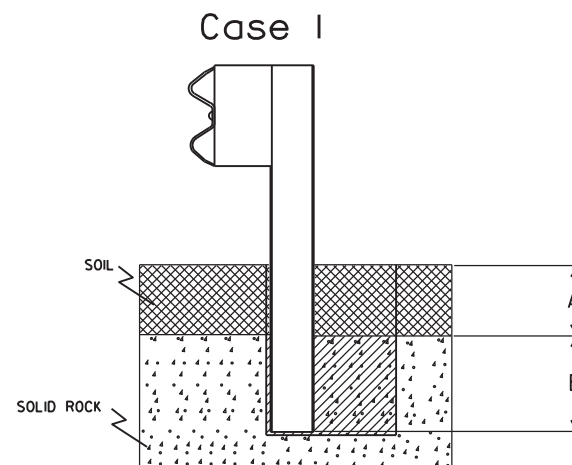
FOR DESIGN SPEEDS OF 50 MPH OR LESS ALL CURB FACES, AS SHOWN ON STD. DRWG. CG-1, MAY BE USED. FOR DESIGN SPEEDS OF 55 MPH OR MORE TYPE "E" CURB FACE SHALL BE USED.



SECTION A-A

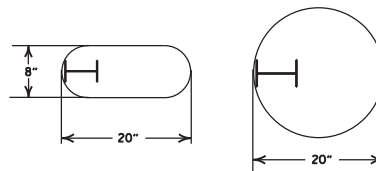


DETAIL OF CONNECTION



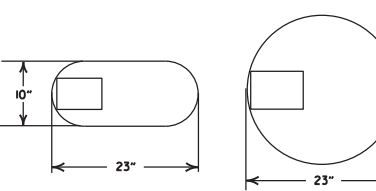
Plan View Steel Posts

Either hole configuration acceptable



Plan View Wood Posts

Either hole configuration acceptable



Notes: For overlying soil depths (A) ranging from 0 to 18", the depth of required drilling (B) is equal to 24".

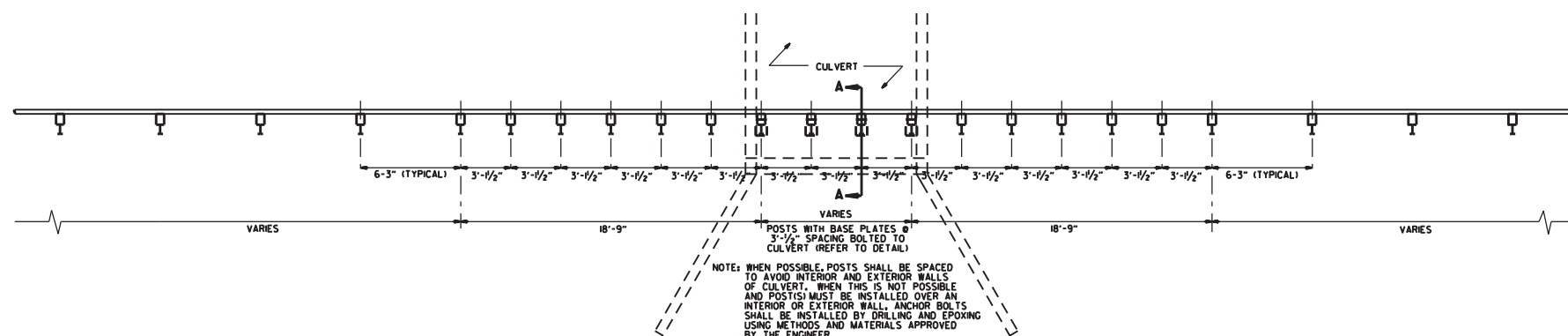
Zone A: Backfill according to Section 617.03(a).

Zone B: Backfill hole in 6" lifts with material meeting the requirements of Section 802.02(c) - Alternate gradation. Compact to 95% maximum dry density per ASTM D-698.

Notes: For overlying soil depths (A) ranging from 18" to 44", the depth of required drilling (B) is equal to either 12" or 44" minus the depth of soil whichever is less.

Zone A & B: Backfill according to Section 617.03(a).

DETAIL OF POST PLACEMENT IN SOLID ROCK (W-BEAM)



PLAN LAYOUT OF TYPE A GUARDRAIL AT LOW-FILL CULVERTS

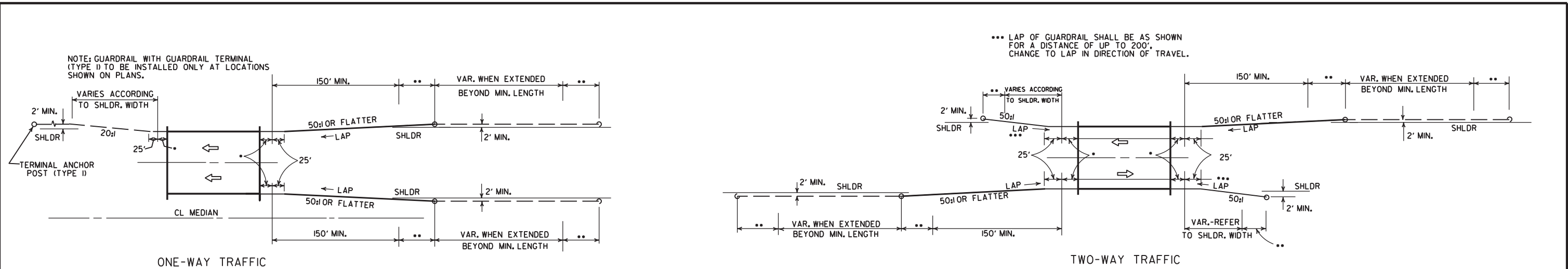
NOTE: THIS DETAIL IS TO BE USED ONLY WHEN THE COVER OVER THE CULVERT DOES NOT PERMIT FULL EMBEDMENT OF GUARDRAIL POSTS AS SHOWN ON STD. DWG. GR-6.

DATE	REVISION	FILMED
11-07-19	RENUMBERED, RENAMED, REVISED REFERENCE	
11-16-17	REVISED GUARDRAIL HEIGHT	
07-14-10	RAISED HEIGHT OF GUARDRAIL 1"	
04-12-07	REVISED DETAIL OF GUARDRAIL PLACEMENT BEHIND CURB	
11-10-05	ADDED GUARDRAIL PLACEMENT BEHIND CURB; REVISED DETAIL OF CONNECTION	
11-18-04	REVISED POST PLACEMENT IN ROCK & CULVERT CONNECTION DETAILS. ADDED DETAIL FOR GUARDRAIL PLACEMENT AT LOW-FILL CULVERTS	
03-30-00	REMOVED CONCRETE INSERT ANCHOR	
08-12-98	CHANGED STEEL SPACER BLOCK TO WOOD BLOCKOUT, ADDED DET. OF GUARDRAIL CONNECTION TO R.C. BOX CULV'T., DELETED DET. OF STEEL LINE POST CONN. & ADDED DET. OF GUARDRAIL PLACE. BEHIND CURB & DET. OF POSTPLACE. IN SOLID ROCK	
04-03-96	PLACED ARROWS AT CUT STEEL WASHERS	4-3-96
10-18-96	REV. ASTM REF. TO AASHTO	
11-22-95	ADDED OPTIONAL HOLES	
06-02-94	REVISED ALTERNATE POST SIZE	
08-05-93	REVISED STEEL POST SIZE	
10-01-92	REDRAWN & REVISED	10-1-92
08-02-90	DEL. WASHER ON ANCHOR ASSEMBLY	8-2-90
07-15-88	CONFORMED TO 1988 SPECS	
03-04-88	REVISED ANCHOR NOTE	
10-30-87	REVISED ANCHOR ASSEMBLY	712-10-30-87
10-30-87	REVISED PLACEMENT BEHIND CURB	547-10-30-87
10-09-87	REDRAWN & REVISED	803-10-9-87

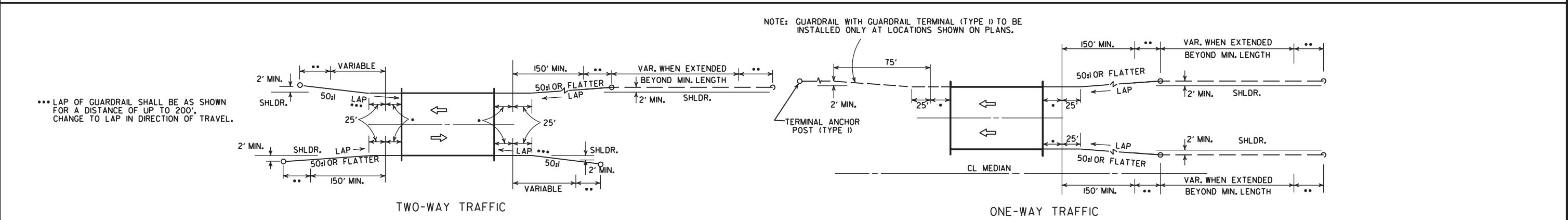
ARKANSAS STATE HIGHWAY COMMISSION

GUARDRAIL DETAILS

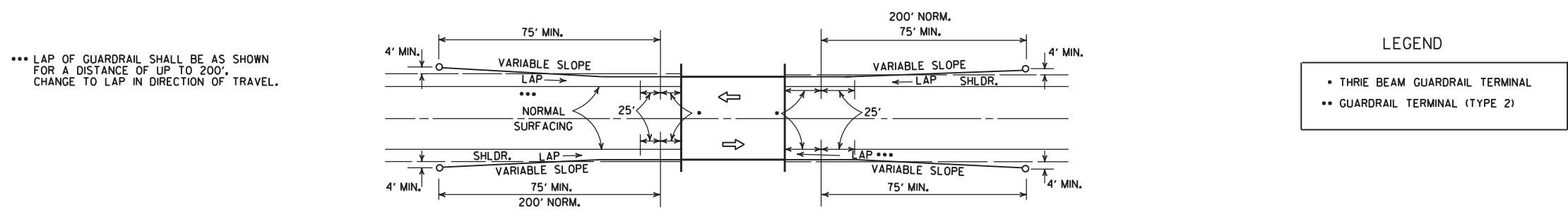
STANDARD DRAWING GR-7



METHODS OF INSTALLATION OF GUARDRAIL AT LESS THAN FULL SHOULDER WIDTH BRIDGES USING GUARDRAIL TERMINAL (TYPE 2)

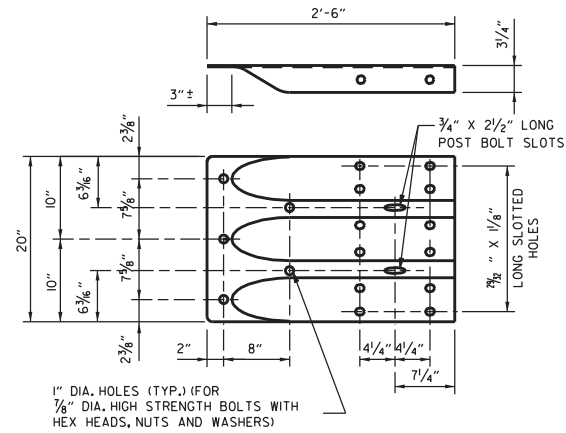


METHOD OF INSTALLATION OF GUARDRAIL AT FULL SHOULDER WIDTH BRIDGES USING GUARDRAIL TERMINAL (TYPE 2)

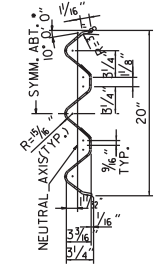


METHOD OF INSTALLATION OF GUARDRAIL USING GUARDRAIL TERMINAL (TYPE 1) (FULL SHOULDER WIDTH OR LESS BRIDGES)

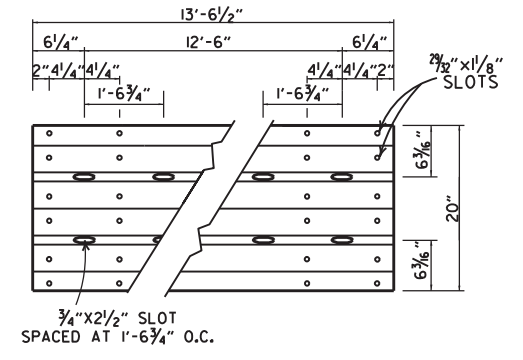
ARKANSAS STATE HIGHWAY COMMISSION		
GUARDRAIL DETAILS		
STANDARD DRAWING GR-8		
11-07-19	RENUMBERED AND RENAMED	
4-17-08	REVISED LAYOUTS	
11-10-05	REMOVED GUARDRAIL NOTES AND DETAILS	
11-16-01	DELETED NOTE-METHOD OF INSTALLATION OF GUARDRAIL USING GUARDRAIL TERM. (TY. 1)	
1-12-00	ADDED CONSTRUCTION NOTE	1-12-00
6-26-97	REVISED LAYOUT	
10-1-92	REDRAWN & REVISED	10-1-92
	ADDED NOTE	
10-9-87	REDRAWN & REVISED	
DATE	REVISION	DATE FILM



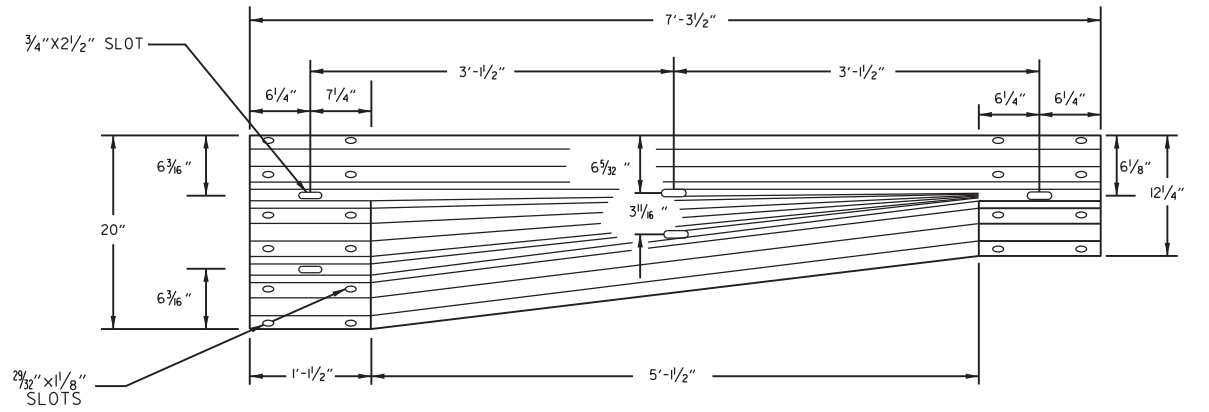
SPECIAL END SHOE



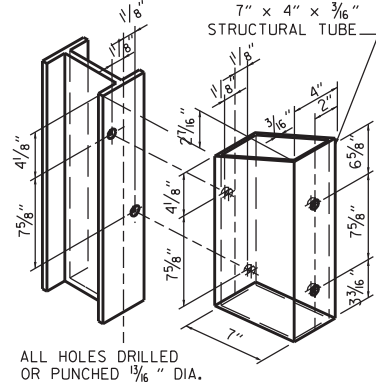
SECTION THRU THRIE BEAM RAIL



THRIE BEAM RAIL

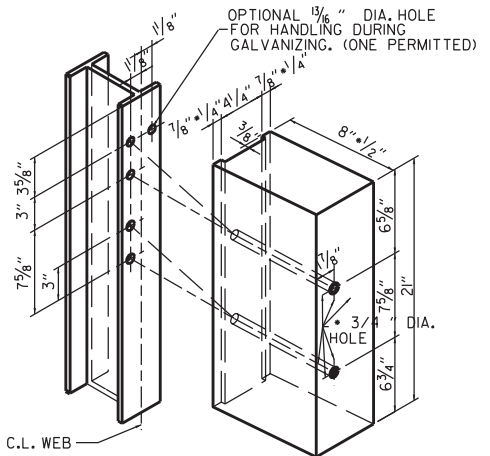


TRANSITION SECTION



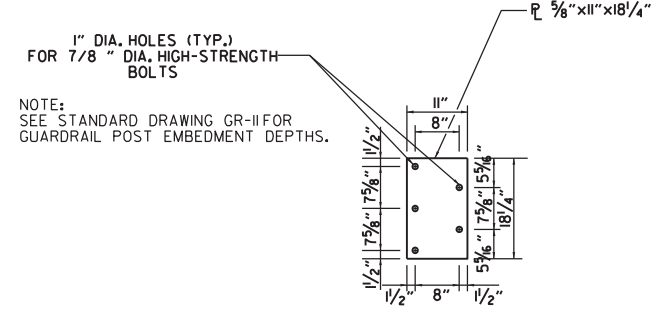
ATTACH BLOCKOUT TO POST USING 3/8" DIA. HEX HEAD BOLTS WITH 1/2" O.D. CUT STEEL WASHERS AND NUT.

STRUCTURAL STEEL TUBING BLOCKOUT DETAIL



HOLE PUNCHING DETAIL FOR STEEL POST & WOOD OR PLASTIC BLOCKOUTS

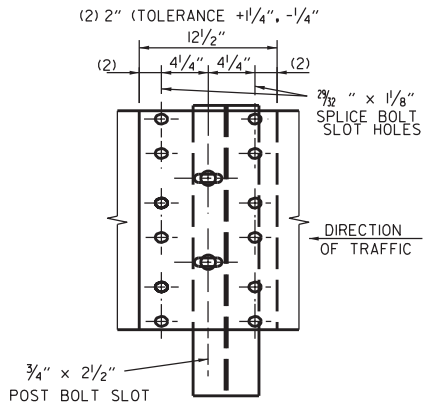
NOTE: BLOCKS SHALL BE THE SAME TYPE THROUGHOUT THE PROJECT LIMITS.



NOTE: SEE STANDARD DRAWING GR-II FOR GUARDRAIL POST EMBEDMENT DEPTHS.

CONNECTOR PLATE

CONNECTOR PLATE SHALL BE AASHTO M270, GR. 36 AND SHALL BE GALVANIZED AFTER FABRICATION. GALVANIZING SHALL CONFORM TO SUBSECTION 807.19 OF THE STANDARD SPECIFICATIONS. CONNECTOR PLATE TO BE BOLTED TO SPECIAL END SHOE USING 7/8" DIA. HIGH STRENGTH BOLTS, WITH THE HEADS PLACED ON THE TRAFFIC FACE. WASHERS SHALL BE USED UNDER THE HEAD AND NUT. BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED AND SHALL CONFORM TO SUBSECTION 807.06.

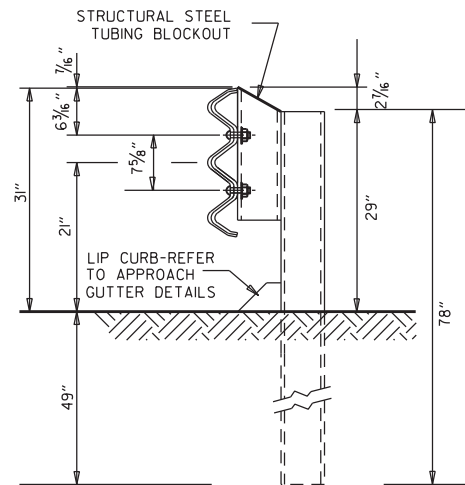


THRIE BEAM RAIL SPLICE AT POST

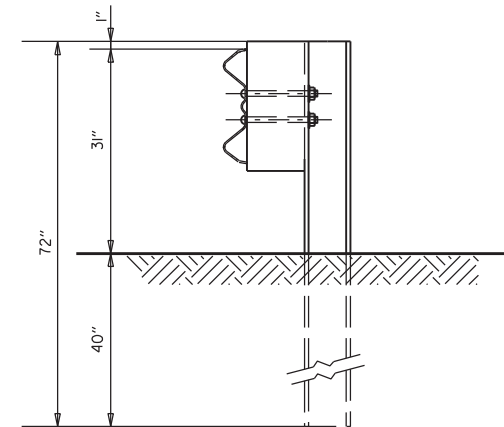
GENERAL NOTES:
 THE THRIE BEAM RAIL, SPECIAL END SHOE, AND THE TRANSITION SECTION SHALL BE MADE OF STEEL AND SHALL BE 12 GAGE. ZINC COATING SHALL BE TYPE 1.
 RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION.
 ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3"4" BEYOND IT.
 ALL LAP SPLICES, INCLUDING SPECIAL END SHOES, SHALL BE MADE IN THE DIRECTION SHOWN ON STANDARD DRAWINGS GR-8 & GR-13.
 REFER TO STD. DRWG. GR-II FOR POST DETAILS.
 USE THRIE BEAM GUARDRAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB.
 THRIE BEAM POSTS SHALL BE SAME MATERIAL AS W-BEAM POSTS FOR ENTIRE JOB.
 WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9,7F (1400 F) OR NO. 1 1350 F SOUTHERN PINE.

II-07-19	RENAMED AND REVISED REFERENCES	
II-16-17	REVISED TRANSITION SECTION, GUARD RAIL HEIGHT, AND GENERAL NOTES; MOVED THRIE BEAM GUARD RAIL CONNECTIONS AT BRIDGES ENDS TO STD. DRWG. GR-12	
07-14-10	RAISED HEIGHT OF W-BEAM 1"	
II-29-07	ADDED PLASTIC BLOCKOUTS	
II-10-05	ADDED NOTE FOR ATTACHING STEEL BLOCKOUT	
II-18-04	REVISED GENERAL NOTES	
10-9-03	REVISED GENERAL NOTES	
04-10-03	REVISED GENERAL NOTES	
08-22-02	REVISED NOTE (2)	
06-29-00	MOVED DIMENSION LINES	
05-18-00	ADDED NOTE	
03-30-00	DRAWN & ISSUED	
DATE	REVISION	FILMED

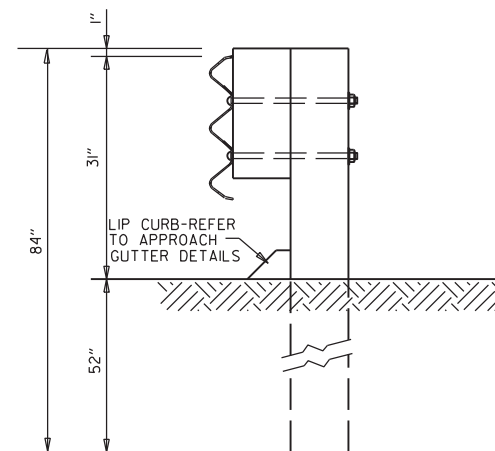
ARKANSAS STATE HIGHWAY COMMISSION
 GUARDRAIL DETAILS
 STANDARD DRAWING GR-10



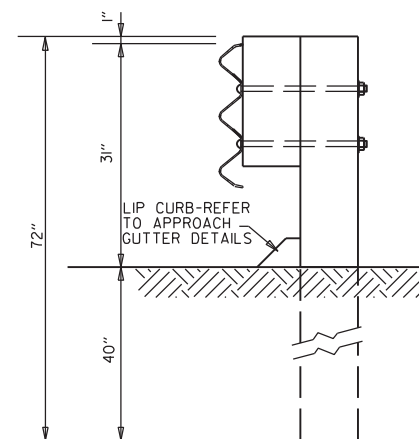
THRIE BEAM RAIL WITH STEEL TUBING BLOCKOUT AND STEEL POST
POSTS 1-7



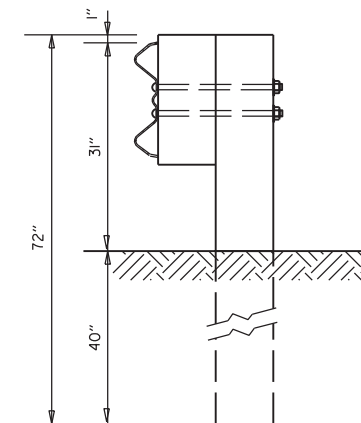
W-BEAM TO THRIE BEAM TRANSITION RAIL WITH WOOD OR PLASTIC BLOCKOUT AND STEEL POST
POST 8



THRIE BEAM RAIL WITH WOOD OR PLASTIC BLOCKOUTS & WOOD POSTS
POSTS 1-6



THRIE BEAM RAIL WITH WOOD OR PLASTIC BLOCKOUT & WOOD POST
POST 7

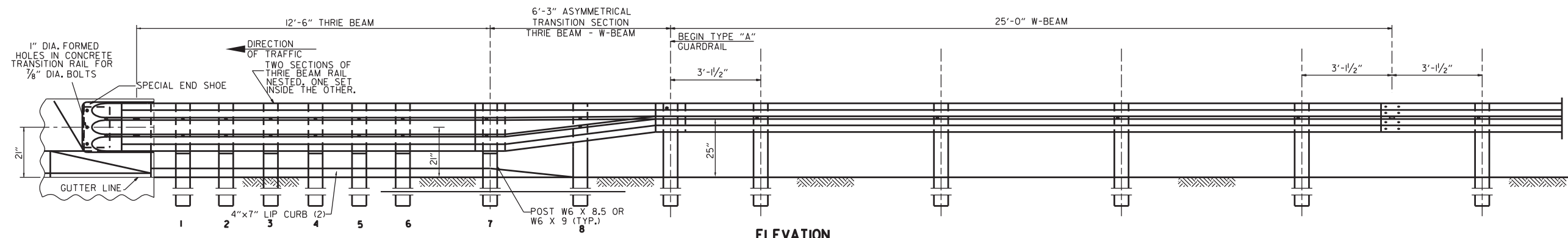


W-BEAM TO THRIE BEAM TRANSITION RAIL WITH WOOD OR PLASTIC BLOCKOUT & WOOD POST
POST 8

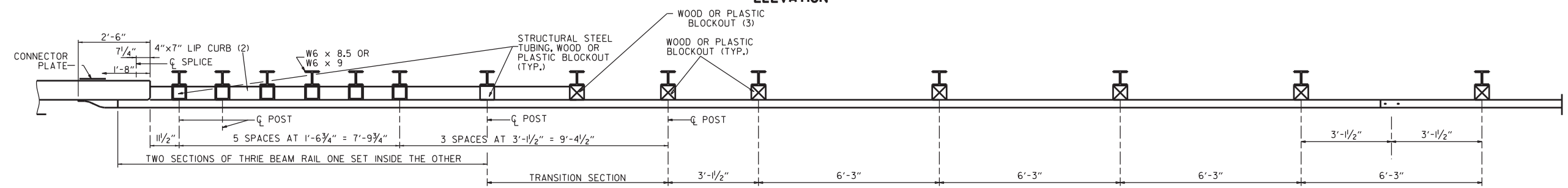
GENERAL NOTES:
RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION.

WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7f (1400 f) OR NO. 1 1350 f SOUTHERN PINE.

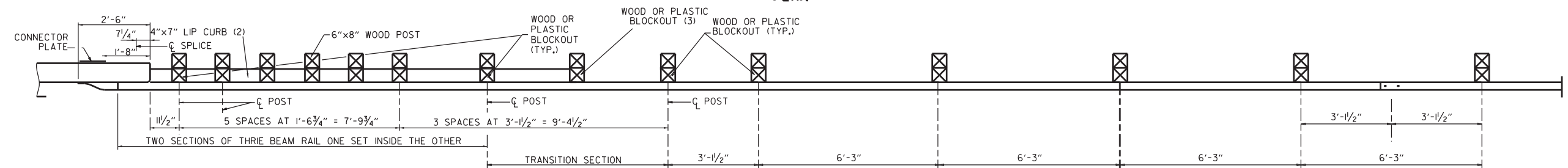
			ARKANSAS STATE HIGHWAY COMMISSION
			GUARDRAIL DETAILS
			STANDARD DRAWING GR-II
11-07-19	RENAMED		
11-16-17	REVISED GUARDRAIL HEIGHT, CHANGED STD. DWG. NUMBER FROM GR-10A TO GR-II		
07-14-10	REVISED POST 8 DIMENSIONS		
11-29-07	ADDED PLASTIC BLOCKOUTS		
08-22-02	REVISED LIP CURB NOTE		
03-30-00	DRAWN & ISSUED		
DATE	REVISION	FILMED	



ELEVATION



PLAN



PLAN

- (1) VERIFY BOLT SPACING FROM RAIL TRANSITION PRODUCER.
- (2) REFER TO APPROACH GUTTER DETAILS.
- (3) LENGTH OF BLOCKOUT ON POST 8 TO BE MODIFIED TO FIT RAIL WIDTH.

THRIE BEAM GUARDRAIL CONNECTION AT BRIDGE ENDS

GENERAL NOTES:

THE THRIE BEAM RAIL, SPECIAL END SHOE, AND THE TRANSITION SECTION SHALL BE MADE OF STEEL AND SHALL BE 12 GAGE. ZINC COATING SHALL BE TYPE I.

RAIL POSTS SHALL BE SET PERPENDICULAR TO THE ROADWAY PROFILE GRADE AND VERTICALLY IN CROSS SECTION.

ALL BOLTS SHALL BE SUFFICIENT LENGTH TO EXTEND THROUGH THE FULL THICKNESS OF THE NUT AND NO MORE THAN 3/4" BEYOND IT.

ALL LAP SPLICES, INCLUDING SPECIAL END SHOES, SHALL BE MADE IN THE DIRECTION SHOWN ON STANDARD DRAWINGS GR-8 & GR-13.

REFER TO STD. DRWG. GR-II FOR POST DETAILS.


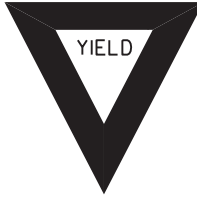



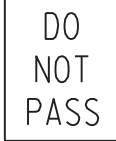



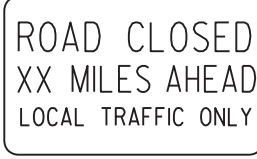










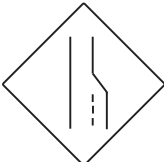




















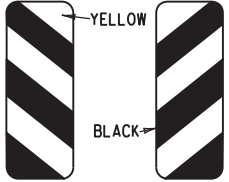


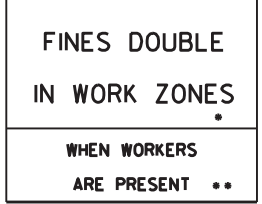
USE THRIE BEAM GUARDRAIL COMPONENTS OF SAME MATERIAL FOR ENTIRE JOB.

THRIE BEAM POSTS SHALL BE SAME MATERIAL AS W-BEAM POSTS FOR ENTIRE JOB.

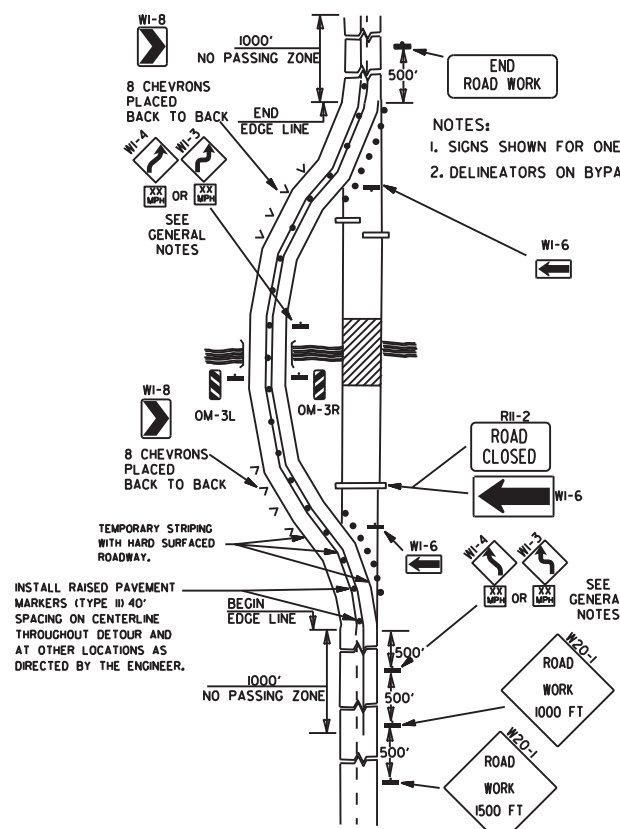
POSTS SHALL NOT BE PLACED AT SPLICE LOCATIONS ALONG W-BEAM RAILS.

WOOD POSTS & WOOD BLOCKS SHALL BE EITHER DENSE NO. 1 STRUCTURAL OR BETTER 9.7f (1400 f) OR NO. 1 1350 f SOUTHERN PINE.

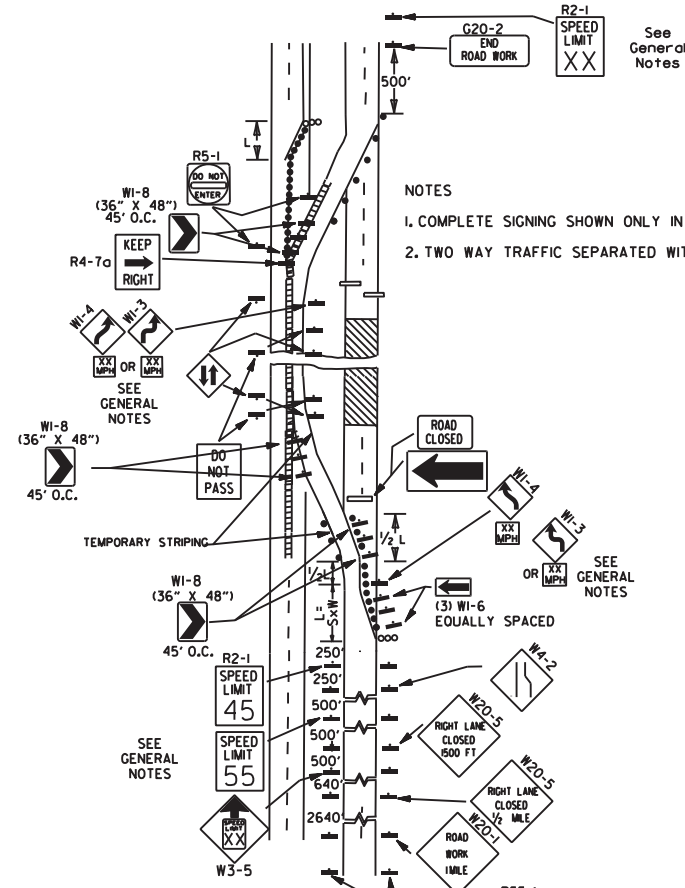
			ARKANSAS STATE HIGHWAY COMMISSION
			GUARDRAIL DETAILS
			STANDARD DRAWING GR-12
05-14-20	REVISED NOTES		
11-07-19	RENAMED & REVISED REFERENCES		
11-16-17	RE-DRAWN FROM STD. DWG. GR-10 & ISSUED		
DATE	REVISION	FILMED	

							ADVANCE DISTANCES (XXXX)		
<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>500 FT 1/2 MILE 1000 FT 3/4 MILE 1500 FT 1 MILE AHEAD</p>		
<p>GENERAL NOTES:</p> <ol style="list-style-type: none"> ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION. TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER. EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED. SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE. SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3. POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS. FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS. MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT. R55-SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN. <p>• NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.</p>									
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>W21-5a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>			
<p>W1-3</p>  <p>STD. 48"x48"</p>	<p>W1-4</p>  <p>STD. 48"x48"</p>	<p>W1-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>W1-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>			
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>		
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>W1-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>		
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60"</p> <p>• USE 6" C LETTERS •• USE 4" D LETTERS</p>		

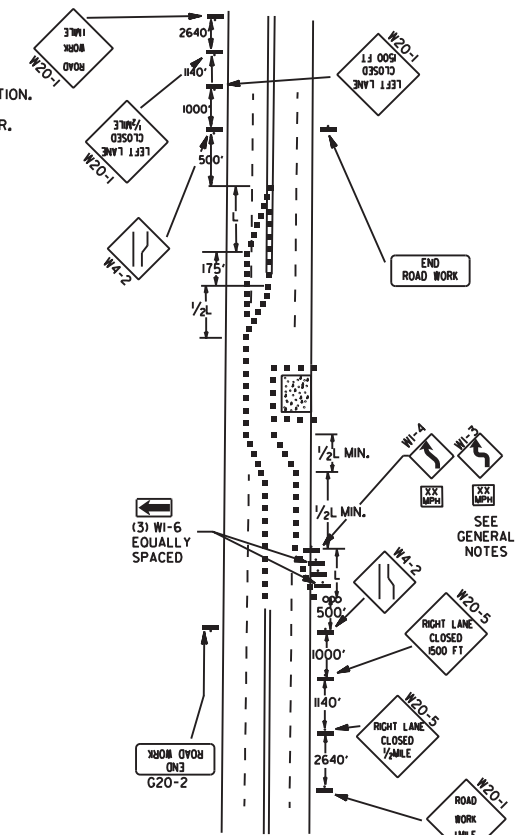
DATE	REVISION	FILMED
11-07-19	REVISED FOR MASH	
4-13-17	DELETED RSP-1 & ADDED W21-5a	
9-2-15	REVISED REDUCED SPEED LIMIT AHEAD SIGNS REVISED ROAD WORK NEXT XX MILES	
12-15-11	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	
DATE	REVISION	FILMED



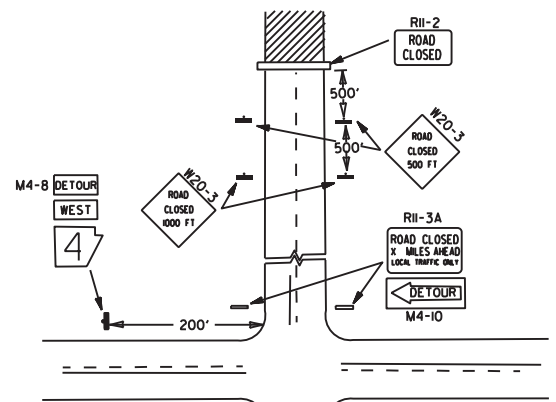
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.

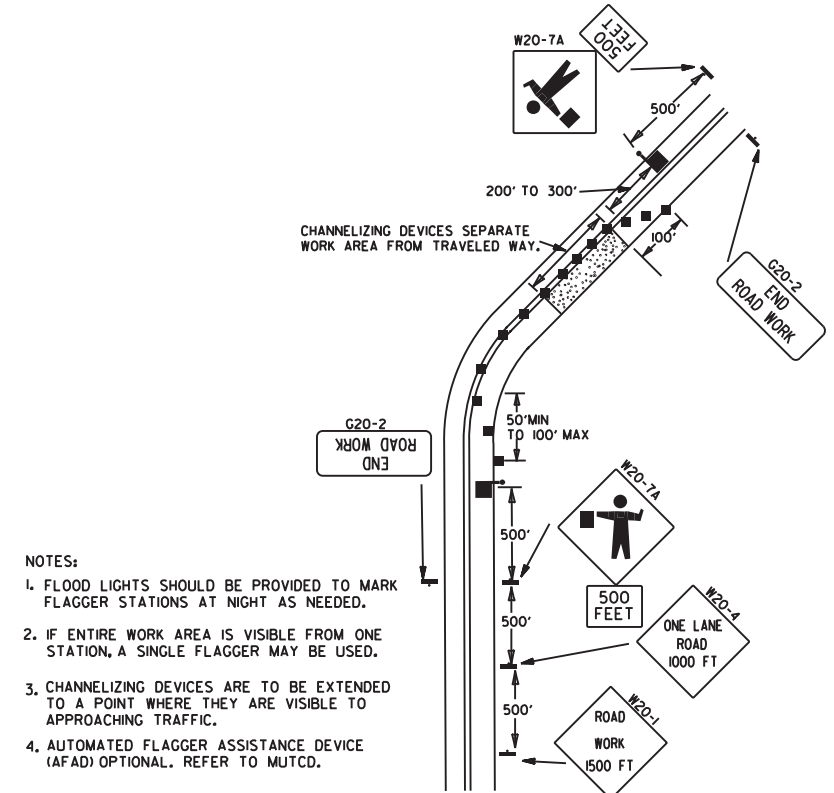


(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



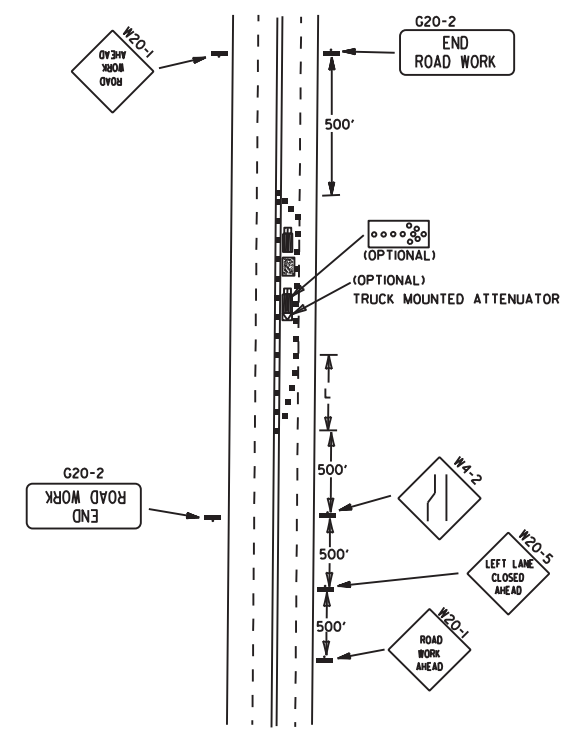
NOTES:
 1. REGULATORY TRAFFIC CONTROL DEVICES TO BE MODIFIED AS NEEDED FOR THE DURATION OF THE DETOUR.
 2. STREET NAMES MAY BE USED WHEN DESIRABLE FOR DIRECTING DETOURED TRAFFIC.

(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.



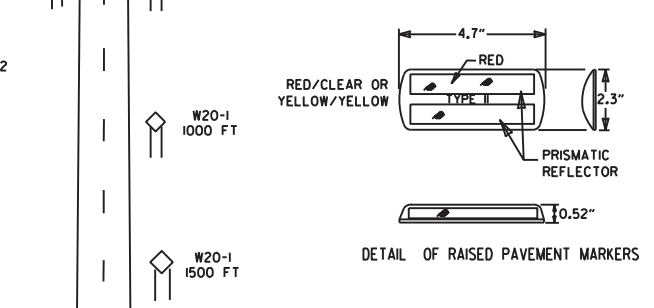
NOTES:
 1. FLOOD LIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
 2. IF ENTIRE WORK AREA IS VISIBLE FROM ONE STATION, A SINGLE FLAGGER MAY BE USED.
 3. CHANNELIZING DEVICES ARE TO BE EXTENDED TO A POINT WHERE THEY ARE VISIBLE TO APPROACHING TRAFFIC.
 4. AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD) OPTIONAL. REFER TO MUTCD.

(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

- KEY:
- FLAGGER
 - ▬ POSITIVE BARRIER
 - ∞ ARROW PANEL (IF REQUIRED)
 - ▬ TYPE III BARRICADE
 - ▬ CHANNELIZING DEVICE
 - TRAFFIC DRUM
 - RAISED PAVEMENT MARKER



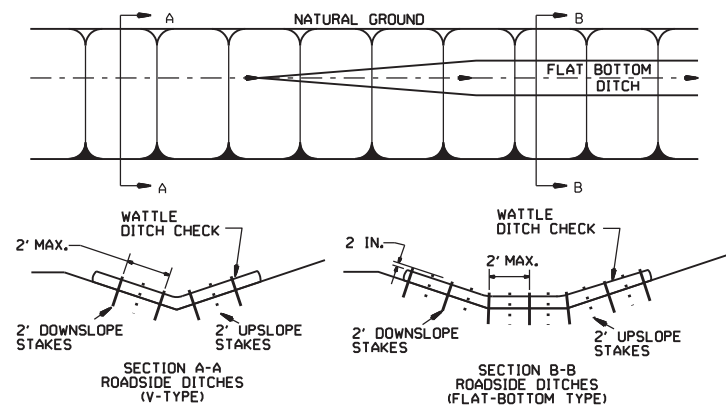
TYPICAL ADVANCE WARNING SIGN PLACEMENT
 TAPER FORMULAE:
 L=5XW FOR SPEEDS OF 45MPH OR MORE.
 L= $\frac{WS^2}{60}$ FOR SPEEDS OF 40MPH OR LESS.
 WHERE:
 L= MINIMUM LENGTH OF TAPER.
 S= NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.
 W= WIDTH OF OFFSET.

- GENERAL NOTES:
1. THE MAINTENANCE DIVISION SHALL CONDUCT A BALL BANK STUDY TO DETERMINE THE ADVISORY SPEED LIMIT PRIOR TO OPENING TO TRAFFIC. THE ADVISORY SPEED WILL BE POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
 2. WHEN THE EXISTING SPEED LIMIT IS 45MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-(45) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-145MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(45) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-(45) SHALL BE OMITTED. ADDITIONAL R2-155MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-(45) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
 5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
 6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 7. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
 8. DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ADOT QUALIFIED PRODUCTS LIST.
 9. ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

DATE	REVISION	FILMED
05-20-21	REVISED NOTE 7	
11-07-19	REVISED NOTE 4, ADDED NOTE 9	
9-2-15	REVISED NOTE 2, ADDED NOTE 8, REVISED DRAWING (A) & REPLACED R2-5A WITH W3-5	
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-11-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

GENERAL NOTES

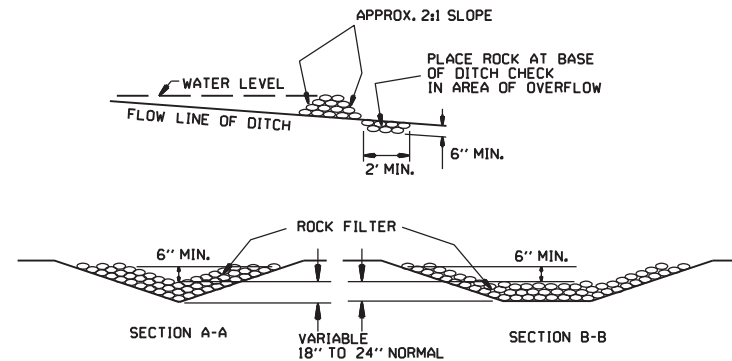
INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.



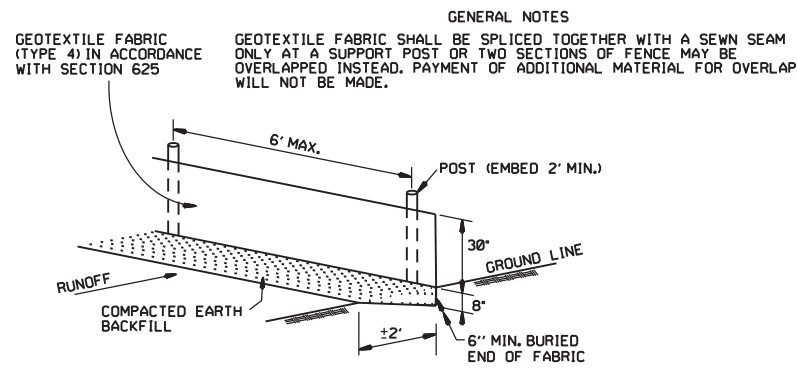
WATTLE DITCH CHECK (E-1)



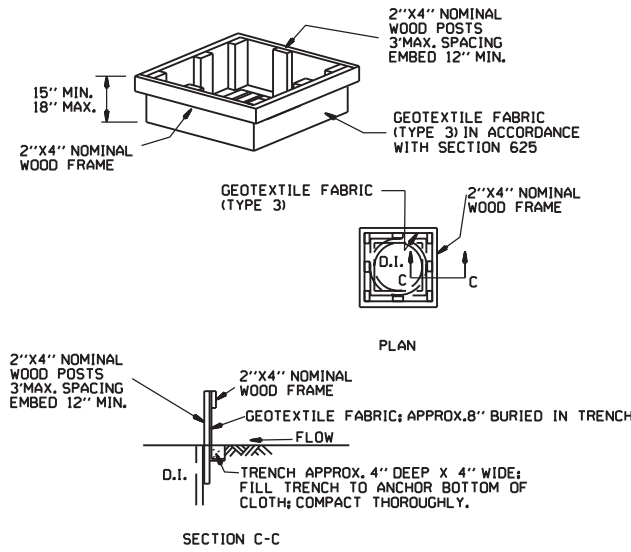
SAND BAG DITCH CHECK (E-5)



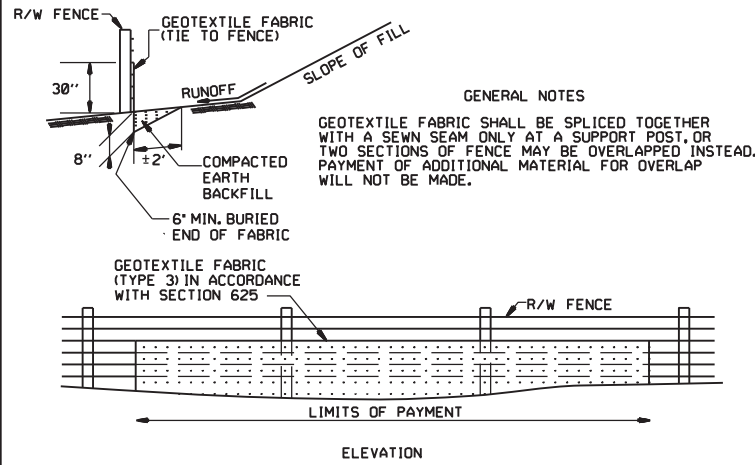
ROCK DITCH CHECK (E-6)



SILT FENCE (E-11)

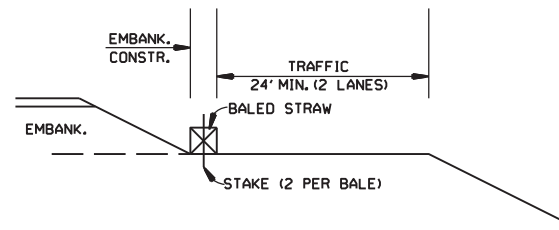


DROP INLET SILT FENCE (E-7)

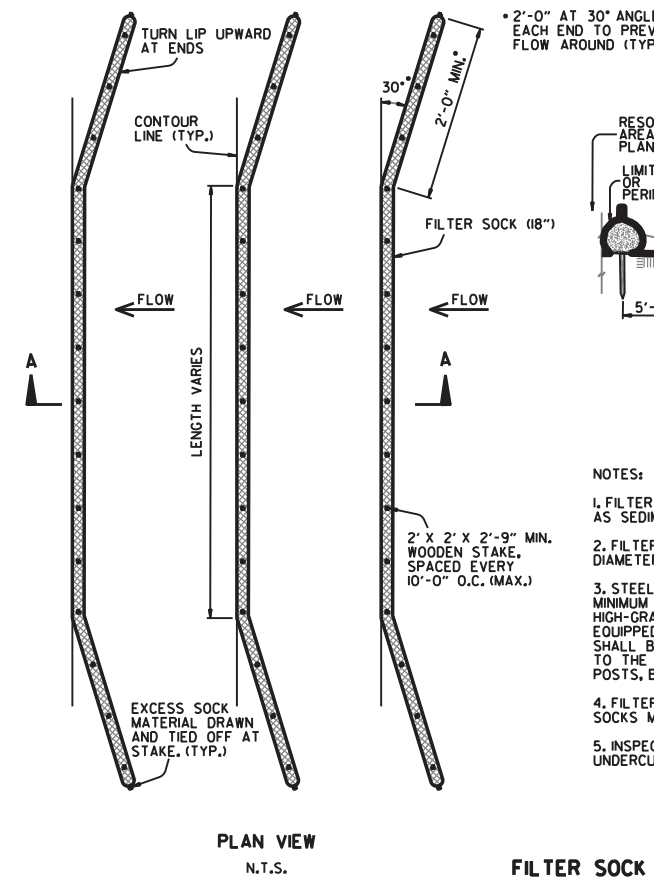


SILT FENCE ON R/W FENCE (E-4)

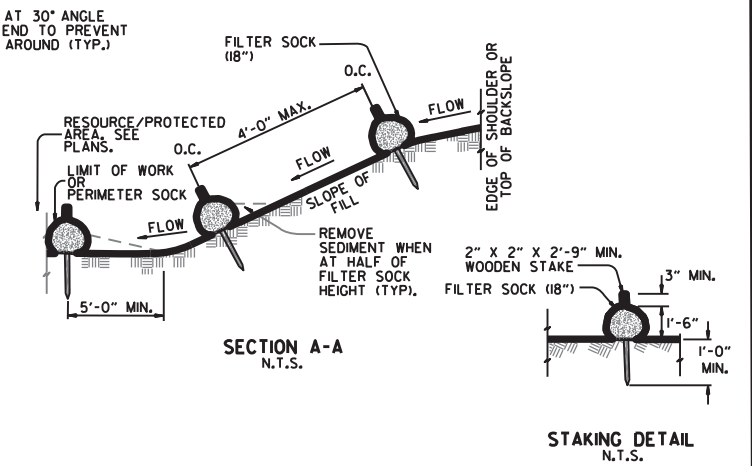
- GENERAL NOTES
1. STRAW BALES SHALL BE INSTALLED SO THAT THE BINDINGS ARE ORIENTED AROUND THE SIDES RATHER THAN ALONG THE TOPS AND BOTTOMS OF THE BALES. THE BALES SHALL BE A MINIMUM OF 30 INCHES IN LENGTH.
 2. NO GAPS SHALL BE LEFT BETWEEN BALES.
 3. BALED STRAW FILTER BARRIERS COMPLETED AND ACCEPTED WILL BE MEASURED BY THE BALE IN PLACE AS AUTHORIZED BY THE ENGINEER AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID PER BALE FOR BALED STRAW DITCH CHECKS.



BALED STRAW FILTER BARRIER (E-2)



PLAN VIEW
N.T.S.

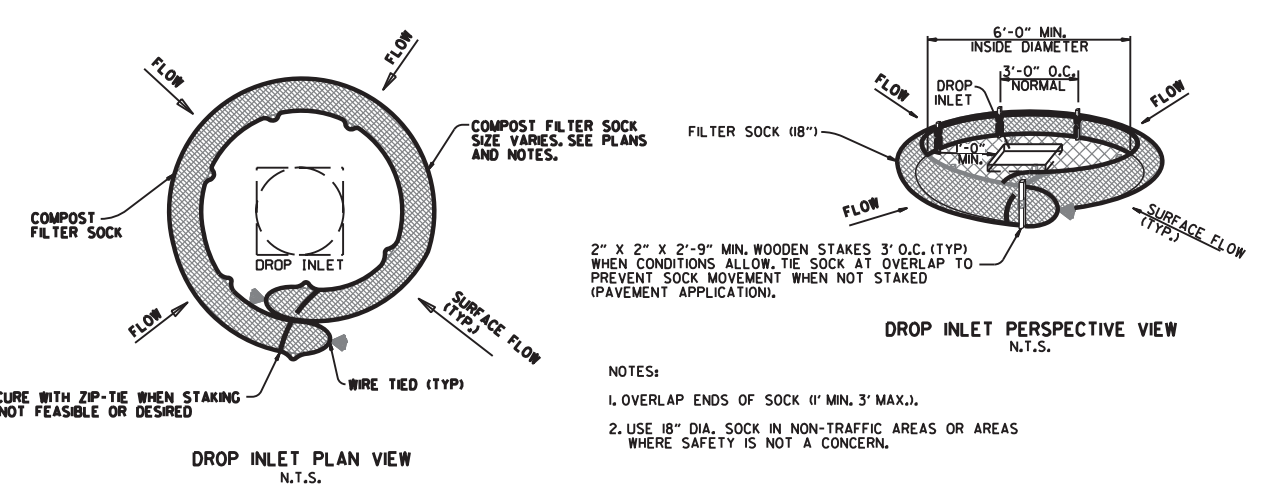


SECTION A-A
N.T.S.

STAKING DETAIL
N.T.S.

- NOTES:
1. FILTER SOCKS CAN BE PLACED AT THE TOP, ON THE FACE, AND AT THE TOE OF SLOPES AS SEDIMENT-TRAPPING DEVICES FOR SHEET FLOW RUNOFF.
 2. FILTER SOCKS ARE TYPICALLY SUPPLIED AND INSTALLED WITH 18 INCH DIAMETERS. DIAMETER TOLERANCE IS 2 INCHES, AS FILTER SOCKS TEND TO FLATTEN OUT WHEN PLACED.
 3. STEEL POSTS MAY BE USED AND SHALL BE ROLLED FROM HIGH CARBON STEEL AND HAVE A MINIMUM OF 1.25 LB./FT. POSTS SHALL BE HOT-DIPPED GALVANIZED OR PAINTED WITH HIGH-GRADE WEATHER RESISTANT BROWN OR BLACK STEEL PAINT. STEEL POSTS SHALL BE EQUIPPED WITH ANCHOR PLATE HAVING A MINIMUM AREA OF 14 SQUARE INCHES. POSTS SHALL BE STUDDED, EMBOSSED, OR PUNCHED. POSTS AND ANCHOR PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A702. NO ADDITIONAL PAYMENT WILL BE PROVIDED FOR STEEL POSTS, BUT PRICE WILL BE CONSIDERED SUBIADINARY TO "FILTER SOCK (18")."
 4. FILTER SOCKS MAY BE UP TO 250 FEET LONG. WHEN USED ON LONG SLOPES, FILTER SOCKS MAY BE JOINTED OR STAGGERED AS SHOWN IN DETAILS.
 5. INSPECT FILTER SOCKS AFTER EACH RUNOFF EVENT. REMOVE AND REPLACE IF SIGNS OF UNDERCUTTING OR DOWNSTREAM RILLS ARE OBSERVED.

FILTER SOCK ALONG SLOPE (E-3)



DROP INLET PLAN VIEW
N.T.S.

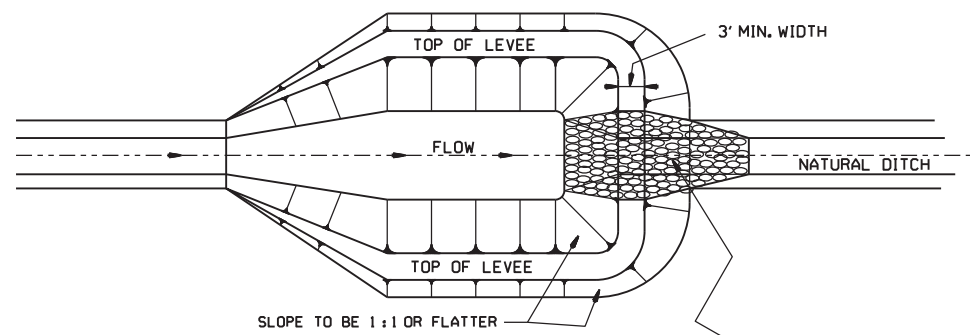
DROP INLET PERSPECTIVE VIEW
N.T.S.

- NOTES:
1. OVERLAP ENDS OF SOCK (1' MIN. 3' MAX.).
 2. USE 18" DIA. SOCK IN NON-TRAFFIC AREAS OR AREAS WHERE SAFETY IS NOT A CONCERN.

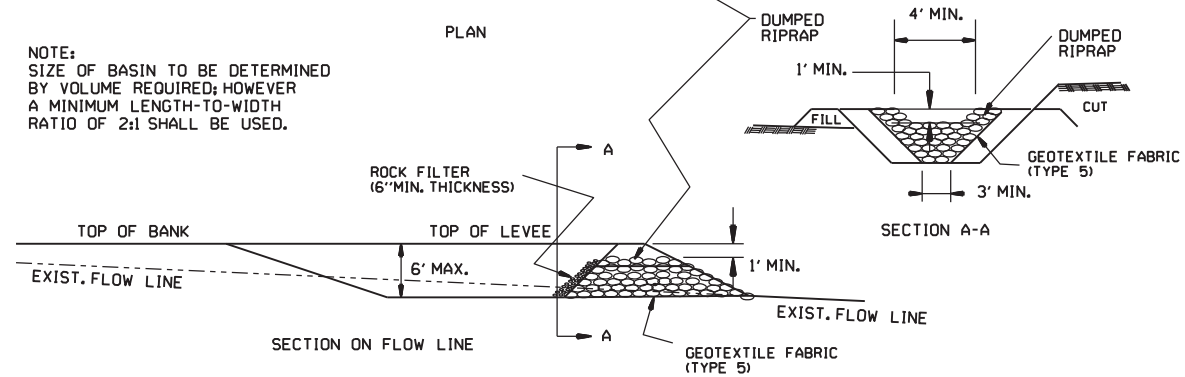
COMPOST FILTER SOCK DROP INLET PROTECTION (E-13)

11-16-17	ADDED FILTER SOCK E-3 AND E-13	
12-15-11	DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK	
11-18-98	ADDED NOTES	
07-02-98	ADDED BALED STRAW FILTER BARRIER (E-2)	
07-20-95	REVISED SILT FENCE E-4 AND E-11	7-20-95
07-15-94	REV. E-4 & E-11 MIN. 13" BURIED END OF FABRIC	
06-02-94	REVISED E-1, 4, 7 & 11 DELETED E-2 & 3	6-2-94
04-01-93	REDRAWN	
10-01-92	REDRAWN	
08-02-76	ISSUED R.D.M.	298-7-28-76
DATE	REVISION	FILMED

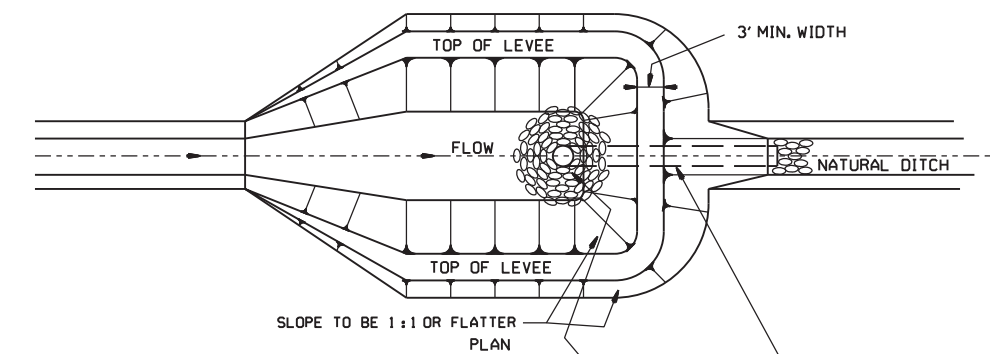
ARKANSAS STATE HIGHWAY COMMISSION
 TEMPORARY EROSION CONTROL DEVICES
 STANDARD DRAWING TEC-1



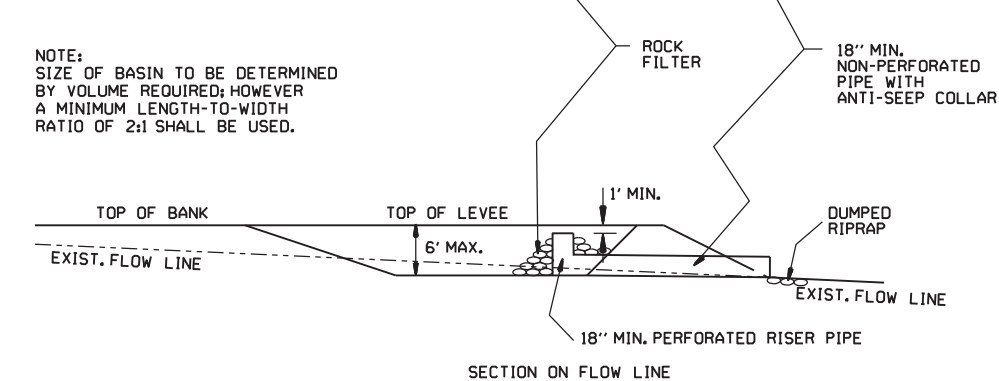
NOTE:
SIZE OF BASIN TO BE DETERMINED
BY VOLUME REQUIRED; HOWEVER
A MINIMUM LENGTH-TO-WIDTH
RATIO OF 2:1 SHALL BE USED.



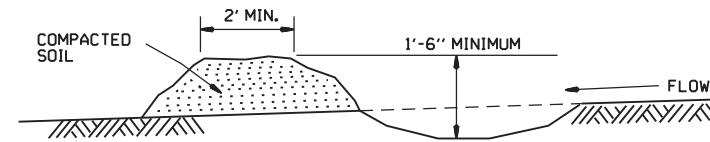
SEDIMENT BASIN WITH RIPRAP OUTLET (E-9)



NOTE:
SIZE OF BASIN TO BE DETERMINED
BY VOLUME REQUIRED; HOWEVER
A MINIMUM LENGTH-TO-WIDTH
RATIO OF 2:1 SHALL BE USED.

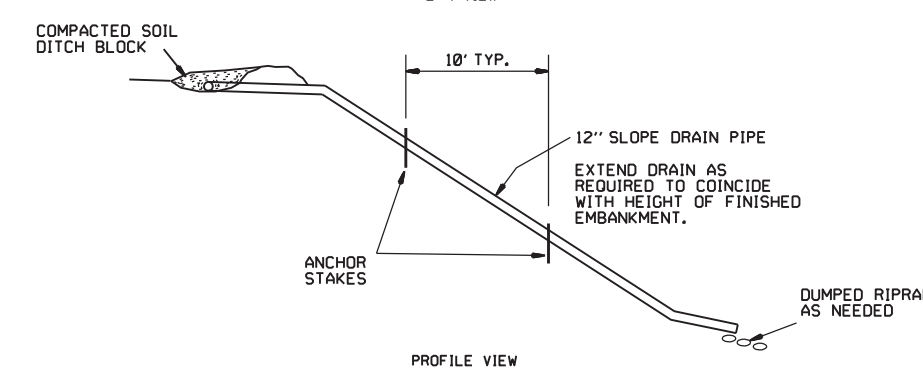
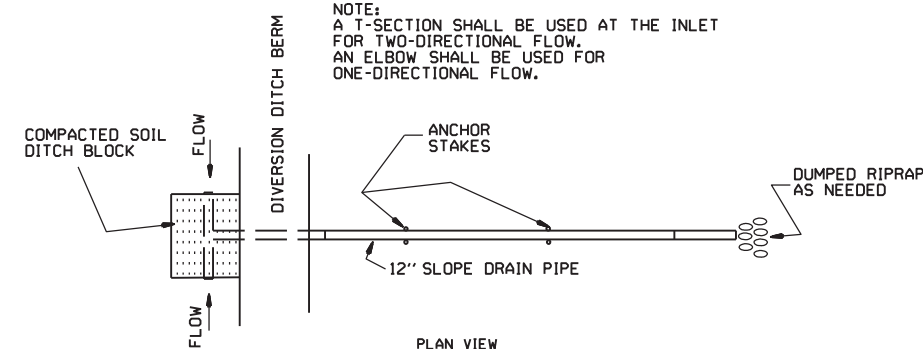


SEDIMENT BASIN WITH PIPE OUTLET (E-10)

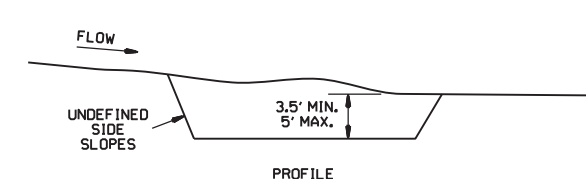
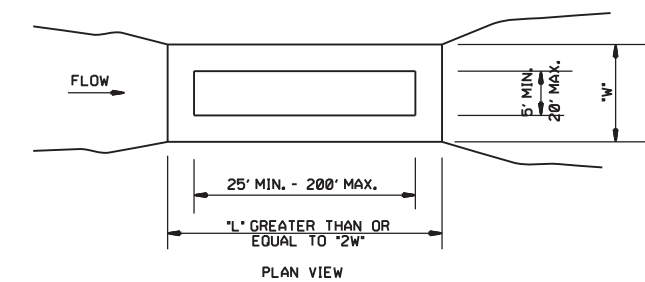


DIVERSION DITCH (E-8)

NOTE:
A T-SECTION SHALL BE USED AT THE INLET
FOR TWO-DIRECTIONAL FLOW.
AN ELBOW SHALL BE USED FOR
ONE-DIRECTIONAL FLOW.



SLOPE DRAIN (E-12)



SEDIMENT BASIN (E-14)

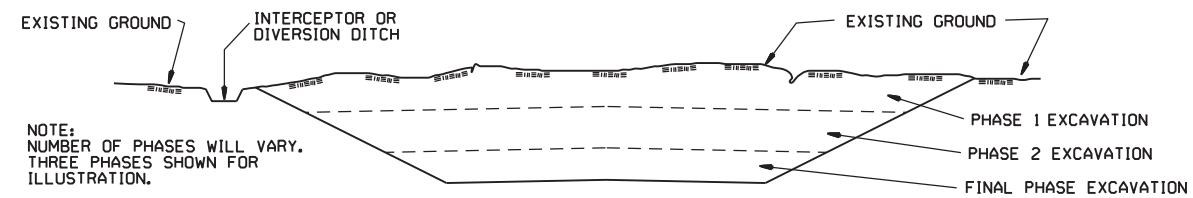
			ARKANSAS STATE HIGHWAY COMMISSION
			TEMPORARY EROSION CONTROL DEVICES
6-2-94	Revised E-8 & E-12; Added E-14 & Deleted E-13		
4-1-93	ISSUED		
DATE	REVISION	FILMED	STANDARD DRAWING TEC-2

CLEARING AND GRUBBING

CONSTRUCTION SEQUENCE

1. PLACE PERIMETER CONTROLS (I.E. SILT FENCES, DIVERSION DITCHES, SEDIMENT BASINS, ETC.)
2. PERFORM CLEARING AND GRUBBING OPERATION.

EXCAVATION



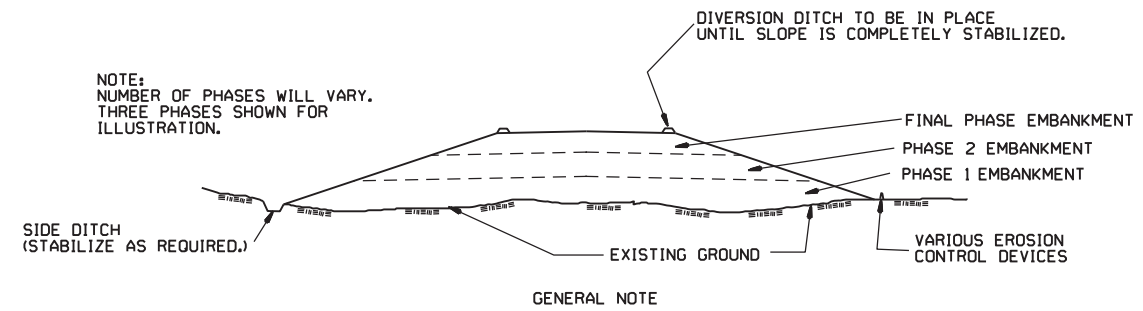
GENERAL NOTE

ALL CUT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE EXCAVATED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

CONSTRUCTION SEQUENCE

1. EXCAVATE AND STABILIZE INTERCEPTOR AND/OR DIVERSION DITCHES.
2. PERFORM PHASE 1 EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING.
3. PERFORM PHASE 2 EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING.
4. PERFORM FINAL PHASE OF EXCAVATION. PLACE PERMANENT OR TEMPORARY SEEDING. STABILIZE DITCHES, CONSTRUCT DITCH CHECKS, DIVERSION DITCHES, SEDIMENT BASINS, OR OTHER EROSION CONTROL DEVICES AS REQUIRED.

EMBANKMENT



GENERAL NOTE

ALL EMBANKMENT SLOPES SHALL BE DRESSED, PREPARED, SEEDED, AND MULCHED AS THE WORK PROGRESSES. SLOPES SHALL BE CONSTRUCTED AND STABILIZED IN EQUAL INCREMENTS NOT TO EXCEED 25 FEET, MEASURED VERTICALLY.

CONSTRUCTION SEQUENCE

1. CONSTRUCT DIVERSION DITCHES, DITCH CHECKS, SEDIMENT BASINS, SILT FENCES, OR OTHER EROSION CONTROL DEVICES AS SPECIFIED.
2. PLACE PHASE 1 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
3. PLACE PHASE 2 EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PROVIDE DIVERSION DITCHES AND SLOPE DRAINS IF EMBANKMENT CONSTRUCTION IS TO BE TEMPORARILY ABANDONED FOR A PERIOD OF GREATER THAN 21 DAYS.
4. PLACE FINAL PHASE OF EMBANKMENT WITH PERMANENT OR TEMPORARY SEEDING. PLACE DIVERSION DITCHES AND SLOPE DRAINS AND MAINTAIN UNTIL ENTIRE SLOPE IS STABILIZED.

			ARKANSAS STATE HIGHWAY COMMISSION
			TEMPORARY EROSION CONTROL DEVICES
11-03-94	CORRECTED SPELLING		
6-2-94	Drawn & Issued		6-2-94
DATE	REVISION		FILMED
			STANDARD DRAWING TEC-3

