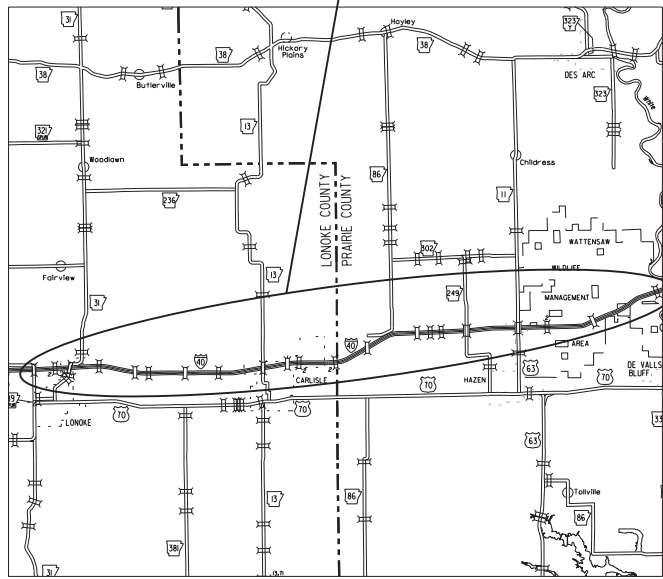


"A FULLY CONTROLLED ACCESS FACILITY"
 ARKANSAS DEPARTMENT OF TRANSPORTATION
 CONSTRUCTION PLANS FOR STATE HIGHWAY

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/11/2022				6	ARK.			
				JOB NO.		061687	1	63
				HWY. 31 - EAST (S)				

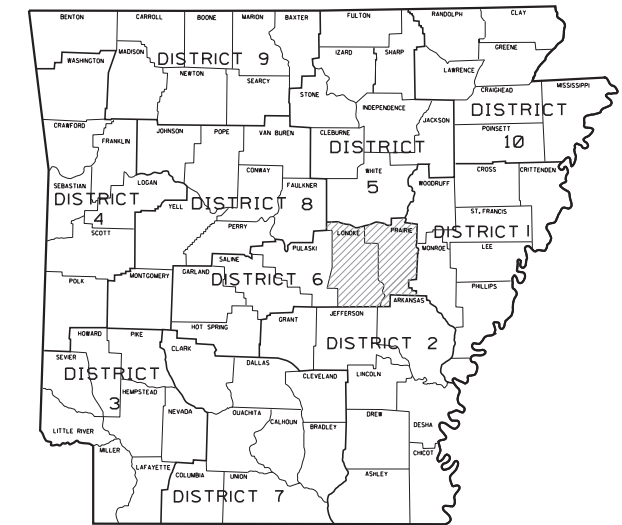
PROJECT LOCATION



VICINITY MAP

HWY. 31 - EAST (S)
 LONOKE & PRAIRIE COUNTIES
 ROUTE 40 SECTIONS 41 & 42
 JOB 061687
 FED. AID PROJ. NHPP - 40 - 4(87)

NOT TO SCALE



ARKANSAS HIGHWAY DISTRICT 6

FOR BRIDGE DATA AND STRUCTURES OVER 20'-0" SPAN SEE SHEET NO. 2.

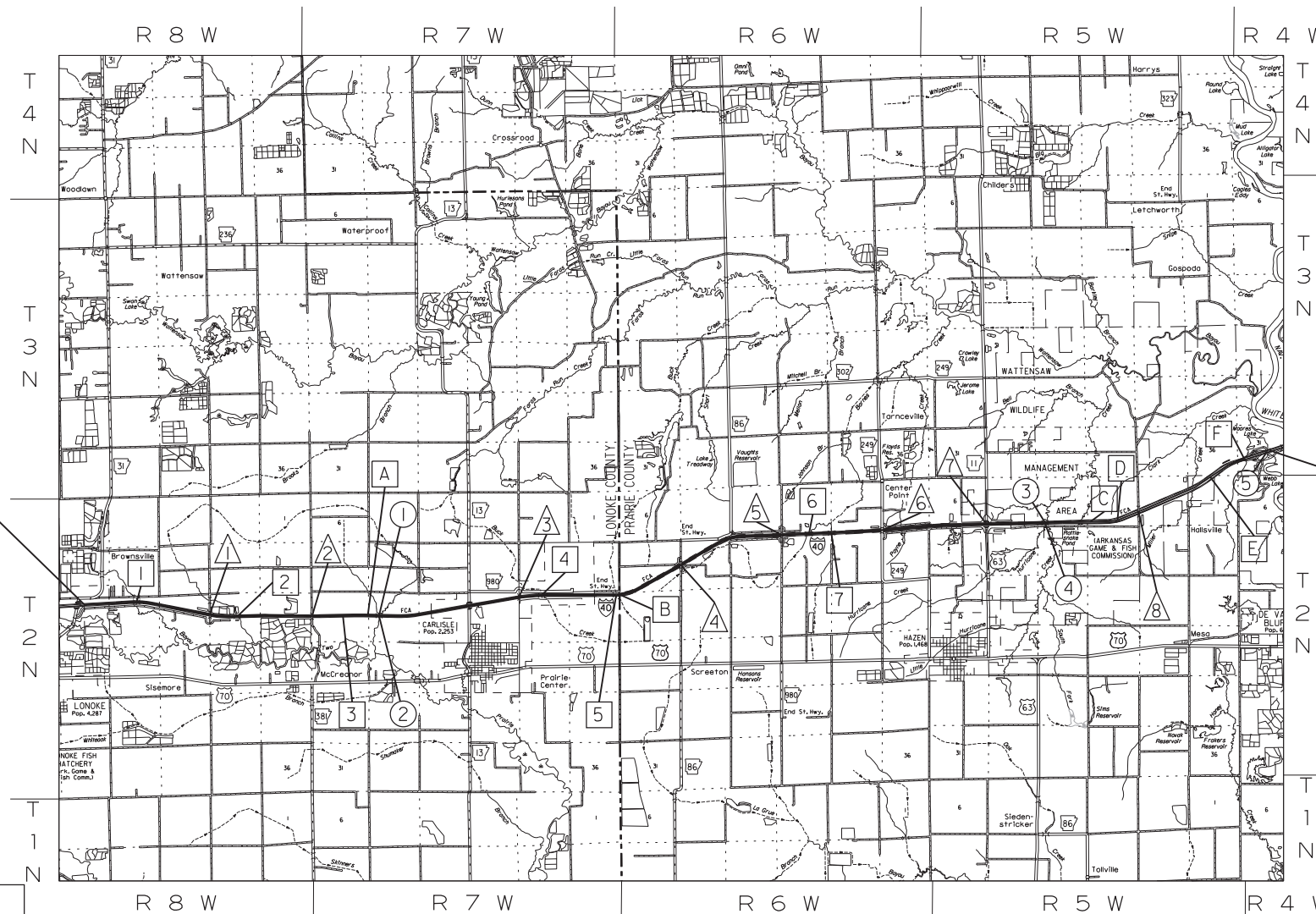
STA. 587+22.80
 BEGIN JOB 061687
 L.M. 174.90

EQUATIONS:

- A** STA. 885+00.00 BK. = STA. 884+89.09 AHD.
- B** STA. 1152+07.89 BK. = STA. 1152+14.44 AHD.
- C** STA. 1679+21.68 BK. = STA. 1679+07.70 AHD.
- D** STA. 1702+27.00 BK. = STA. 1702+00.00 AHD.
- E** STA. 1799+24.50 BK. = STA. 1798+46.50 AHD.
- F** STA. 1834+16.00 BK. = STA. 1834+34.70 AHD.

PROJECT COORDINATES

	BEGIN	MID-POINT	END
LATITUDE	N 34°48'15"	N 34°48'40"	N 34°50'15"
LONGITUDE	W 91°52'59"	W 91°40'23"	W 91°27'58"
STATION	587+22.80	1228+29.65	1868+34.40



DESIGN TRAFFIC DATA

DESIGN YEAR	-----	2042
2022 ADT	-----	40000
2042 ADT	-----	52000
2042 DHV	-----	5720
DIRECTIONAL DISTRIBUTION	-----	60%
TRUCKS	-----	56%
DESIGN SPEED	-----	70 MPH

STA. 1868+34.40
 END JOB 061687



DIGITALLY SIGNED 10/11/2022

GROSS LENGTH OF PROJECT	128,216.24 FEET OR 24.283 MILES
NET LENGTH OF ROADWAY	127,817.71 FEET OR 24.208 MILES
NET LENGTH OF BRIDGES	398.53 FEET OR 0.075 MILES
NET LENGTH OF PROJECT	127,817.71 FEET OR 24.208 MILES

BRIDGE DATA

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
						061687	2	63

② BRIDGE DATA AND STRUCTURES OVER 20'-0" SPAN

- | | | |
|---|---|---|
| <p>① STA. 891+19.30 BRIDGE END
BRIDGE NO. A3654
EXISTING 75'-0" R.C. SLAB UNIT
40'-0" CLEAR ROADWAY
STA. 891+94.30 BRIDGE END
POLYMER OVERLAY</p> | <p>② STA. 891+19.30 BRIDGE END
BRIDGE NO. B3654
EXISTING 75'-0" R.C. SLAB UNIT
40'-0" CLEAR ROADWAY
STA. 891+94.30 BRIDGE END
POLYMER OVERLAY</p> | <p>③ STA. 1614+45.25 BRIDGE END (LT)
BRIDGE NO. A3709
EXISTING 145'-0" R.C. SLAB UNIT
30° RT. FWD SKEW
40'-0" CLEAR ROADWAY
STA. 1614+90.25 BRIDGE END (LT)
POLYMER OVERLAY</p> |
| <p>④ STA. 1614+45.25 BRIDGE END (RT)
BRIDGE NO. B3709
EXISTING 145'-0" R.C. SLAB UNIT
30° RT. FWD SKEW
40'-0" CLEAR ROADWAY
STA. 1614+90.25 BRIDGE END (RT)
POLYMER OVERLAY</p> | <p>⑤ STA. 1865+82.40 BRIDGE END
BRIDGE NO. 06893
EXISTING 252'-0" R.C. AND STEEL
80'-0" CLEAR ROADWAY
STA. 1868+34.40 BRIDGE END
RETAIN</p> | |



DIGITALLY SIGNED 10/28/2022

BRIDGE DATA (OVERPASSES)

- | | | | |
|---|--|---|---|
| <p>△ STA. 13+93.11 BRIDGE END
BRIDGE NO. 03652 LILLY ROAD
EXISTING 215.30' COMP. IBEAM SPANS
20'-0" CLEAR ROADWAY
STA. 16+08.41 BRIDGE END
RETAIN</p> | <p>△ STA. 13+93.85 BRIDGE END
BRIDGE NO. 03653 NORTHCUT ROAD
EXISTING 212.30' COMP. IBEAM SPANS
20'-0" CLEAR ROADWAY
STA. 16+06.15 BRIDGE END
RETAIN</p> | <p>△ STA. 13+91.85 BRIDGE END
BRIDGE NO. 03656 PAUSCHERT ROAD
EXISTING 216.30' COMP. IBEAM SPANS
24'-0" CLEAR ROADWAY
STA. 16+08.15 BRIDGE END
RETAIN</p> | <p>△ STA. 13+78.69 BRIDGE END
BRIDGE NO. 03670 HALIJAN ROAD
EXISTING 242.62' COMP. IBEAM UNIT
24'-0" CLEAR ROADWAY
STA. 16+21.31 BRIDGE END
RETAIN</p> |
| <p>△ STA. 13+93.86 BRIDGE END
BRIDGE NO. 03671 ANDERSON ROAD
EXISTING 212.28' COMP. IBEAM UNIT
24'-0" CLEAR ROADWAY
STA. 16+06.14 BRIDGE END
RETAIN</p> | <p>△ STA. 9+10.09 BRIDGE END
BRIDGE NO. 03672 HWY. 249
EXISTING 216.24' COMP. IBEAM UNIT
24'-0" CLEAR ROADWAY
STA. 11+26.33 BRIDGE END
RETAIN</p> | <p>△ STA. 36+12.20 BRIDGE END
BRIDGE NO. 03708 U.S. HWY. 63
EXISTING 216.20' COMP. IBEAM UNIT
28'-0" CLEAR ROADWAY
STA. 38+28.40 BRIDGE END
RETAIN</p> | <p>△ STA. 32+15.92 BRIDGE END
BRIDGE NO. 03710 FIRE TOWER ROAD
EXISTING 348'-0" COMP. IBEAM UNIT
24'-0" CLEAR ROADWAY
STA. 35+99.61 BRIDGE END
RETAIN</p> |

STRUCTURES OVER 20'-0" SPAN

- | | | | |
|---|---|--|---|
| <p>① STA. 633+55 - IN PLACE
DBL. 10' X 7' X 241' R.C. BOX CULVERT
SPAN = 22.08'
RETAIN</p> | <p>② STA. 741+68 - IN PLACE
DBL. 10' X 8' X 216' R.C. BOX CULVERT
30° RT. FORWARD SKEW
SPAN = 25.59'
RETAIN</p> | <p>③ STA. 858+63 - IN PLACE
DBL. 8' X 5' X 198' R.C. BOX CULVERT
30° RT. FORWARD SKEW
SPAN = 20.69'
RETAIN</p> | <p>④ STA. 1065+98 - IN PLACE
DBL. 10' X 6' X 176' R.C. BOX CULVERT
SPAN = 22.08'
RETAIN</p> |
| <p>⑤ STA. 1144+72 - IN PLACE
TRP. 10' X 4' X 186' R.C. BOX CULVERT
SPAN = 32.92'
RETAIN</p> | <p>⑥ STA. 1365+03 - IN PLACE
TRP. 10' X 8' X 198' R.C. BOX CULVERT
SPAN = 33.00'
RETAIN</p> | <p>⑦ STA. 1388+73 - IN PLACE
TRP. 10' X 8' X 204' R.C. BOX CULVERT
SPAN = 22.17'
RETAIN</p> | |

BRIDGE DATA AND
STRUCTURES OVER 20'-0" SPAN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.	061687	3	63	

2 INDEX OF SHEETS AND STANDARD DRAWINGS



DIGITALLY SIGNED 10/28/2022

INDEX OF SHEETS

SHEET NO.	TITLE	BRIDGE NO.	DRWG.NO.
1	TITLE SHEET		
2	BRIDGE DATA AND STRUCTURES OVER 20'-0" SPAN		
3	INDEX OF SHEETS AND STANDARD DRAWINGS		
4	GOVERNING SPECIFICATIONS AND GENERAL NOTES		
5 - 7	TYPICAL SECTIONS OF IMPROVEMENT		
8 - 10	SPECIAL DETAILS		
11	TEMPORARY EROSION CONTROL DETAILS		
12 - 21	MAINTENANCE OF TRAFFIC DETAILS		
22 - 25	QUANTITY SHEETS		
26	SCHEDULE OF BRIDGE QUANTITIES	03652, 03653, A&B3654, 03656, 03670, 03671, 03672, 03708, A&B3709	61771
27	SUMMARY OF QUANTITIES AND REVISIONS		
28 - 58	PLAN SHEETS		
59	DETAILS OF LATEX MODIFIED CONCRETE OVERLAY WITH GRADE RAISE	03656	61772
60	DETAILS OF LATEX MODIFIED CONCRETE OVERLAY	03670, 03671	61773
61	DETAILS OF LATEX MODIFIED CONCRETE OVERLAY WITH GRADE RAISE	03708	61774
62	MISCELLANEOUS DETAILS	03656, 03670, 03671 & 03708	61775
63	APPROACH SLAB DETAILS	03708	61776

BRIDGE STANDARD DRAWINGS

DRWG.NO.	TITLE	DATE
55064	STANDARD DETAILS FOR JOINT REPAIRS & MODIFICATION	11-07-19

ROADWAY STANDARD DRAWINGS

DRWG.NO.	TITLE	DATE
PM-1	PAVEMENT MARKING DETAILS	02-27-20
PM-2	PERMANENT PAVEMENT MARKING ON ACCESS CONTROLLED ROADWAYS	05-14-20
PU-1	DETAILS OF PIPE UNDERDRAIN	12-08-16
TC-1	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	11-07-19
TC-2	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	05-20-21
TC-3	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION	08-12-21
TC-4	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION-TEMPORARY PRECAST BARRIER	11-07-19
TC-5	STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION-TEMPORARY PRECAST BARRIER	11-07-19
TEC-1	TEMPORARY EROSION CONTROL DEVICES	11-16-17
TR-1A	DETAILS OF STANDARD TURNOUT FOR ENTRANCE & EXIT RAMP (NON-REINFORCED)	08-22-02

S:\donaldson 10/24/2022 3:34:39 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_INDEX.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/11/2022				6	ARK.			
10/28/2022								
11/8/2022								
				JOB NO.		061687	4	63

2 GOVERNING SPECIFICATIONS AND GENERAL NOTES

GOVERNING SPECIFICATIONS

ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS:

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
FHWA-1273	REQUIRED CONTRACT PROVISIONS FEDERAL-AID CONSTRUCTION CONTRACTS
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - NOTICE TO CONTRACTORS
FHWA-1273	SUPPLEMENT - SPECIFIC EQUAL EMPLOYMENT OPPORTUNITY RESPONSIBILITIES (23 U.S.C. 140)
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - GOALS AND TIMETABLES
FHWA-1273	SUPPLEMENT - EQUAL EMPLOYMENT OPPORTUNITY - FEDERAL STANDARDS
FHWA-1273	SUPPLEMENT - POSTERS AND NOTICES REQUIRED FOR FEDERAL-AID PROJECTS
FHWA-1273	SUPPLEMENT - TRAINING PROGRAM - JOB 061687
FHWA-1273	SUPPLEMENT - WAGE RATE DETERMINATION
100-3	CONTRACTOR'S LICENSE
100-4	DEPARTMENT NAME CHANGE
102-2	ISSUANCE OF PROPOSALS
105-4	MAINTENANCE DURING CONSTRUCTION
107-2	RESTRAINING CONDITIONS
108-1	LIQUIDATED DAMAGES
108-2	WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER
303-1	AGGREGATE BASE COURSE
306-1	QUALITY CONTROL AND ACCEPTANCE
307-1	CEMENT
308-1	CEMENT
400-1	TACK COATS
400-4	DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES
400-5	PERCENT AIR VOIDS FOR ACHM MIX DESIGNS
400-6	LIQUID ANTI-STRIP ADDITIVE
400-7	TRACKLESS TACK
404-3	DESIGN OF ASPHALT MIXTURES
410-1	CONSTRUCTION REQUIREMENTS AND ACCEPTANCE OF ASPHALT CONCRETE PLANT MIX COURSES
410-2	DEVICES FOR MEASURING DENSITY FOR ROLLING PATTERNS
410-4	EVALUATION OF ACHM SUBLOT REPLACEMENT MATERIAL
501-2	CEMENT
603-1	LANE CLOSURE NOTIFICATION
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
604-3	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES (MASH)
621-1	FILTER SOCKS
800-1	STRUCTURES
802-4	CEMENT
804-2	REINFORCING STEEL FOR STRUCTURES
JOB 061687	ASSESSMENT OF WORKING DAYS - MAINTENANCE OF TRAFFIC
JOB 061687	BIDDING REQUIREMENTS AND CONDITIONS
JOB 061687	BRIDGE DECK REPAIR FOR POLYMER OVERLAYS
JOB 061687	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB 061687	CARGO PREFERENCE ACT REQUIREMENTS
JOB 061687	COLD MILLING - MILL & INLAY
JOB 061687	CONCRETE BRIDGE DECK CURING AND SURFACE TREATMENT RESTRICTIONS
JOB 061687	CONSTRUCTION PROJECT INFORMATION SIGN
JOB 061687	DISADVANTAGED BUSINESS ENTERPRISE BIDDER'S RESPONSIBILITIES
JOB 061687	ENHANCED THERMOPLASTIC PAVEMENT MARKING
JOB 061687	FLEXIBLE BEGINNING OF WORK - CALENDAR DAY CONTRACT
JOB 061687	GOALS FOR DISADVANTAGED BUSINESS ENTERPRISE PARTICIPATION
JOB 061687	JOINT REHABILITATION FOR BRIDGE DECKS
JOB 061687	LIQUIDATED DAMAGES PROCEDURE FOR BID LETTINGS
JOB 061687	LONGITUDINAL JOINT DENSITIES FOR ACHM SURFACE COURSES
JOB 061687	MAINTENANCE OF TRAFFIC
JOB 061687	MANDATORY ELECTRONIC CONTRACT
JOB 061687	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB 061687	PARTNERING REQUIREMENTS
JOB 061687	PERCENT WITHIN LIMITS
JOB 061687	POLYMER OVERLAY
JOB 061687	PRICE ADJUSTMENT FOR ASPHALT BINDER
JOB 061687	PRICE ADJUSTMENT FOR FUEL
JOB 061687	PROHIBITION OF CERTAIN TELECOMMUNICATIONS AND VIDEO SURVEILLANCE SERVICES OR EQUIPMENT
JOB 061687	RESTRICTIONS ON THE USE OF RECYCLED ASPHALT PAVEMENT MATERIAL
JOB 061687	SITE USE (A+C METHOD) - CALENDAR DAY CONTRACT
JOB 061687	SPECIAL CLEARING
JOB 061687	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB 061687	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB 061687	UTILITY ADJUSTMENTS
JOB 061687	VALUE ENGINEERING
JOB 061687	WARM MIX ASPHALT
JOB 061687	WELLHEAD PROTECTION



DIGITALLY SIGNED 11/8/2022

GENERAL NOTES

1. ALL PIPE LINES, POWER, TELEPHONE, AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
2. ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
3. ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
4. THE SEQUENCE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS IS A GENERAL OUTLINE FOR THE CONSTRUCTION OF THIS PROJECT, AND IN NO WAY IS IT INTENDED TO COVER EVERY ITEM IN THE PROJECT. ITEMS NOT CRITICAL TO THE CONSTRUCTION SEQUENCE MAY BE CONSTRUCTED IN ANY STAGE AS APPROVED BY THE RESIDENT ENGINEER.

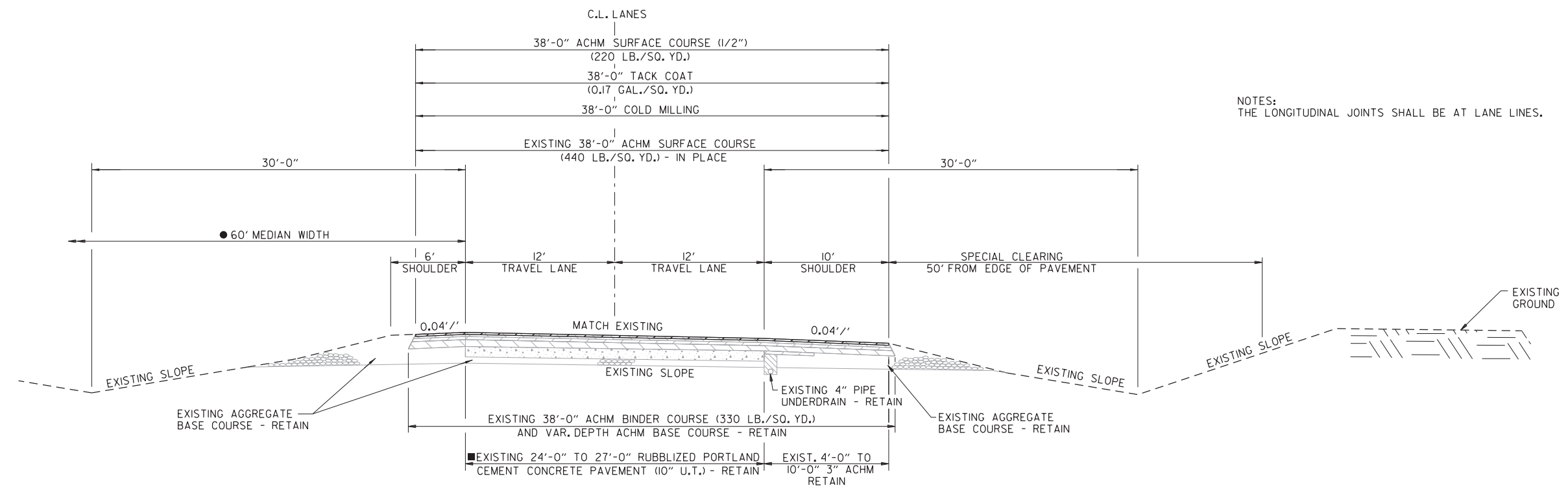
S:\Donaldson
 WORKSPACE - AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_SPECS_NOTES.dgn
 11/7/2022 4:09:10 PM
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061687	5	63	

2 TYPICAL SECTIONS OF IMPROVEMENT



DIGITALLY SIGNED 8/29/2022



NOTES:
THE LONGITUDINAL JOINTS SHALL BE AT LANE LINES.

I-40 TYPICAL SECTION
(SHOWN IN THE DIRECTION OF TRAFFIC)

STA. 587+22.80 TO STA. 621+38.00 LT. MAIN LANES
 STA. 655+05.86 TO STA. 758+69.44 LT. MAIN LANES
 STA. 772+64.82 TO STA. 863+25.04 LT. MAIN LANES
 STA. 876+10.21 TO STA. 885+00.00 LT. MAIN LANES
 STA. 884+89.09 TO STA. 890+84.30 LT. MAIN LANES
 STA. 892+29.30 TO STA. 1041+43.96 LT. MAIN LANES
 STA. 1065+84.95 TO STA. 1139+18.06 LT. MAIN LANES
 STA. 1174+09.36 TO STA. 1264+04.45 LT. MAIN LANES
 STA. 1296+71.61 TO STA. 1614+08.03 LT. MAIN LANES
 STA. 1616+26.75 TO STA. 1675+55.60 LT. MAIN LANES
 STA. 1701+22.00 TO STA. 1702+27.00 LT. MAIN LANES
 STA. 1702+00.00 TO STA. 1799+24.50 LT. MAIN LANES
 STA. 1798+46.50 TO STA. 1809+83.80 LT. MAIN LANES
 STA. 1832+06.80 TO STA. 1834+16.00 LT. MAIN LANES

STA. 587+22.80 TO STA. 708+95.69 RT. MAIN LANES
 STA. 739+16.80 TO STA. 814+34.25 RT. MAIN LANES
 STA. 826+04.20 TO STA. 885+00.00 RT. MAIN LANES
 STA. 884+89.09 TO STA. 890+84.30 RT. MAIN LANES
 STA. 892+29.30 TO STA. 910+82.44 RT. MAIN LANES
 STA. 937+12.13 TO STA. 1139+18.06 RT. MAIN LANES
 STA. 1174+09.36 TO STA. 1264+04.45 RT. MAIN LANES
 STA. 1296+71.61 TO STA. 1614+57.19 RT. MAIN LANES
 STA. 1616+75.49 TO STA. 1678+87.60 RT. MAIN LANES
 STA. 1704+00.00 TO STA. 1769+74.10 RT. MAIN LANES
 STA. 1800+46.50 TO STA. 1814+92.20 RT. MAIN LANES

■ THE EXISTING PORTLAND CEMENT CONCRETE PAVEMENT (10" U.T.) WAS REMOVED IN A PREVIOUS PROJECT FROM THE STATION RANGES LISTED BELOW. IN THESE RANGES, 6" OF AGGREGATE BASE COURSE (CLASS 7) EXISTS IN PLACE OF THE RUBBLIZED CONCRETE SLAB.

STA. 703+50.00 TO STA. 721+00.00 LT. & RT. MAIN LANES
 STA. 814+50.00 TO STA. 827+50.00 LT. & RT. MAIN LANES
 STA. 880+50.00 TO STA. 890+84.00 LT. & RT. MAIN LANES
 STA. 892+29.00 TO STA. 901+50.00 LT. & RT. MAIN LANES
 STA. 1608+75.00 TO STA. 1614+57.25 RT. MAIN LANES
 STA. 1608+75.00 TO STA. 1614+08.75 LT. MAIN LANES

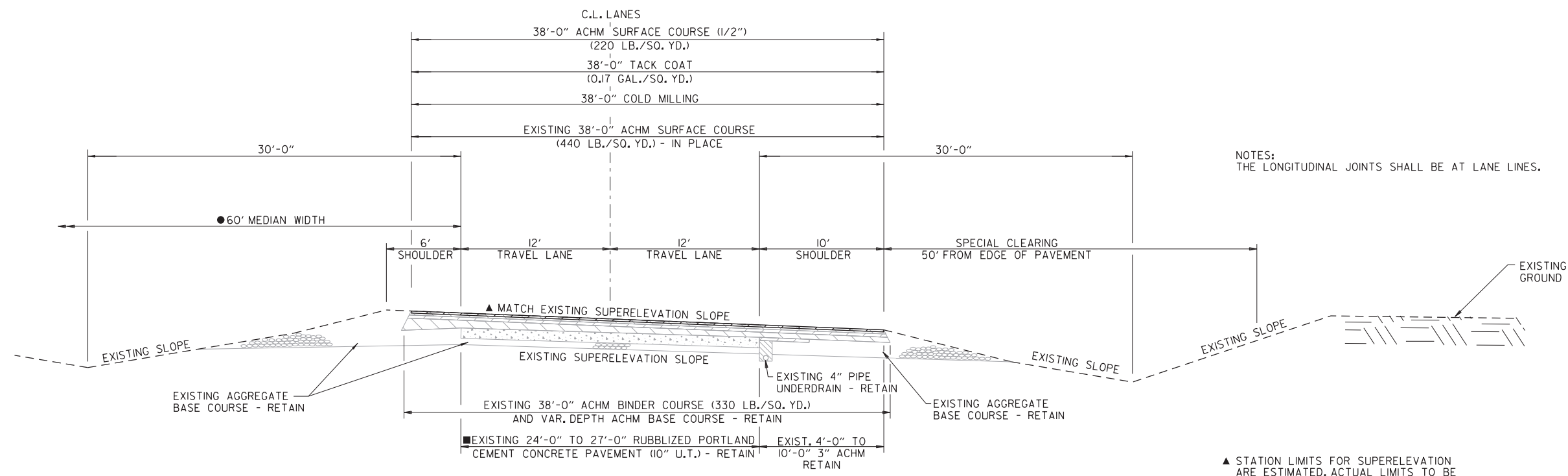
● 176' MEDIAN FROM STA. 1702+02.32 TO STA. 1811+65.14

S:\donaldson 8/26/2022 10:05:52 PM
 WORKSPACE: AHTD
 L:\2017\17017580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_TYP_01.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						061687	6	63
				2 TYPICAL SECTIONS OF IMPROVEMENT				



DIGITALLY SIGNED 8/29/2022



NOTES:
THE LONGITUDINAL JOINTS SHALL BE AT LANE LINES.

I-40 SUPERELEVATED SECTION
(SHOWN IN THE DIRECTION OF TRAFFIC)

- | | |
|---|---|
| <p>STA. 621+38.00 TO STA. 655+05.86 LT. MAIN LANES
 STA. 758+69.44 TO STA. 772+64.82 LT. MAIN LANES
 STA. 863+25.04 TO STA. 876+10.21 LT. MAIN LANES
 STA. 1041+43.96 TO STA. 1065+84.95 LT. MAIN LANES
 STA. 1139+18.06 TO STA. 1152+07.89 LT. MAIN LANES
 STA. 1152+14.44 TO STA. 1174+09.36 LT. MAIN LANES
 STA. 1264+04.45 TO STA. 1296+71.61 LT. MAIN LANES
 STA. 1675+55.60 TO STA. 1679+21.68 LT. MAIN LANES
 STA. 1679+07.70 TO STA. 1701+22.00 LT. MAIN LANES
 STA. 1809+83.80 TO STA. 1832+06.80 LT. MAIN LANES</p> | <p>STA. 708+95.69 TO STA. 739+16.80 RT. MAIN LANES
 STA. 814+34.25 TO STA. 826+04.20 RT. MAIN LANES
 STA. 910+82.44 TO STA. 937+12.13 RT. MAIN LANES
 STA. 1139+18.06 TO STA. 1152+07.89 RT. MAIN LANES
 STA. 1152+14.44 TO STA. 1174+09.36 RT. MAIN LANES
 STA. 1264+04.45 TO STA. 1296+71.61 RT. MAIN LANES
 STA. 1678+87.60 TO STA. 1679+21.68 RT. MAIN LANES
 STA. 1679+07.70 TO STA. 1702+27.00 RT. MAIN LANES
 STA. 1702+00.00 TO STA. 1704+00.00 RT. MAIN LANES
 STA. 1769+74.10 TO STA. 1799+24.50 RT. MAIN LANES
 STA. 1798+46.50 TO STA. 1800+46.50 RT. MAIN LANES
 STA. 1814+92.20 TO STA. 1834+16.00 RT. MAIN LANES</p> |
|---|---|

- ▲ STATION LIMITS FOR SUPERELEVATION ARE ESTIMATED. ACTUAL LIMITS TO BE DETERMINED IN THE FIELD.
- THE EXISTING PORTLAND CEMENT CONCRETE PAVEMENT (10" U.T.) WAS REMOVED IN A PREVIOUS PROJECT FROM THE STATION RANGES LISTED BELOW. IN THESE RANGES, 6" OF AGGREGATE BASE COURSE (CLASS 7) EXISTS IN PLACE OF THE RUBBLIZED CONCRETE SLAB.
- 176' MEDIAN FROM STA. 1702+02.32 TO STA. 1811+65.14

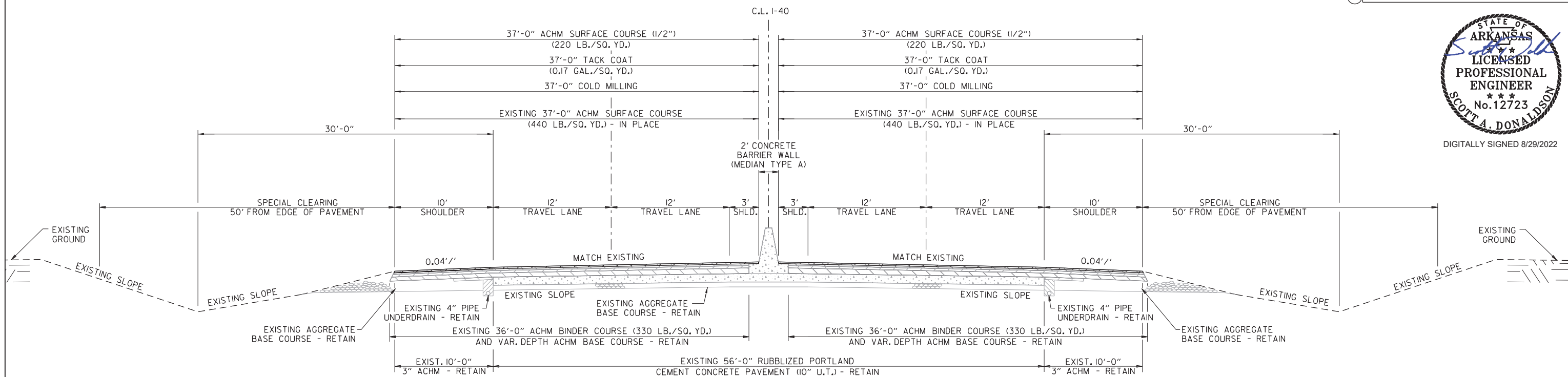
S:\adonaldson\8/26/2022 10:05:52 PM
 WORKSPACE: AHTD
 L:\2017\17017580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_TYP_01.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		7	63
				JOB NO.	061687			

2 TYPICAL SECTIONS OF IMPROVEMENT



DIGITALLY SIGNED 8/29/2022

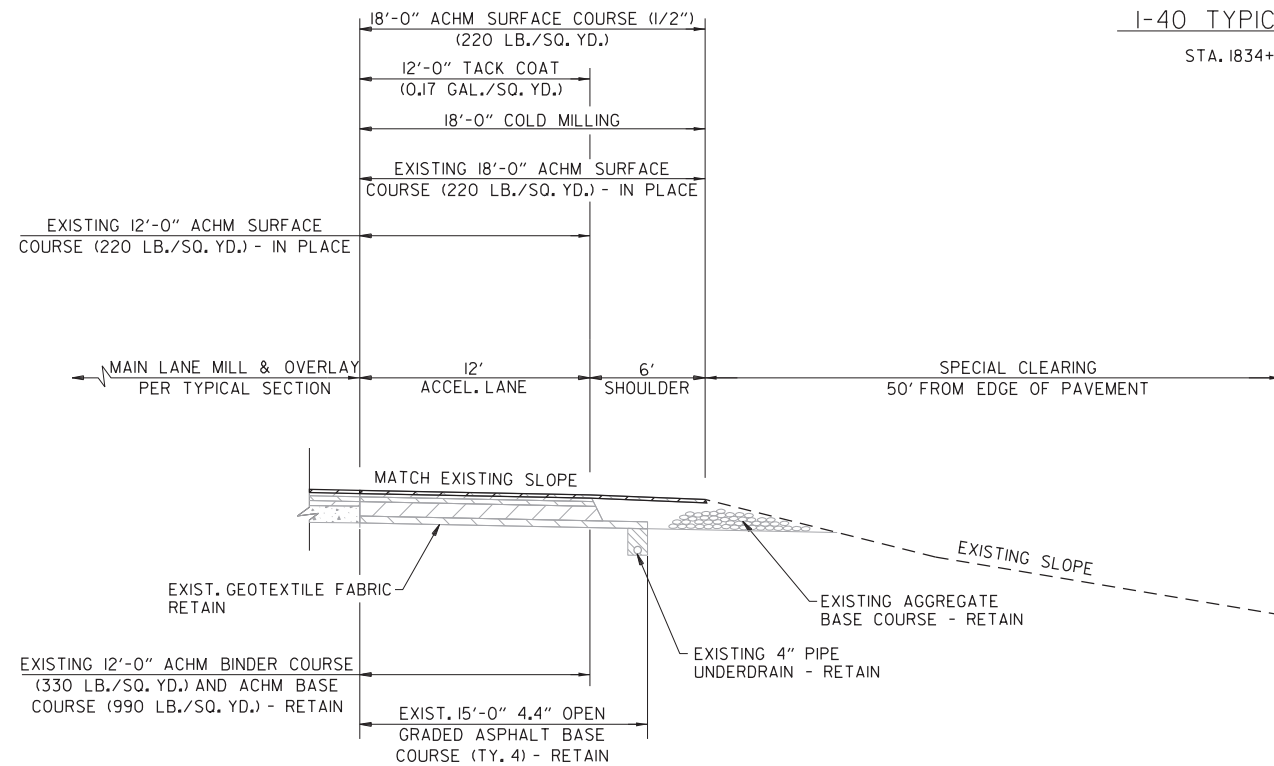


NOTES:
LONGITUDINAL JOINTS SHALL BE AT LANE LINES.

▲ 10' SHOULDER FROM STA. 969+93.01 TO STA. 978+09.25.

I-40 TYPICAL BARRIER SECTION

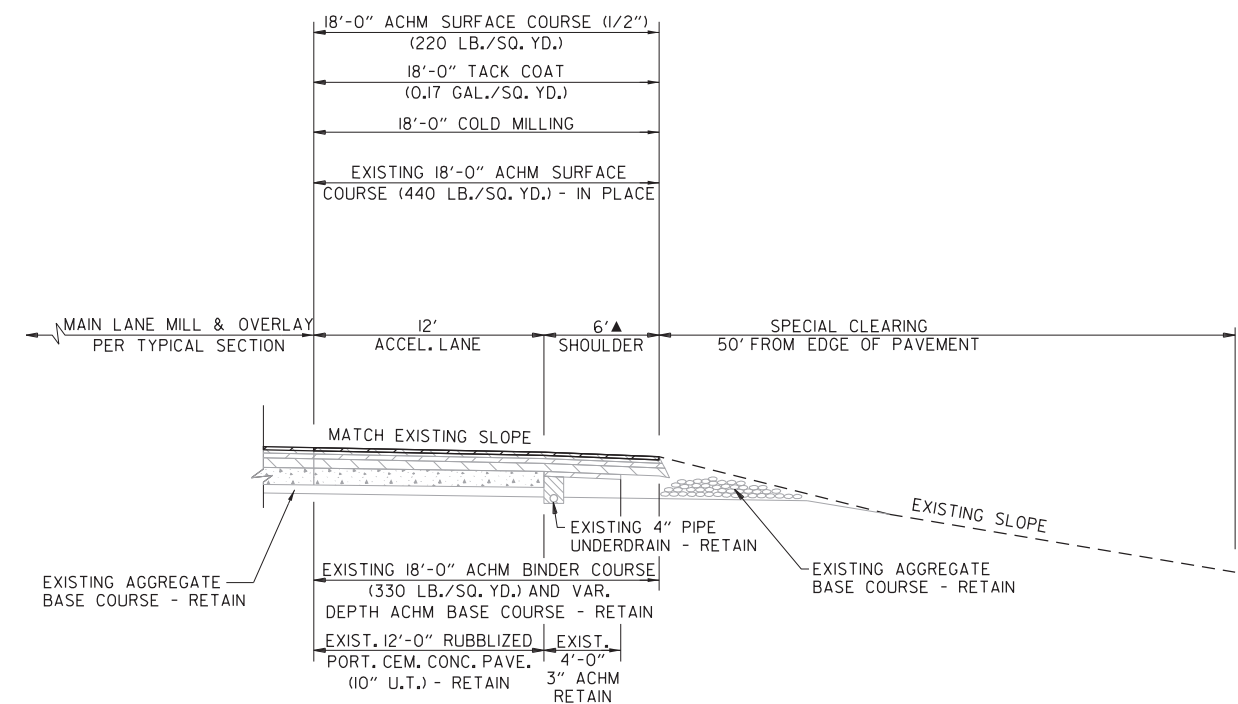
STA. 1834+34.70 TO STA. 1865+40.89



AUXILIARY LANE TYPICAL SECTION

(SHOWN IN THE DIRECTION OF TRAFFIC)

STA. 1533+00.00 TO STA. 1539+30.00 LT. MAIN LANES
 STA. 1566+87.00 TO STA. 1573+17.00 RT. MAIN LANES
 STA. 1832+45.00 TO STA. 1838+75.00 LT. MAIN LANES
 STA. 1859+12.00 TO STA. 1865+42.00 RT. MAIN LANES



AUXILIARY LANE TYPICAL SECTION

(SHOWN IN THE DIRECTION OF TRAFFIC)

STA. 976+23.00 TO STA. 978+64.00 LT. MAIN LANES
 STA. 1002+24.00 TO STA. 1005+31.00 RT. MAIN LANES
 STA. 1539+30.00 TO STA. 1545+56.00 LT. MAIN LANES
 STA. 1560+38.00 TO STA. 1566+87.00 RT. MAIN LANES
 STA. 1838+75.00 TO STA. 1844+80.00 LT. MAIN LANES
 STA. 1856+23.00 TO STA. 1859+12.00 RT. MAIN LANES

TYPICAL SECTIONS OF IMPROVEMENT

S.A. Donaldson 8/26/2022 10:05:53 PM
 WORKSPACE: AHTD
 L:\2017\071580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687.TYP_01.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061687	8	63	

2 SPECIAL DETAILS

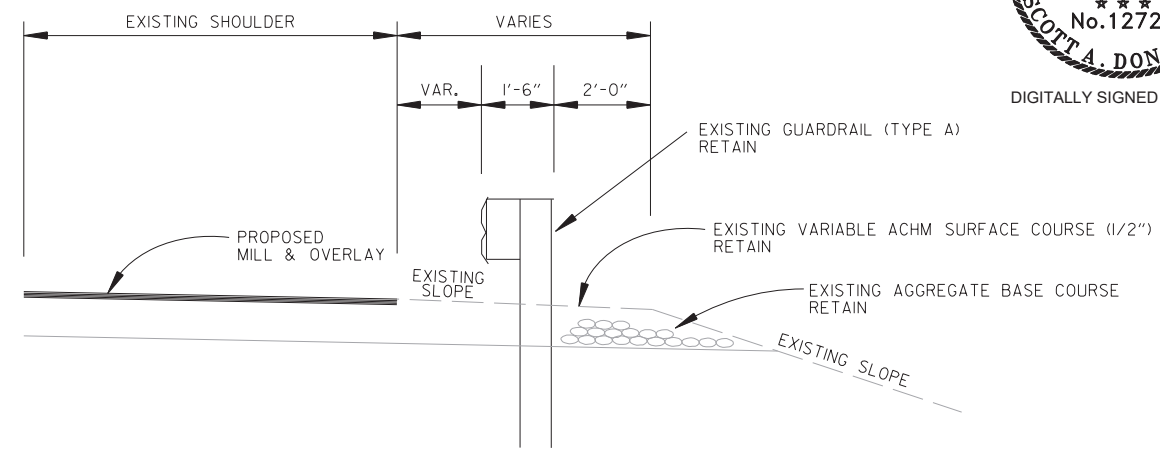


DIGITALLY SIGNED 8/29/2022

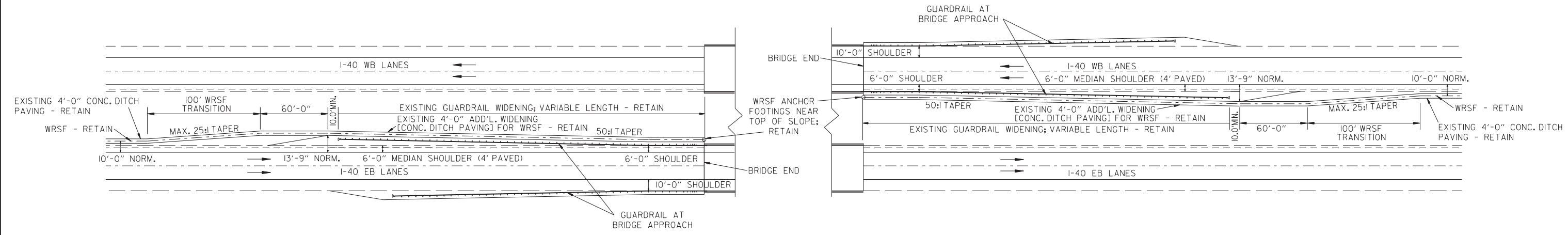


6.0" Radius, 1.3" Border, Black on Orange;
Job XXXXXX C 2K; *Start Date Mo Year* C 2K;
Est Completion Mo Year C 2K; *IDRIVE* Arial;
ARKANSAS.COM Arial;

CONSTRUCTION PROJECT INFORMATION SIGN



EXISTING GUARDRAIL
I-40 MAIN LANES



DETAIL OF WIRE ROPE SAFETY FENCE AT EXISTING BRIDGE ENDS

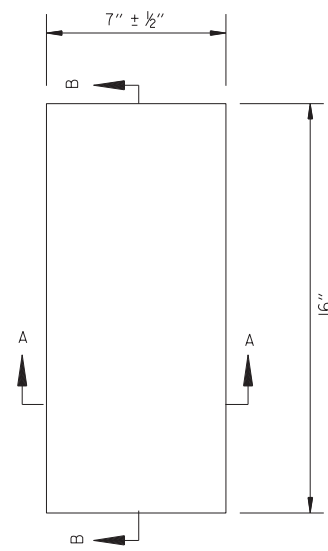
SADonaldson 8/26/2022 10:05:53 PM WORKSPACE: AHTD L:\2017\07580 - BB0608 Hwy. 31 - Prairie Co Line\Drawings\061687_SD_01.dgn REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						061687	9	63

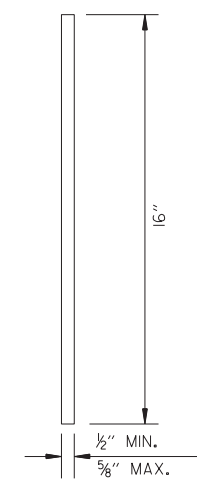
2 SPECIAL DETAILS



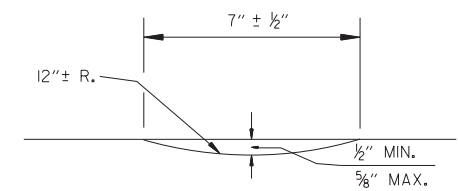
DIGITALLY SIGNED 8/29/2022



PLAN

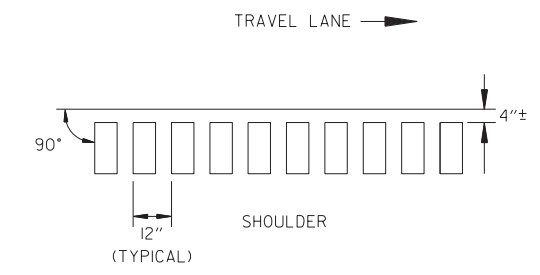


SECTION B-B

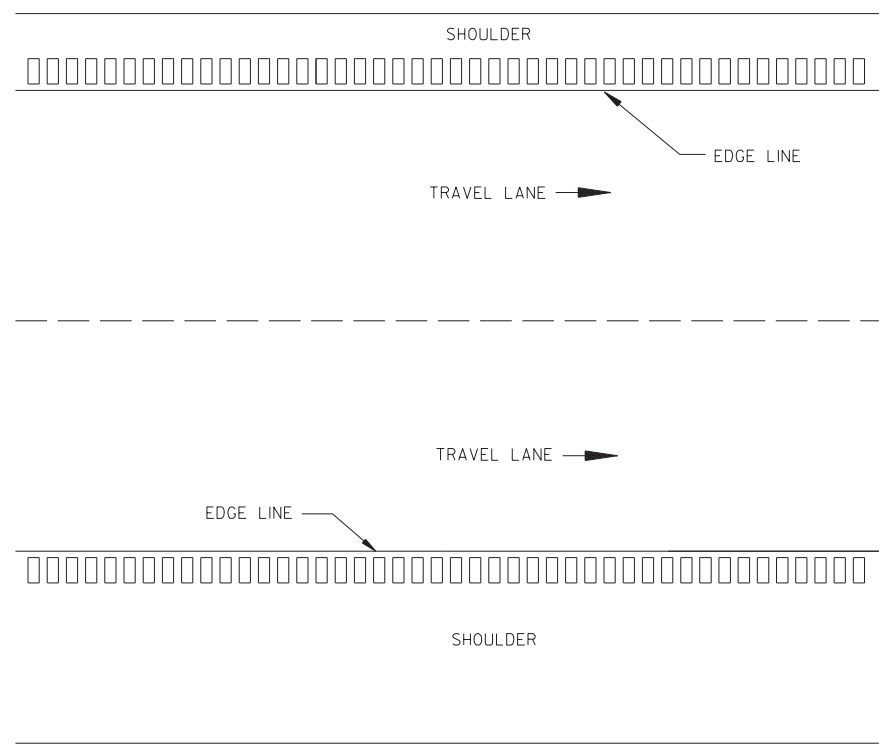


SECTION A-A

DETAILS OF RUMBLE STRIPS



LOCATION PLAN OF RUMBLE STRIPS
LEFT OR RIGHT SHOULDER



PLAN VIEW

NOTES:

1. ALIGNMENT OF RUMBLE STRIPS SHALL GENERALLY BE STRAIGHT AND OFFSET APPROXIMATELY 4" FROM THE OUTER EDGE OF THE EDGE LINE. THIS OFFSET MAY BE ADJUSTED TO ACCOMMODATE VARIATIONS IN THE EDGE LINE.
2. THE 1/2" DEPTH SHALL GENERALLY APPLY FOR THE ENTIRE 16" LENGTH. SOME VARIATION TO SUIT SHOULDER SLOPE BREAKS MAY BE NECESSARY.
3. RUMBLE STRIPS SHALL NOT BE INSTALLED ON BRIDGE DECKS, APPROACH SLABS, OR ACROSS TRANSVERSE JOINTS OF CONCRETE SHOULDERS.

S.A. Donaldson 8/26/2022 10:05:53 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_SD_01.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
						JOB NO. 061687	10	63

2 SPECIAL DETAILS

THIS SHEET INTENTIONALLY LEFT BLANK

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						061687	11	63
				TEMPORARY EROSION CONTROL DETAILS				

2

LEGEND

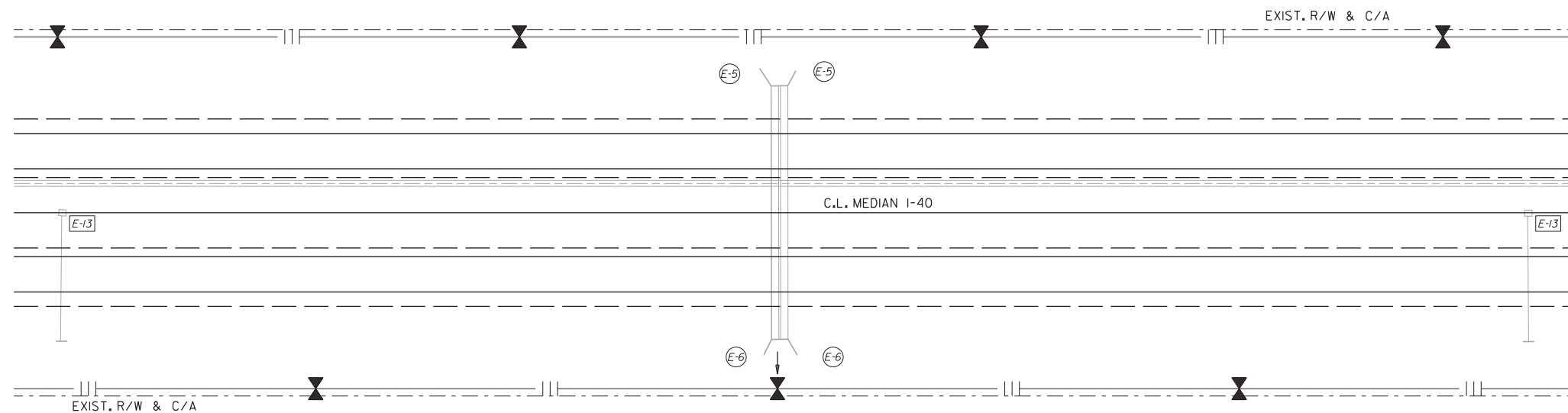
(E-5) = SAND BAG DITCH CHECKS (E-6) = ROCK DITCH CHECKS

[E-13] = FILTER SOCK DROP INLET PROTECTION

EROSION CONTROL MEASURES TO BE PLACED DURING APPROPRIATE STAGES. THESE DEVICES SHALL BE LEFT IN PLACE AS LONG AS REQUIRED TO CONTROL EROSION.



DIGITALLY SIGNED 8/29/2022



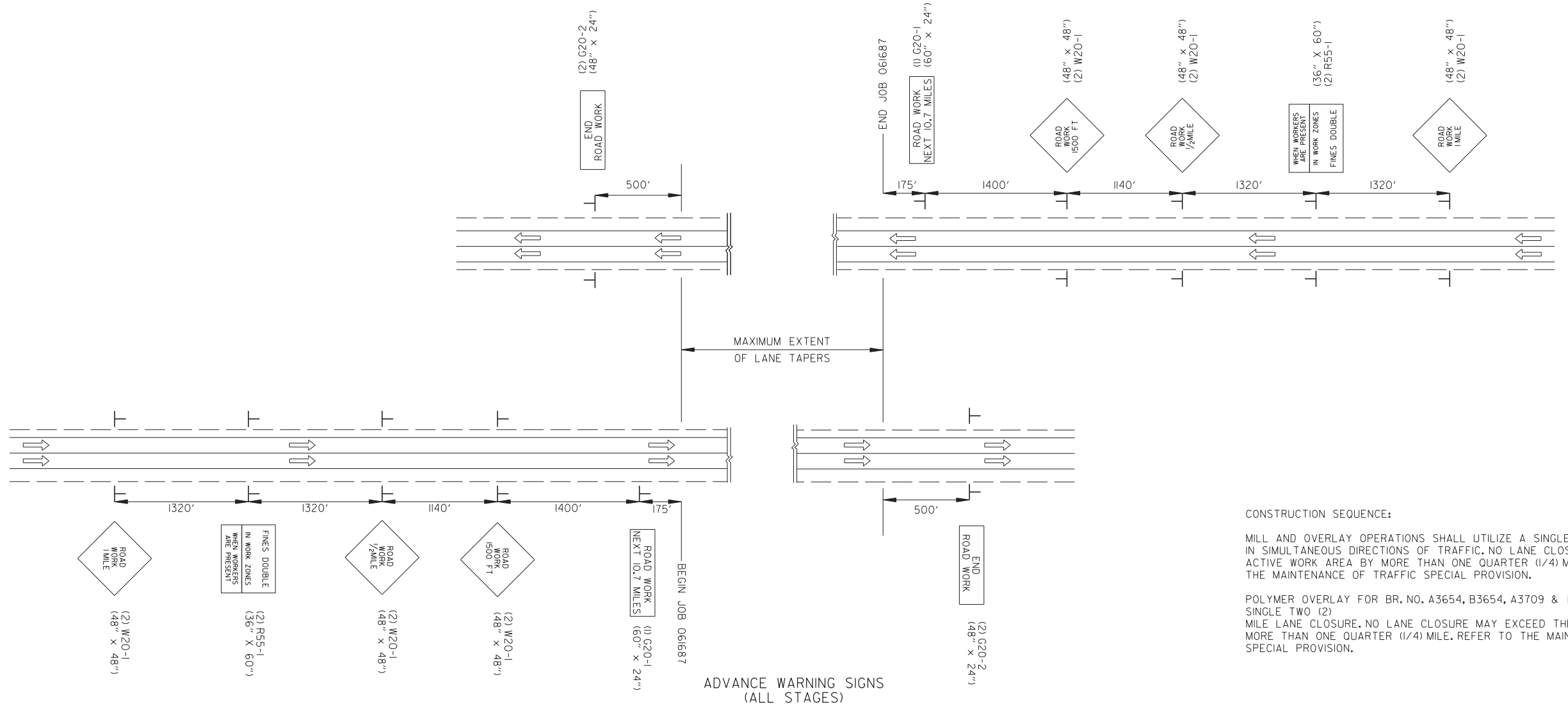
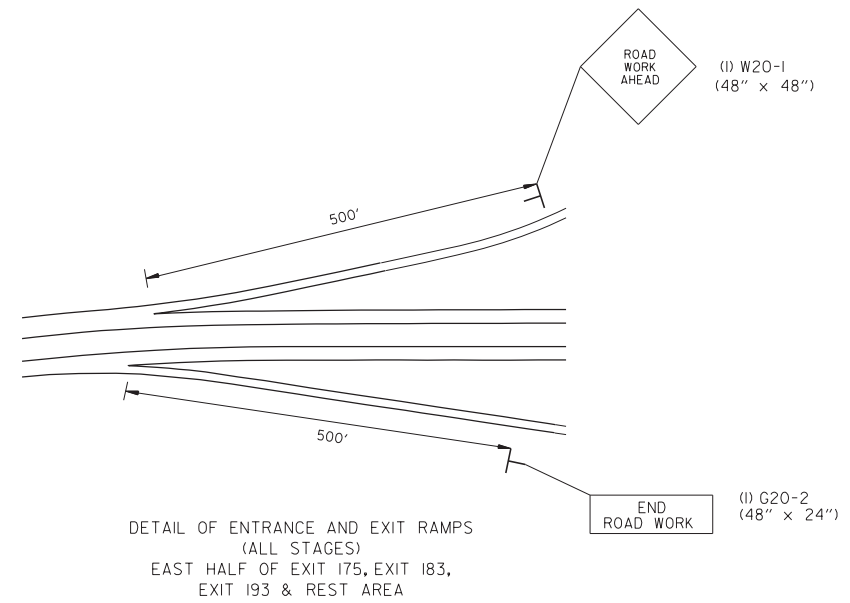
TYPICAL EROSION CONTROL DEVICE PLACEMENT

NOTE:
 DETAILED PLAN SHEETS FOR TEMPORARY EROSION CONTROL HAVE NOT BEEN PROVIDED SINCE THERE ARE NO EXPECTED SOIL DISTURBANCE ACTIVITIES INCLUDED IN THE PROJECT. AS A SUBSTITUTE, ESTIMATED QUANTITIES FOR EROSION CONTROL DEVICES HAVE BEEN PROVIDED TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER. THE PLAN VIEW ABOVE SHOWS TYPICAL USES OF EACH DEVICE. IT SHOWS DROP INLET SILT FENCE AT MEDIAN INLETS, AND DITCH CHECKS IN ROADSIDE DITCHES.

S:\Donaldson 8/26/2022 10:05:54 PM
 WORKSPACE: AHTD
 L:\2017\17017580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_140_EC_ALL.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061687	12	63	

2 MAINTENANCE OF TRAFFIC DETAILS



PORTABLE CHANGEABLE MESSAGE SIGN TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER
 ANY MESSAGES DISPLAYED WILL BE COORDINATED WITH THE DISTRICT

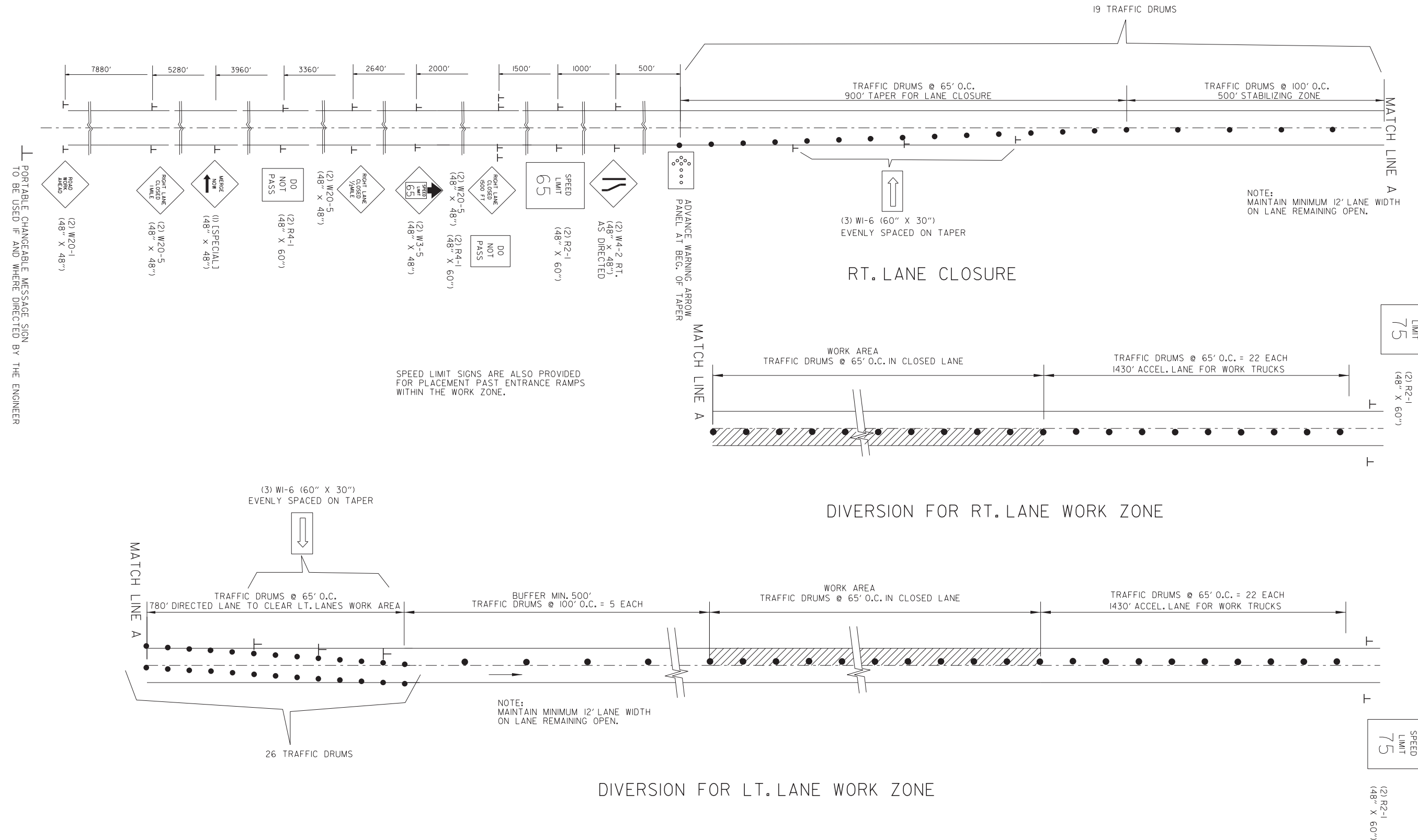
CONSTRUCTION SEQUENCE:
 MILL AND OVERLAY OPERATIONS SHALL UTILIZE A SINGLE FOUR (4) MILE LANE CLOSURE IN SIMULTANEOUS DIRECTIONS OF TRAFFIC. NO LANE CLOSURE MAY EXCEED THE ACTIVE WORK AREA BY MORE THAN ONE QUARTER (1/4) MILE. REFER TO THE MAINTENANCE OF TRAFFIC SPECIAL PROVISION.
 POLYMER OVERLAY FOR BR. NO. A3654, B3654, A3709 & B3709 SHALL UTILIZE A SINGLE TWO (2) MILE LANE CLOSURE. NO LANE CLOSURE MAY EXCEED THE ACTIVE WORK AREA BY MORE THAN ONE QUARTER (1/4) MILE. REFER TO THE MAINTENANCE OF TRAFFIC SPECIAL PROVISION.

ADVANCE WARNING SIGNS
 MAINTENANCE OF TRAFFIC DETAILS

PORTABLE CHANGEABLE MESSAGE SIGN TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER
 ANY MESSAGES DISPLAYED WILL BE COORDINATED WITH THE DISTRICT

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061687	13	63	

2 MAINTENANCE OF TRAFFIC DETAILS

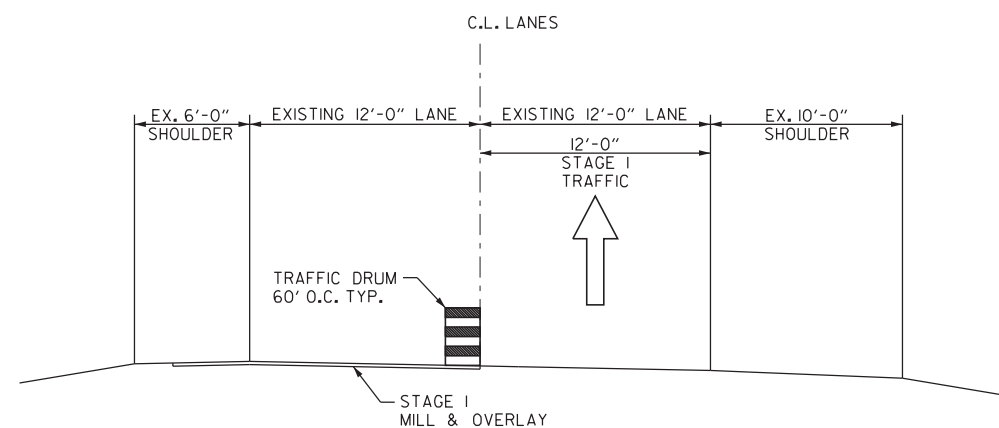


S.A. Donaldson 8/26/2022 10:06:55 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy. 31 - Prairie Co Line Drawings\061687.MOT_SD_01.dgn
 REVISED DATE:

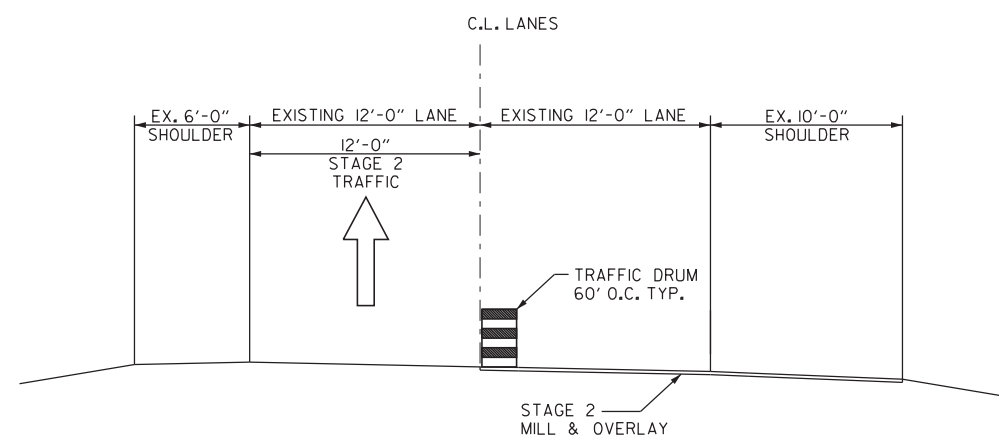
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061687	14	63	
② MAINTENANCE OF TRAFFIC DETAILS								



DIGITALLY SIGNED 8/29/2022



LOCATION OF TRAFFIC DRUMS FOR MAINTENANCE OF TRAFFIC
STAGE 1
(SHOWN IN DIRECTION OF TRAFFIC)



LOCATION OF TRAFFIC DRUMS FOR MAINTENANCE OF TRAFFIC
STAGE 2
(SHOWN IN DIRECTION OF TRAFFIC)

CONSTRUCTION OF SEQUENCE:

STAGE 1

MILL & OVERLAY INSIDE LANE & SHOULDER.

STAGE 2

MILL & OVERLAY OUTSIDE LANE & SHOULDER.
INSTALL RUMBLE STRIPS & PERMANENT PAVEMENT MARKINGS.

NOTES:

MILL AND OVERLAY OPERATIONS SHALL UTILIZE A SINGLE FOUR (4) MILE LANE CLOSURE IN SIMULTANEOUS DIRECTIONS OF TRAFFIC. NO LANE CLOSURE MAY EXCEED THE ACTIVE WORK AREA BY MORE THAN ONE QUARTER (1/4) MILE. REFER TO THE MAINTENANCE OF TRAFFIC SPECIAL PROVISION.

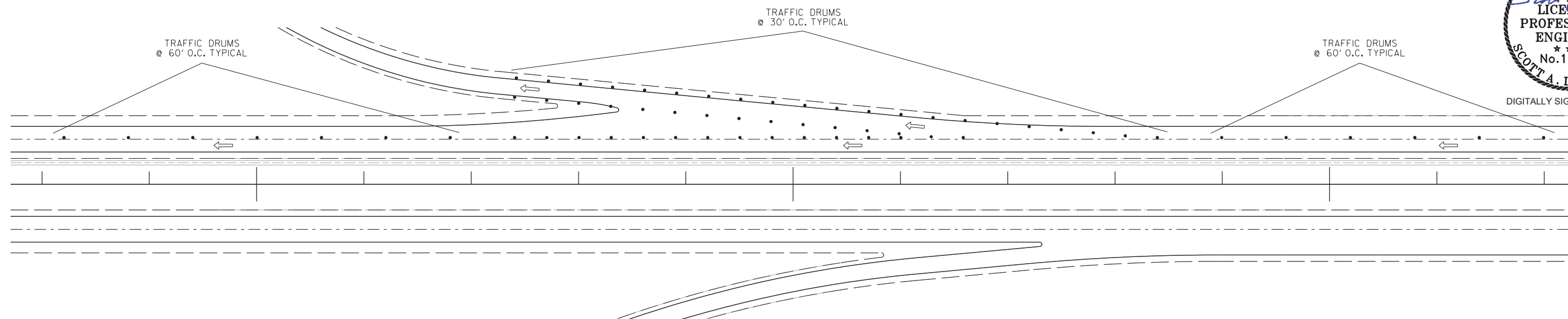
POLYMER OVERLAY FOR BR. NO. A3654, B3654, A3709 & B3709 SHALL UTILIZE A SINGLE TWO (2) MILE LANE CLOSURE. NO LANE CLOSURE MAY EXCEED THE ACTIVE WORK AREA BY MORE THAN ONE QUARTER (1/4) MILE. REFER TO THE MAINTENANCE OF TRAFFIC SPECIAL PROVISION.

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061687	15	63	

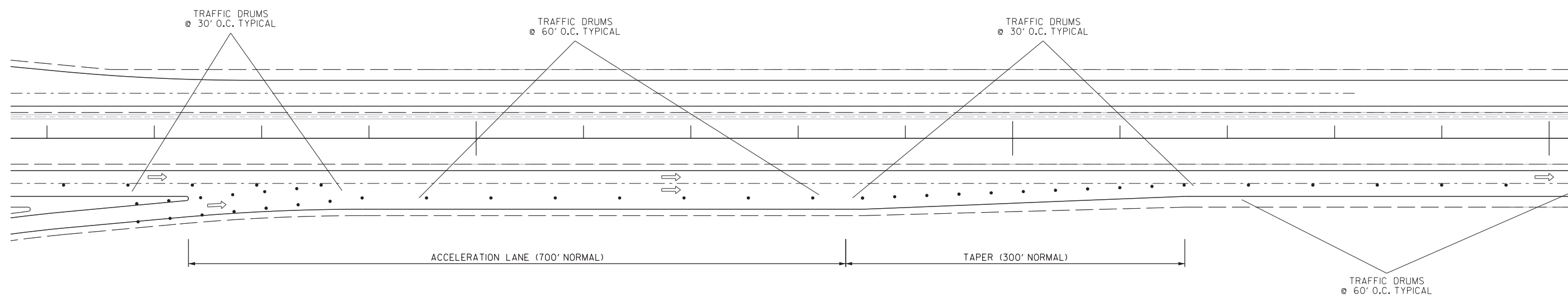
2 MAINTENANCE OF TRAFFIC DETAILS



DIGITALLY SIGNED 8/29/2022



EXIT RAMP - TYPICAL TRAFFIC DRUM LAYOUT
OUTSIDE LANE CLOSURE



ENTRANCE RAMP - TYPICAL TRAFFIC DRUM LAYOUT
ACCELERATION LANE CLOSURE

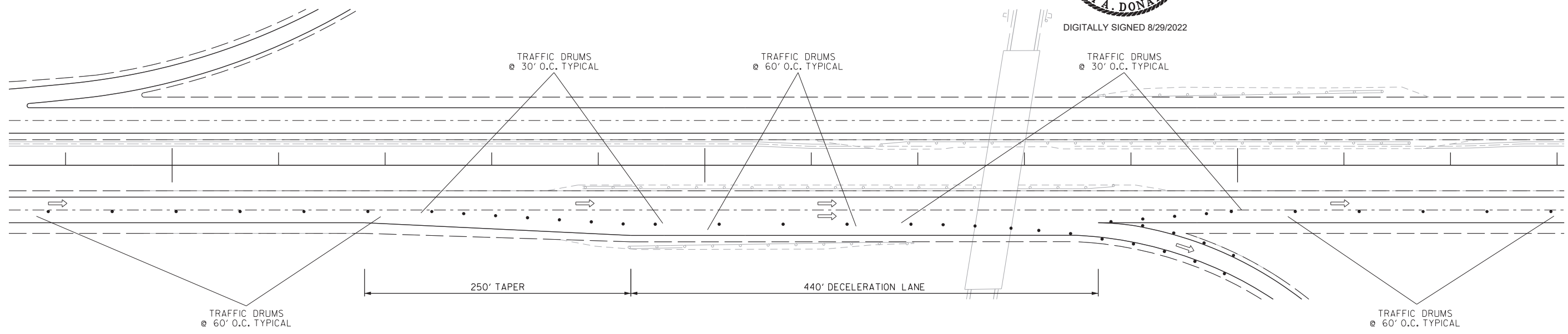
DETAIL OF RAMPS WITH LANE CLOSURE
MAINTENANCE OF TRAFFIC DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061687	16	63	

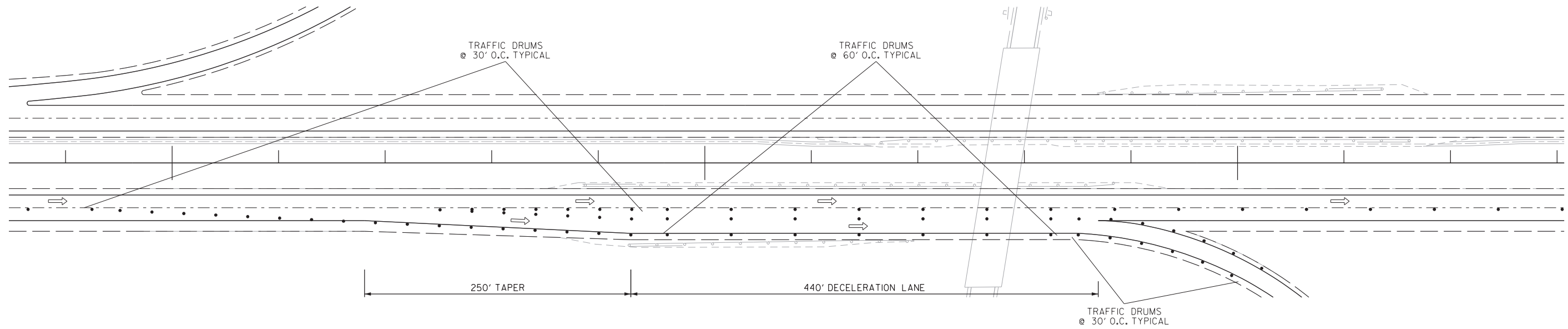


DIGITALLY SIGNED 8/29/2022

2 MAINTENANCE OF TRAFFIC DETAILS



EXIT RAMP - TYPICAL TRAFFIC DRUM LAYOUT
DECELERATION LANE CLOSURE



EXIT RAMP - TYPICAL TRAFFIC DRUM LAYOUT
OUTSIDE LANE CLOSURE

DETAIL OF RAMPS WITH LANE CLOSURE
MAINTENANCE OF TRAFFIC DETAILS

8/26/2022 10:09:01 PM
 S.A. Donaldson
 WORKSPACE: AHTD
 L:\2017\107580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687.M01_SD_02.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
						061687	17	63
				② MAINTENANCE OF TRAFFIC DETAILS				

THIS SHEET INTENTIONALLY LEFT BLANK

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
						061687	18	63
				② MAINTENANCE OF TRAFFIC DETAILS				

THIS SHEET INTENTIONALLY LEFT BLANK

S:\donaldson 10/24/2022 3:48:28 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_MOT_NOR.dwg
 REVISED DATE:

DETOUR MAP FOR
 POLYMER OVERLAY OF BR. NO. 03653
 MAINTENANCE OF TRAFFIC DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
						061687	19	63
				② MAINTENANCE OF TRAFFIC DETAILS				

THIS SHEET INTENTIONALLY LEFT BLANK

S:\donaldson 10/26/2022 12:33:27 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_MOT_PAS_01.dgn
 REVISED DATE:

HYDRODEMOLITION OF BR. NO. 03656
 MAINTENANCE OF TRAFFIC DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
						061687	20	63
				② MAINTENANCE OF TRAFFIC DETAILS				

THIS SHEET INTENTIONALLY LEFT BLANK

S:\adonaldson 10/24/2022 3:48:29 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_MOT_SD.BRG.02.dgn
 REVISED DATE:

HYDRODEMOLITION FOR BR. NO. 03670 & 03671
 POLYMER OVERLAY FOR BR. NO. 03672
 MAINTENANCE OF TRAFFIC DETAILS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
						061687	21	63
				② MAINTENANCE OF TRAFFIC DETAILS				

THIS SHEET INTENTIONALLY LEFT BLANK

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.		061687	22	63

(2) QUANTITIES



DIGITALLY SIGNED 10/28/2022

ADVANCE WARNING SIGNS AND DEVICES

SIGN NUMBER	DESCRIPTION	SIGN SIZE	STAGE 1	STAGE 2	MAXIMUM NUMBER REQUIRED	TOTAL SIGNS REQUIRED		CONSTRUCTION PROJECT INFORMATION SIGN UPDATE	TRAFFIC DRUMS	* ADVANCE WARNING ARROW PANEL	* PORTABLE CHANGEABLE MESSAGE SIGN
			LIN. FT. - EACH			NO.	SQ. FT.				
W20-1	ROAD WORK 1500 FT.	48"x48"	32	4	32	32	512.0				
W20-1	ROAD WORK 1/2 MILE	48"x48"	4	4	4	4	64.0				
W20-1	ROAD WORK 1 MILE	40"x40"	4	4	4	4	64.0				
W20-1	ROAD WORK AHEAD	48"x48"	13	7	13	13	208.0				
G20-2	END ROAD WORK	48"x24"	25	7	25	25	200.0				
G20-1	ROAD WORK NEXT 25.3 MILES	60"x24"	2	2	2	2	20.0				
R4-1	DO NOT PASS	48"x60"	8	8	8	8	160.0				
R55-1	FINES DOUBLE WHEN	36"x60"	4	4	4	4	60.0				
W20-5	RIGHT LANE CLOSED 1 MILE	48"x48"	4	4	4	4	64.0				
W3-5	SPEED LIMIT 65 ↑	48"x48"	8	4	8	8	128.0				
W20-5	RIGHT LANE CLOSED 1/2 MILE	48"x48"	4	4	4	4	64.0				
W20-5	RIGHT LANE CLOSED 1500 FT	48"x48"	4	4	4	4	64.0				
W4-2 RT	LANE ENDS, MERGE LEFT	48"x48"	4	4	4	4	64.0				
R2-1	SPEED LIMIT 65	48"x60"	8	4	8	8	160.0				
SPECIAL	MERGE NOW	48"x48"	2	2	2	2	32.0				
W1-6	ARROW	60"x30"	12	6	12	12	150.0				
R2-1	SPEED LIMIT 75	48"x60"	4	4	4	4	80.0				
SPECIAL	CONSTRUCTION PROJECT INFORMATION SIGN	48"x96"	2	2	2	2	64.0				
	CONSTRUCTION PROJECT INFORMATION SIGN UPDATE							4			
	TRAFFIC DRUMS		848	953	953				953		
	ADVANCE WARNING ARROW PANEL		4	4	4					300	
	PORTABLE CHANGEABLE MESSAGE SIGN		7	7	7						60
TOTALS:							2158.0	4	953	300	60

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

NOTE: THE QUANTITY OF TRAFFIC DRUMS PROVIDED IS FOR BOTH SIDES OF THE ROADWAY FOR A 4 MILE WORK AREA. HOWEVER, THE INSTALLATION OF TRAFFIC DRUMS SHALL NEVER EXCEED THE ACTUAL WORK AREA BY MORE THAN 1/4 MILE, UNLESS APPROVED BY THE ENGINEER.

* QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.
TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

S:\donaldson 10/24/2022 3:49:23 PM WORKSPACE: AHTD L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_01Y_01.dgn REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
11/8/2022						061687	23	63
							QUANTITIES	

2

CONSTRUCTION PAVEMENT MARKINGS AND PERMANENT PAVEMENT MARKINGS

DESCRIPTION	STAGE 1	STAGE 2	ENTIRE JOB	CONSTRUCTION PAVEMENT MARKINGS	RAISED PAVEMENT MARKERS	ENHANCED THERMOPLASTIC PAVEMENT MARKING		
					TYPE II (WHITE/RED) EACH	6"		12"
						WHITE	YELLOW	WHITE
LIN. FT. - EACH					LIN. FT.			
CONSTRUCTION PAVEMENT MARKINGS	286601	236487		523088				
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED)			3912		3912			
ENHANCED THERMOPLASTIC PAVEMENT MARKING WHITE (6")			325561			325561		
ENHANCED THERMOPLASTIC PAVEMENT MARKING YELLOW (6")			260020				260020	
ENHANCED THERMOPLASTIC PAVEMENT MARKING WHITE (12")			6260					6260
TOTALS:				523088	3912	325561	260020	6260

NOTE: THIS IS A HIGH TRAFFIC VOLUME ROAD AS DEFINED IN SECTION 604.03, STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.



DIGITALLY SIGNED 11/8/2022

TEMPORARY EROSION CONTROL

STATION	STATION	LOCATION	SAND BAG DITCH CHECKS	ROCK DITCH CHECKS	FILTER SOCK (12")	*SEDIMENT REMOVAL & DISPOSAL
			(E-5) BAG	(E-6) CU.YD.	(E-13) LIN. FT.	CU. YD.
*ENTIRE PROJECT TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.			1188	100	2900	107
TOTALS:			1188	100	2900	107

*NOTE: QUANTITIES ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

ACHM PATCHING OF EXISTING ROADWAY

DESCRIPTION	TON
*ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	600
TOTAL:	600

* NOTE: QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

SPECIAL CLEARING

STATION	STATION	LOCATION	SPECIAL CLEARING
			STATION
587+23	1868+72	LT. & RT. OF MAIN LANES	1281
1680+00	1825+00	50' IN MEDIAN	145
TOTAL:			1426

NOTE: CLEAR TO ROW AT BOX CULVERTS AND BRIDGES.

ASPHALT CONCRETE PATCHING FOR MAINTENANCE OF TRAFFIC

LOCATION	TON	TACK COAT
		GALLON
*ENTIRE PROJECT - TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER	607	1214
TOTALS:	607	1214

BASIS OF ESTIMATE:
ASPHALT CONCRETE PATCHING FOR MAINTENANCE OF TRAFFIC...25 TON/MILE
TACK COAT FOR MAINTENANCE OF TRAFFIC.....50 GAL./MILE
* NOTE: QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.

QUANTITIES

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/11/2022				6	ARK.			
10/28/2022						061687	24	63
				JOB NO.	061687		24	63
				(2) QUANTITIES				



DIGITALLY SIGNED 10/28/2022

COLD MILLING ASPHALT PAVEMENT (BOX 1 OF 2)

STATION	STATION	LOCATION	AVG. WIDTH	COLD MILLING ASPHALT PAVEMENT
			FEET	SQ. YD.
MAIN LANES				
587-22.80	621+38.00	I-40 LT. MAIN LANES	38.00	14419.73
621-38.00	655+05.86	I-40 LT. MAIN LANES	38.00	14219.85
655-05.86	758+69.44	I-40 LT. MAIN LANES	38.00	43757.34
758-69.44	772+64.82	I-40 LT. MAIN LANES	38.00	5891.60
772-64.82	863+25.04	I-40 LT. MAIN LANES	38.00	38254.26
863-25.04	876+10.21	I-40 LT. MAIN LANES	38.00	5426.27
876-10.21	885+00.00	I-40 LT. MAIN LANES	38.00	3756.89
884-89.09	890+84.30	I-40 LT. MAIN LANES	38.00	2513.11
892-29.30	1041+43.96	I-40 LT. MAIN LANES	38.00	62973.01
1041+43.96	1065+84.95	I-40 LT. MAIN LANES	38.00	10306.40
1065+84.95	1139+18.06	I-40 LT. MAIN LANES	38.00	30962.02
1139+18.06	1152+07.89	I-40 LT. MAIN LANES	38.00	5445.95
1152+14.44	1174+09.36	I-40 LT. MAIN LANES	38.00	9267.44
1174+09.36	1264+04.45	I-40 LT. MAIN LANES	38.00	37979.27
1264+04.45	1296+71.61	I-40 LT. MAIN LANES	38.00	13794.68
1296+71.61	1614+08.03	I-40 LT. MAIN LANES	38.00	133998.22
1616+26.75	1675+55.60	I-40 LT. MAIN LANES	38.00	25032.92
1675+55.60	1679+21.68	I-40 LT. MAIN LANES	38.00	1545.67
1679+07.70	1701+22.00	I-40 LT. MAIN LANES	38.00	9349.27
1701+22.00	1702+27.00	I-40 LT. MAIN LANES	38.00	443.33
1702+00.00	1799+24.50	I-40 LT. MAIN LANES	38.00	41059.00
1798+46.50	1809+83.80	I-40 LT. MAIN LANES	38.00	4801.93
1809+83.80	1832+06.80	I-40 LT. MAIN LANES	38.00	9388.00
1832+06.80	1834+16.00	I-40 LT. MAIN LANES	38.00	883.29
1834+34.70	1865+40.89	I-40 LT. MAIN LANES	37.00	12769.89
RT. MAIN LANES				
587-22.80	708+95.69	I-40 RT. MAIN LANES	38.00	51396.65
708-95.69	739+16.80	I-40 RT. MAIN LANES	38.00	12755.80
739-16.80	814+34.25	I-40 RT. MAIN LANES	38.00	31740.34
814-34.25	826+04.20	I-40 RT. MAIN LANES	38.00	4939.79
826-04.20	885+00.00	I-40 RT. MAIN LANES	38.00	24893.38
884-89.09	890+84.30	I-40 RT. MAIN LANES	38.00	2513.11
892-29.30	910+82.44	I-40 RT. MAIN LANES	38.00	7824.37
910-82.44	937+12.13	I-40 RT. MAIN LANES	38.00	11103.14
937-12.13	1139+18.06	I-40 RT. MAIN LANES	38.00	85313.93
1139+18.06	1152+07.89	I-40 RT. MAIN LANES	38.00	5445.95
1152+14.44	1174+09.36	I-40 RT. MAIN LANES	38.00	9267.44
1174+09.36	1264+04.45	I-40 RT. MAIN LANES	38.00	37979.27
1264+04.45	1296+71.61	I-40 RT. MAIN LANES	38.00	13794.68
1296+71.61	1614+08.03	I-40 RT. MAIN LANES	38.00	134205.78
1616+26.75	1675+55.60	I-40 RT. MAIN LANES	38.00	26228.91
1675+55.60	1679+21.68	I-40 RT. MAIN LANES	38.00	143.89
1679+07.70	1702+27.00	I-40 RT. MAIN LANES	38.00	9792.60
1702+00.00	1704+00.00	I-40 RT. MAIN LANES	38.00	844.44
1704+00.00	1769+74.10	I-40 RT. MAIN LANES	38.00	27757.31
1769+74.10	1799+24.50	I-40 RT. MAIN LANES	38.00	12457.24
1798+46.50	1800+46.50	I-40 RT. MAIN LANES	38.00	844.44
1800+46.50	1814+92.20	I-40 RT. MAIN LANES	38.00	6104.07
1814+92.20	1834+16.00	I-40 RT. MAIN LANES	38.00	8122.71
1834+34.70	1865+40.89	I-40 RT. MAIN LANES	37.00	12769.89
SUBTOTAL (BOX 1 OF 2):				1076476.47

NOTE: AVERAGE MILLING DEPTH 2".

STOCKPILE LOCATIONS: LONOKE COUNTY: 420 DEE DEE LANE, LONOKE, AR 72085
GPS COORDINATES: 34.812091° N, -91.888604° W

PRAIRIE COUNTY: 47 SAMPLE ROAD, HAZEN, AR 72064 (500 C.Y.)
GPS COORDINATES: 34.79687° N, -91.57624° W

COLD MILLING ASPHALT PAVEMENT (BOX 2 OF 2)

STATION	STATION	LOCATION	AVG. WIDTH	COLD MILLING ASPHALT PAVEMENT
			FEET	SQ. YD.
ADDITIONAL FOR RAMPS				
587+22.80	588+06.58	HWY. 31 RAMP 2 ACCEL LANE	8.00	74.47
588+06.58	591+06.58	HWY. 31 RAMP 2 TAPER	4.00	133.33
969+93.01	972+93.01	HWY. 13 RAMP 4 TAPER	6.00	200.00
972+93.01	978+63.85	HWY. 13 RAMP 4 ACCEL LANE	12.00	761.12
978+63.87	983+63.87	HWY. 13 RAMP 4	25.00	1388.89
981+80.65	984+30.65	HWY. 13 RAMP 1 TAPER	4.00	111.11
984+30.65	988+69.27	HWY. 13 RAMP 1 DECEL LANE	8.00	389.88
988+69.27	993+69.27	HWY. 13 RAMP 1	25.00	1388.89
993+37.41	998+37.41	HWY. 13 RAMP 3	25.00	1388.89
996+30.60	998+37.44	HWY. 13 RAMP 3 RECOVERY AREA	VAR.	91.21
997+31.88	1002+31.88	HWY. 13 RAMP 2	25.00	1388.89
998+37.44	1002+91.04	HWY. 13 RAMP 3 EXIT	VAR.	545.59
1002+31.88	1008+60.46	HWY. 13 RAMP 2 ACCEL LANE	8.00	558.74
1008+60.46	1011+60.46	HWY. 13 RAMP 2 TAPER	4.00	133.33
1533+00.00	1535+98.31	HWY. 63 RAMP 4 TAPER	4.00	132.58
1535+98.31	1545+56.00	HWY. 63 RAMP 4 ACCEL LANE	8.00	851.28
1540+51.94	1544+83.57	HWY. 63 RAMP 1 TURNOUT	VAR.	505.26
1544+83.57	1549+83.57	HWY. 63 RAMP 1	25.00	1388.89
1545+56.00	1550+56.00	HWY. 63 RAMP 4	25.00	1388.89
1555+38.07	1560+38.07	HWY. 63 RAMP 2	25.00	1388.89
1556+42.96	1561+42.96	HWY. 63 RAMP 3	25.00	1388.89
1560+38.07	1570+17.00	HWY. 63 RAMP 2 ACCEL LANE	8.00	870.16
1561+42.96	1566+34.69	HWY. 63 RAMP 3 TURNOUT	VAR.	526.96
1570+17.00	1573+17.00	HWY. 63 RAMP 2 TAPER	4.00	133.33
1832+44.69	1834+16.00	REST AREA RAMP 4 TAPER	4.00	76.14
1834+34.70	1835+45.40	REST AREA RAMP 4 TAPER	5.00	61.50
1835+45.40	1844+79.94	REST AREA RAMP 4 ACCEL LANE	8.00	830.70
1844+79.94	1849+79.94	REST AREA RAMP 4	25.00	1388.89
1840+22.47	1841+49.50	REST AREA RAMP 1 TAPER	4.00	56.46
1841+49.50	1846+49.50	REST AREA RAMP 1	25.00	1388.89
1852+02.28	1857+02.28	REST AREA RAMP 2	25.00	1388.89
1857+02.28	1859+25.14	REST AREA RAMP 2 ACCEL LANE	8.00	198.10
1859+16.15	1864+16.15	REST AREA RAMP 3	25.00	1388.89
1859+25.14	1865+42.02	REST AREA RAMP 2 TAPER	4.00	274.17
1864+16.15	1865+42.02	REST AREA RAMP 3 TAPER	4.00	55.94
SUBTOTAL (BOX 2 OF 2):				24238.04
SUBTOTAL (BOX 1 OF 2):				1076476.47
TOTAL:				1100714.51

NOTE: AVERAGE MILLING DEPTH 2".

RUMBLE STRIPS IN ASPHALT SHOULDERS

STATION	STATION	LOCATION	* RUMBLE STRIPS IN ASPHALT SHOULDERS
			LIN.FT.
587+23	885+00	LEFT OF LEFT MAIN LANES	29777
884+89	890+84	LEFT OF LEFT MAIN LANES	595
892+29	1152+08	LEFT OF LEFT MAIN LANES	25979
1152+14	1614+02	LEFT OF LEFT MAIN LANES	46188
1616+27	1679+22	LEFT OF LEFT MAIN LANES	6295
1679+07	1799+24	LEFT OF LEFT MAIN LANES	12017
1798+46	1865+42	LEFT OF LEFT MAIN LANES	6696
587+23	885+00	RIGHT OF LEFT MAIN LANES	29777
884+89	890+84	RIGHT OF LEFT MAIN LANES	595
892+29	1152+08	RIGHT OF LEFT MAIN LANES	25979
1152+14	1614+15	RIGHT OF LEFT MAIN LANES	46201
1616+27	1679+22	RIGHT OF LEFT MAIN LANES	6295
1679+07	1799+24	RIGHT OF LEFT MAIN LANES	12017
1798+46	1865+42	RIGHT OF LEFT MAIN LANES	6696
TOTAL:			510201

* NOTE: QUANTITY ESTIMATED.
SEE SECTION 104.03 OF THE STD. SPECS.
TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

10/24/2022 3:49:30 PM
 S:\donaldson\WORKSPACE\AHTD\112017107580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_01.dwg
 REVISION DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/11/2022				6	ARK.			
10/28/2022						061687	26	63

△ A&B3654 & A&B3709 QUANTITIES 61771

SCHEDULE OF BRIDGE QUANTITIES - JOB NO. 061687

I-40 LOG MILE	UNIT OF STRUCTURE	ITEM NO.	SP & 509	SS & 802	SP & 803	SP & 803	SS & 804	SS & 804	SS & 809	821	SP JOB 061687	SP JOB 061687	SP JOB 061687	SP JOB 061687	SP JOB 061687	
		ITEM	JOINT REHABILITATION (TYPE A)	GROOVING	CLASS 1 PROTECTIVE SURFACE TREATMENT	CLASS 3 PROTECTIVE SURFACE TREATMENT	REINFORCING STEEL-BRIDGE (GRADE 60)	EPOXY COATED REINFORCING STEEL (GRADE 60)	SILICONE JOINT SEALANT	MODIFICATION OF EXISTING BRIDGE STRUCTURE (BRIDGE NO.)	HYDRODEMOLITION -CLASS I	BRIDGE DECK REPAIR FOR LATEX MODIFIED CONCRETE OVERLAYS	LATEX MODIFIED CONCRETE OVERLAY (1/2" THICK)	BRIDGE DECK REPAIR FOR POLYMER OVERLAYS	POLYMER OVERLAY	
		UNIT	LINEAR FOOT	SO. YD.	GALLON	LINEAR FOOT	LBS.	LBS.	LINEAR FOOT	LUMP SUM	SO. YD.	SO. FT.	SO. YD.	SO. FT.	SO. YD.	
△ 177.29	EXISTING BRIDGE NO. 03652						-190							-213	-473	
△ 179.34	EXISTING BRIDGE NO. 03653						-180							-210	-467	
180.68	EXISTING BRIDGE NO. A3654	160						130						150	333	
180.68	EXISTING BRIDGE NO. B3654	160						130						150	333	
△ 183.54	EXISTING BRIDGE NO. 03656			-500.0	11.4	-428	-440		124	+	-3 641	-3 514		-643		
△ 186.94	EXISTING BRIDGE NO. 03670			-560.1	-12.8	-480	-490				-640	576		-642		
△ 189.10	EXISTING BRIDGE NO. 03671			-492.4	-11.2	-422	-440				-563	-506		-565		
△ 191.13	EXISTING BRIDGE NO. 03672						-220							257	-571	
△ 193.22	EXISTING BRIDGE NO. 03708			-594.5	-13.3	-428	-510		143		-3 963	-599		-3 965		
194.39	EXISTING BRIDGE NO. A3709	277					250							290	644	
194.39	EXISTING BRIDGE NO. B3709	277					250							290	644	
TOTALS FOR JOB NO. 061687			874	△ 2,147.0	△ 48.7	△ 1,758	△ 2,970	△ 260	△ 267		△ 2,807	△ 2,195	△ 2,815	△ 880	△ 1,954	△ 3,465

REFERENCE TABLE △

BR. NO.	EXISTING DRAWING NUMBERS	APPLICABLE STANDARD DRAWING NUMBERS
-03652	+1991	----
-03653	+1992	----
A&B3654	30712	----
-03656	+1995, 12003, 12006, 1898	----
-03670	+2033, 14990, 15010, 15011	----
-03671	+2034, 14990, 15010	----
-03672	+2035	----
-03708	+2735, 14990, 42233, 42235, 15030	-55064
A&B3709	12737, 12738, 12739	2381A

△ Removed Bridges From Project
Made By: WMM Date: 10/18/22
Ck'd By: JHR Date: 10/18/22

- ① Contractor shall remove and reconstruct the damaged end post on west side of End Bent No. 1. No direct payment will be made for this work. It shall be considered subsidiary to other items in the Contract. Refer to existing end bent plans for details of end post.
- ② The quantity shown is for estimating and bidding purposes only. Actual quantity, if any, will be determined in the field.
- ③ Includes approach gutters.
- ④ Modification of Existing Bridge Structure (Bridge No. 03656) includes modification of existing backwalls required for LMC grade raise. See Std. Dwg. No. 55065.
- ⑤ Existing bridge deck has remnants of an asphalt overlay near the bridge ends.
- ⑥ Existing bridge has slider plate joints to be partially removed and replaced with poured silicone joints.



Digitally Signed 10/28/2022
BRIDGE ENGINEER

SCHEDULE OF BRIDGE QUANTITIES
HWY. 31 - EAST (S)
LONOKE/PRAIRIE COUNTY
ROUTE 40 SEC. 41 & 42
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

DRAWN BY: HEW DATE: JAN. 2020 FILENAME: b061687-ql.dgn
CHECKED BY: WMM DATE: JAN. 2020 SCALE: NO SCALE
DESIGNED BY: BWD DATE: JAN. 2020
BRIDGE NO. A&B3654 & A&B3709 △ DRAWING NO. 61771

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/11/2022				6	ARK.			
10/28/2022						061687	27	63
11/8/2022								

2 SUMMARY OF QUANTITIES AND REVISIONS



DIGITALLY SIGNED 11/8/2022

SUMMARY OF QUANTITIES

ITEM NUMBER	ITEM	QUANTITY	UNIT
SP	SPECIAL CLEARING	1426	STATION
SS & 401	TACK COAT	188335	GAL.
SP, SS, & 407	MINERAL AGGREGATE IN ACHM SURFACE COURSE (1/2")	115146	TON
SP, SS, & 407	ASPHALT BINDER (PG 76-22) IN ACHM SURFACE COURSE (1/2")	5933	TON
SP & 412	COLD MILLING ASPHALT PAVEMENT	1100715	SQ. YD.
SP, SS, & 414	ASPHALT CONCRETE PATCHING FOR MAINTENANCE OF TRAFFIC	607	TON
SP, SS, & 415	ACHM PATCHING OF EXISTING ROADWAY	600	TON
601	MOBILIZATION	1.00	LUMP SUM
SP, SS, & 603	MAINTENANCE OF TRAFFIC	1.00	LUMP SUM
SS & 604	SIGNS	2158	SQ. FT.
SP, SS, & 604	CONSTRUCTION PROJECT INFORMATION SIGN UPDATE	4	EACH
SS & 604	TRAFFIC DRUMS	953	EACH
604	CONSTRUCTION PAVEMENT MARKINGS	523088	LIN. FT.
SS & 604	ADVANCE WARNING ARROW PANEL	300	DAY
SP, SS, & 604	PORTABLE CHANGEABLE MESSAGE SIGN	60	WEEK
621	SAND BAG DITCH CHECKS	1188	BAG
621	SEDIMENT REMOVAL AND DISPOSAL	107	CU. YD.
621	ROCK DITCH CHECKS	100	CU. YD.
SS & 621	FILTER SOCK (12")	2900	LIN. FT.
635	ROADWAY CONSTRUCTION CONTROL	1.00	LUMP SUM
642	RUMBLE STRIPS IN ASPHALT SHOULDERS	510201	LIN. FT.
SP	ENHANCED THERMOPLASTIC PAVEMENT MARKING WHITE (6")	325561	LIN. FT.
SP	ENHANCED THERMOPLASTIC PAVEMENT MARKING WHITE (12")	6260	LIN. FT.
SP	ENHANCED THERMOPLASTIC PAVEMENT MARKING YELLOW (6")	260020	LIN. FT.
721	RAISED PAVEMENT MARKERS (TYPE II)	3912	EACH
STRUCTURES OVER 20' SPAN			
SP & 509	JOINT REHABILITATION (TYPE A)	874	LIN. FT.
636	BRIDGE CONSTRUCTION CONTROL	1.00	LUMP SUM
SS & 804	REINFORCING STEEL-BRIDGE (GRADE 60)	500	POUND
SS & 804	EPOXY COATED REINFORCING STEEL (GRADE 60)	260	POUND
SP	BRIDGE DECK REPAIR FOR POLYMER OVERLAYS	880	SQ. FT.
SP	POLYMER OVERLAY	1954	SQ. YD.

REVISIONS

DATE	REVISION	SHEET NUMBER
10/11/2022	UPDATED FAP NUMBER ON TITLE SHEET. ADDED SS 621-1 FILTER SOCKS. UPDATED THE "PRICE ADJUSTMENT FOR FUEL" SPECIAL PROVISION TO A NEWER VERSION. ADDED JOB NUMBER TO THE "UNDERDRAIN INSPECTION, FLUSHING, AND REHABILITATION" SPECIAL PROVISION. ADDED THE "UTILITY ADJUSTMENTS" SPECIAL PROVISION. ADDED STOCKPILE LOCATIONS UNDER COLD MILLING QUANTITY BOX. REMOVED "SP" FROM THE GROOVING PAY ITEM NUMBER ON SCHEDULE OF BRIDGE QUANTITIES AND SUMMARY OF QUANTITIES.	1, 4, 24, 26, 27
10/28/2022	UPDATED SHEETS TO REMOVE THE WORK ON ALL THE OVERPASS BRIDGES. REMOVED STANDARD DRAWING NUMBER 55065. REMOVED THE "BRIDGE DECK REPAIR FOR LATEX MODIFIED CONCRETE OVERLAYS", "HYDRODEMOLITION - CLASS 1", "LATEX MODIFIED CONCRETE OVERLAY", "MANAGEMENT OF HYDRODEMOLITION WASTEWATER", "PORTABLE TRAFFIC SIGNAL SYSTEM", AND "SPECIAL SAFETY REQUIREMENTS FOR BRIDGES" SPECIAL PROVISIONS. REMOVED SOME SPECIAL DETAILS. REMOVED MAINTENANCE OF TRAFFIC SHEETS FOR THE OVERPASS WORK. UPDATED QUANTITIES FOR REMOVING OVERPASS WORK. UPDATED PLAN SHEET NOTES FOR REMOVING OVERPASS WORK. UPDATED BRIDGE DETAIL SHEETS FOR REMOVING OVERPASS WORK.	2-4, 10, 17-27, 51-57, 59-63
11/8/2022	REMOVED SS 600-2 INCIDENTAL CONSTRUCTION AND "UNDERDRAIN INSPECTION, FLUSHING, AND REHABILITATION" SPECIAL PROVISION FROM THE GOVERNING SPECIFICATIONS LIST. REMOVED QUANTITIES FOR FLUSHING UNDERDRAIN, 4" PIPE UNDERDRAINS, UNDERDRAIN OUTLET PROTECTORS AND UNDERDRAIN VIDEO INSPECTION.	4, 23, 27

S:\Donaldson 11/8/2022 8:43:31 AM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_01Y_SUM.dgn
 REVISED DATE:

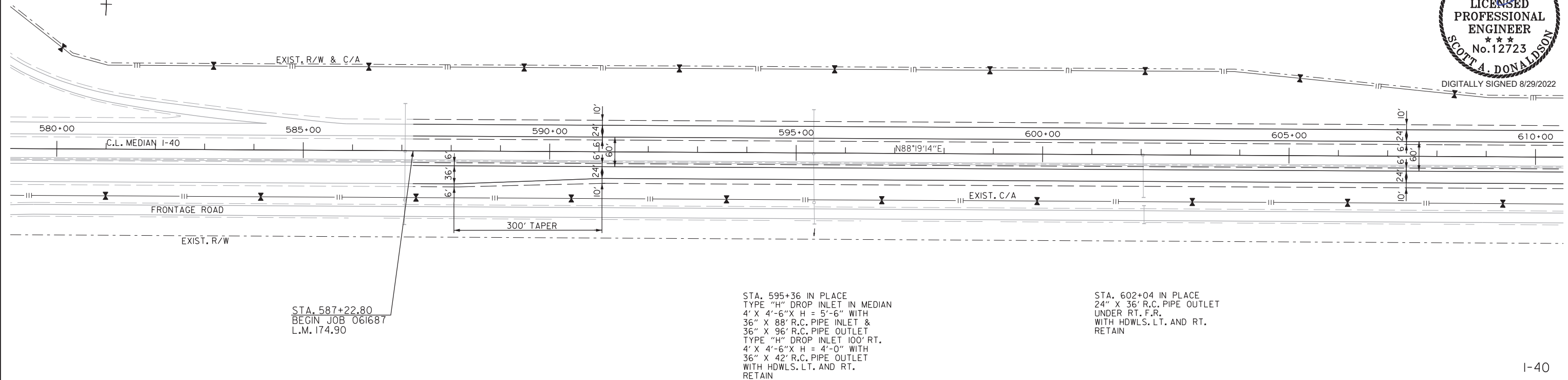
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		28	63
				JOB NO.	061687		PLAN STA. 580+00 - STA. 640+00	



DIGITALLY SIGNED 8/29/2022

STA. 587+07 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
WITH 36" X 96' R.C. PIPE INLET &
36" X 99' R.C. PIPE OUTLET
TYPE "H" DROP INLET 100' RT.
4' X 4'-6" X H = 4'-0" WITH
36" X 50' R.C. PIPE OUTLET
WITH HDWLS. LT. AND RT.
RETAIN

STA. 602+04 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 84' R.C. PIPE OUTLET
WITH HDWL. RT.
RETAIN



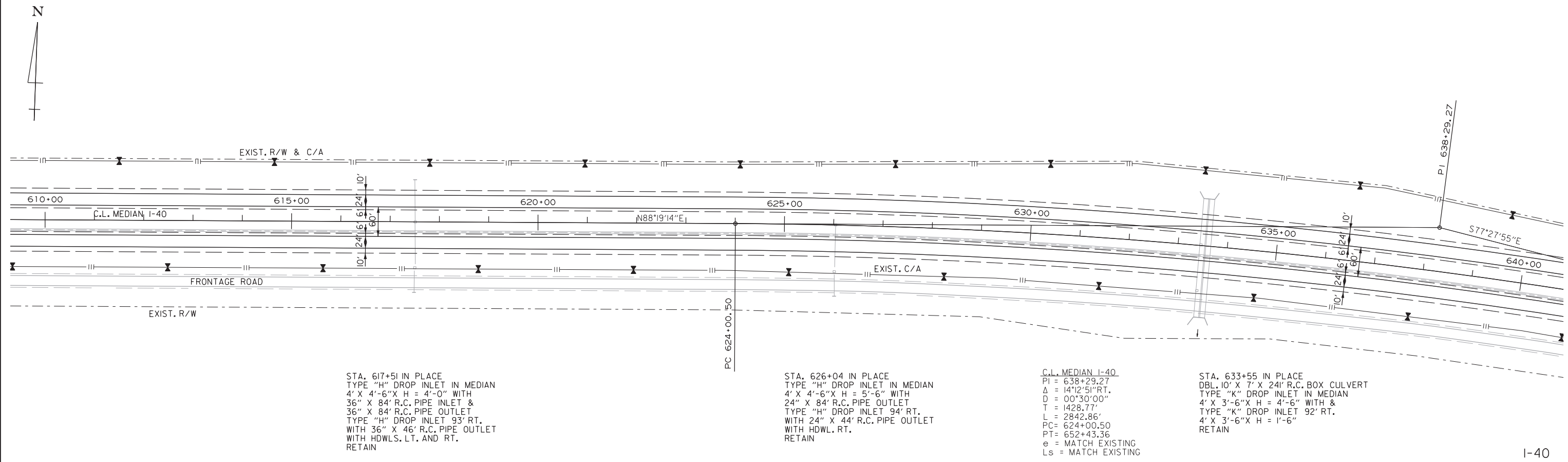
STA. 587+22.80
BEGIN JOB 061687
L.M. 174.90

STA. 595+36 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 4'-6" X H = 5'-6" WITH
36" X 88' R.C. PIPE INLET &
36" X 96' R.C. PIPE OUTLET
TYPE "H" DROP INLET 100' RT.
4' X 4'-6" X H = 4'-0" WITH
36" X 42' R.C. PIPE OUTLET
WITH HDWLS. LT. AND RT.
RETAIN

STA. 602+04 IN PLACE
24" X 36' R.C. PIPE OUTLET
UNDER RT. F.R.
WITH HDWLS. LT. AND RT.
RETAIN

I-40

S:\adonaldson\8/26/2022 10:55:57 PM
WORKSPACE: AHTD
L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_I40_POI.dgn
REVISED DATE:



STA. 617+51 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 4'-6" X H = 4'-0" WITH
36" X 84' R.C. PIPE INLET &
36" X 84' R.C. PIPE OUTLET
TYPE "H" DROP INLET 93' RT.
WITH 36" X 46' R.C. PIPE OUTLET
WITH HDWLS. LT. AND RT.
RETAIN

STA. 626+04 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 4'-6" X H = 5'-6" WITH
24" X 84' R.C. PIPE OUTLET
TYPE "H" DROP INLET 94' RT.
WITH 24" X 44' R.C. PIPE OUTLET
WITH HDWL. RT.
RETAIN

C.L. MEDIAN I-40
PI = 638+29.27
Δ = 14°12'51" RT.
D = 00°30'00"
T = 1428.77'
L = 2842.86'
PC = 624+00.50
PT = 652+43.36
e = MATCH EXISTING
Ls = MATCH EXISTING

STA. 633+55 IN PLACE
DBL. 10' X 7' X 24' R.C. BOX CULVERT
TYPE "K" DROP INLET IN MEDIAN
4' X 3'-6" X H = 4'-6" WITH &
TYPE "K" DROP INLET 92' RT.
4' X 3'-6" X H = 1'-6"
RETAIN

I-40

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		29	63
				JOB NO.		061687	PLAN STA. 640+00 - STA. 700+00	



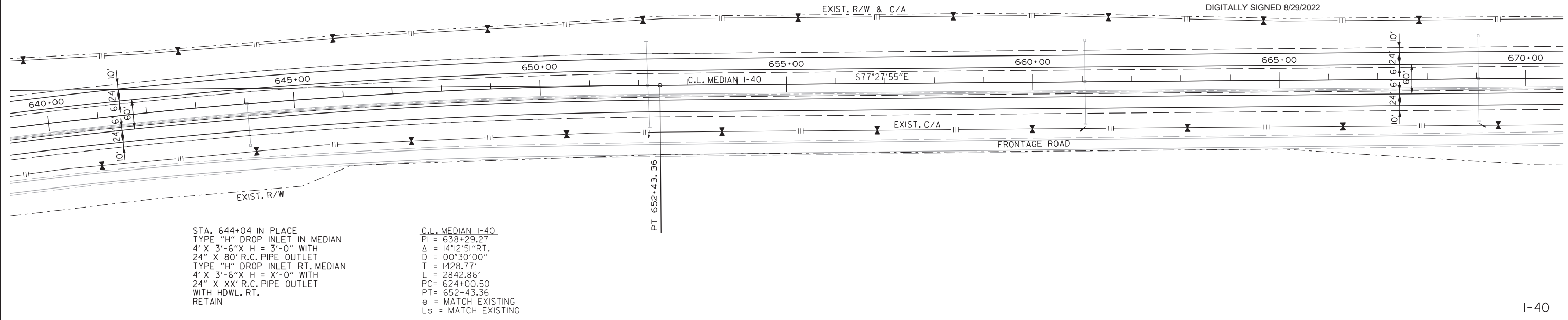
DIGITALLY SIGNED 8/29/2022



STA. 652+18 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 6' X H = 5'-6" WITH
48" X 88' R.C. PIPE INLET &
48" X 84' R.C. PIPE OUTLET
WITH HDWLS. LT. AND RT.
RETAIN

STA. 661+05 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 80' R.C. PIPE INLET &
24" X 84' R.C. PIPE OUTLET
WITH 24" STEEL GRATE
ASSEMBLY LT. & HDWL. RT.
RETAIN

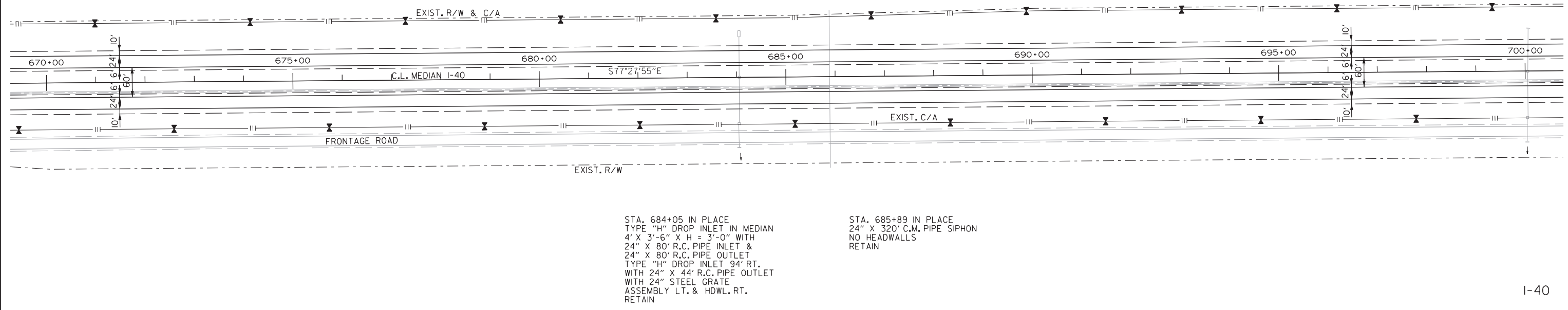
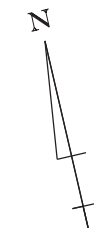
STA. 669+03 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 4' X H = 4'-0" WITH
30" X 80' R.C. PIPE INLET &
30" X 84' R.C. PIPE OUTLET
WITH 24" STEEL GRATE
ASSEMBLY LT. & HDWL. RT.
RETAIN



STA. 644+04 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 80' R.C. PIPE OUTLET
TYPE "H" DROP INLET RT. MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X XX' R.C. PIPE OUTLET
WITH HDWL. RT.
RETAIN

C.L. MEDIAN I-40
PI = 638+29.27
 Δ = 14°12'51" RT.
D = 00°30'00"
T = 1428.77'
L = 2842.86'
PC = 624+00.50
PT = 652+43.36
e = MATCH EXISTING
Ls = MATCH EXISTING

I-40



STA. 684+05 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 80' R.C. PIPE INLET &
24" X 80' R.C. PIPE OUTLET
TYPE "H" DROP INLET 94' RT.
WITH 24" X 44' R.C. PIPE OUTLET
WITH 24" STEEL GRATE
ASSEMBLY LT. & HDWL. RT.
RETAIN

STA. 685+89 IN PLACE
24" X 320' C.M. PIPE SIPHON
NO HEADWALLS
RETAIN

I-40

8/26/2022 10:55:58 PM
 S:\donaldson\WORKSPACE\AHTD\1\2017\07580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_I40_P02.dgn
 L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_I40_P02.dgn
 REVISION DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		30	63
				JOB NO.		061687	PLAN STA. 700+00 - STA. 760+00	

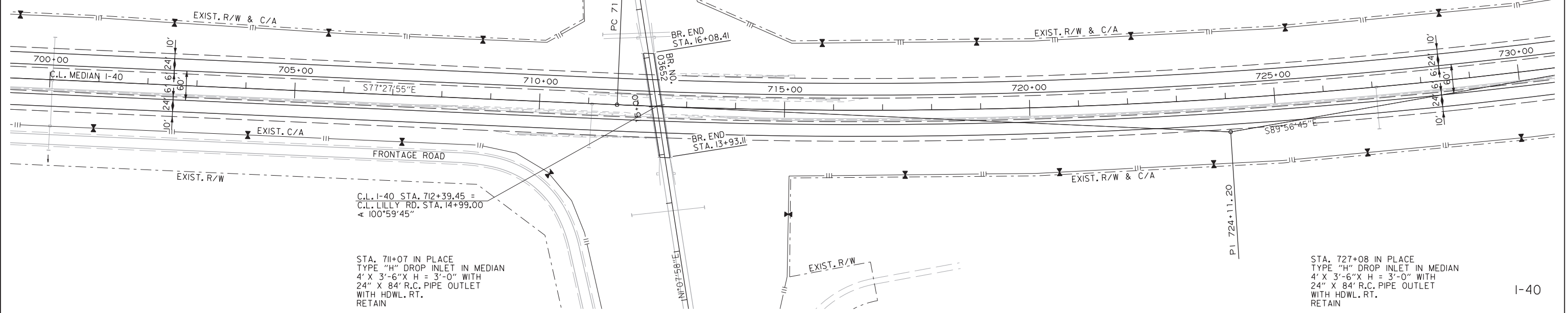


STA. 700+05 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-9" WITH
24" X 84' R.C. PIPE INLET &
24" X 84' R.C. PIPE OUTLET
DROP INLET 94' RT. WITH
24" X 44' R.C. PIPE OUTLET
WITH HDWLS. LT. AND RT.
RETAIN

STA. 13+93.11 BRIDGE END
BRIDGE NO. 03652 LILLY ROAD
EXISTING 215.30' COMP. I-BEAM SPANS
20'-0" CLEAR ROADWAY
STA. 16+08.41 BRIDGE END
POLYMER OVERLAY

GUARDRAIL
STA. 708+33 TO 713+33 LT. OF RT. MAIN LANES RETAIN
STA. 709+60 TO 712+29 RT. OF RT. MAIN LANES RETAIN
STA. 711+13 TO 716+13 RT. OF LT. MAIN LANES RETAIN
STA. 712+40 TO 715+09 LT. OF LT. MAIN LANES RETAIN

C.L. MEDIAN I-40
PI = 724+11.20
Δ = 12°28'50" LT.
D = 00°30'00"
T = 1253.01'
L = 2496.11'
PC = 711+58.19
PT = 736+54.30
e = MATCH EXISTING
Ls = MATCH EXISTING

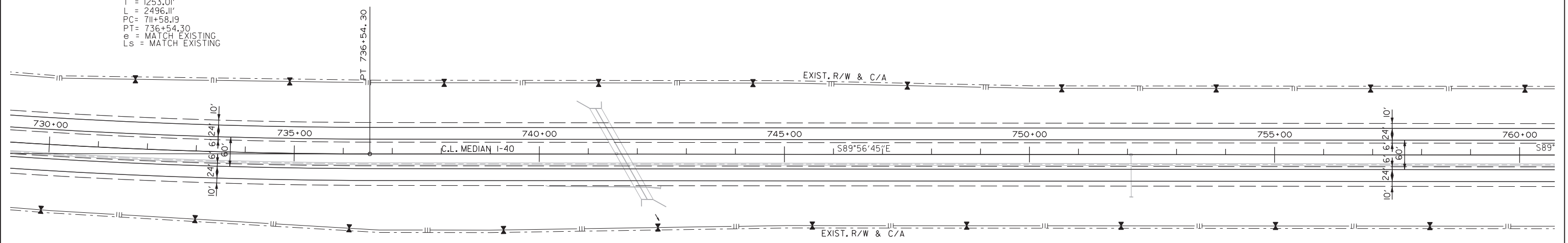


C.L. I-40 STA. 712+39.45 =
C.L. LILLY RD. STA. 14+99.00
Δ = 100°59'45"

STA. 711+07 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 84' R.C. PIPE OUTLET
WITH HDWL. RT.
RETAIN

STA. 727+08 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 84' R.C. PIPE OUTLET
WITH HDWL. RT.
RETAIN

C.L. MEDIAN I-40
PI = 724+11.20
Δ = 12°28'50" LT.
D = 00°30'00"
T = 1253.01'
L = 2496.11'
PC = 711+58.19
PT = 736+54.30
e = MATCH EXISTING
Ls = MATCH EXISTING



STA. 741+68 IN PLACE
DBL. 10' X 8' X 216' R.C. BOX CULVERT
TYPE "K" DROP INLET IN MEDIAN
4' X 3'-6" X H = 4'-3"
30° RT. FWD. SKEW
RETAIN

STA. 752+07 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 84' R.C. PIPE OUTLET
WITH HDWL. RT.
RETAIN

S:\Donaldson 8/26/2022 10:59:59 PM
WORKSPACE: AHTD
L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_I40_P03.dgn
REVISED DATE:



I-40

I-40

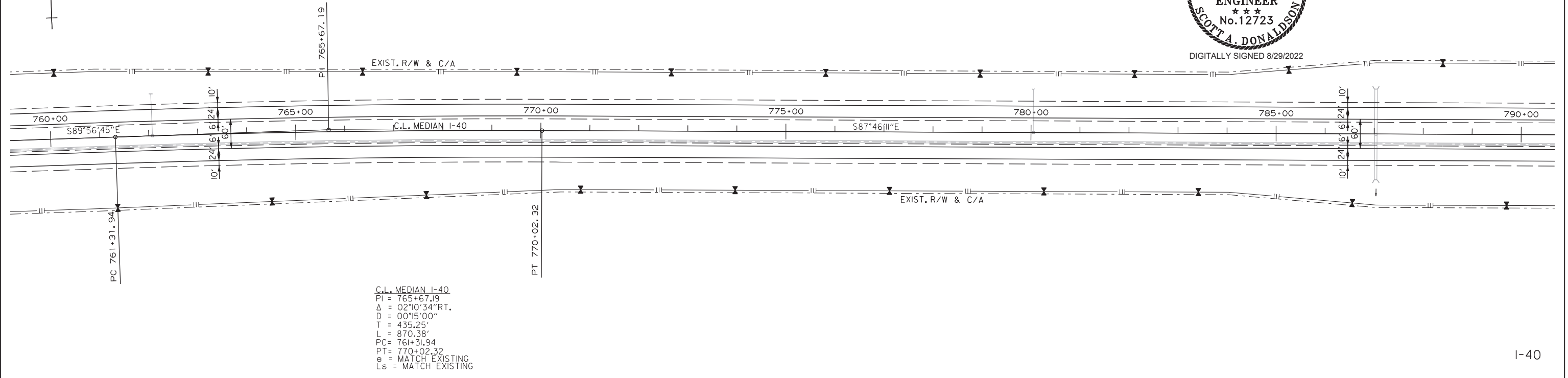
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		31	63
				JOB NO.	061687			
				PLAN STA. 760+00 - STA. 820+00				



STA. 762+08 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 84' R.C. PIPE OUTLET
WITH HDWL. RT.
RETAIN

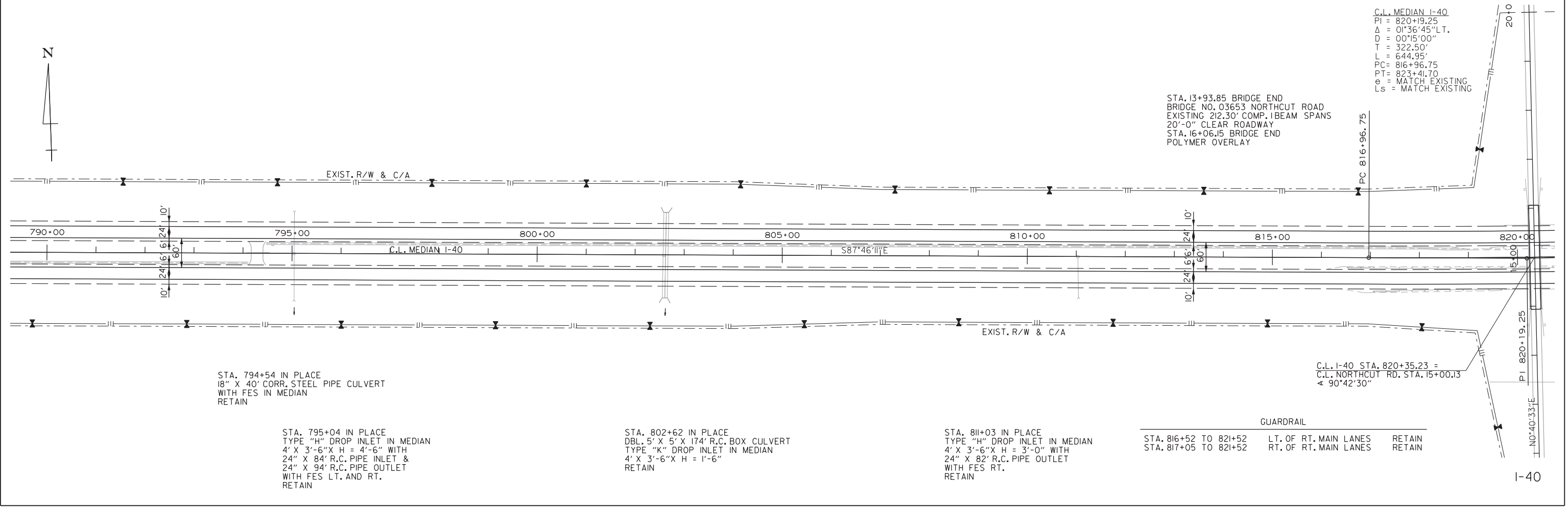
STA. 780+05 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 86' R.C. PIPE OUTLET
WITH FES LT.
RETAIN

STA. 787+04 IN PLACE
5' X 4' X 185' R.C. BOX CULVERT
TYPE "K" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-9"
RETAIN



C.L. MEDIAN I-40
PI = 765+67.19
Δ = 02°10'34" RT.
D = 00°15'00"
T = 435.25'
L = 870.38'
PC = 761+31.94
PT = 770+02.32
e = MATCH EXISTING
Ls = MATCH EXISTING

I-40



C.L. MEDIAN I-40
PI = 820+19.25
Δ = 01°36'45" LT.
D = 00°15'00"
T = 322.50'
L = 644.95'
PC = 816+96.75
PT = 823+41.70
e = MATCH EXISTING
Ls = MATCH EXISTING

STA. 13+93.85 BRIDGE END
BRIDGE NO. 03653 NORTH CUT ROAD
EXISTING 212.30' COMP. I-BEAM SPANS
20'-0" CLEAR ROADWAY
STA. 16+06.15 BRIDGE END
POLYMER OVERLAY

STA. 794+54 IN PLACE
18" X 40' CORR. STEEL PIPE CULVERT
WITH FES IN MEDIAN
RETAIN

STA. 795+04 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 4'-6" WITH
24" X 84' R.C. PIPE INLET &
24" X 94' R.C. PIPE OUTLET
WITH FES LT. AND RT.
RETAIN

STA. 802+62 IN PLACE
DBL. 5' X 5' X 174' R.C. BOX CULVERT
TYPE "K" DROP INLET IN MEDIAN
4' X 3'-6" X H = 1'-6"
RETAIN

STA. 811+03 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 82' R.C. PIPE OUTLET
WITH FES RT.
RETAIN

GUARDRAIL
STA. 816+52 TO 821+52 LT. OF RT. MAIN LANES RETAIN
STA. 817+05 TO 821+52 RT. OF RT. MAIN LANES RETAIN

C.L. I-40 STA. 820+35.23 =
C.L. NORTHCUT RD. STA. 15+00.13
Δ 90°42'30"

I-40

S:\adonaldson 8/26/2022 10:00 PM
 WORKSPACE: AHTD
 L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_H40_P04.dgn
 REVISED DATE:

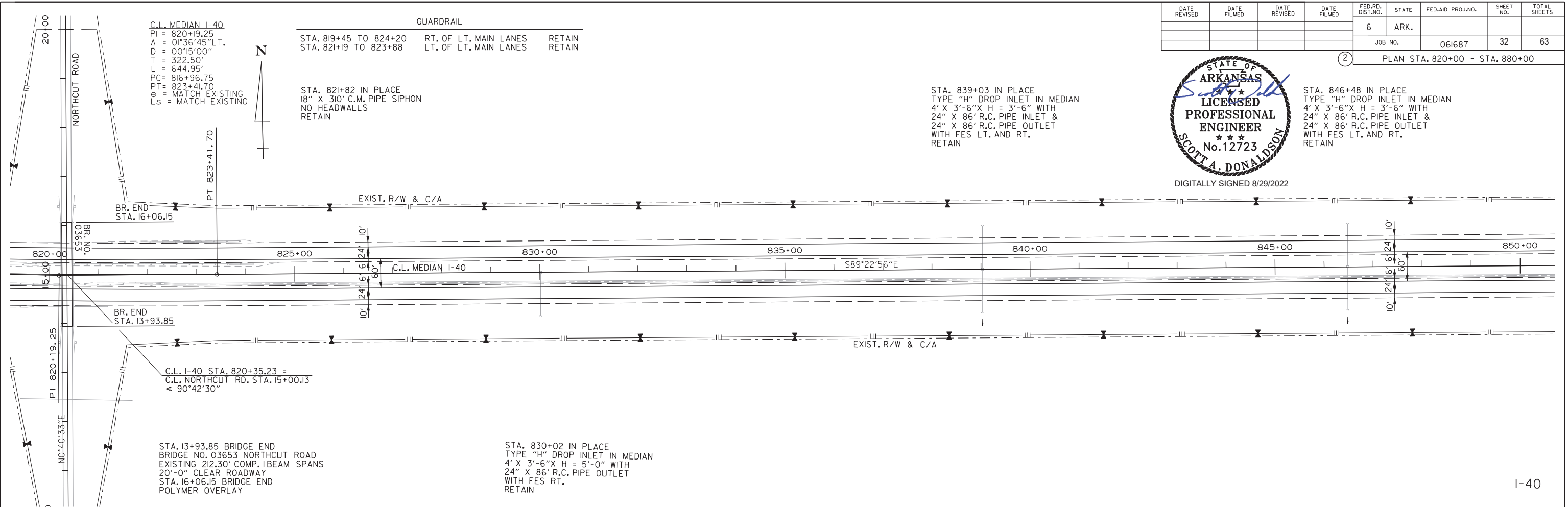
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		32	63
				JOB NO.	061687			

② PLAN STA. 820+00 - STA. 880+00

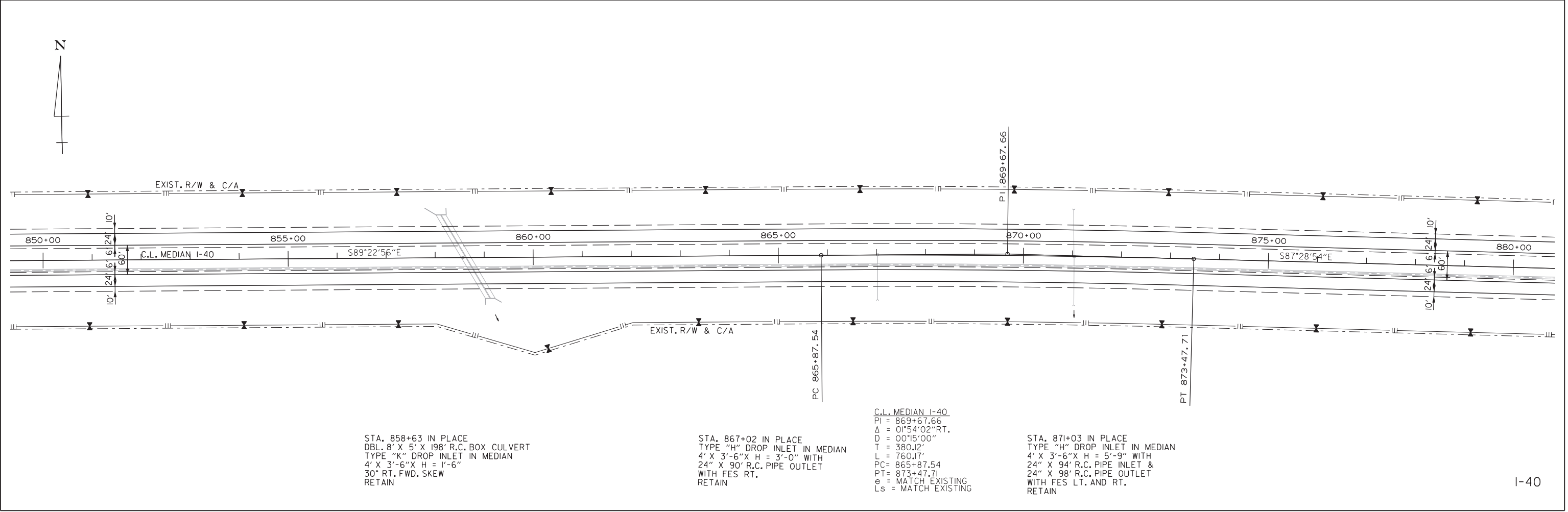


DIGITALLY SIGNED 8/29/2022

STA. 846+48 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-6" WITH
24" X 86' R.C. PIPE INLET &
24" X 86' R.C. PIPE OUTLET
WITH FES LT. AND RT.
RETAIN



I-40



I-40

S:\Donaldson 8/26/2022 10:01PM
 WORKSPACE: AHTD
 L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_I40_P05.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		33	63
				JOB NO.		061687		
				PLAN STA. 880+00 - STA. 940+00				

2



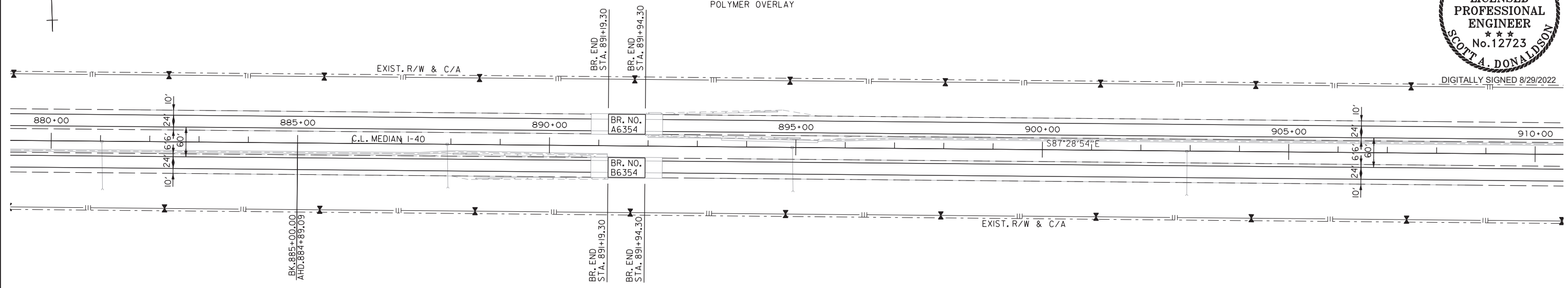
DIGITALLY SIGNED 8/29/2022



GUARDRAIL

STA. 888+00 TO 891+19	LT. OF RT. MAIN LANES	RETAIN
STA. 888+50 TO 891+19	RT. OF RT. MAIN LANES	RETAIN
STA. 891+94 TO 894+63	LT. OF LT. MAIN LANES	RETAIN
STA. 891+94 TO 895+13	RT. OF LT. MAIN LANES	RETAIN

STA. 891+19.30 BRIDGE END
BRIDGE NO. A3654
EXISTING 75'-0" R.C. SLAB UNIT
40'-0" CLEAR ROADWAY
STA. 891+94.30 BRIDGE END
POLYMER OVERLAY



STA. 881+03 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 94" R.C. PIPE OUTLET
WITH FES RT.
RETAIN

STA. 887+93 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 86" R.C. PIPE OUTLET
WITH FES RT.
RETAIN

STA. 891+19.30 BRIDGE END
BRIDGE NO. B3654
EXISTING 75'-0" R.C. SLAB UNIT
40'-0" CLEAR ROADWAY
STA. 891+94.30 BRIDGE END
POLYMER OVERLAY

STA. 894+93 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 86" R.C. PIPE OUTLET
WITH FES RT.
RETAIN

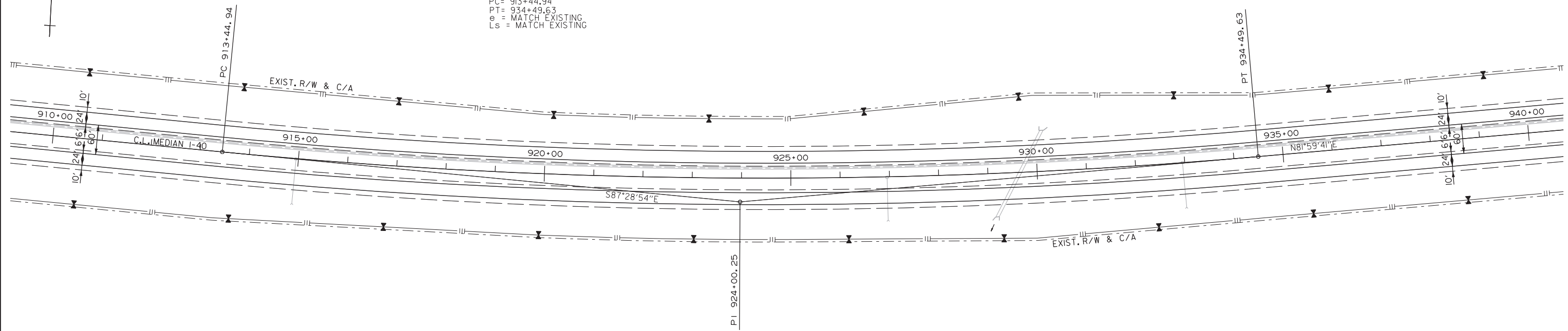
STA. 902+94 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 86" R.C. PIPE OUTLET
WITH FES RT.
RETAIN

I-40



C.L. MEDIAN I-40
PI = 924+00.25
Δ = 10°31'25" LT.
D = 00°30'00"
T = 1055.31'
L = 2104.69'
PC = 913+44.94
PT = 934+49.63
e = MATCH EXISTING
Ls = MATCH EXISTING

STA. 929+65 IN PLACE
6' X 4' X 198" R.C. BOX CULVERT
30° LT. FWD. SKEW
RETAIN



STA. 914+95 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 90" R.C. PIPE OUTLET
WITH FES RT.
RETAIN

STA. 926+95 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 86" R.C. PIPE OUTLET
WITH FES RT.
RETAIN

STA. 932+95 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 90" R.C. PIPE OUTLET
WITH FES RT.
RETAIN

I-40

S:\adonaldson\8/26/2022 10:05 PM
 WORKSPACE: AHTD
 L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P06.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		34	63

PLAN STA. 940+00 - STA. 1000+00



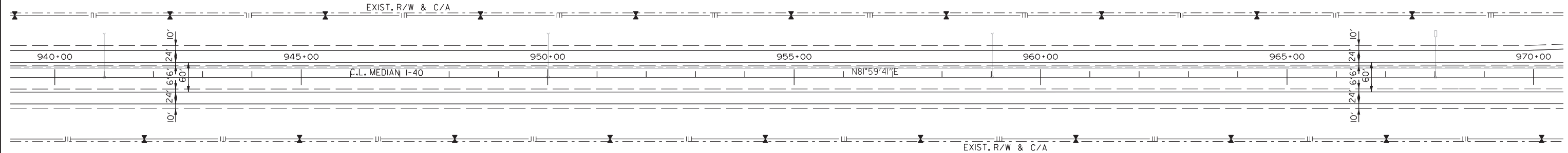
DIGITALLY SIGNED 8/29/2022

STA. 968+01 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 80' R.C. PIPE OUTLET
WITH 24" STEEL GRATE ASSEMBLY LT.
RETAIN

STA. 941+00 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 86' R.C. PIPE OUTLET
WITH FES RT.
RETAIN

STA. 950+01 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 86' R.C. PIPE OUTLET
WITH FES RT.
RETAIN

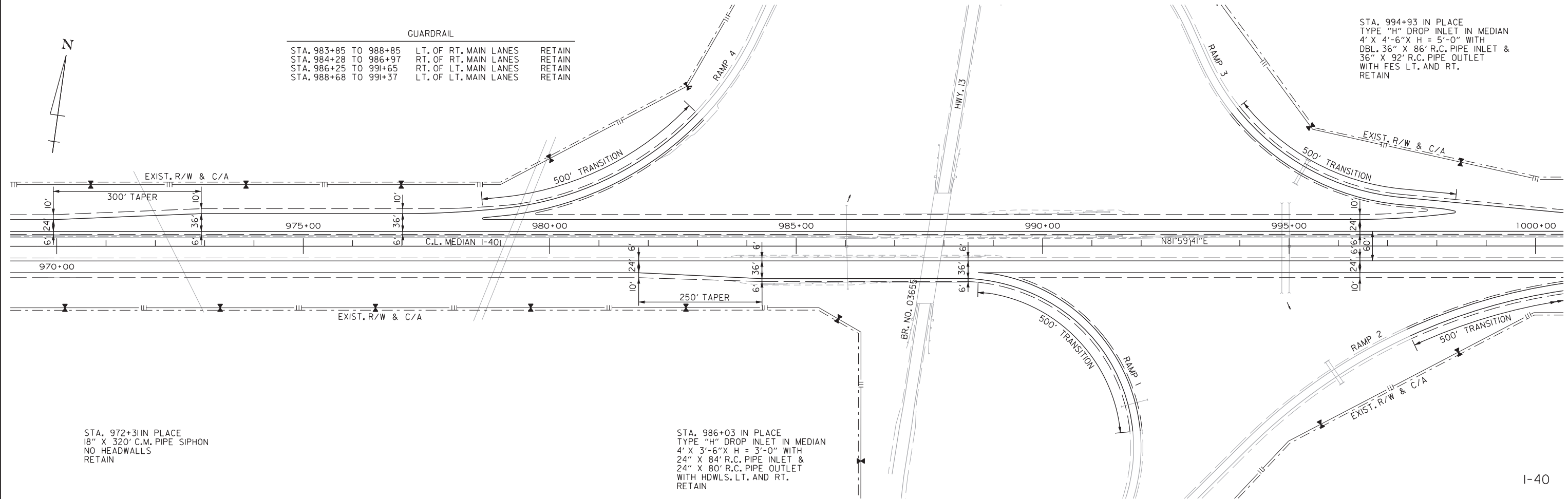
STA. 959+02 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 86' R.C. PIPE OUTLET
WITH FES RT.
RETAIN



I-40

GUARDRAIL			
STA. 983+85 TO 988+85	LT. OF RT. MAIN LANES	RETAIN	
STA. 984+28 TO 986+97	RT. OF RT. MAIN LANES	RETAIN	
STA. 986+25 TO 991+65	RT. OF LT. MAIN LANES	RETAIN	
STA. 988+68 TO 991+37	LT. OF LT. MAIN LANES	RETAIN	

STA. 994+93 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 4'-6" X H = 5'-0" WITH
DBL. 36" X 86' R.C. PIPE INLET &
36" X 92' R.C. PIPE OUTLET
WITH FES LT. AND RT.
RETAIN



STA. 972+31 IN PLACE
18" X 320' C.M. PIPE SIPHON
NO HEADWALLS
RETAIN

STA. 986+03 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 84' R.C. PIPE INLET &
24" X 80' R.C. PIPE OUTLET
WITH HDWLS. LT. AND RT.
RETAIN

I-40

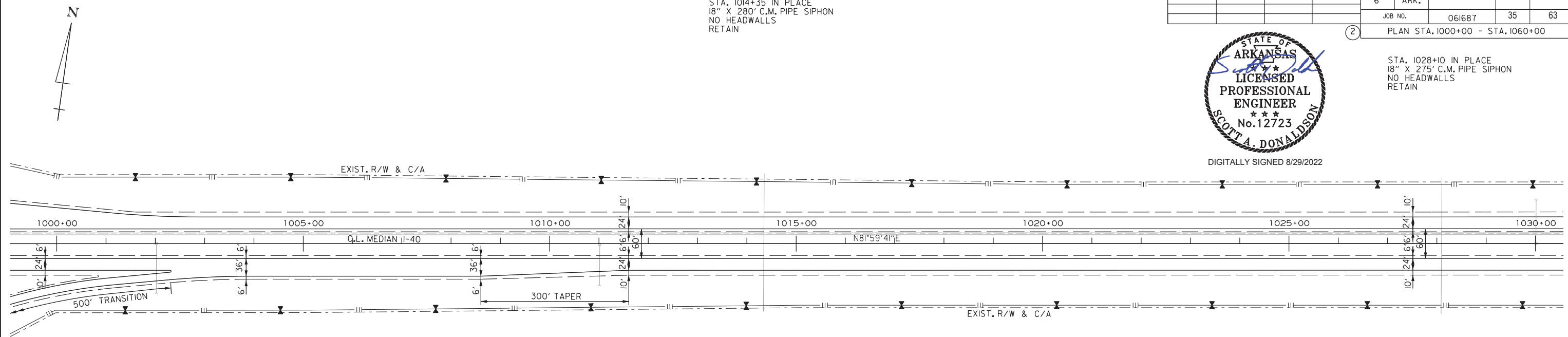
8/26/2022 10:07 PM
 S.Adonaldson
 WORKSPACE: AHTD
 L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P07.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		35	63
				JOB NO.	061687		PLAN STA. 1000+00 - STA. 1060+00	



STA. 1028+10 IN PLACE
18" X 275' C.M. PIPE SIPHON
NO HEADWALLS
RETAIN

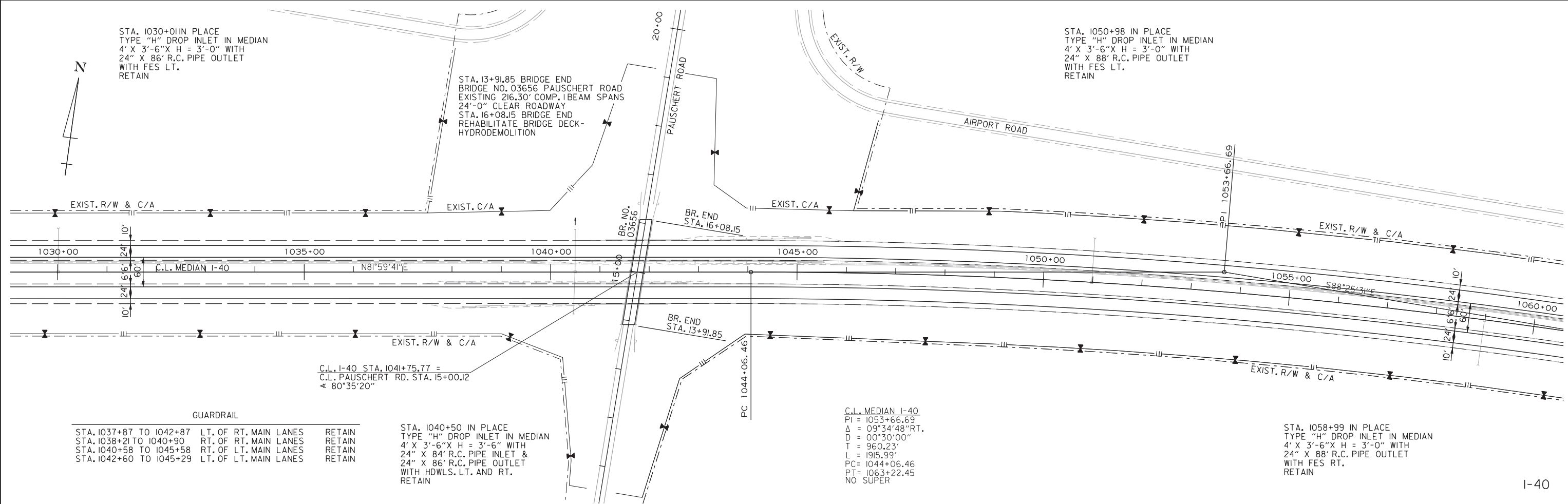
STA. 1014+35 IN PLACE
18" X 280' C.M. PIPE SIPHON
NO HEADWALLS
RETAIN



STA. 1002+02 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 98' R.C. PIPE OUTLET
WITH FES RT.
RETAIN

STA. 1011+02 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 82' R.C. PIPE OUTLET
WITH FES RT.
RETAIN

I-40



STA. 1030+01 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 86' R.C. PIPE OUTLET
WITH FES LT.
RETAIN

STA. 13+91.85 BRIDGE END
BRIDGE NO. 03656 PAUSCHERT ROAD
EXISTING 216.30' COMP. I BEAM SPANS
24'-0" CLEAR ROADWAY
STA. 16+08.15 BRIDGE END
REHABILITATE BRIDGE DECK-
HYDRODEMOLITION

STA. 1050+98 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 88' R.C. PIPE OUTLET
WITH FES LT.
RETAIN

GUARDRAIL

STA. 1037+87 TO 1042+87	LT. OF RT. MAIN LANES	RETAIN
STA. 1038+21 TO 1040+90	RT. OF RT. MAIN LANES	RETAIN
STA. 1040+58 TO 1045+58	RT. OF LT. MAIN LANES	RETAIN
STA. 1042+60 TO 1045+29	LT. OF LT. MAIN LANES	RETAIN

STA. 1040+50 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-6" WITH
24" X 84' R.C. PIPE INLET &
24" X 86' R.C. PIPE OUTLET
WITH HDWLS, LT. AND RT.
RETAIN

C.L. MEDIAN I-40
PI = 1053+66.69
Δ = 09°34'48" RT.
D = 00°30'00"
T = 960.23'
L = 1915.99'
PC = 1044+06.46
PT = 1063+22.45
NO SUPER

STA. 1058+99 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 88' R.C. PIPE OUTLET
WITH FES RT.
RETAIN

I-40

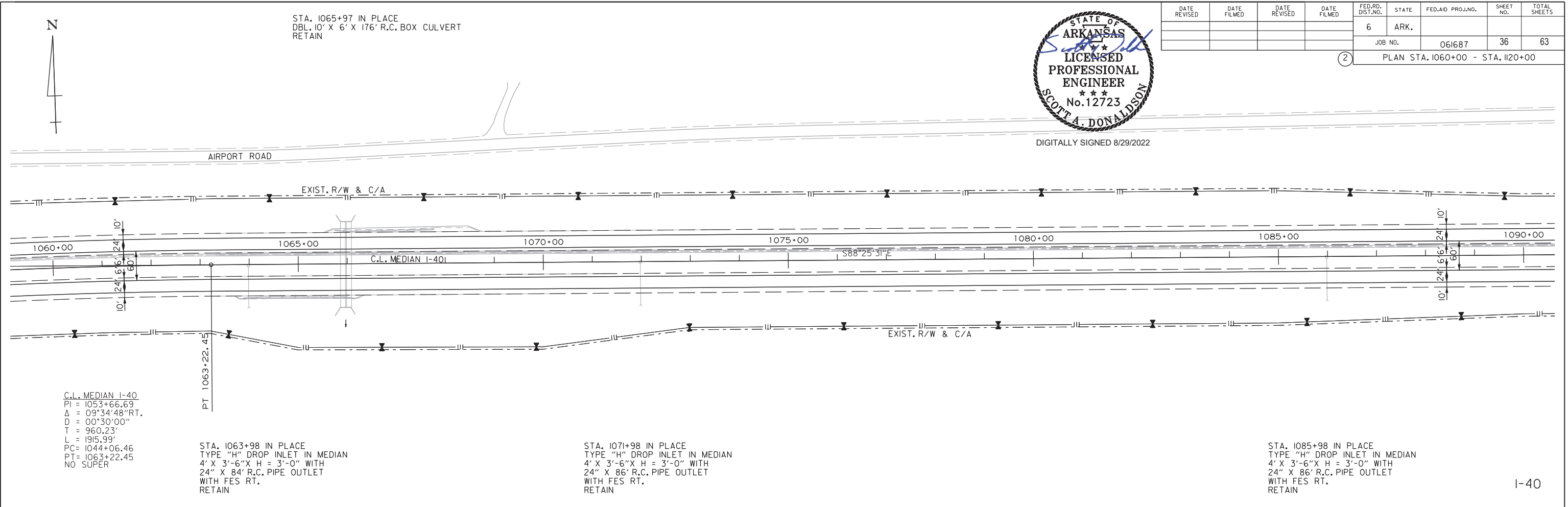
S:\adonaldson 8/26/2022 10:03 PM
 WORKSPACE: AHTD
 L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P08.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		36	63
				JOB NO.	061687		PLAN STA. 1060+00 - STA. 1120+00	



DIGITALLY SIGNED 8/29/2022

STA. 1065+97 IN PLACE
DBL. 10' X 6' X 176' R.C. BOX CULVERT
RETAIN



C.L. MEDIAN I-40
PI = 1053+66.69
 $\Delta = 09^{\circ}34'48''$ RT.
D = 00'30'00"
T = 960.23'
L = 1915.99'
PC = 1044+06.46
PT = 1063+22.45
NO SUPER

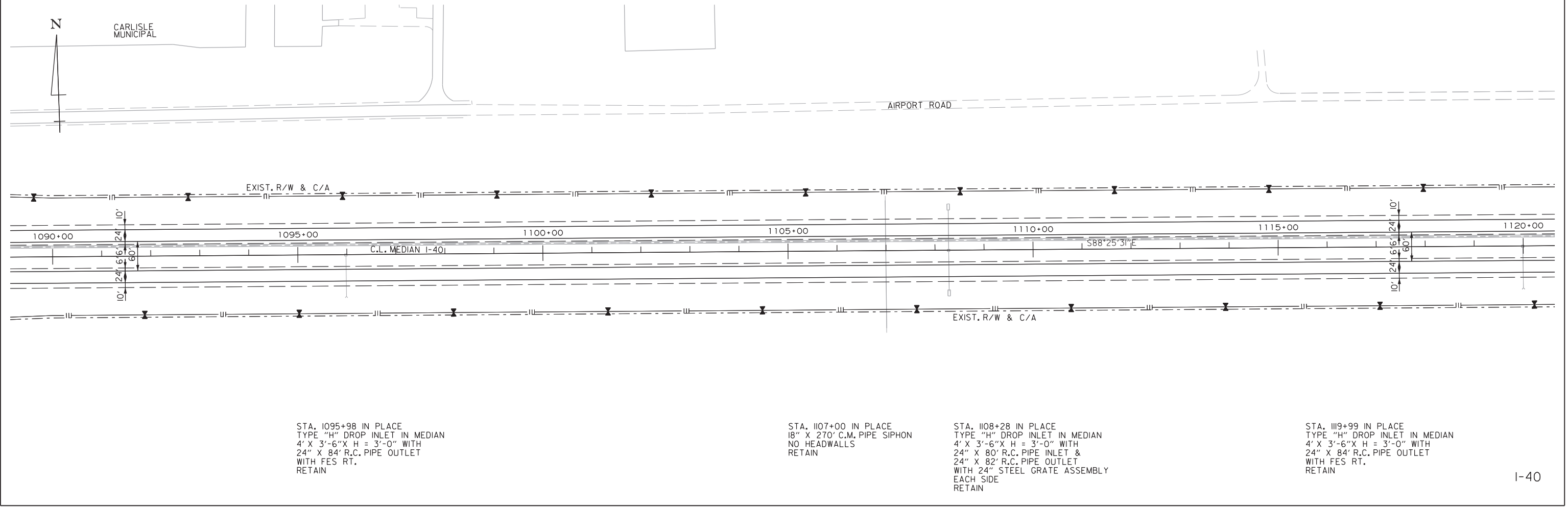
STA. 1063+98 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 84' R.C. PIPE OUTLET
WITH FES RT.
RETAIN

STA. 1071+98 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 86' R.C. PIPE OUTLET
WITH FES RT.
RETAIN

STA. 1085+98 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 86' R.C. PIPE OUTLET
WITH FES RT.
RETAIN

I-40

CARLISLE MUNICIPAL



STA. 1095+98 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 84' R.C. PIPE OUTLET
WITH FES RT.
RETAIN

STA. 1107+00 IN PLACE
18" X 270' C.M. PIPE SIPHON
NO HEADWALLS
RETAIN

STA. 1108+28 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 80' R.C. PIPE INLET &
24" X 82' R.C. PIPE OUTLET
WITH 24" STEEL GRATE ASSEMBLY
EACH SIDE
RETAIN

STA. 1119+99 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 84' R.C. PIPE OUTLET
WITH FES RT.
RETAIN

I-40

8/26/2022 10:00 PM
 S:\adonaldson\WORKSPACE\AHTD\1\20171017580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_140_P09.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
						061687	37	63
				PLAN STA. 1120+00 - STA. 1150+00				



DIGITALLY SIGNED 8/29/2022

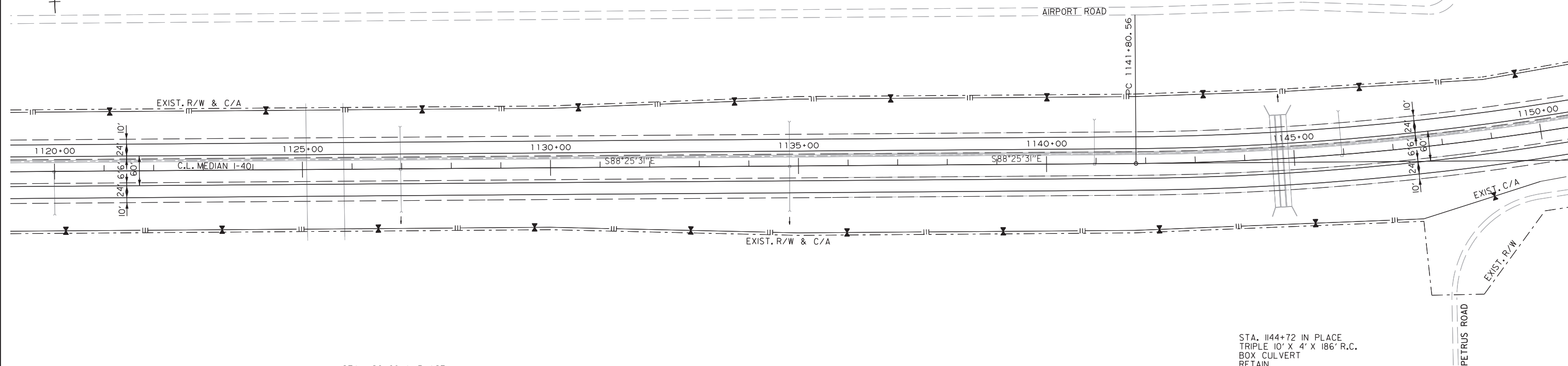
STA. 1125+08 IN PLACE
18" X 270' C.M. PIPE SIPHON
NO HEADWALLS
RETAIN

STA. 1125+83 IN PLACE
18" X 270' C.M. PIPE SIPHON
NO HEADWALLS
RETAIN

STA. 1134+82 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 4'-0" WITH
24" X 88' R.C. PIPE INLET &
24" X 88' R.C. PIPE OUTLET
WITH FES LT. AND RT.
RETAIN

STA. 1140+97 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-6" WITH
24" X 88' R.C. PIPE OUTLET
WITH FES LT.
RETAIN

C.L. MEDIAN I-40
PI = 1157+00.79
 $\Delta = 29^{\circ}35'51''$ LT.
D = 01'00'00"
T = 1513.68'
L = 2959.75'
PC = 1141+80.56
PT = 1171+46.86
e = MATCH EXISTING
Ls = MATCH EXISTING



STA. 1126+98 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-0" WITH
24" X 84' R.C. PIPE INLET &
24" X 84' R.C. PIPE OUTLET
WITH FES LT. AND RT.
RETAIN

STA. 1144+72 IN PLACE
TRIPLE 10' X 4' X 186' R.C.
BOX CULVERT
RETAIN

STA. 1145+96 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H = 3'-6" WITH
24" X 88' R.C. PIPE OUTLET
WITH FES LT.
RETAIN

S:\adonaldson 8/26/2022 10:01 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P10.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		38	63
				JOB NO.	061687			

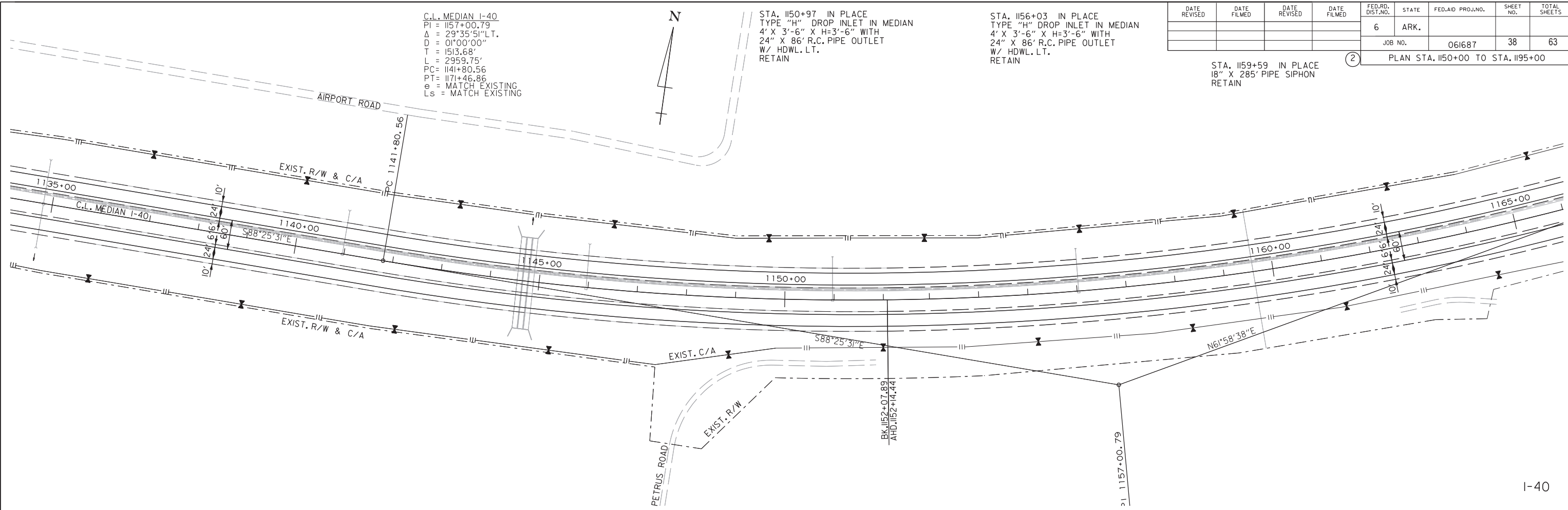
PLAN STA. 1150+00 TO STA. 1195+00

STA. 1159+59 IN PLACE
18" X 285' PIPE SIPHON
RETAIN

C.L. MEDIAN I-40
PI = 1157+00.79
 $\Delta = 29^{\circ}35'51''$ L.T.
D = 01°00'00"
T = 1513.68'
L = 2959.75'
PC = 1141+80.56
PT = 1171+46.86
e = MATCH EXISTING
Ls = MATCH EXISTING

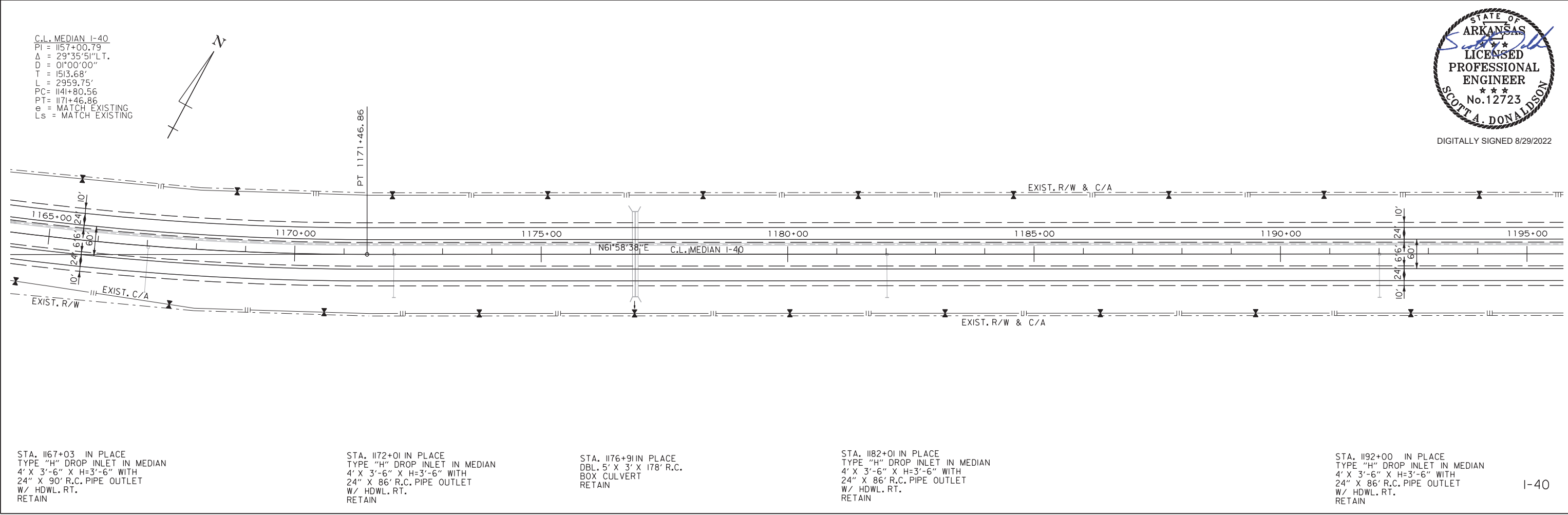
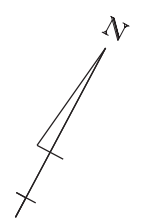
STA. 1150+97 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. LT.
RETAIN

STA. 1156+03 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. LT.
RETAIN



I-40

C.L. MEDIAN I-40
PI = 1157+00.79
 $\Delta = 29^{\circ}35'51''$ L.T.
D = 01°00'00"
T = 1513.68'
L = 2959.75'
PC = 1141+80.56
PT = 1171+46.86
e = MATCH EXISTING
Ls = MATCH EXISTING



I-40

STA. 1167+03 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 90' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1172+01 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1176+91 IN PLACE
DBL. 5' X 3' X 178" R.C.
BOX CULVERT
RETAIN

STA. 1182+01 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1192+00 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN



DIGITALLY SIGNED 8/29/2022

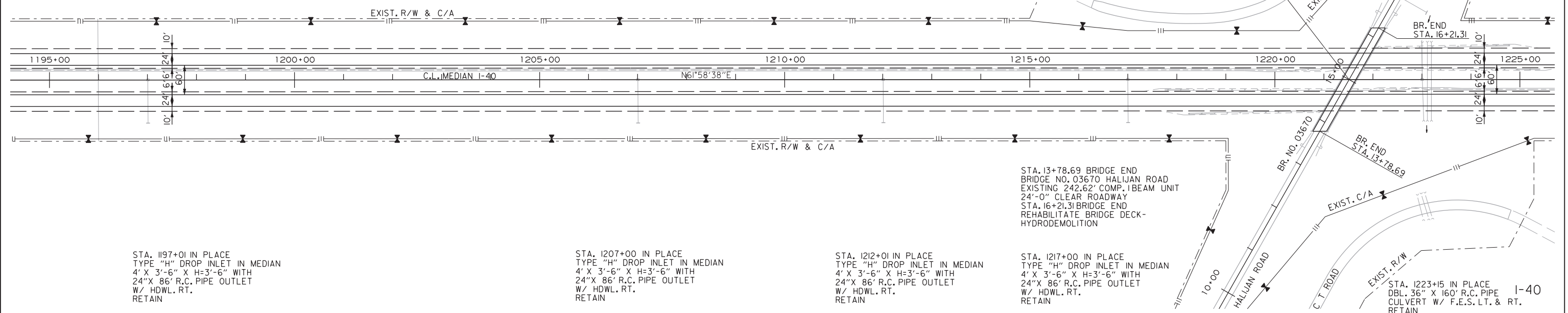
S:\Donaldson 8/26/2022 10:42 PM
 WORKSPACE: AHTD
 L:\2017\101580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P11.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		39	63
				JOB NO.	061687		PLAN STA. 1195+00 TO STA. 1255+00	

STA. 1196+00 IN PLACE
18" X 250' PIPE SIPHON
RETAIN

GUARDRAIL
 STA. 1217+75 TO 1222+75 LT. OF RT. MAIN LANES RETAIN
 STA. 1218+23 TO 1220+92 RT. OF RT. MAIN LANES RETAIN
 STA. 1220+41 TO 1225+41 RT. OF LT. MAIN LANES RETAIN
 STA. 1222+11 TO 1224+80 LT. OF LT. MAIN LANES RETAIN

STA. 1223+01 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 5'-6" X H=4'-6" WITH
36" X 74" R.C. PIPE INLET
36" X 82" R.C. PIPE OUTLET
W/ F.E.S. LT. & RT.
RETAIN



STA. 1197+01 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86" R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1207+00 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86" R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1212+01 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86" R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

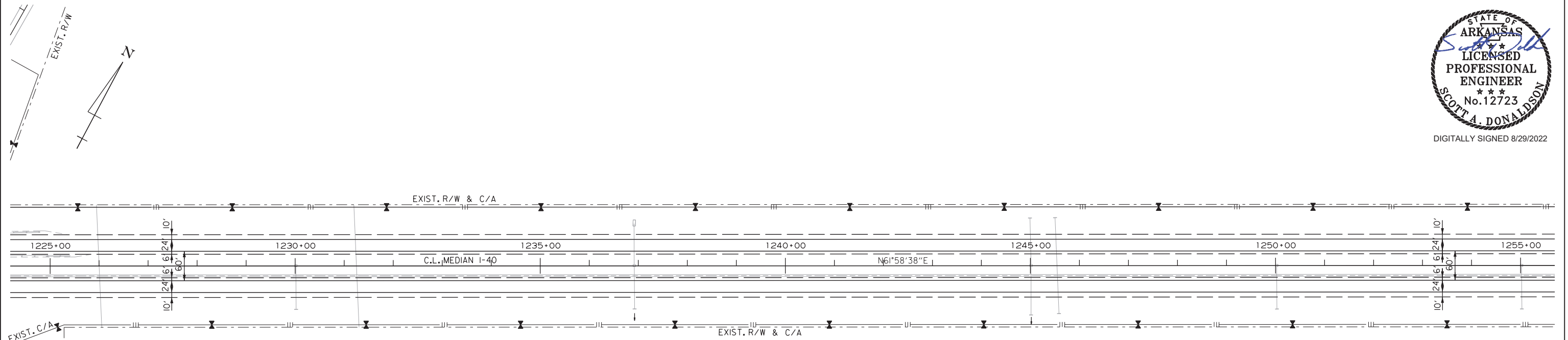
STA. 13+78.69 BRIDGE END
BRIDGE NO. 03670 HALIJAN ROAD
EXISTING 242.62' COMP. I BEAM UNIT
24'-0" CLEAR ROADWAY
STA. 16+21.31 BRIDGE END
REHABILITATE BRIDGE DECK-
HYDRODEMOLITION

STA. 1217+00 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86" R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1223+15 IN PLACE
DBL. 36" X 160" R.C. PIPE
CULVERT W/ F.E.S. LT. & RT.
RETAIN



DIGITALLY SIGNED 8/29/2022



STA. 1226+00 IN PLACE
18" X 250' PIPE SIPHON
RETAIN

STA. 1230+01 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86" R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1231+25 IN PLACE
18" X 250' PIPE SIPHON
RETAIN

STA. 1236+92 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 84" R.C. PIPE INLET
& 24" X 86" R.C. PIPE OUTLET
W/ STEEL GRATE ASSEMBLY LT.
& W/ HDWL. RT.
RETAIN

STA. 1245+01 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=5'-6" WITH
24" X 94" R.C. PIPE INLET
& 24" X 98" R.C. PIPE OUTLET
W/ HDWLS. LT. & RT.
RETAIN

STA. 1245+54 IN PLACE
24" X 196" R.C. PIPE CULVERT
W/ HDWLS. LT. & RT.
RETAIN

STA. 1250+02 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86" R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

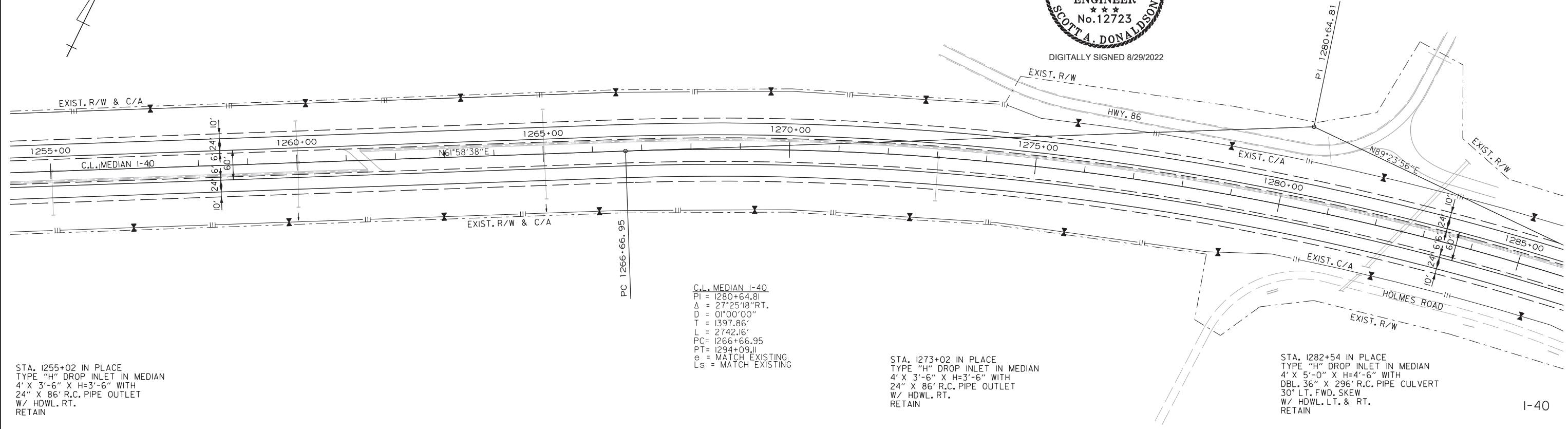
SADonaldson 8/26/2022 10:05 PM WORKSPACE: AHTD L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P12.dgn REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		40	63
				JOB NO.		061687	PLAN STA. 1255+00 TO STA. 1315+00	



STA. 1260+03 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 84' R.C. PIPE INLET
& 24" X 86' R.C. PIPE OUTLET
W/ HDWLS. LT. & RT.
RETAIN

STA. 1265+02 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE INLET
& 24" X 90' R.C. PIPE OUTLET
W/ HDWLS. LT. & RT.
RETAIN



STA. 1255+02 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

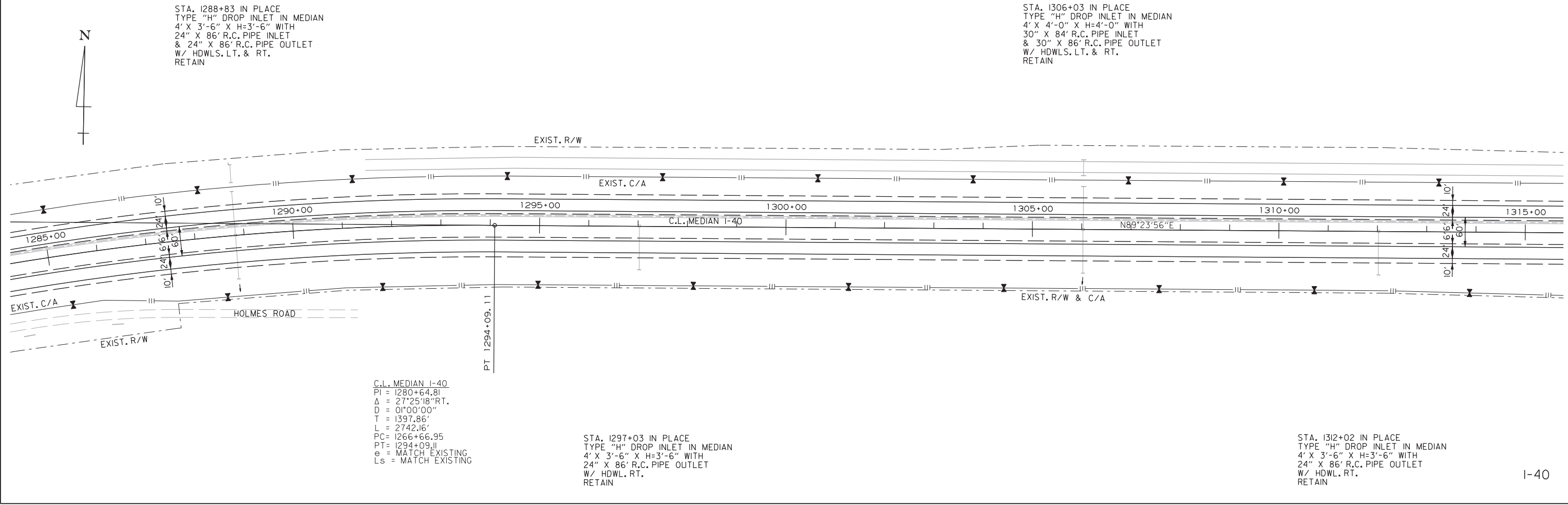
C.L. MEDIAN I-40
PI = 1280+64.81
Δ = 27°25'18" RT.
D = 01°00'00"
T = 1397.86'
L = 2742.16'
PC = 1266+66.95
PT = 1294+09.11
e = MATCH EXISTING
Ls = MATCH EXISTING

STA. 1273+02 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1282+54 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 5'-0" X H=4'-6" WITH
DBL. 36" X 296' R.C. PIPE CULVERT
30" LT. FWD. SKEW
W/ HDWL. LT. & RT.
RETAIN

STA. 1288+83 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE INLET
& 24" X 86' R.C. PIPE OUTLET
W/ HDWLS. LT. & RT.
RETAIN

STA. 1306+03 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 4'-0" X H=4'-0" WITH
30" X 84' R.C. PIPE INLET
& 30" X 86' R.C. PIPE OUTLET
W/ HDWLS. LT. & RT.
RETAIN



C.L. MEDIAN I-40
PI = 1280+64.81
Δ = 27°25'18" RT.
D = 01°00'00"
T = 1397.86'
L = 2742.16'
PC = 1266+66.95
PT = 1294+09.11
e = MATCH EXISTING
Ls = MATCH EXISTING

STA. 1297+03 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1312+02 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

8/26/2022 10:16 PM
 S.A.Donaldson
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P13.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		41	63
				JOB NO.		061687	PLAN STA. 1315+00 TO STA. 1375+00	

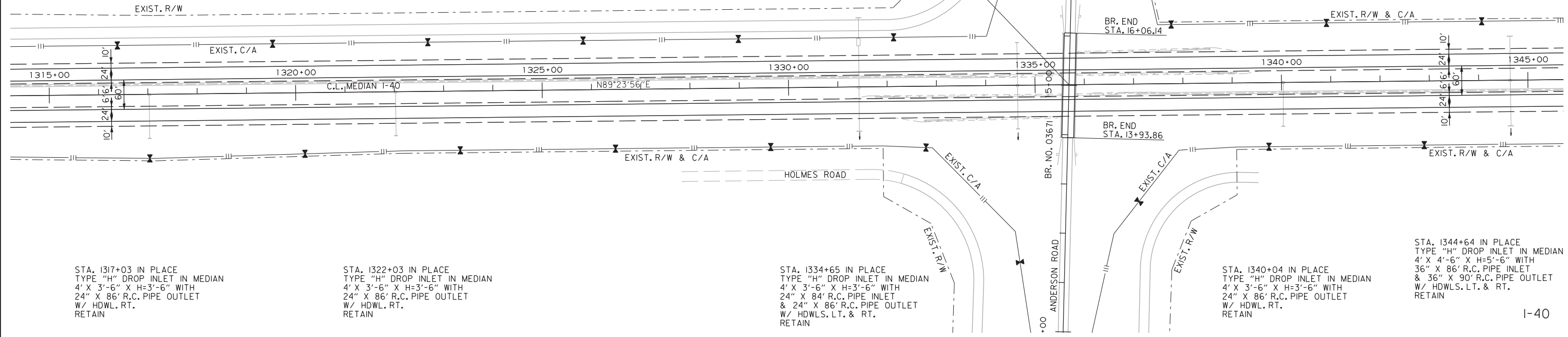


GUARDRAIL
 STA. 1331+75 TO 1336+75 LT. OF RT. MAIN LANES RETAIN
 STA. 1332+75 TO 1335+44 RT. OF RT. MAIN LANES RETAIN
 STA. 1334+17 TO 1339+17 RT. OF LT. MAIN LANES RETAIN
 STA. 1335+88 TO 1338+57 LT. OF LT. MAIN LANES RETAIN

STA. 1331+43 IN PLACE
 36" X 42' R.C. PIPE INLET
 & 36" X 86' R.C. PIPE OUTLET
 TYPE "H" DROP INLET IN MEDIAN
 4' X 4'-6" X H=4'-6" I-OPENING
 W/ HDWL. RT. & DROP INLET LT.
 RETAIN

C.L. I-40 STA. 1335+68.96 =
 C.L. ANDERSON RD. STA. 15+01.28
 4 88°02'17"

STA. 13+93.86 BRIDGE END
 BRIDGE NO. 03671 ANDERSON ROAD
 EXISTING 212.28' COMP. I-BEAM UNIT
 24'-0" CLEAR ROADWAY
 STA. 16+06.14 BRIDGE END
 REHABILITATE BRIDGE DECK-
 HYDRODEMOLITION



STA. 1317+03 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 86' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

STA. 1322+03 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 86' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

STA. 1334+65 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 84' R.C. PIPE INLET
 & 24" X 86' R.C. PIPE OUTLET
 W/ HDWLS. LT. & RT.
 RETAIN

STA. 1340+04 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 86' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

STA. 1344+64 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 4'-6" X H=5'-6" WITH
 36" X 86' R.C. PIPE INLET
 & 36" X 90' R.C. PIPE OUTLET
 W/ HDWLS. LT. & RT.
 RETAIN

I-40

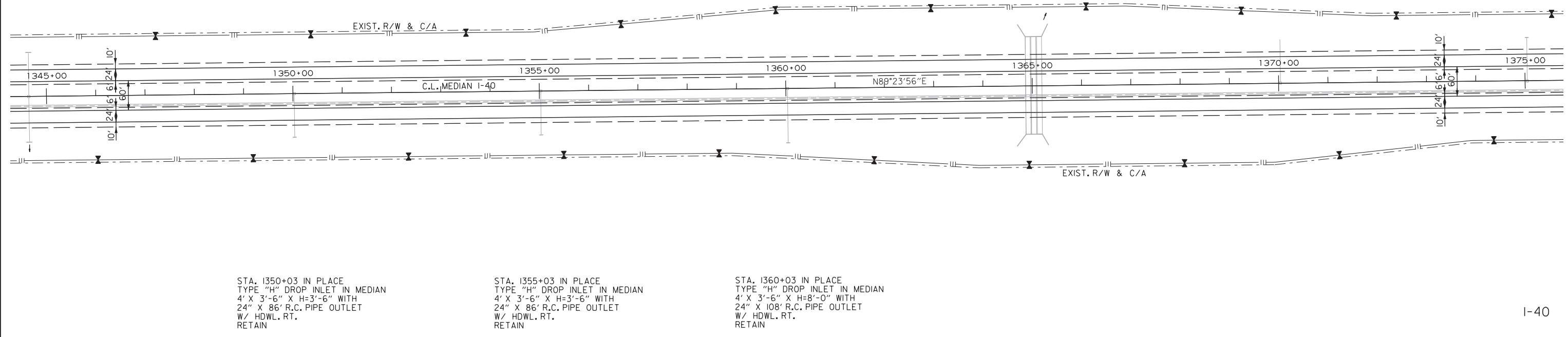


STA. 1365+03 IN PLACE
 TRIPLE 10' X 8' X 198' R.C. BOX CULVERT
 TYPE "K" DROP INLET IN MEDIAN
 4' X 3'-0" X H=4'-6" 2-OPENINGS
 RETAIN

STA. 1370+04 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=11'-0" WITH
 24" X 86' R.C. PIPE OUTLET
 RETAIN



DIGITALLY SIGNED 8/29/2022



STA. 1350+03 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 86' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

STA. 1355+03 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 86' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

STA. 1360+03 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=8'-0" WITH
 24" X 108' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

I-40

S:\adonaldson 8/26/2022 10:01:17 PM
 WORKSPACE: AHTD
 L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P14.dgn
 REVISED DATE:

STA. 1375+05 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. LT.
RETAIN

STA. 1381+04 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. LT.
RETAIN

STA. 1386+06 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=9'-0" WITH
24" X 106' R.C. PIPE OUTLET
W/ HDWL. LT.
RETAIN

STA. 1390+55 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=11'-6" WITH
24" X 116' R.C. PIPE OUTLET
W/ HDWL. LT.
RETAIN

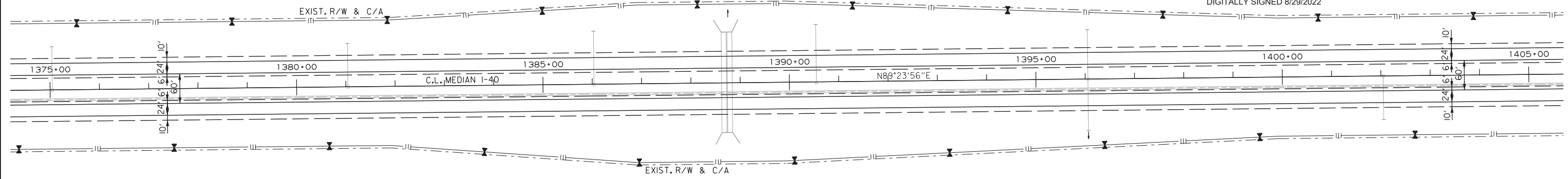
STA. 1396+06 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=6'-0" WITH
24" X 98' R.C. PIPE INLET
& 24" X 102' R.C. PIPE OUTLET
W/ HDWLS. LT. & RT.
RETAIN

STA. 1388+73 IN PLACE
DBL. 10' X 8' X 204' R.C.
BOX CULVERT
RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		42	63
				JOB NO.	061687		PLAN STA. 1375+00 TO STA. 1435+00	

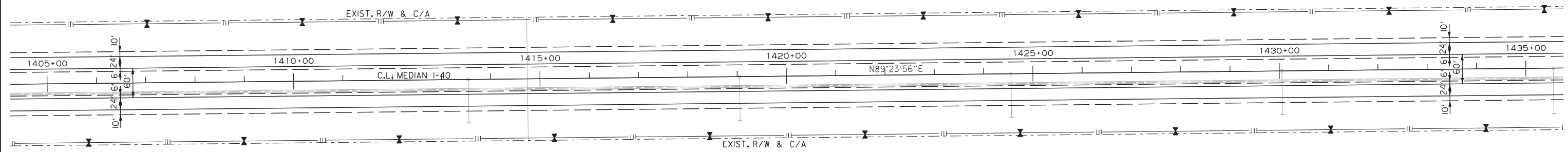


DIGITALLY SIGNED 8/29/2022



STA. 1402+05 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

I-40



STA. 1413+55 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1414+75 IN PLACE
18" X 250' PIPE SIPHON
RETAIN

STA. 1419+05 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1424+56 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1430+05 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=3'-6" WITH
24" X 86' R.C. PIPE OUTLET
W/ HDWL. RT.
RETAIN

I-40

S:\Donaldson 8/26/2022 10:08 PM
 WORKSPACE: AHTD
 L:\2017\107580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P15.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		43	63
				JOB NO.	061687			
				PLAN STA. 1435+00 TO STA. 1495+00				



STA. 1463+06 IN PLACE
 DBL. 8' X 4' X 253' R.C. BOX CULVERT
 TYPE "K" DROP INLET IN MEDIAN
 4' X 4' X H=5'-6" 2-OPENINGS &
 TYPE "K" DROP INLET 102' LT.
 4' X 4' X H=1'-9" 2-OPENINGS
 RETAIN

C.L. MEDIAN I-40
 PI = 1439+06.67
 Δ = 03°01'27" LT.
 D = 00°30'00"
 T = 302.48'
 L = 604.82'
 PC = 1436+04.19
 PT = 1442+09.01
 e = NO SUPER

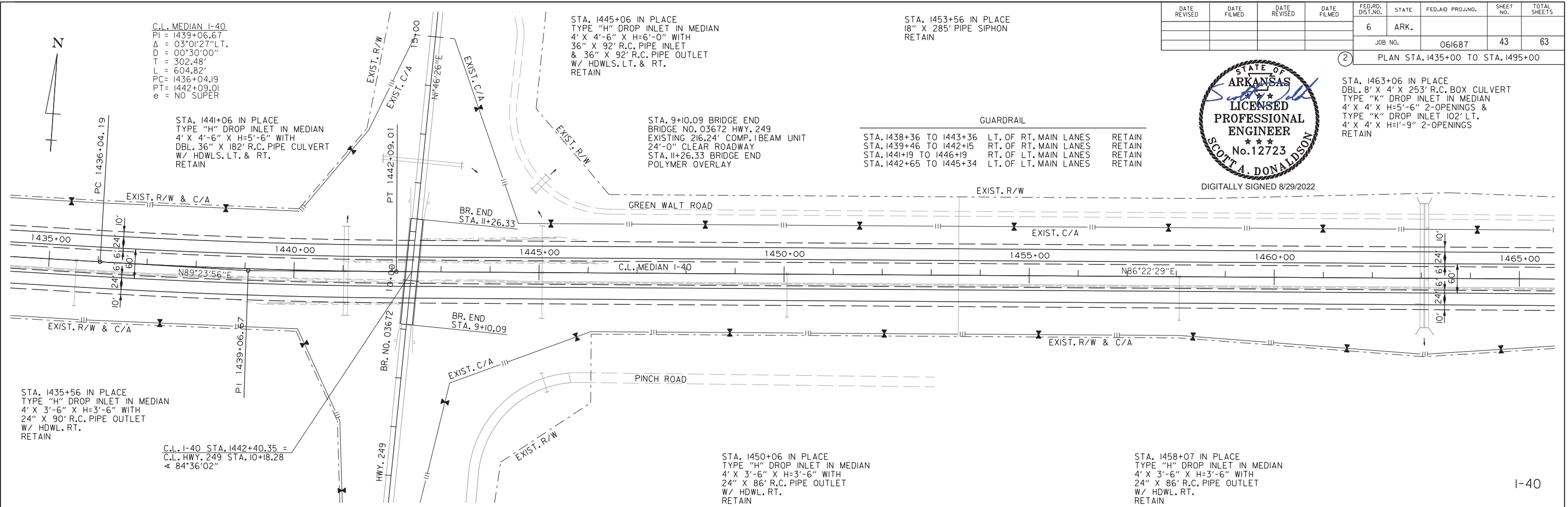
STA. 1445+06 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 4'-6" X H=6'-0" WITH
 36" X 92' R.C. PIPE INLET
 & 36" X 92' R.C. PIPE OUTLET
 W/ HDWLS. LT. & RT.
 RETAIN

STA. 1453+56 IN PLACE
 18" X 285' PIPE SIPHON
 RETAIN

STA. 1441+06 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 4'-6" X H=5'-6" WITH
 DBL. 36" X 182' R.C. PIPE CULVERT
 W/ HDWLS. LT. & RT.
 RETAIN

STA. 9+10.09 BRIDGE END
 BRIDGE NO. 03672 HWY. 249
 EXISTING 216.24' COMP. I-BEAM UNIT
 24'-0" CLEAR ROADWAY
 STA. 11+26.33 BRIDGE END
 POLYMER OVERLAY

	GUARDRAIL	
STA. 1438+36 TO 1443+36	LT. OF RT. MAIN LANES	RETAIN
STA. 1439+46 TO 1442+15	RT. OF RT. MAIN LANES	RETAIN
STA. 1441+19 TO 1446+19	RT. OF LT. MAIN LANES	RETAIN
STA. 1442+65 TO 1445+34	LT. OF LT. MAIN LANES	RETAIN



STA. 1435+56 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 90' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

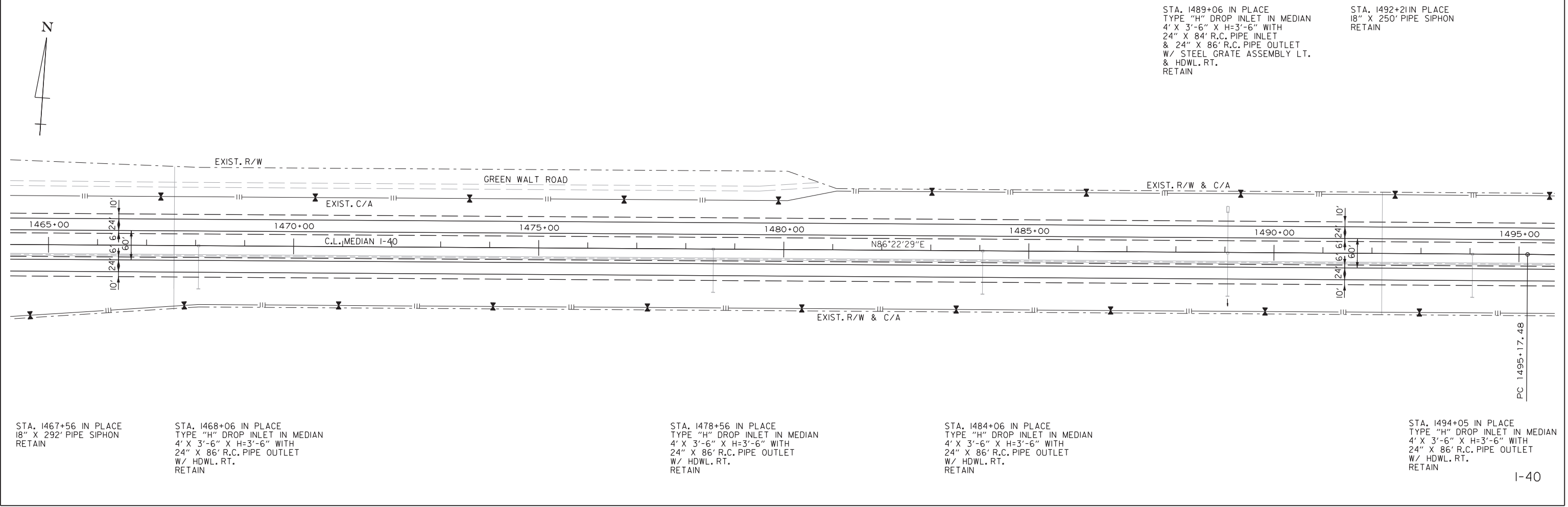
C.L. I-40 STA. 1442+40.35 =
 C.L. HWY. 249 STA. 10+18.28
 4 84'36"02"

STA. 1450+06 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 86' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

STA. 1458+07 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 86' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

I-40

S:\adonaldson 8/26/2022 10:09 PM
 WORKSPACE: AHTD
 L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P16.dgn
 REVISED DATE:



STA. 1467+56 IN PLACE
 18" X 292' PIPE SIPHON
 RETAIN

STA. 1468+06 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 86' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

STA. 1478+56 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 86' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

STA. 1484+06 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 86' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

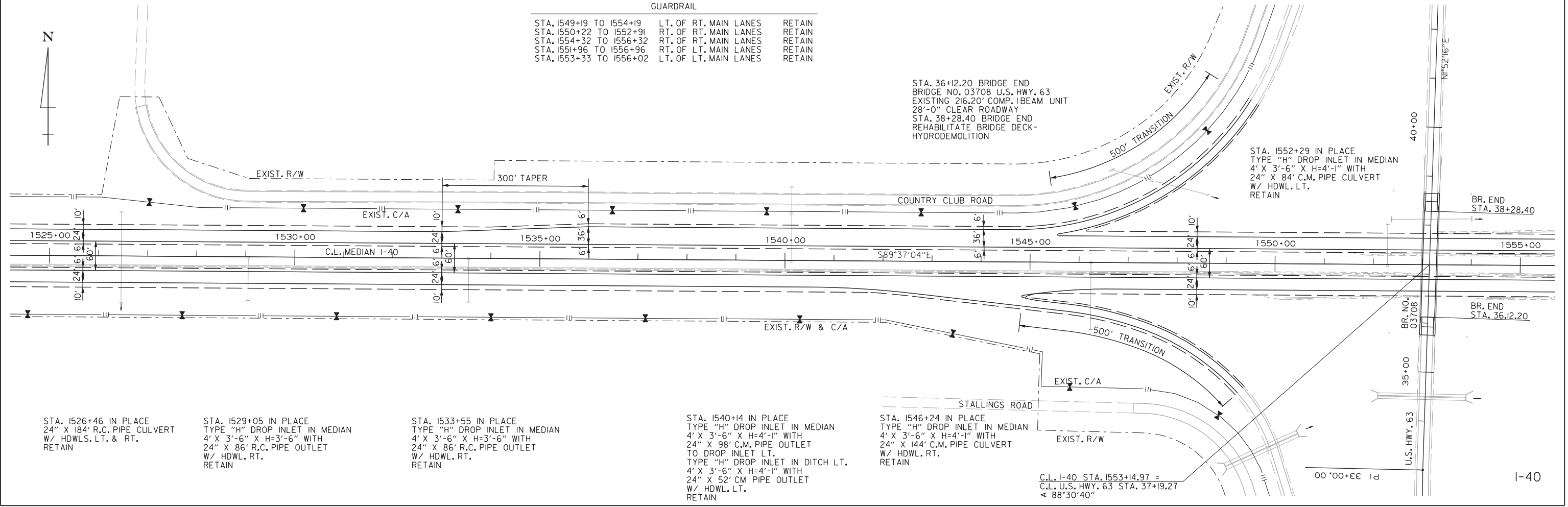
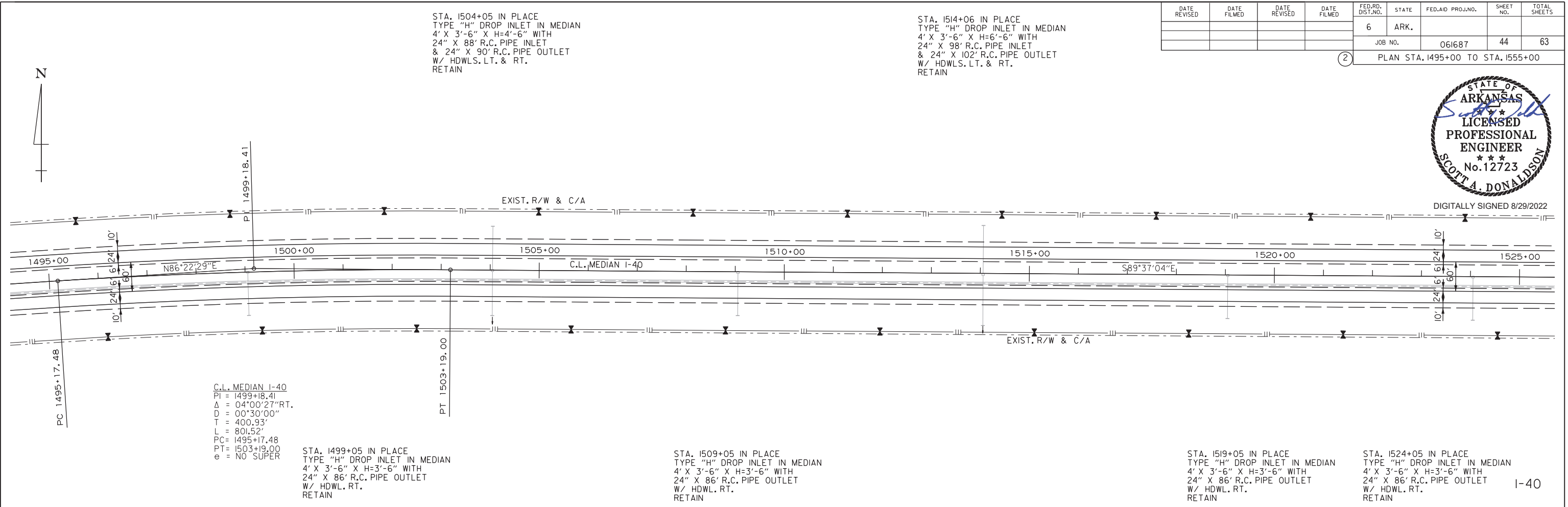
STA. 1494+05 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=3'-6" WITH
 24" X 86' R.C. PIPE OUTLET
 W/ HDWL. RT.
 RETAIN

I-40

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		44	63
				JOB NO.	061687			
				PLAN STA. 1495+00 TO STA. 1555+00				



DIGITALLY SIGNED 8/29/2022



S:\adonaldson 8/26/2022 10:22 PM
 WORKSPACE: AHTD
 L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_140_P17.dgn
 REVISED DATE:

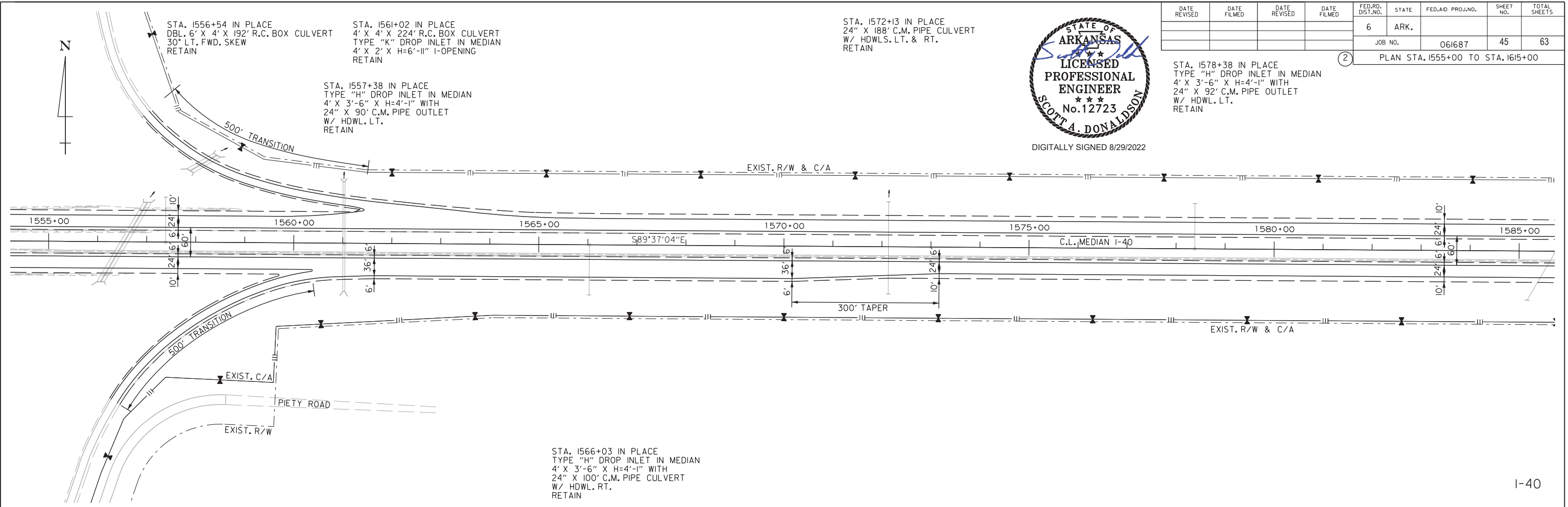
I-40

I-40

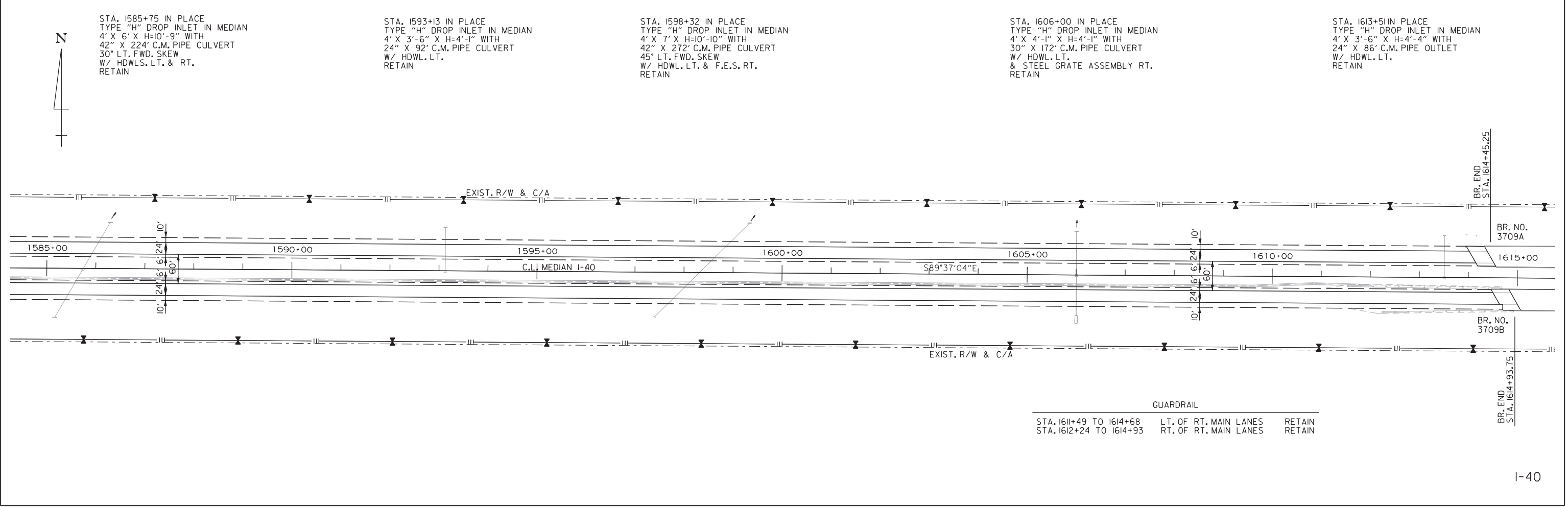
DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		45	63
				JOB NO.	061687		PLAN STA. 1555+00 TO STA. 1615+00	



STA. 1578+38 IN PLACE
 TYPE "H" DROP INLET IN MEDIAN
 4' X 3'-6" X H=4'-1" WITH
 24" X 92' C.M. PIPE OUTLET
 W/ HDWL. LT.
 RETAIN



I-40



I-40

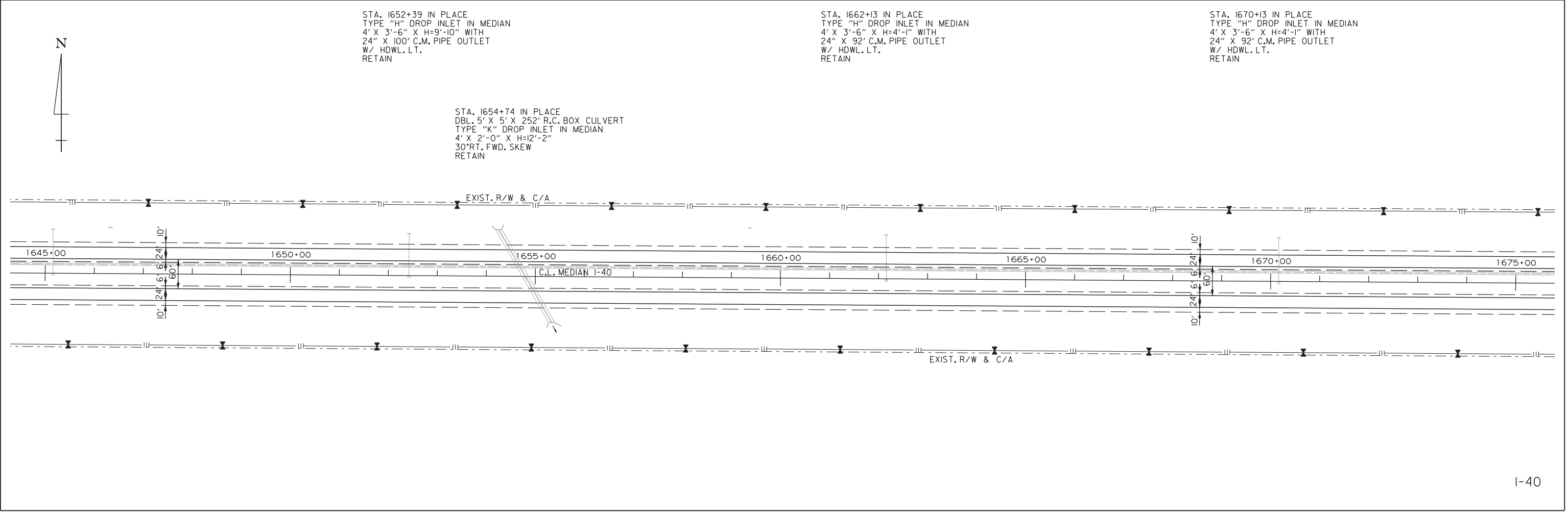
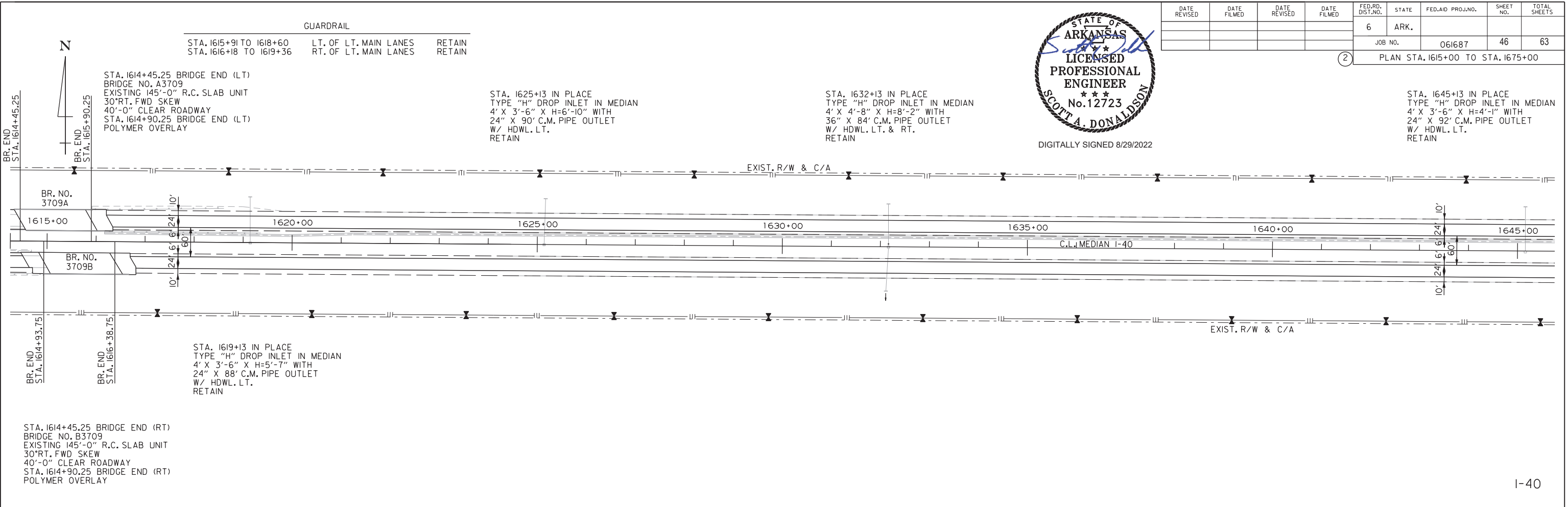
S.A. Donaldson
 WORKSPACE: AHTD
 L:\2017\07580 - BB0608 Hwy. 31 - Prairie Co Line Drawings\061687_I40_P18.dgn
 REVISED DATE:

GUARDRAIL
 STA. 1611+49 TO 1614+68 LT. OF RT. MAIN LANES RETAIN
 STA. 1612+24 TO 1614+93 RT. OF RT. MAIN LANES RETAIN

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.	061687	46	63	
				PLAN STA. 1615+00 TO STA. 1675+00				



DIGITALLY SIGNED 8/29/2022



8/26/2022 10:24 PM
 S.A. Donaldson
 WORKSPACE: AHTD
 L:\2017\107580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P19.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		47	63
				JOB NO.		061687	PLAN STA. 1675+00 TO STA. 1735+00	



DIGITALLY SIGNED 8/29/2022

C.L. MEDIAN I-40
 PI = 1689+97.22
 Δ = 21°32'00"LT.
 D = 01°00'00"
 T = 1089.52'
 L = 2153.33'
 PC = 1679+07.70
 PT = 1700+61.03
 e = MATCH EXISTING
 Ls = MATCH EXISTING

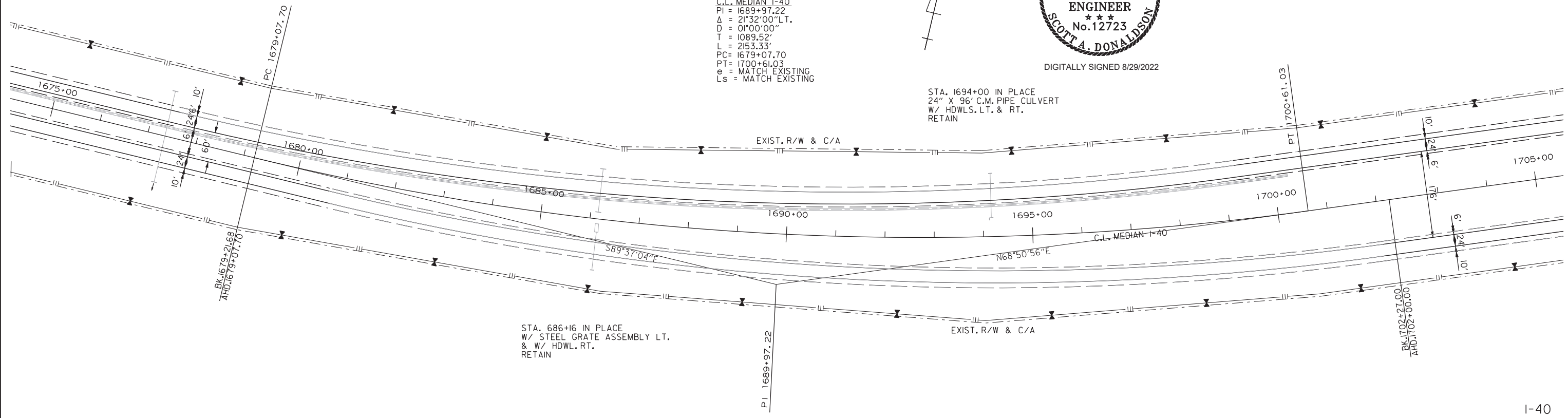


STA. 1677+35 IN PLACE
 24" X 190' C.M. PIPE CULVERT
 W/ HDWLS. LT. & RT.
 RETAIN

STA. 1686+15 IN PLACE
 36" X 84' C.M. PIPE CULVERT
 W/ HDWLS. LT. & RT.
 RETAIN

STA. 1694+00 IN PLACE
 24" X 96' C.M. PIPE CULVERT
 W/ HDWLS. LT. & RT.
 RETAIN

STA. 686+16 IN PLACE
 W/ STEEL GRATE ASSEMBLY LT.
 & W/ HDWL. RT.
 RETAIN



I-40

C.L. I-40 STA. 1716+43.95 =
 C.L. FIRE TOWER RD. STA. 33+69.70
 N68°50'56"

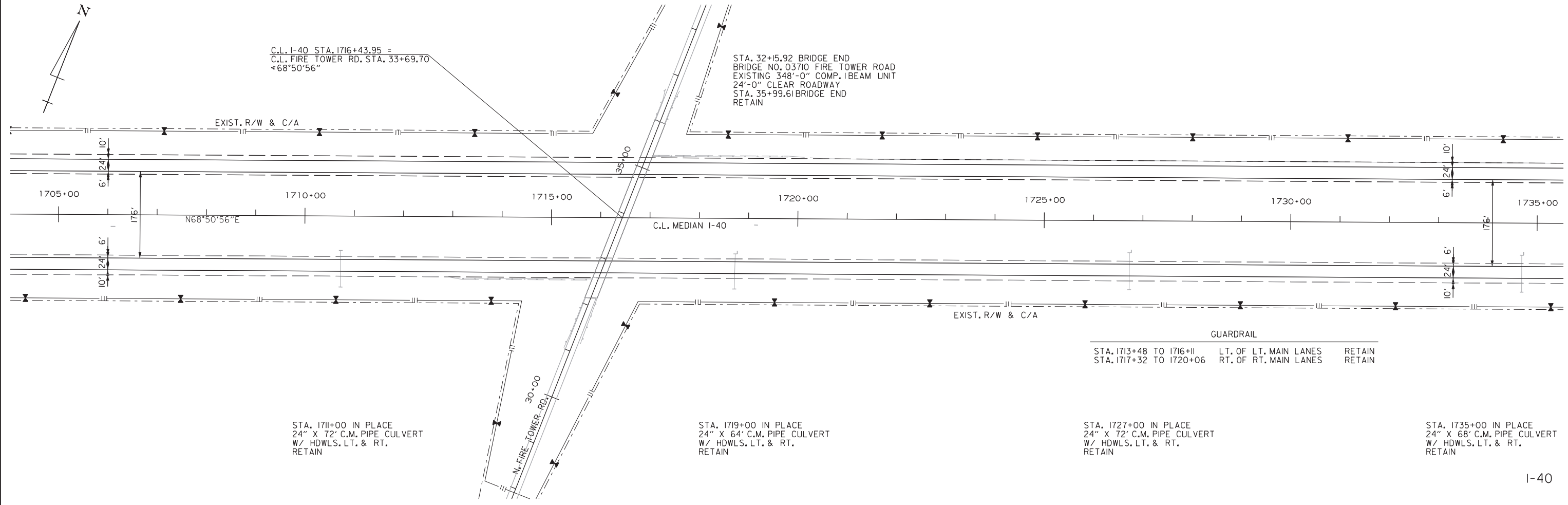
STA. 32+15.92 BRIDGE END
 BRIDGE NO. 03710 FIRE TOWER ROAD
 EXISTING 348'-0" COMP. I-BEAM UNIT
 24'-0" CLEAR ROADWAY
 STA. 35+99.61 BRIDGE END
 RETAIN

STA. 1711+00 IN PLACE
 24" X 72' C.M. PIPE CULVERT
 W/ HDWLS. LT. & RT.
 RETAIN

STA. 1719+00 IN PLACE
 24" X 64' C.M. PIPE CULVERT
 W/ HDWLS. LT. & RT.
 RETAIN

STA. 1727+00 IN PLACE
 24" X 72' C.M. PIPE CULVERT
 W/ HDWLS. LT. & RT.
 RETAIN

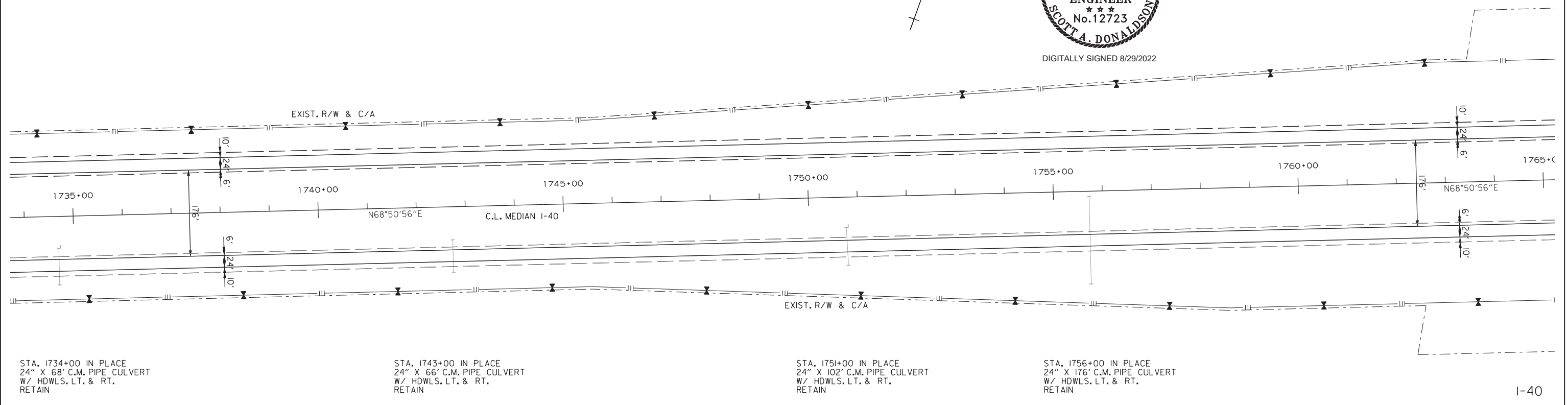
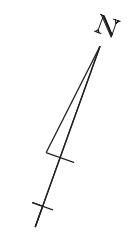
STA. 1735+00 IN PLACE
 24" X 68' C.M. PIPE CULVERT
 W/ HDWLS. LT. & RT.
 RETAIN



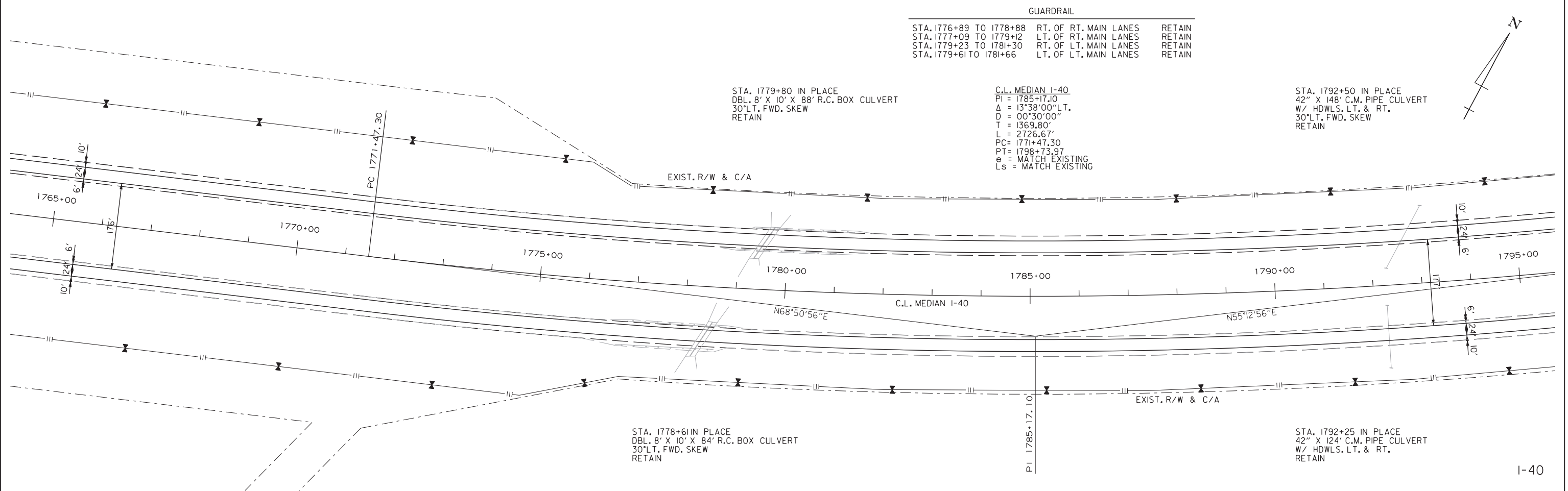
I-40

8/26/2022 10:28 PM
 S:\adonaldson\WORKSPACE\AHTD\170717580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_I40_P20.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		48	63
				JOB NO.		061687	PLAN STA. 1735+00 TO STA. 1795+00	



GUARDRAIL		
STA. 1776+89 TO 1778+88	RT. OF RT. MAIN LANES	RETAIN
STA. 1777+09 TO 1779+12	LT. OF RT. MAIN LANES	RETAIN
STA. 1779+23 TO 1781+30	RT. OF LT. MAIN LANES	RETAIN
STA. 1779+61 TO 1781+66	LT. OF LT. MAIN LANES	RETAIN



S.A. Donaldson
 8/26/2022 10:31 PM
 WORKSPACE: AHTD
 L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_I40_P21.dgn
 REVISED DATE:

I-40

I-40

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		49	63
				JOB NO.		061687	PLAN STA. 1795+00 TO STA. 1855+00	



DIGITALLY SIGNED 8/29/2022

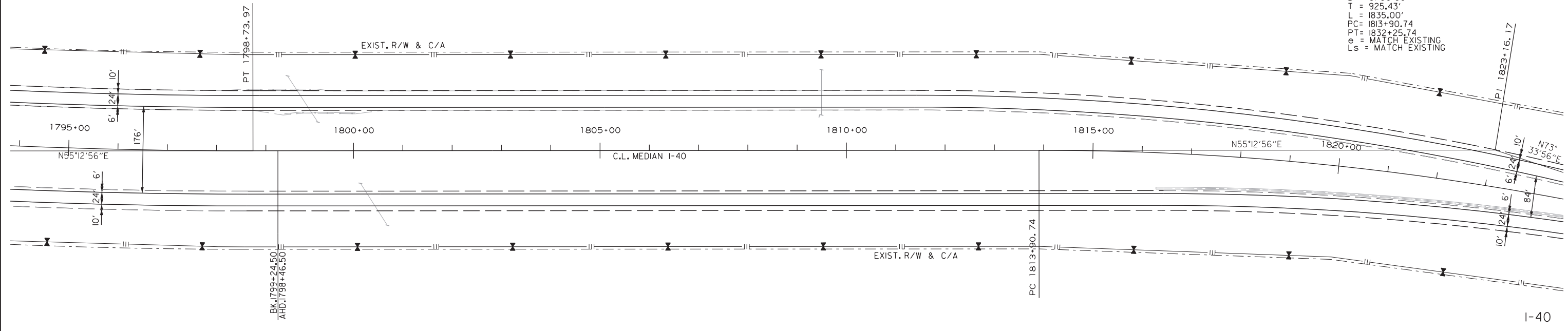
STA. 1799+47 IN PLACE
24" X 124" R.C. BOX CULVERT
45° RT. FWD. SKEW
RETAIN

GUARDRAIL
STA. 1798+79 TO 1779+79 LT. OF LT. MAIN LANES RETAIN
STA. 1799+10 TO 1800+54 RT. OF LT. MAIN LANES RETAIN

STA. 1800+92 IN PLACE
24" X 132" R.C. BOX CULVERT
45° RT. FWD. SKEW
RETAIN

STA. 1810+00 IN PLACE
24" X 92" BOX CULVERT
W/ HDWL. LT.
RETAIN

C.L. MEDIAN I-40
PI = 1823+16.17
Δ = 18°21'00" RT.
D = 01°00'00"
T = 925.43'
L = 1835.00'
PC = 1813+90.74
PT = 1832+25.74
e = MATCH EXISTING
Ls = MATCH EXISTING



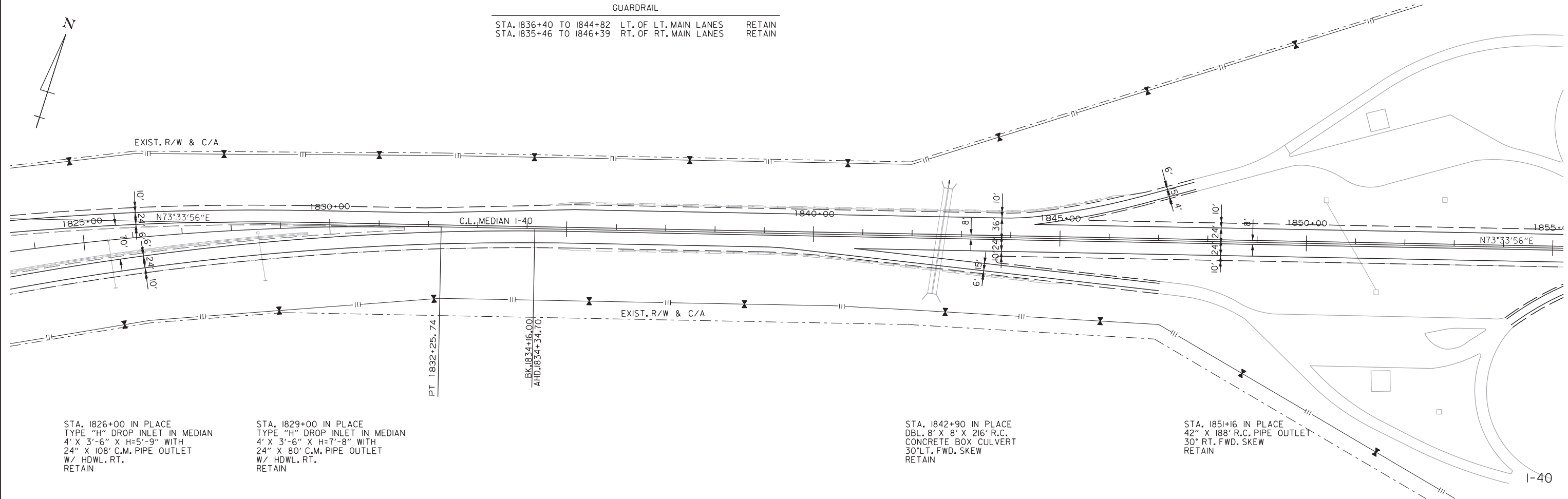
GUARDRAIL
STA. 1836+40 TO 1844+82 LT. OF LT. MAIN LANES RETAIN
STA. 1835+46 TO 1846+39 RT. OF RT. MAIN LANES RETAIN

STA. 1826+00 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=5'-9" WITH
24" X 108' C.M. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1829+00 IN PLACE
TYPE "H" DROP INLET IN MEDIAN
4' X 3'-6" X H=7'-8" WITH
24" X 80' C.M. PIPE OUTLET
W/ HDWL. RT.
RETAIN

STA. 1842+90 IN PLACE
DBL. 8' X 8' X 216" R.C.
CONCRETE BOX CULVERT
30° LT. FWD. SKEW
RETAIN

STA. 1851+16 IN PLACE
42" X 188" R.C. PIPE OUTLET
30° RT. FWD. SKEW
RETAIN



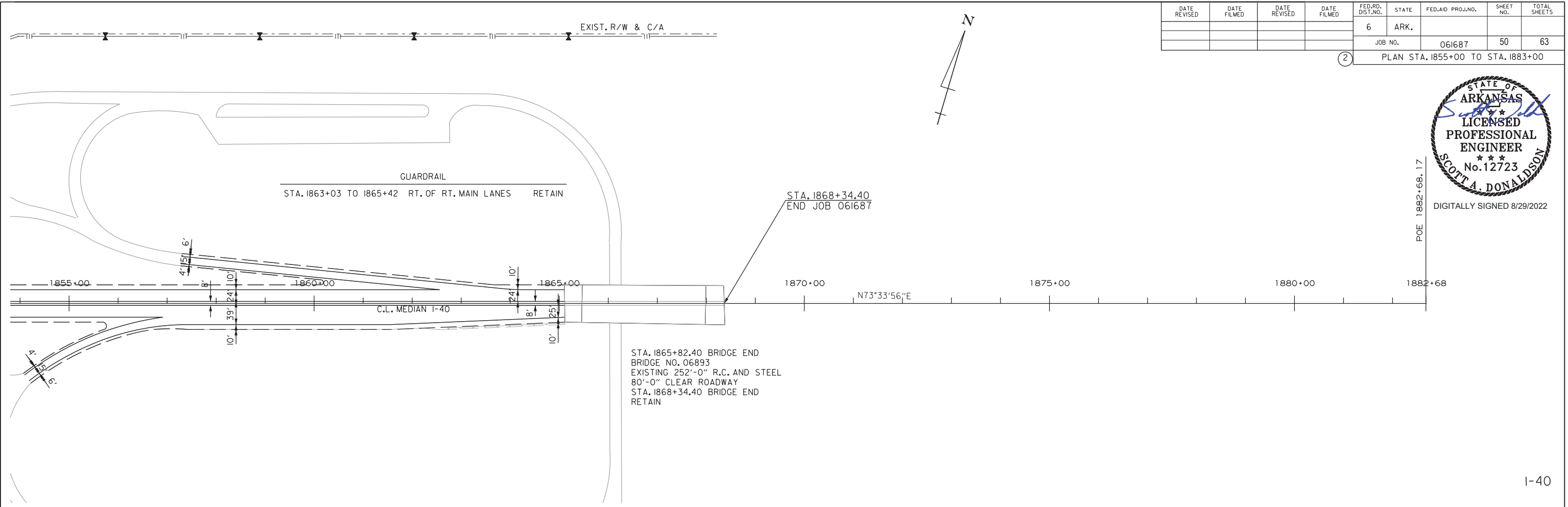
S:\adonaldson\8/26/2022 10:35 PM
 WORKSPACE: AHTD
 L:\2017\07580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P22.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.		061687	50	63
				PLAN STA. 1855+00 TO STA. 1883+00				

2



DIGITALLY SIGNED 8/29/2022



STA. 1865+82.40 BRIDGE END
 BRIDGE NO. 06893
 EXISTING 252'-0" R.C. AND STEEL
 80'-0" CLEAR ROADWAY
 STA. 1868+34.40 BRIDGE END
 RETAIN

POE 1882+68.17

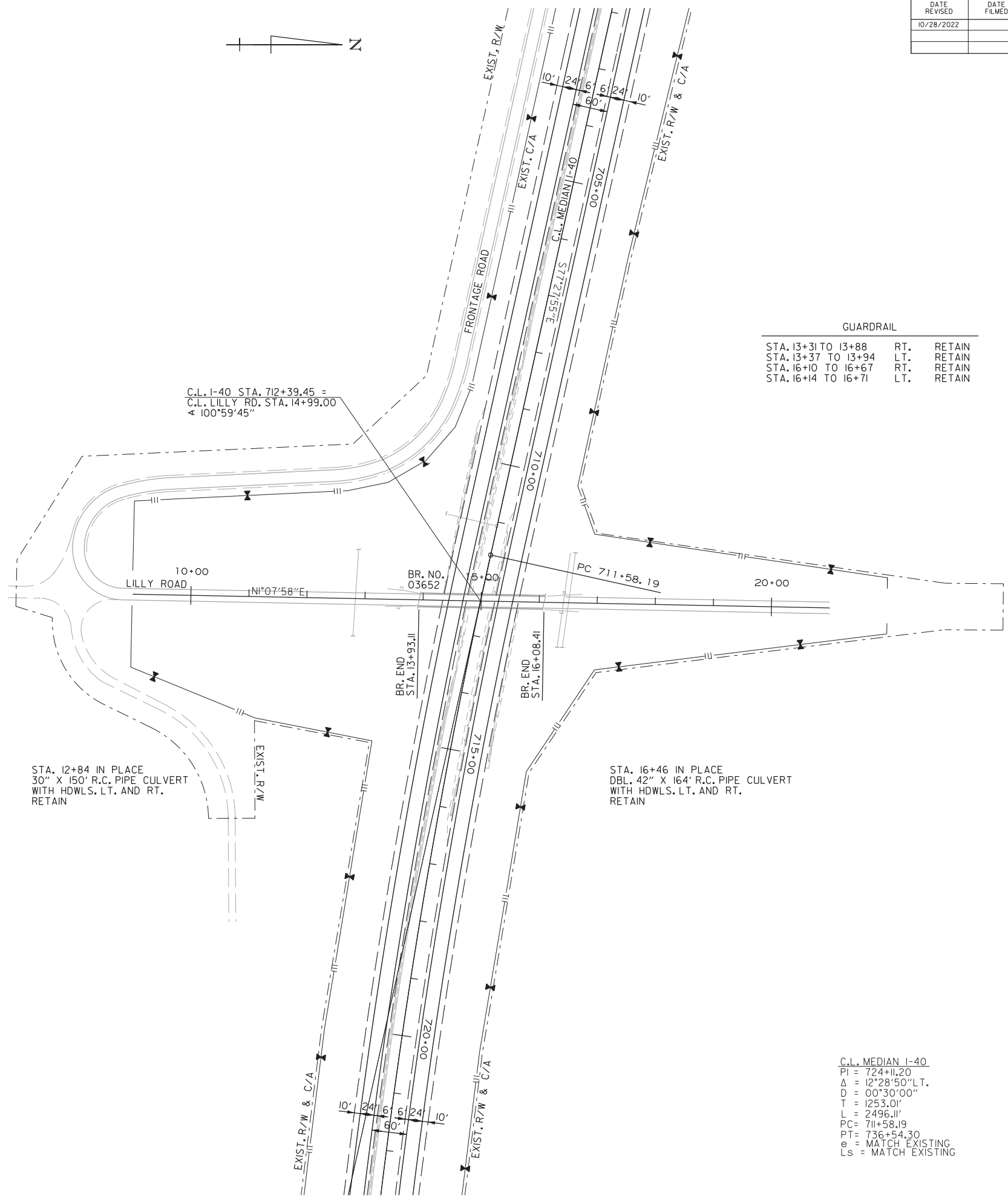
S:\adonaldson 8/26/2022 10:35 PM
 WORKSPACE: AHTD
 L:\2017\107580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_140_P23.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.		061687	51	63
				PLAN STA. 9+00 - STA. 21+00				

2



DIGITALLY SIGNED 10/28/2022



GUARDRAIL		
STA. 13+31 TO 13+88	RT.	RETAIN
STA. 13+37 TO 13+94	LT.	RETAIN
STA. 16+10 TO 16+67	RT.	RETAIN
STA. 16+14 TO 16+71	LT.	RETAIN

STA. 13+93.11 BRIDGE END
BRIDGE NO. 03652 LILLY ROAD
EXISTING 215.30' COMP. I-BEAM SPANS
20'-0" CLEAR ROADWAY
STA. 16+08.41 BRIDGE END
RETAIN

STA. 12+84 IN PLACE
30" X 150' R.C. PIPE CULVERT
WITH HDWLS. LT. AND RT.
RETAIN

STA. 16+46 IN PLACE
DBL. 42" X 164' R.C. PIPE CULVERT
WITH HDWLS. LT. AND RT.
RETAIN

C.L. MEDIAN I-40
PI = 724+11.20
Δ = 12°28'50" LT.
D = 00°30'00"
T = 1253.01'
L = 2496.11'
PC = 711+58.19
PT = 736+54.30
e = MATCH EXISTING
Ls = MATCH EXISTING

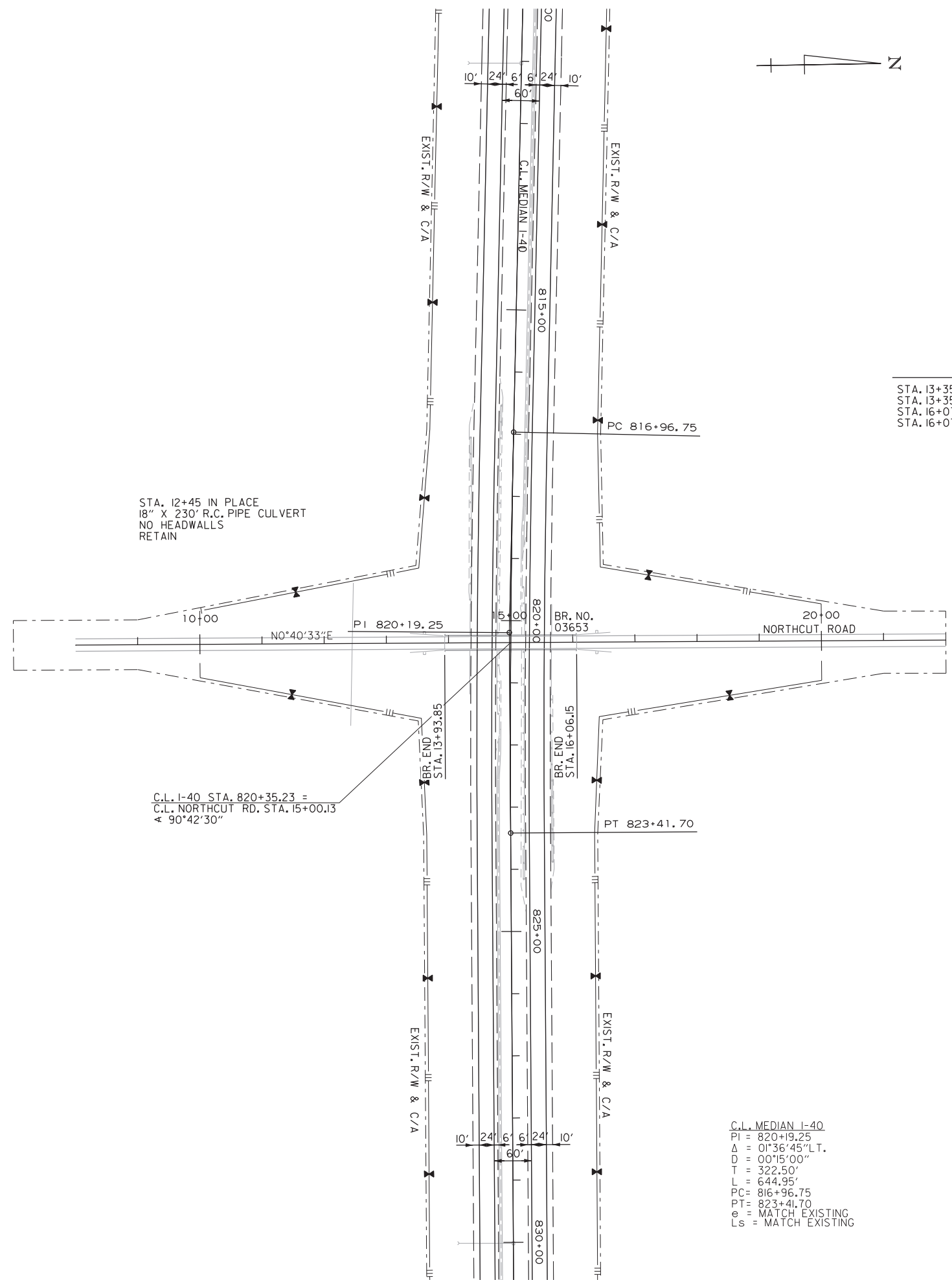
LILLY ROAD

S:\Donaldson 10/24/2022 3:51:04 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy. 31 - Prairie Co Line\Drawings\061687.LIL.L01.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.		061687	52	63
				PLAN STA. 8+00 - STA. 22+00				



DIGITALLY SIGNED 10/28/2022



GUARDRAIL			
STA. 13+35 TO 13+92	LT.	RT.	RETAIN
STA. 16+07 TO 16+64	LT.	RT.	RETAIN

STA. 12+45 IN PLACE
18" X 230' R.C. PIPE CULVERT
NO HEADWALLS
RETAIN

STA. 13+93.85 BRIDGE END
BRIDGE NO. 03653 NORTH CUT ROAD
EXISTING 212.30' COMP. I-BEAM SPANS
20'-0" CLEAR ROADWAY
STA. 16+06.15 BRIDGE END
RETAIN

C.L. I-40 STA. 820+35.23 =
C.L. NORTH CUT RD. STA. 15+00.13
← 90°42'30"

C.L. MEDIAN I-40
PI = 820+19.25
Δ = 01°36'45" L.T.
D = 00°15'00"
T = 322.50'
L = 644.95'
PC = 816+96.75
PT = 823+41.70
e = MATCH EXISTING
Ls = MATCH EXISTING

NORTH CUT ROAD

S:\adonaldson 10/24/2022 3:51:04 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy. 31 - Prairie Co Line Drawings\061687_NOR_F01.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.	061687		53	63
				PLAN STA. 8+00 - STA. 22+00				

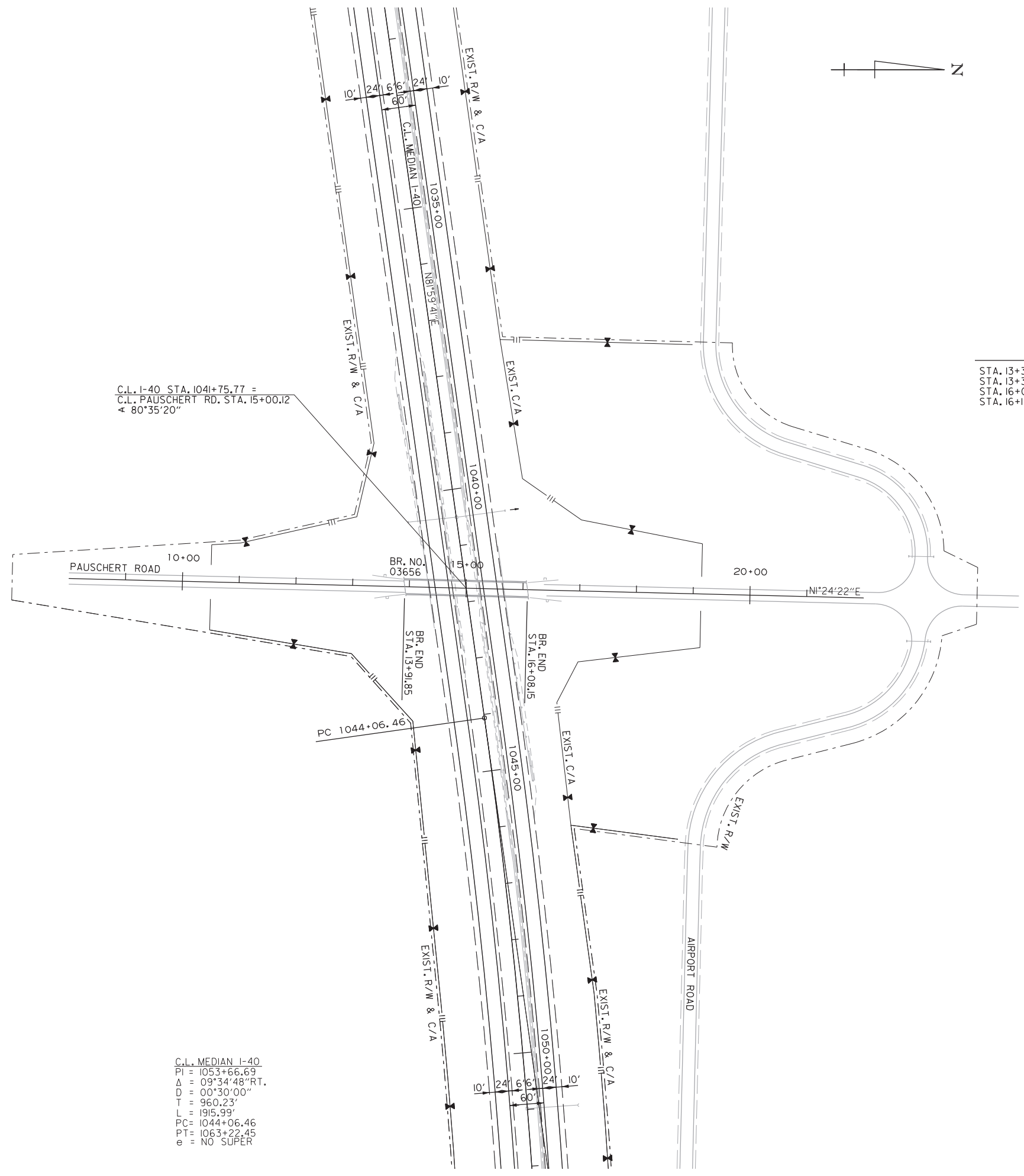
2



DIGITALLY SIGNED 10/28/2022



GUARDRAIL		
STA. 13+32 TO 13+89	LT.	RETAIN
STA. 13+36 TO 13+93	RT.	RETAIN
STA. 16+08 TO 16+65	LT.	RETAIN
STA. 16+13 TO 16+70	RT.	RETAIN



C.L. I-40 STA. 1041+75.77 =
 C.L. PAUSCHERT RD. STA. 15+00.12
 < 80°35'20"

STA. 13+91.85 BRIDGE END
 BRIDGE NO. 03656 PAUSCHERT ROAD
 EXISTING 216.30' COMP. I-BEAM SPANS
 24'-0" CLEAR ROADWAY
 STA. 16+08.15 BRIDGE END
 RETAIN

C.L. MEDIAN I-40
 PI = 1053+66.69
 Δ = 09°34'48" RT.
 D = 00°30'00"
 T = 960.23'
 L = 1915.99'
 PC = 1044+06.46
 PT = 1063+22.45
 e = NO SUPER

PAUSCHERT ROAD

S:\Donaldson 10/24/2022 3:51:05 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_PAS_P01.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.		061687	54	63
				PLAN STA. 7+00 TO STA. 23+00				

2



DIGITALLY SIGNED 10/28/2022



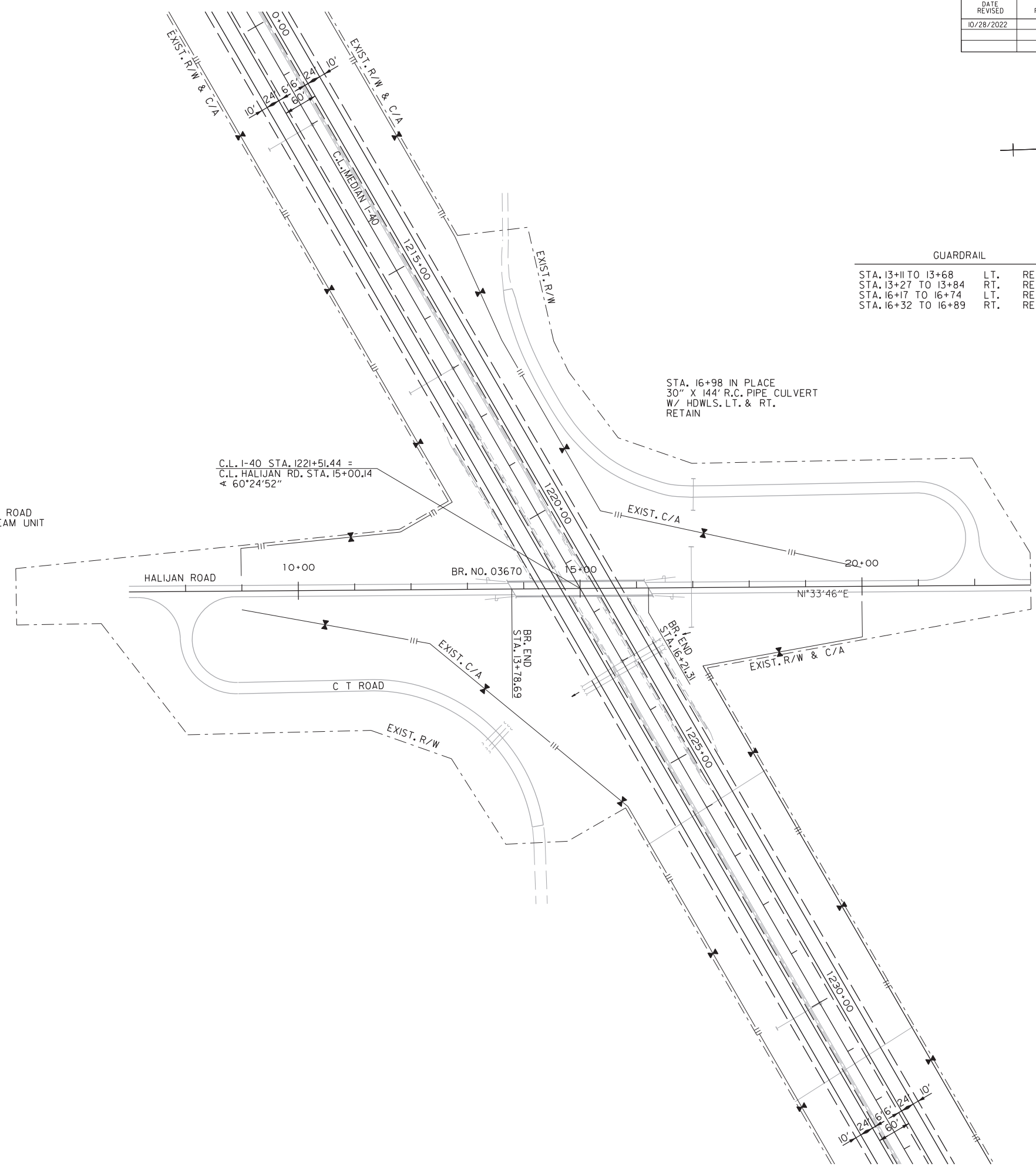
GUARDRAIL

STA. 13+11 TO 13+68	LT.	RETAIN
STA. 13+27 TO 13+84	RT.	RETAIN
STA. 16+17 TO 16+74	LT.	RETAIN
STA. 16+32 TO 16+89	RT.	RETAIN

STA. 16+98 IN PLACE
30" X 144' R.C. PIPE CULVERT
W/ HDWLS. LT. & RT.
RETAIN

C.L. I-40 STA. 1221+51.44 =
C.L. HALIJAN RD. STA. 15+00.14
∠ 60°24'52"

STA. 13+78.69 BRIDGE END
BRIDGE NO. 03670 HALIJAN ROAD
EXISTING 242.62' COMP. I BEAM UNIT
24'-0" CLEAR ROADWAY
STA. 16+21.31 BRIDGE END
RETAIN



HALIJAN ROAD

S:\Donaldson 10/24/2022 3:51:05 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_HAL_P01.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.		061687	55	63
				PLAN STA. 8+00 TO STA. 22+00				

2



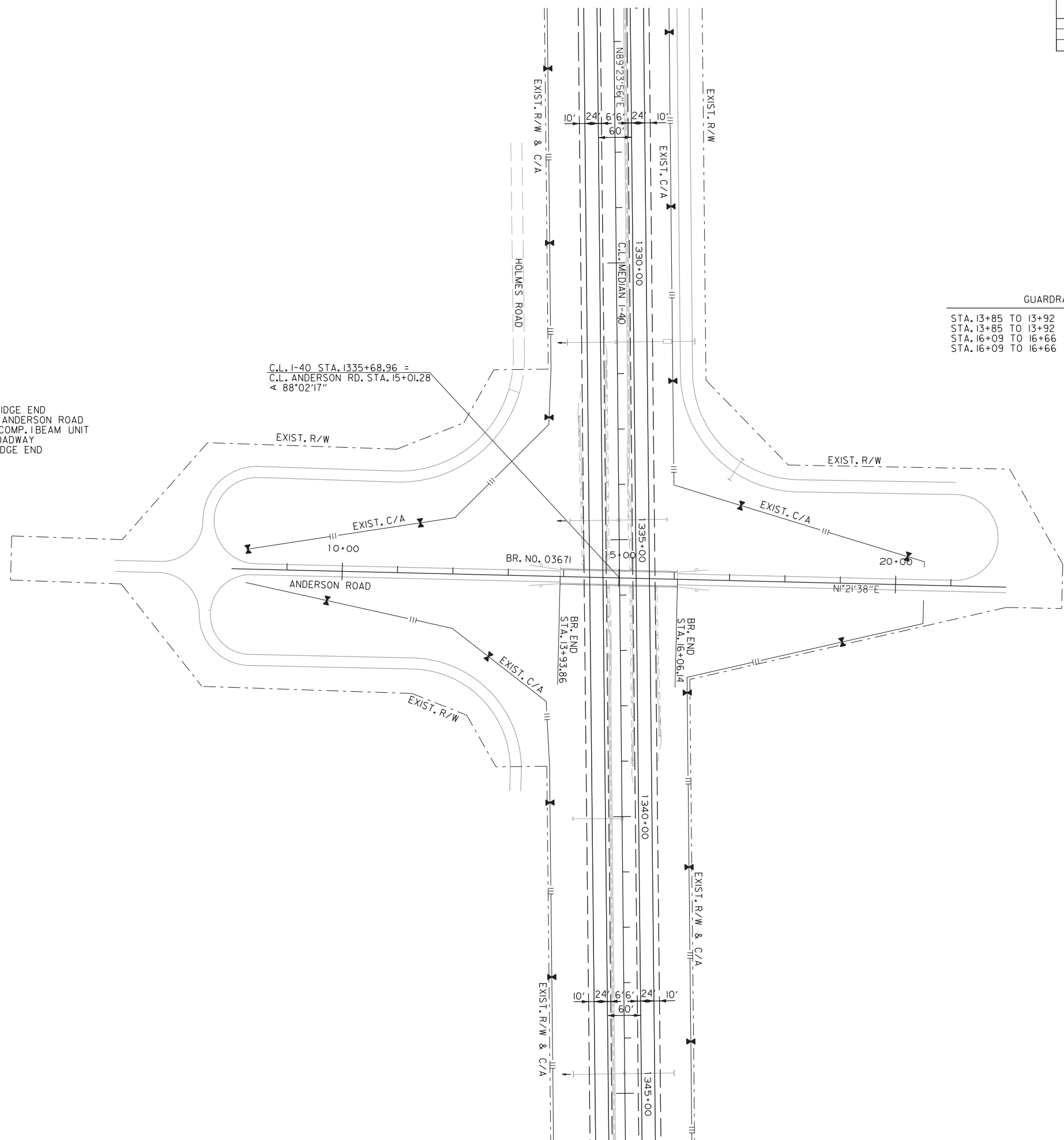
DIGITALLY SIGNED 10/28/2022



GUARDRAIL			
STA. 13+85 TO 13+92	LT.	RT.	RETAIN
STA. 13+85 TO 13+92	LT.	RT.	RETAIN
STA. 16+09 TO 16+66	LT.	RT.	RETAIN
STA. 16+09 TO 16+66	LT.	RT.	RETAIN

STA. 13+93.86 BRIDGE END
BRIDGE NO. 03671 ANDERSON ROAD
EXISTING 212.28' COMP. I-BEAM UNIT
24'-0" CLEAR ROADWAY
STA. 16+06.14 BRIDGE END
RETAIN

C.L. I-40 STA. 1335+68.96 =
C.L. ANDERSON RD. STA. 15+01.28
∠ 88°02'17"



S:\adonaldson 10/24/2022 3:51:05 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687 AND_F01.dgn
 REVISED DATE:

ANDERSON ROAD

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.		061687	56	63
				PLAN STA. 3+00 TO STA. 17+00				

2



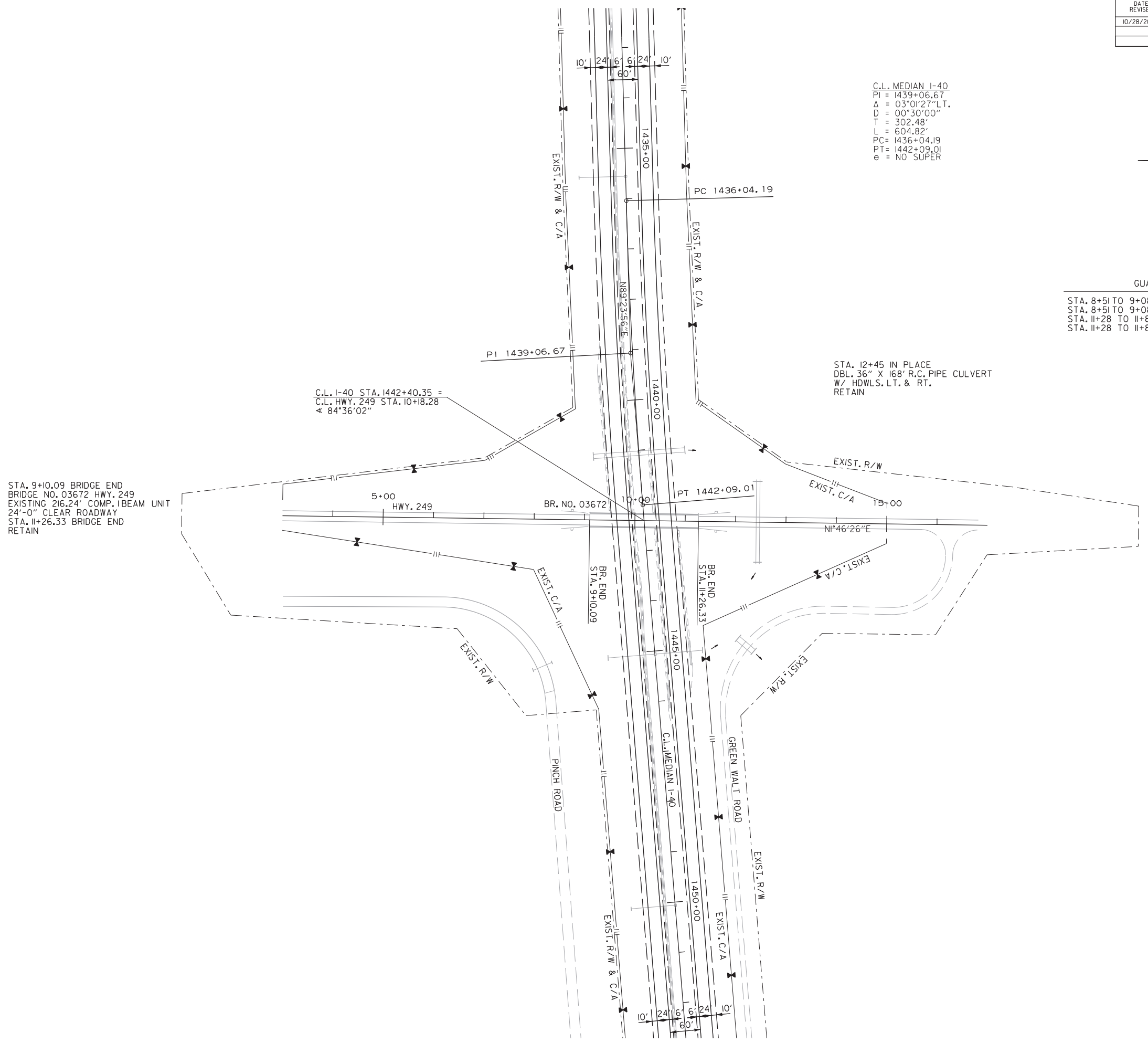
DIGITALLY SIGNED 10/28/2022

C.L. MEDIAN I-40
 PI = 1439+06.67
 $\Delta = 03^{\circ}01'27''$ L.T.
 $D = 00^{\circ}30'00''$
 $T = 302.48'$
 $L = 604.82'$
 $PC = 1436+04.19$
 $PT = 1442+09.01$
 $e = \text{NO SUPER}$



GUARDRAIL

STA. 8+51 TO 9+08	LT.	RETAIN
STA. 8+51 TO 9+08	RT.	RETAIN
STA. 11+28 TO 11+85	LT.	RETAIN
STA. 11+28 TO 11+85	RT.	RETAIN



S:\adonaldson 10/24/2022 3:51:06 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687.249_P01.dgn
 REVISED DATE:

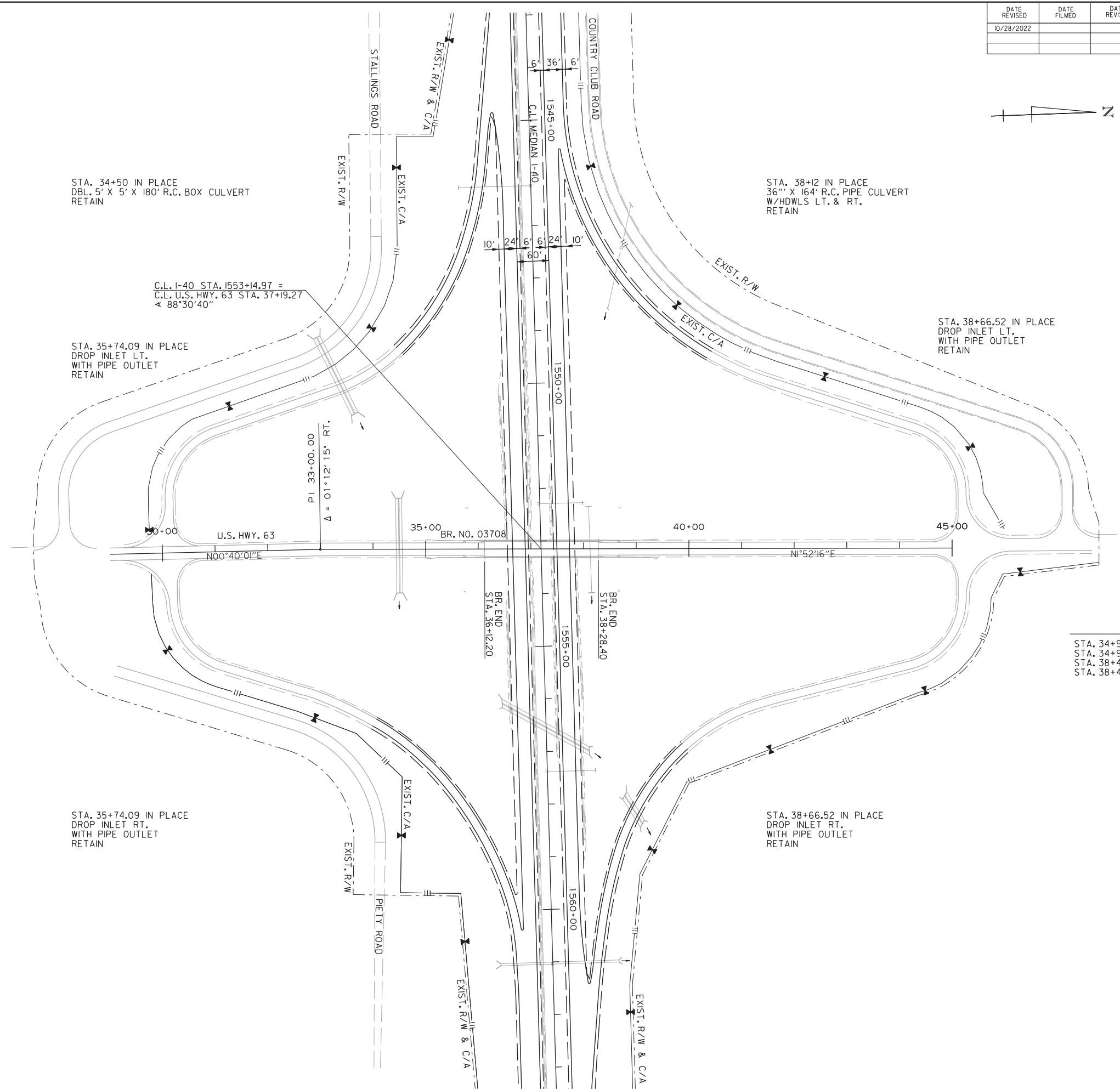
HWY. 249

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.	061687	57	63	
				PLAN STA. 29+00 TO STA. 45+00				

2



DIGITALLY SIGNED 10/28/2022



GUARDRAIL			
STA. 34+99 TO 35+93	LT.	RETAIN	
STA. 34+99 TO 35+93	RT.	RETAIN	
STA. 38+47 TO 39+40	LT.	RETAIN	
STA. 38+47 TO 39+40	RT.	RETAIN	

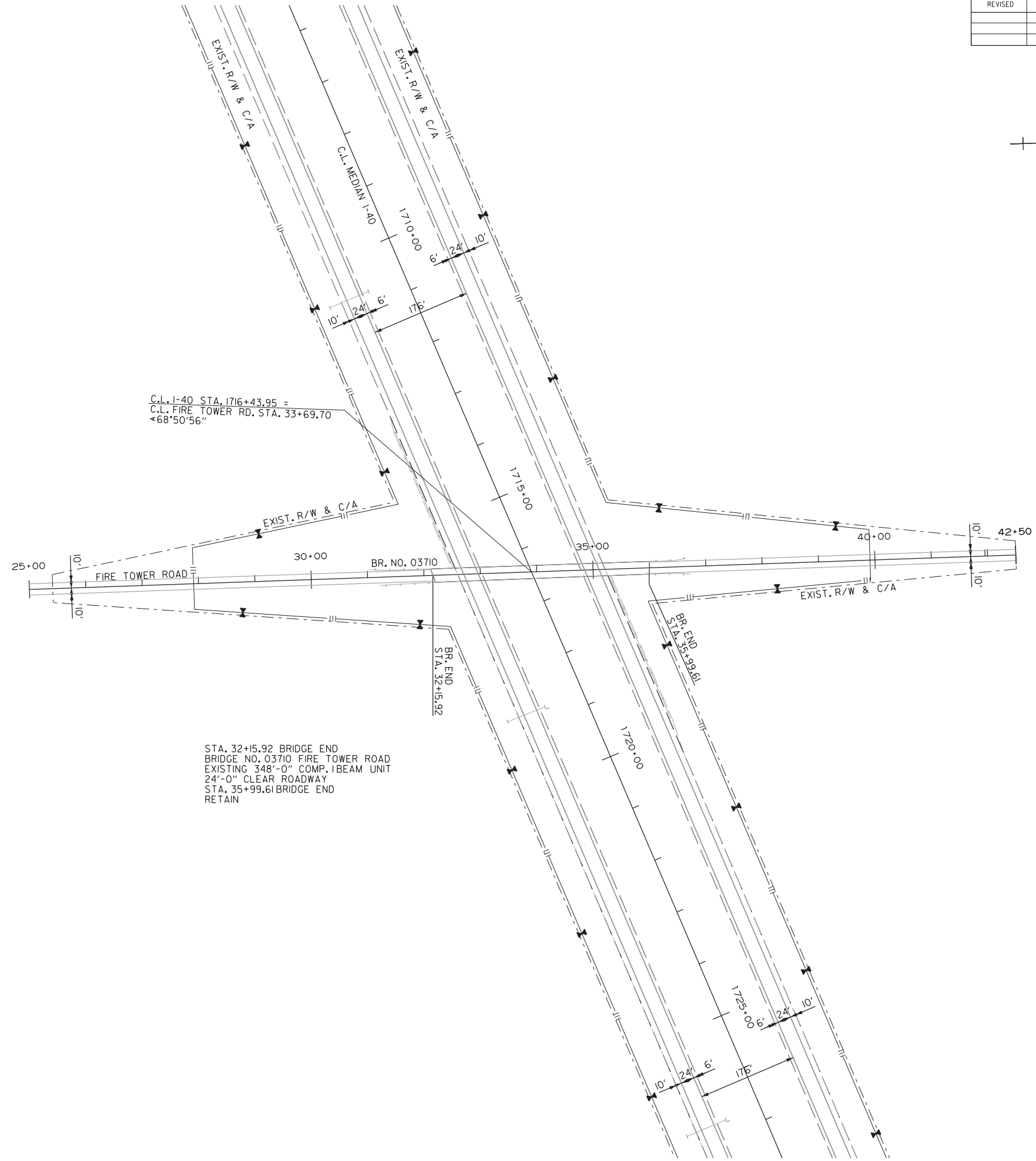
S:\adonaldson 10/24/2022 3:51:06 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\061687_US63_P01.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. RD. DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.		58	63
				JOB NO.		061687		
				PLAN STA. 7+00 TO STA. 23+00				

2



DIGITALLY SIGNED 8/29/2022



FIRE TOWER ROAD

S:\adonaldson 8/26/2022 10:41 PM
 WORKSPACE: AHTD
 L:\2017\1017580 - BB0608 Hwy 31 - Prairie Co Line Drawings\061687_TOWER_P01.dgn
 REVISED DATE:

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
						JOB NO. 061687	59	63
				① 03656	LMC OVERLAY			61772

THIS SHEET INTENTIONALLY LEFT BLANK

DETAILS OF LATEX MODIFIED
 CONCRETE OVERLAY WITH GRADE RAISE
 ROUTE SEC.
 ARKANSAS STATE HIGHWAY COMMISSION
 LITTLE ROCK, ARK.

DRAWN BY: HEW DATE: JAN. 2020 FILENAME: b061687_sl.dgn
 CHECKED BY: JHR DATE: JAN. 2020 SCALE: NO SCALE
 DESIGNED BY: BWD DATE: JAN. 2020
 BRIDGE NO. 03656 DRAWING NO. 61772

BRIDGE ENGINEER

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.		061687	60	63
				① 03670, 03671		LMC OVERLAY		61773

THIS SHEET INTENTIONALLY LEFT BLANK

S:\donaldson 10/24/2022 4:01:00 PM
 WORKSPACE: AHTD
 L:\2017\1701580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\BRIDGE\BB0609_140_S002-BD.dgn
 REVISED DATE:

DETAILS OF LATEX MODIFIED
 CONCRETE OVERLAY
 ROUTE SEC.
 ARKANSAS STATE HIGHWAY COMMISSION
 LITTLE ROCK, ARK.
 DRAWN BY: HEW DATE: JAN. 2020 FILENAME: _____
 CHECKED BY: JHR DATE: JAN. 2020 SCALE: AS SHOWN
 DESIGNED BY: BWD DATE: JAN. 2020
 BRIDGE NO. 03670, 03671 DRAWING NO. 61773

BRIDGE ENGINEER

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.		061687	61	63
				① 03708	LMC OVERLAY			61774

THIS SHEET INTENTIONALLY LEFT BLANK

DETAILS OF LATEX MODIFIED
 CONCRETE OVERLAY WITH GRADE RAISE
 ROUTE SEC.
 ARKANSAS STATE HIGHWAY COMMISSION
 LITTLE ROCK, ARK.

DRAWN BY: HEW DATE: JAN. 2020 FILENAME: _____
 CHECKED BY: JHR DATE: JAN. 2020 SCALE: AS SHOWN
 DESIGNED BY: BWD DATE: JAN. 2020
 BRIDGE NO. 03708 DRAWING NO. 61774

 BRIDGE ENGINEER

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
				JOB NO.		061687	62	63
				① 03656, 03670, 03671 & 03708		MISCELLANEOUS DETAILS	61775	

THIS SHEET INTENTIONALLY LEFT BLANK

S:\Donaldson 10/26/2022 14:16 PM
 WORKSPACE: AHTD
 L:\2017\1701580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\BRIDGE\BB0609_140_S004-BD.dgn
 REVISED DATE:

MISCELLANEOUS DETAILS
 ROUTE SEC.
 ARKANSAS STATE HIGHWAY COMMISSION
 LITTLE ROCK, ARK.

DRAWN BY: HEW DATE: JAN. 2020 FILENAME: _____
 CHECKED BY: JHR DATE: JAN. 2020 SCALE: AS SHOWN
 DESIGNED BY: BWD DATE: JAN. 2020
 BRIDGE NO. 03656, 03670, 03671 & 03708 DRAWING NO. 61775

BRIDGE ENGINEER

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
10/28/2022				6	ARK.			
						061687	63	63
				JOB NO.				
				03708		APPROACH SLAB DETAILS		61776

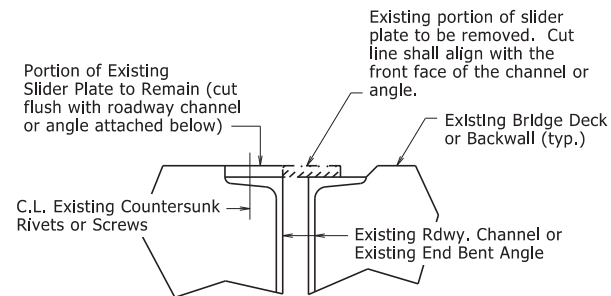
THIS SHEET INTENTIONALLY LEFT BLANK

S:\Donaldson 10/26/2022 14:32:33 PM
 WORKSPACE: AHTD
 L:\2017\1701580 - BB0608 Hwy 31 - Prairie Co Line\Drawings\BRIDGE\BB0609_140_S005-MD.dgn
 REVISED DATE:

APPROACH SLAB DETAILS
 ROUTE SEC.
 ARKANSAS STATE HIGHWAY COMMISSION
 LITTLE ROCK, ARK.
 DRAWN BY: HEW DATE: JAN. 2020 FILENAME: _____
 CHECKED BY: JHR DATE: JAN. 2020 SCALE: AS SHOWN
 DESIGNED BY: BWD DATE: JAN. 2020
 BRIDGE NO. 03708 DRAWING NO. 61776

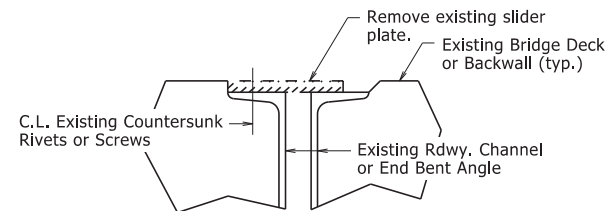
BRIDGE ENGINEER

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.			
				JOB NO.				
				1		JOINT REPAIR - 55064		



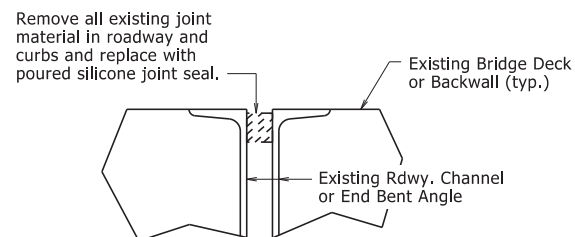
REMOVAL DETAILS AT EXISTING SLIDER PLATE JOINTS

At the direction of the Engineer, the portion of existing slider plate shown shall be removed and replaced with a new plate as shown in "SLIDER PLATE JOINT MODIFICATION". The portion of existing slider plate shall be removed and disposed of in accordance with Section 821. The cut face shall be ground square and flush with the face of the existing angle or channel. Removal and disposal of existing slider plate material will not be paid for directly, but shall be considered subsidiary to the item "Silicone Joint Sealant". Properly functioning slider plates need not be modified.



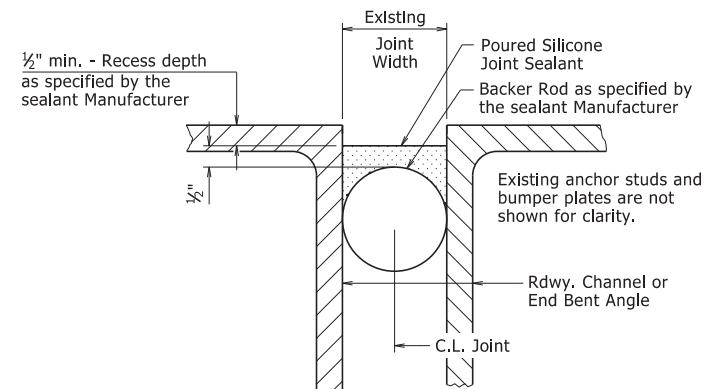
REMOVAL DETAILS AT EXISTING SLIDER PLATE JOINTS WITH GRADE RAISE

The existing slider plate shown shall be removed and replaced with new plates as shown in "JOINT MODIFICATION WITH GRADE RAISE". The existing slider plate shall be removed and disposed of in accordance with Section 821. Removal and disposal of existing slider plate material will not be paid for directly, but shall be considered subsidiary to the item "Silicone Joint Sealant".



REMOVAL DETAILS AT EXISTING FILLED JOINTS

The existing joint material shall be removed and disposed of in accordance with Section 821. Removal and disposal of existing joint material will not be paid for directly, but shall be considered subsidiary to the item "Silicone Joint Sealant".



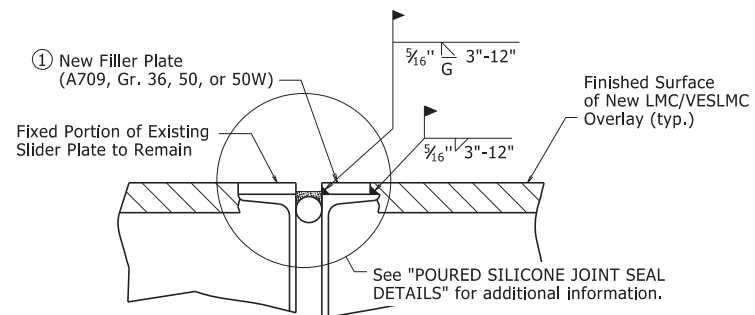
POURED SILICONE JOINT SEAL DETAILS

Existing Joint Seal shall be completely removed, backer rods placed, and Silicone Joint Sealant installed across the entire width of the bridge deck in accordance with these details, Section 809, and the Manufacturer's recommendations. Removal of existing Joint Seal will not be paid for directly, but shall be considered incidental to the item "Silicone Joint Sealant".

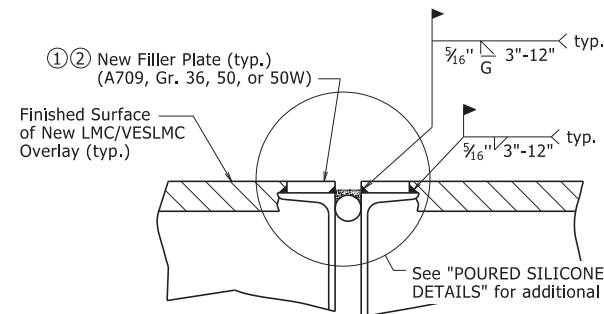
Backer rods shall be extended beyond the length of the poured joint in the initial joint repair area so that the two pieces can be properly spliced together prior to installing sealant for the adjacent joint repair. Manufacturer's recommendations shall be followed to prevent sealant leakage during repair work.

Backer rods shall be appropriately sized and set to the depth shown in the Manufacturer's literature based on the joint width at the time of sealing. Except as noted, do not install more backer rod than can be sealed in the same day. The Contractor shall verify separation of the backer rod from the joint material after joint material has set.

Backer rod shall be notched or otherwise fit around any existing seal supports or bumper plates to maintain its proper depth as defined above.



SLIDER PLATE JOINT MODIFICATION

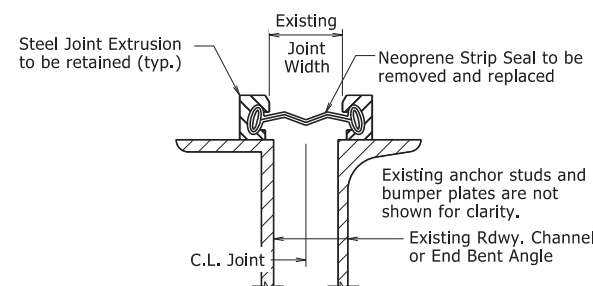


JOINT MODIFICATION WITH GRADE RAISE

1 New field attached plates atop existing roadway channels or angles are required. The plate thickness shall be adjusted as necessary to match surface of finished surface of LMC/VESLMC Overlay and the width shall be 3/8" less than the existing channel flange or angle width to allow for fillet weld as shown.

All new Structural Steel shall be ASTM A709 (Gr. 36, 50, or 50W). The surfaces not in contact with concrete shall be cleaned and painted in accordance with Section 638. Only one coat of paint is required and shall be applied in the fabricator's shop. Grade 50W steel shall not be painted, but shall be cleaned in accordance with Subsection 807.84(e). Structural Steel and Painting will not be paid for directly, but shall be subsidiary to the item "Silicone Joint Sealant".

2 Details shown are for an expansion joint where two bridge units meet. Eliminate filler plate on backwall and proceed with backwall repair in accordance with "BACKWALL REPAIR REMOVAL DETAIL" and "BACKWALL REPAIR INSTALLATION DETAIL" at end bents for bridge decks with grade raise, see Standard Drawing Number 55065.



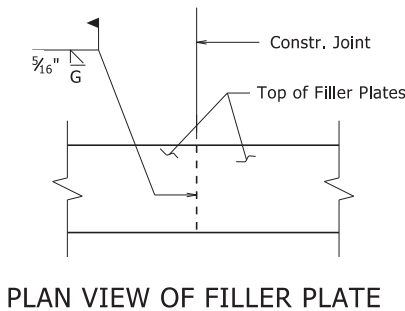
STRIP SEAL JOINT DETAILS

Existing neoprene strip seal joint material shall be completely removed and new neoprene strip seal joint material shall be installed across the entire width of the steel extrusions in accordance with these details, Section 809, and the Manufacturer's recommendations. Prior to installing the new joint material, the Contractor shall clean the steel extrusion at the Engineer's direction and in accordance with the new strip seal joint material Manufacturer's recommendations.

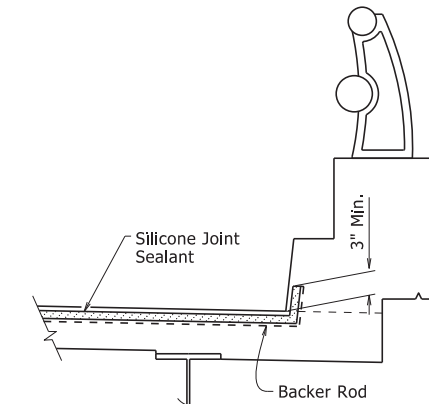
Removal and replacement of the existing neoprene strip seal joint material will require the removal of the parapet slider plates, where present. Parapet slider plates removed for this work shall be reinstalled after installation of the new neoprene strip seal joint material.

The new neoprene strip seal joint material shall provide a movement rating of four inches. The repaired expansion joint shall be capable of sealing the deck surface and parapet area to prevent moisture and other contaminants from descending through the joint.

All work and material associated with removing the existing joint material, cleaning the extrusions, removal and reinstallation of parapet slider plates, and installation of new joint material shall be paid for under the item "Modification of Existing Bridge Structure (Bridge No. _)".

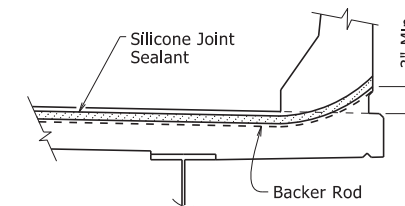


PLAN VIEW OF FILLER PLATE

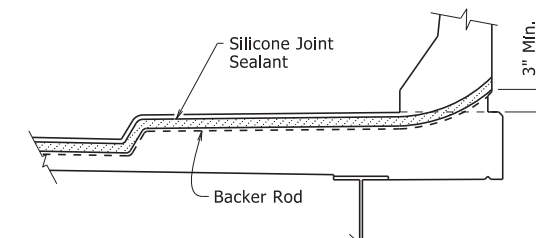


SILICONE JOINT SEAL PLACEMENT AT CURB

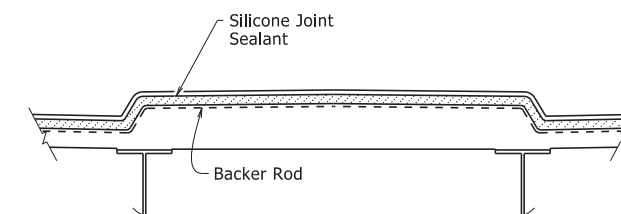
Vertical joints may require forming. The clearance from deck surface to joint material shall be maintained.



SILICONE JOINT SEAL PLACEMENT AT RAIL

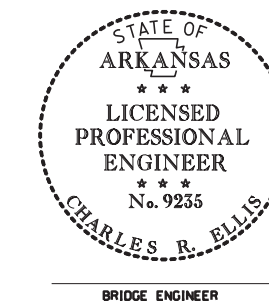


SILICONE JOINT SEAL PLACEMENT AT SIDEWALK

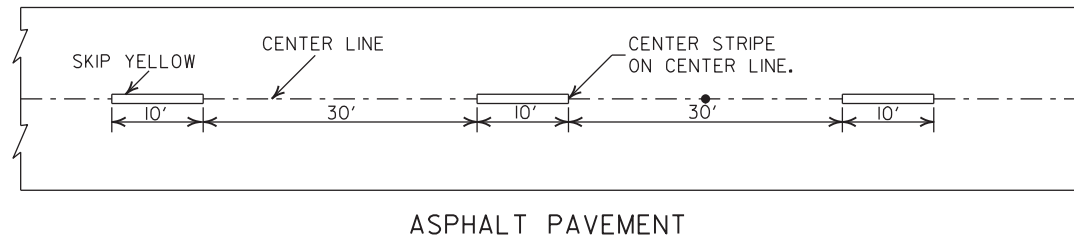
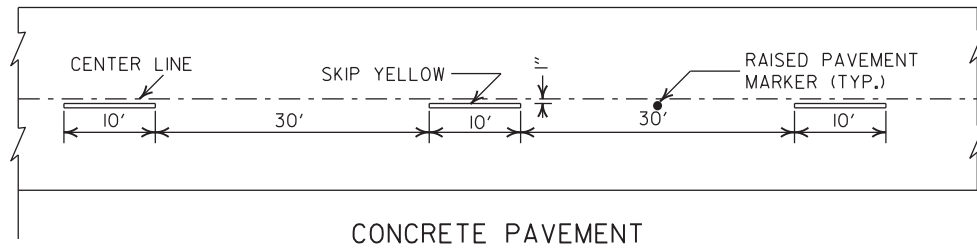


SILICONE JOINT SEAL PLACEMENT AT MEDIAN

This document was originally issued and sealed by Charles R. Ellis, PE No. 9235, on November 7, 2019. This copy is not a signed and sealed document.



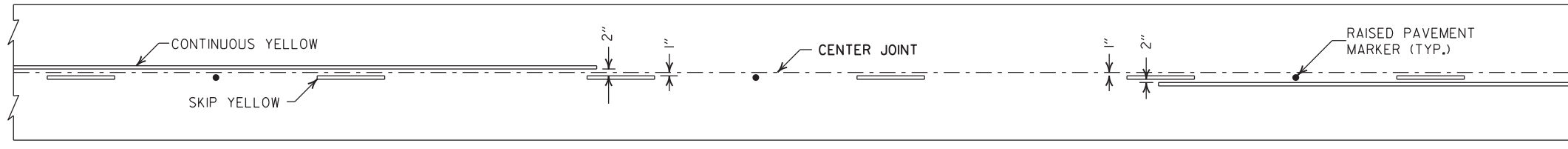
STANDARD DETAILS FOR JOINT REPAIRS & MODIFICATIONS
ARKANSAS STATE HIGHWAY COMMISSION
 LITTLE ROCK, ARK.
 DRAWN BY: KWY DATE: 11/7/2019 FILENAME: b55064.dgn
 CHECKED BY: SWP DATE: 11/7/2019 SCALE: None
 DESIGNED BY: STD. DATE: -----
 BRIDGE ENGINEER
 DRAWING NO. 55064



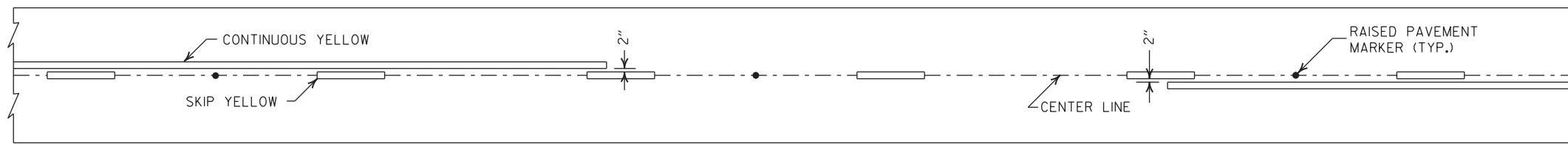
CONCRETE PAVEMENT

ASPHALT PAVEMENT

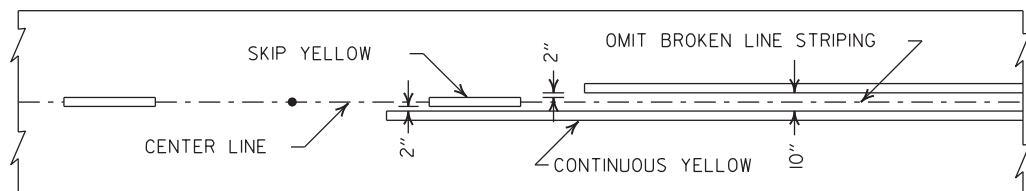
BROKEN LINE STRIPING



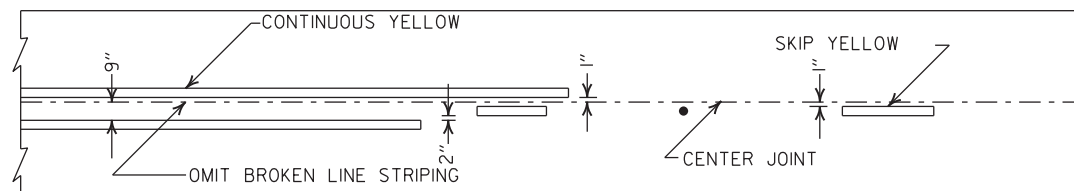
SOLID LINE STRIPING ON CONCRETE PAVEMENT



SOLID LINE STRIPING ON ASPHALT PAVEMENT

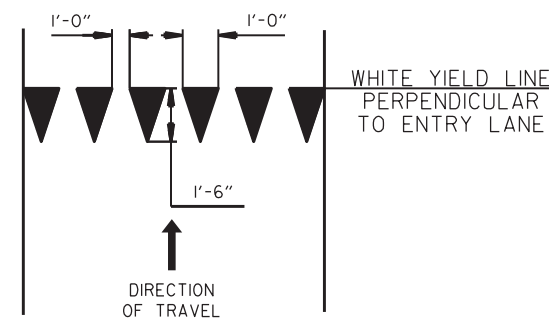


ASPHALT PAVEMENT

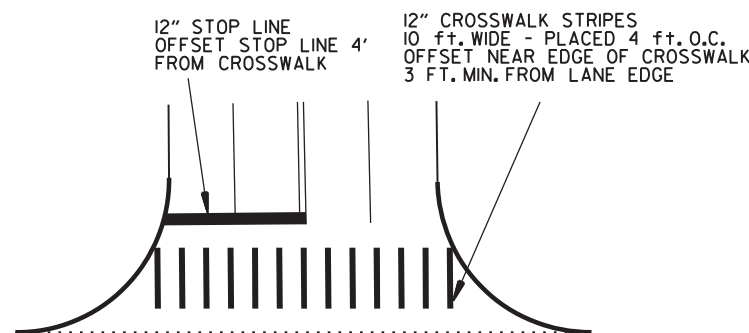


CONCRETE PAVEMENT

STRIPING AT ADJACENT NO PASSING LANES

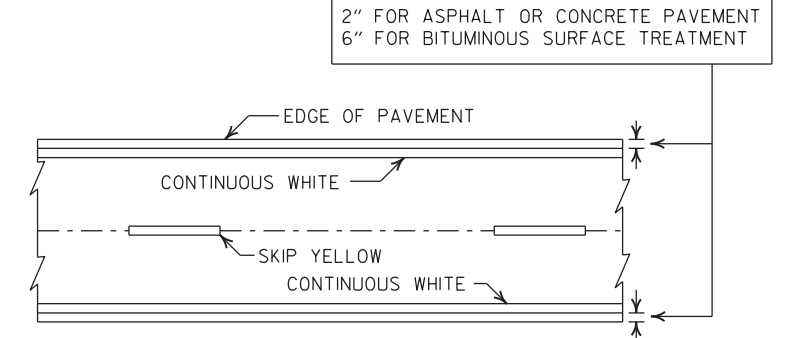


YIELD LINE DETAIL



CROSSWALK AND STOP LINE DETAILS

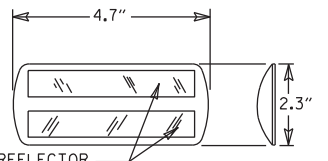
- NOTES:
1. REFER TO THE STRIPING DETAILS FOR PAVEMENT MARKING LINE WIDTHS.
 2. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
 3. RAISED PAVEMENT MARKERS SHALL BE PLACED ON AN 80 FEET SPACING UNLESS OTHERWISE SHOWN IN THE PLANS.



PAVEMENT EDGE LINE MARKING

NOTE:
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.

TYPE II
RED/CLEAR OR
YELLOW/YELLOW



PRISMATIC REFLECTOR

NOTE:
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ARDOT QUALIFIED PRODUCTS LIST.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

2-27-20	REVISED STOP LINE DETAILS	
6-1-17	ADDED YIELD LINE DETAIL	
5-12-16	REVISED LINE WIDTHS, SPACING, & NOTES	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REVISED GENERAL NOTES & REMOVED PLOWABLE PVMT MRKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTLS.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION

PAVEMENT MARKING DETAILS

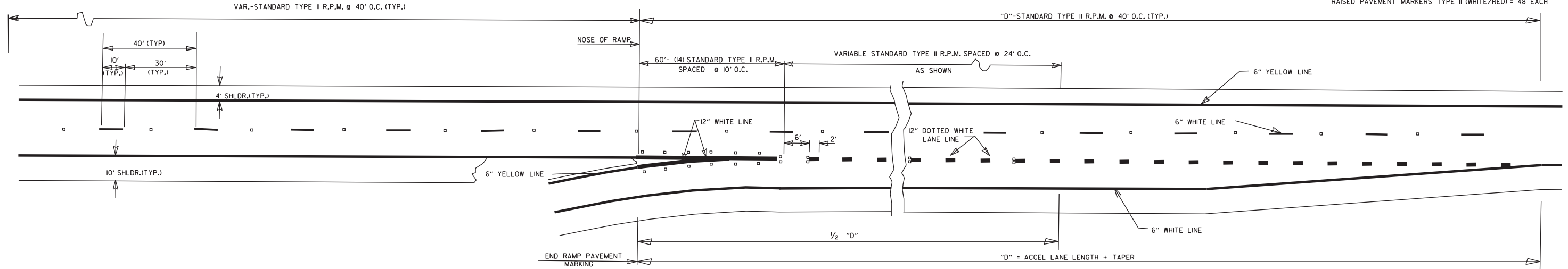
STANDARD DRAWING PM-1

ENTRANCE RAMP

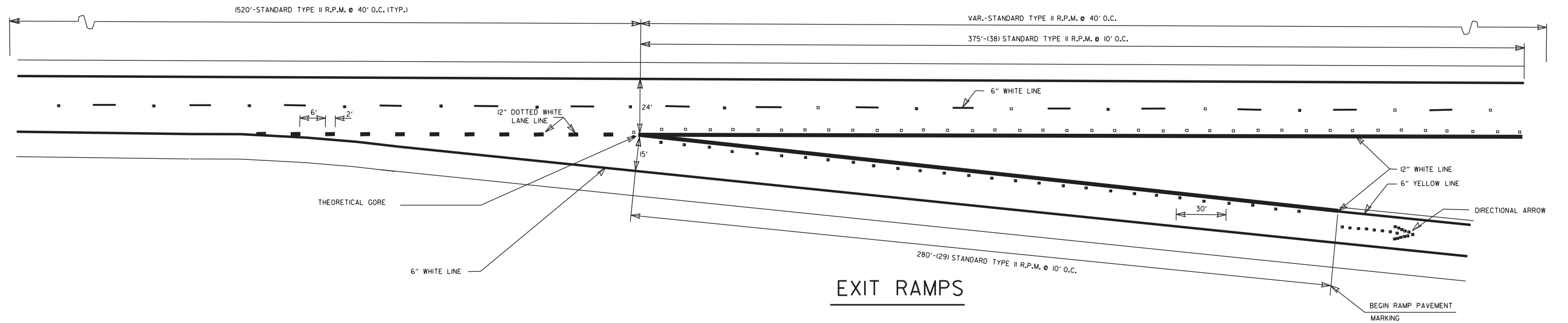
12" WHITE = 370 LIN. FT.
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH

EXIT RAMP

6" WHITE = 280 LIN. FT.
12" WHITE = 815 LIN. FT.
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 38 EACH
RAISED PAVEMENT MARKERS TYPE II (WHITE/RED) = 48 EACH



ENTRANCE RAMP

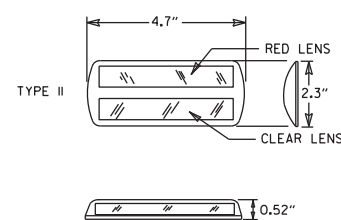


EXIT RAMP

GENERAL NOTES:
THIS DRAWING SHOULD BE CONSIDERED AS TYPICAL ONLY AND THE FINAL LOCATION OF THE STRIPING AND PAVEMENT MARKERS SHALL BE DETERMINED BY THE ENGINEER.

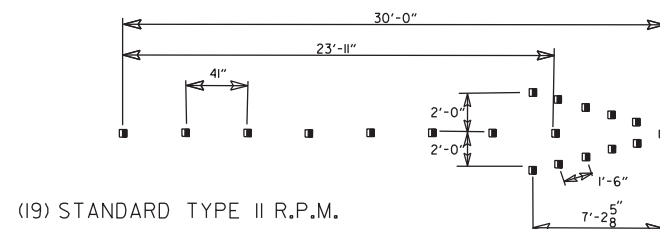
THIS DRAWING SHOULD BE USED IN CONJUNCTION WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST REVISION.

NOTE:
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ADOT QUALIFIED PRODUCTS LIST.



DETAIL OF STANDARD RAISED PAVEMENT MARKERS

NOTE:
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.



DIRECTIONAL ARROWS

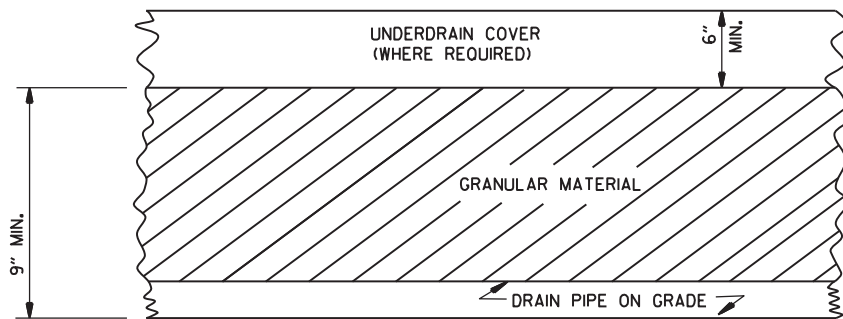
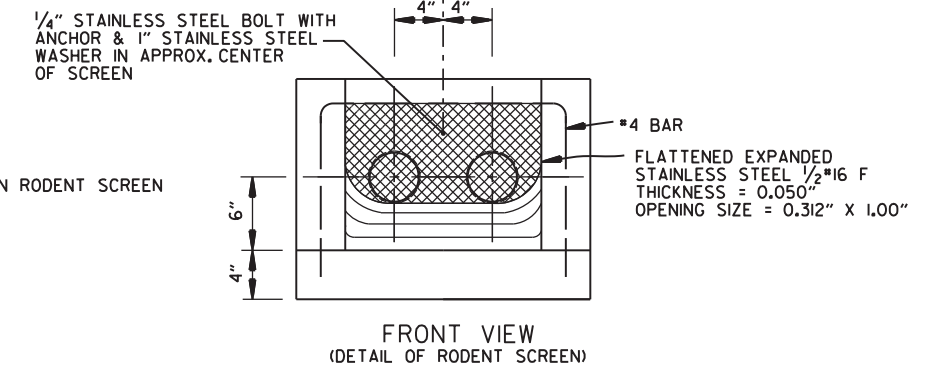
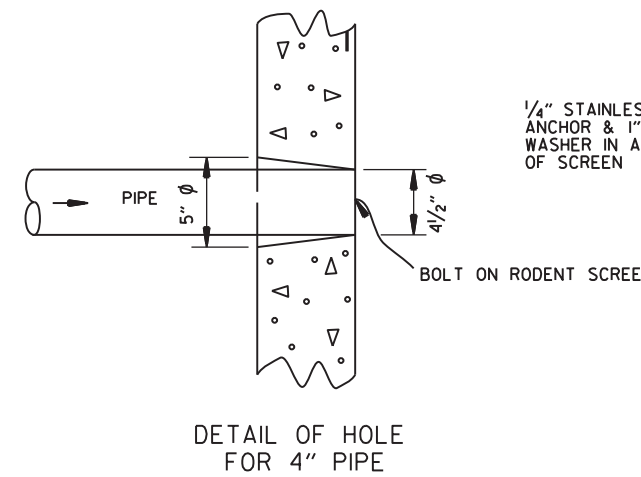
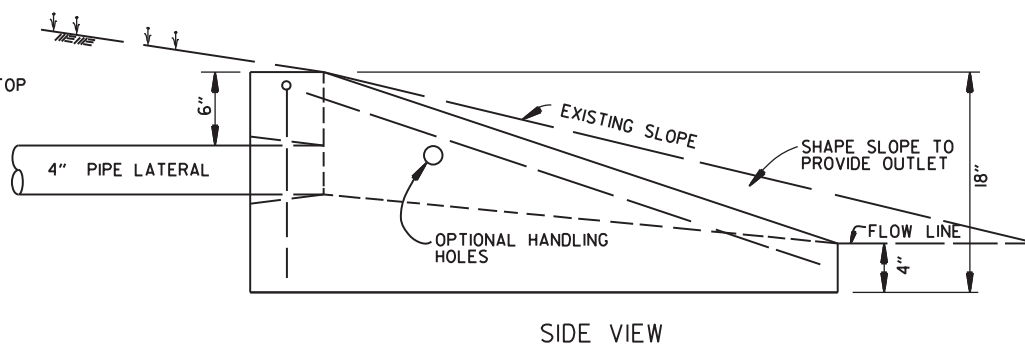
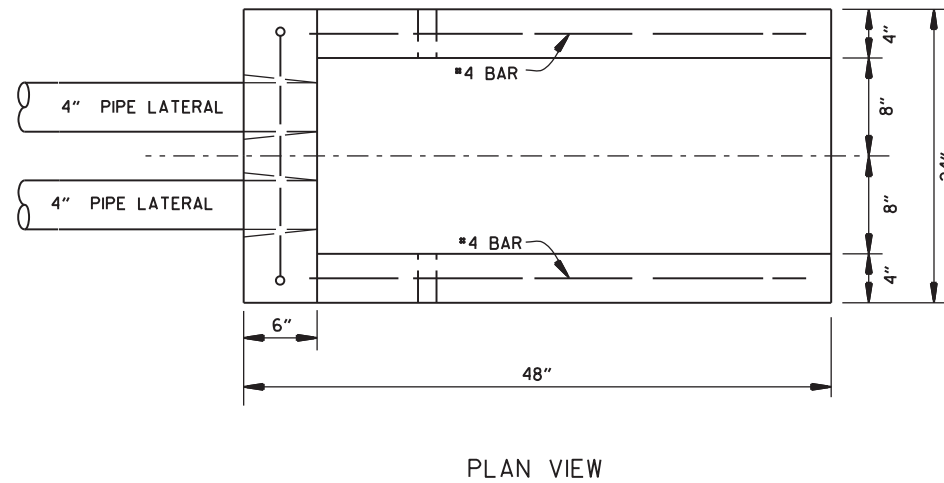
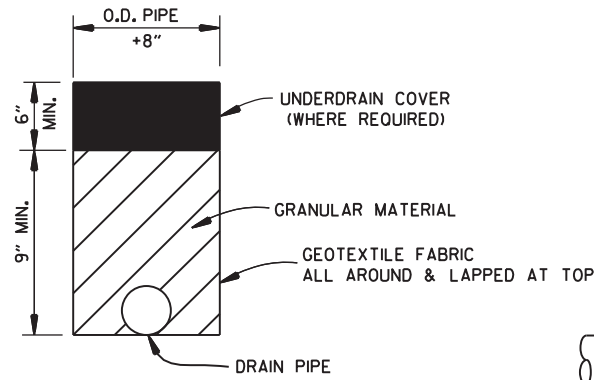
DATE	REVISION	FILMED
05-14-20	REMOVED CROSSHATCH MARKINGS ON EXIT RAMP	
11-07-19	REVISED DOTTED PAV'T MARKINGS; ADDED CROSSHATCH MARKINGS ON EXIT RAMP	
12-8-16	REVISED RAISED PAV'T MARKERS FOR 80' SPACING; REVISED WIDTH OF STRIPING	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
7-26-12	REVISED RPM NOTATION	
12-15-11	REVISED RPMs ACCORDING TO LATEST POLICY	
11-17-10	REMOVED PLOWABLE PAVEMENT MARKERS	
6-3-10	REVISED PER 2009 MUTCD	
11-18-04	REVISED NOTES	
8-22-02	ADDED & REVISED NOTES; REV. ENTRANCE & EXIT RAMP	
5-18-00	REMOVED HASHMARKS	
7-02-98	CHANGED TYPES TO ROMAN NUMERALS	
4-26-96	ADDED DIMENSIONS & QUANTITIES; REVISED LANE WIDTH ON EXIT RAMP	
2-2-95	PLACED IN USE	2-2-95

ARKANSAS STATE HIGHWAY COMMISSION

**PAVEMENT MARKING DETAILS
ON
ACCESS CONTROLLED ROADWAYS**

STANDARD DRAWING PM-2

NOTE:
 1. UNLESS OTHERWISE SPECIFIED ON THE PLANS, THE UNDERDRAIN COVER SHALL BE THOROUGHLY COMPACTED EARTH AND SHALL BE SUBSIDIARY TO PIPE UNDERDRAIN.
 2. GRANULAR MATERIAL SHALL BE WRAPPED WITH GEOTEXTILE FABRIC, LAP FABRIC 12" OR THE WIDTH OF THE TRENCH AT THE TOP.



DETAILS OF PIPE UNDERDRAIN

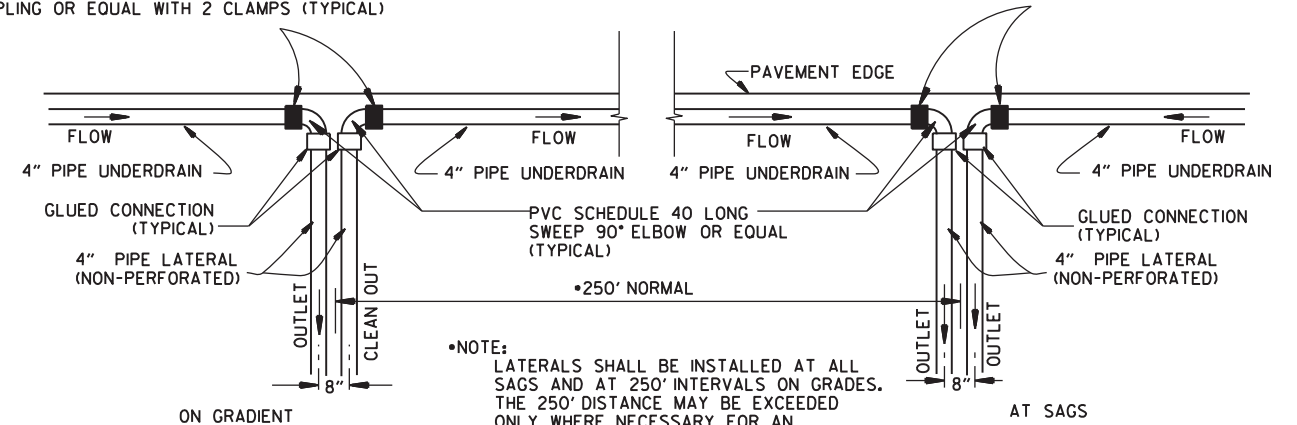
NOTES FOR PIPE UNDERDRAINS

1. GEOTEXTILE FABRIC SHALL MEET THE REQUIREMENTS OF SECTION 625 FOR TYPE I. PAYMENT FOR GEOTEXTILE FABRIC AND GRANULAR FILTER MATERIAL SHALL BE INCLUDED IN THE PRICE BID PER LIN. FT. FOR "4" PIPE UNDERDRAINS" IN ACCORDANCE WITH SECTION 611 OF THE STANDARD SPECIFICATIONS.
2. 4" NON-PERFORATED SCHEDULE 40 PVC PIPE LATERALS WITH OUTLET PROTECTORS SHALL BE INSTALLED AS SHOWN HEREON. LATERALS WILL BE MEASURED AND PAID FOR AS "4" PIPE UNDERDRAINS." UNDERDRAIN OUTLET PROTECTORS WILL BE MEASURED AND PAID FOR BY THE UNIT IN ACCORDANCE WITH SECTION 611 OF THE STANDARD SPECIFICATIONS.
3. EXISTING 4" PIPE UNDERDRAINS MAY BE CONNECTED TO PROPOSED DROP INLETS OR EXTENDED WHERE DIRECTED BY THE ENGINEER. PAYMENT FOR CONNECTING TO DROP INLETS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR "4" PIPE UNDERDRAINS."
4. THE LOCATION OF ALL LATERALS SHALL BE MARKED WITH 4" X 12" PERMANENT PAVEMENT MARKING TAPE (TYPE III WHITE) AT THE OUTSIDE EDGE OF THE SHOULDER, PLACED TRANSVERSE TO TRAFFIC. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS.
5. PAYMENT FOR THE RODENT SCREEN SHALL BE INCLUDED IN THE PRICE BID PER EACH FOR "UNDERDRAIN OUTLET PROTECTORS."
6. ANY EXISTING UNDERDRAINS THAT INTERFERE WITH INSTALLATION OF THE NEW UNDERDRAIN SYSTEM SHALL BE REMOVED AND DISPOSED OF AS DIRECTED BY THE ENGINEER. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS. EXISTING UNDERDRAIN OUTLET PROTECTORS SHALL BE REMOVED UNDER THE ITEM "REMOVAL AND DISPOSAL OF UNDERDRAIN OUTLET PROTECTORS."
7. AT LOCATIONS WHERE A SINGLE LATERAL IS USED THE CONTRACTOR SHALL HAVE THE FOLLOWING OPTIONS: 1. INSTALL OUTLET PROTECTOR AS SHOWN ON STANDARD DRAWING PU-1 AND GROUT THE UNUSED HOLE OR 2. INSTALL AN OUTLET PROTECTOR WITH A SINGLE HOLE.

FERNCO 1056-44 (4" CI/PLASTIC) OR FERNCO 1051-44 (4" AC/DIOR 4" CI/PLASTIC) COUPLING OR EQUAL WITH 2 CLAMPS (TYPICAL)

UNDERDRAIN OUTLET PROTECTORS

FERNCO 1056-44 (4" CI/PLASTIC) OR FERNCO 1051-44 (4" AC/DIOR 4" CI/PLASTIC) COUPLING OR EQUAL WITH 2 CLAMPS (TYPICAL)



NOTE: LATERALS SHALL BE INSTALLED AT ALL SAGS AND AT 250' INTERVALS ON GRADES. THE 250' DISTANCE MAY BE EXCEEDED ONLY WHERE NECESSARY FOR AN ACCEPTABLE OUTLET.

DETAIL OF PIPE UNDERDRAIN LATERALS WHEN PLACED ALONG PAVEMENT EDGE


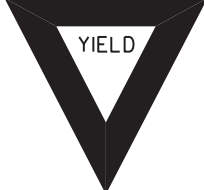

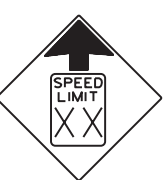





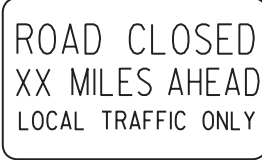










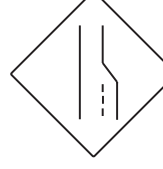



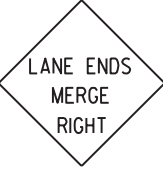













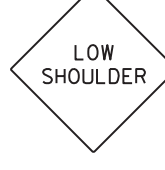

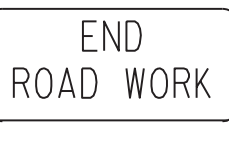
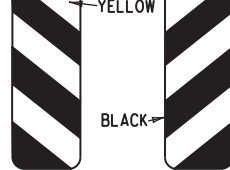
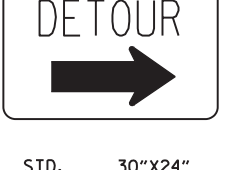

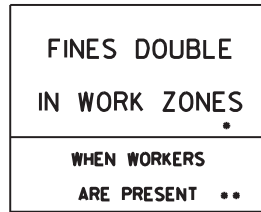
NOTE: PVC PIPE FOR LATERALS SHALL MEET THE REQUIREMENTS OF ASTM D 1785 (LATEST REVISION) FOR SCHEDULE 40 PIPE.

DATE	REVISION	DATE FILMED
12-8-16	ADDED NOTES FOR PIPE UNDERDRAINS, REVISED RODENT SCREEN DETAIL AND NOTES, REMOVED NOTE 1 FOR GRANULAR MATERIAL, ADDED NOTE FOR GEOTEXTILE FABRIC	
4-10-03	REVISED NOTE 3	
1-12-00	REVISED DETAIL OF UNDERDRAIN LATERALS	
11-18-98	REVISED NOTE	
10-18-96	REVISED MIN. DEPTH & GEOTEXTILE FABRIC	
4-26-96	ADDED LATERAL NOTE: 5 1/2" TO 5"	
11-22-95	REVISED LATERALS	
7-20-95	REVISED LATERALS & ADDED NOTE	
11-3-94	REVISED FOR DUAL LATERALS	11-3-94
10-1-92	SUBSTITUTED GEOTEXTILE	10-1-92
8-15-91	ADDED POLYETHYLENE PIPE	8-15-91
11-8-90	DELETED ALTERNATE NOTE	11-8-90
1-25-90	ADDED 4" SNAP ADAPTER	1-25-90
11-30-89	DEL. (SUBGRADE); ADDED (WHERE REQUIRED)	11-30-89
7-15-88	ISSUED P.L.M.	647-7-15-88

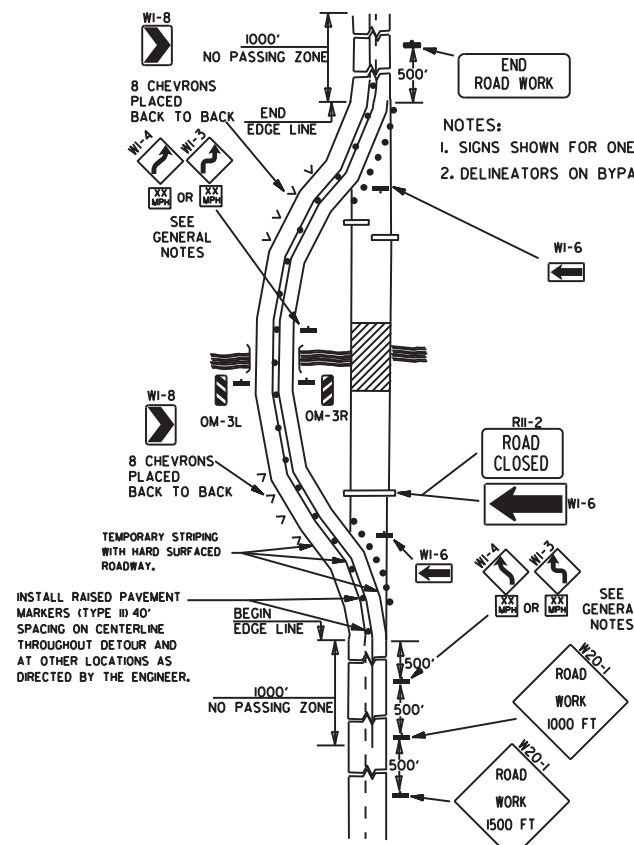
ARKANSAS STATE HIGHWAY COMMISSION

DETAILS OF PIPE UNDERDRAIN

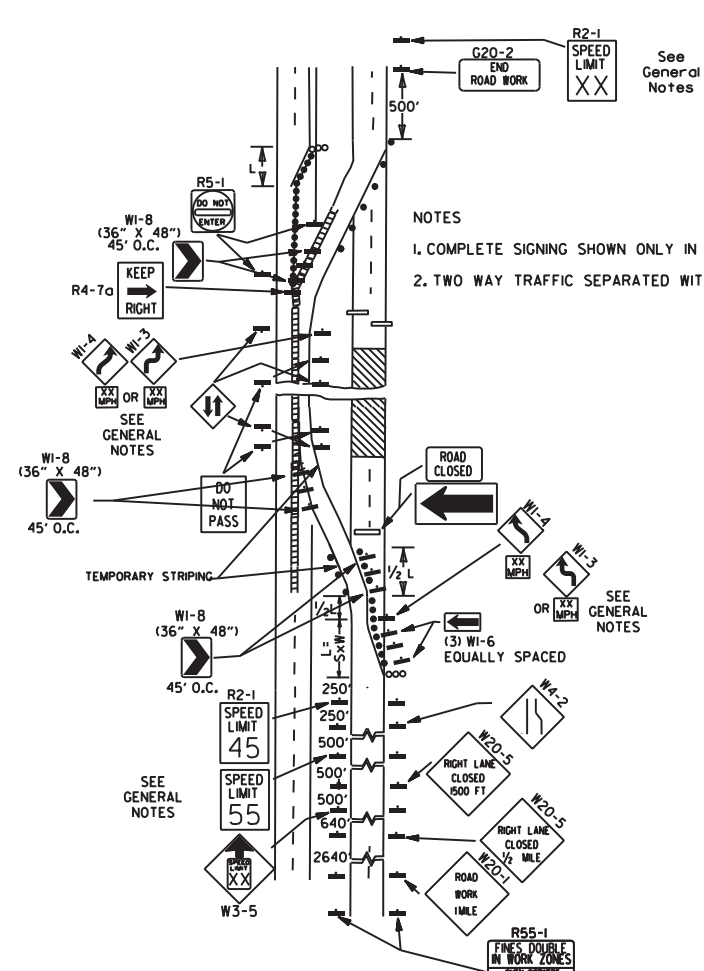
STANDARD DRAWING PU-1

							ADVANCE DISTANCES (XXXX)
<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>500 FT 1/2 MILE 1000 FT 3/4 MILE 1500 FT 1 MILE AHEAD</p> <p>GENERAL NOTES:</p> <ol style="list-style-type: none"> ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION. TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER. EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED. SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE. SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3. POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS. FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS. MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT. R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN. <p>NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.</p>
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>W21-5a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W1-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W1-3</p>  <p>STD. 48"x48"</p>	<p>W1-4</p>  <p>STD. 48"x48"</p>	<p>W1-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>W1-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>W1-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60"</p> <p>• USE 6" C LETTERS •• USE 4" D LETTERS</p>

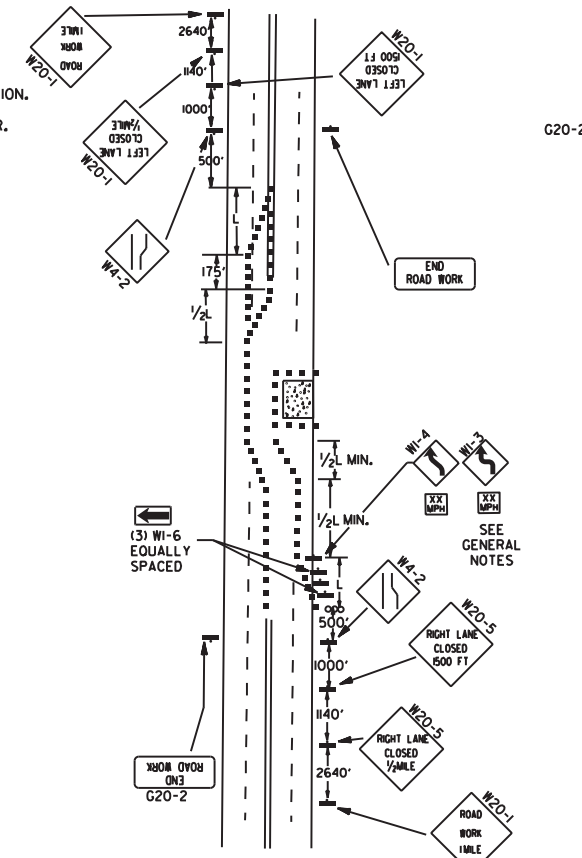
DATE	REVISION	FILMED
11-07-19	REVISED FOR MASH	
4-13-17	DELETED RSP-1 & ADDED W21-5a	
9-2-15	REVISED REDUCED SPEED LIMIT AHEAD SIGNS REVISED ROAD WORK NEXT XX MILES	
12-15-11	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	



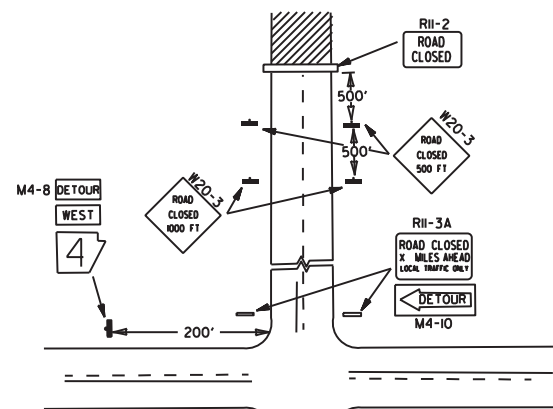
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



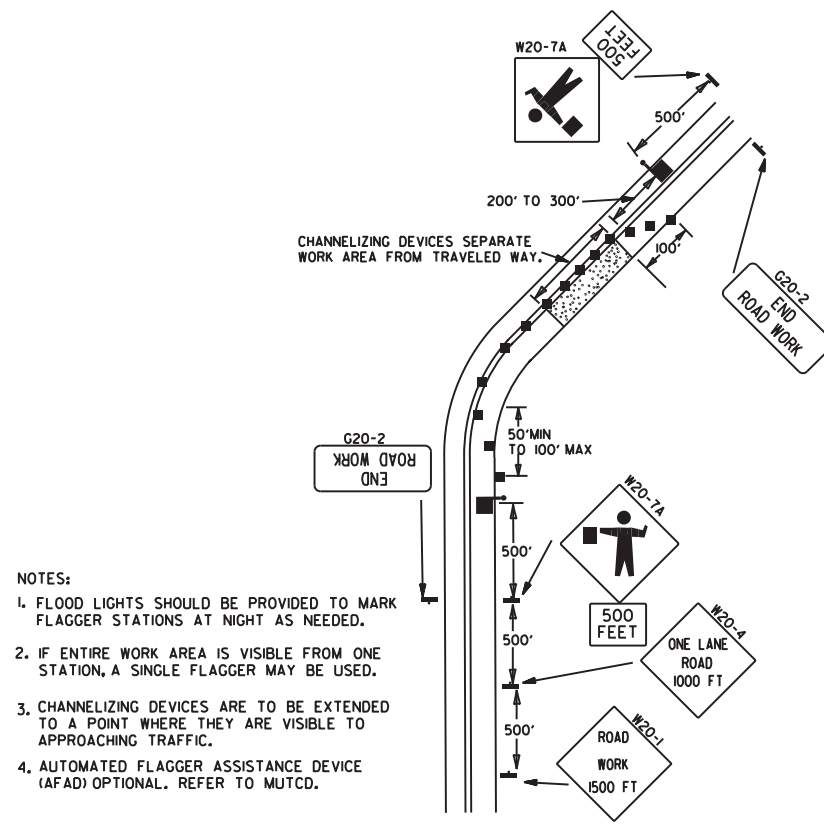
(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.



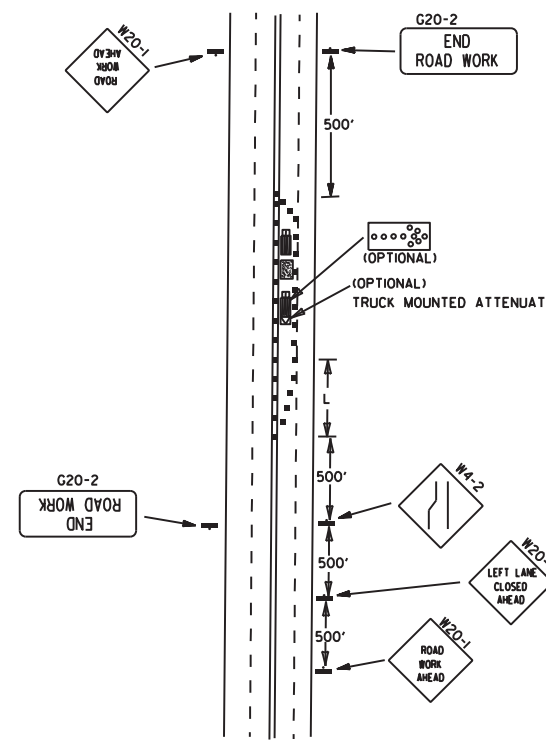
(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.

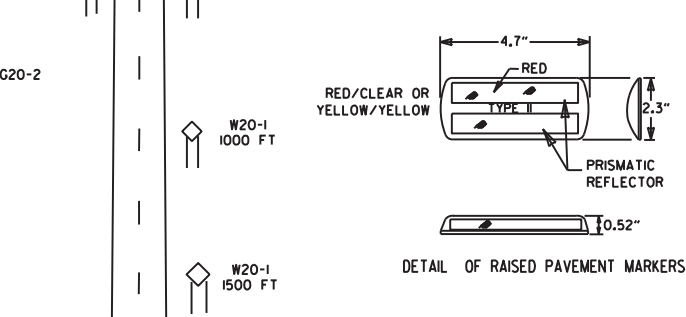


(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

- KEY:
- FLAGGER
 - POSITIVE BARRIER
 - ARROW PANEL (IF REQUIRED)
 - TYPE III BARRICADE
 - CHANNELIZING DEVICE
 - TRAFFIC DRUM
 - RAISED PAVEMENT MARKER



TYPICAL ADVANCE WARNING SIGN PLACEMENT

TAPER FORMULAE:
L=SW FOR SPEEDS OF 45MPH OR MORE.
L= $\frac{WS^2}{60}$ FOR SPEEDS OF 40MPH OR LESS.
WHERE:
L= MINIMUM LENGTH OF TAPER.
S= NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.
W= WIDTH OF OFFSET.

- GENERAL NOTES:
- THE MAINTENANCE DIVISION SHALL CONDUCT A BALL BANK STUDY TO DETERMINE THE ADVISORY SPEED LIMIT PRIOR TO OPENING TO TRAFFIC. THE ADVISORY SPEED WILL BE POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
 - WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(KX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 - WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-1(55)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(KX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 - THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
 - WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
 - PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 - TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
 - DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ADOT QUALIFIED PRODUCTS LIST.
 - ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

DATE	REVISION	FILMED
05-20-21	REVISED NOTE 7	
11-07-19	REVISED NOTE 4, ADDED NOTE 9	
9-2-15	REVISED NOTE 2, ADDED NOTE 8, REVISED DRAWING (A) & REPLACED R2-5A WITH W3-5	
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-11-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

TRAFFIC CONTROL DEVICES

VERTICAL DIFFERENTIAL	LOCATION	TRAFFIC CONTROL	
		≤ 45 MPH	> 45 MPH
≤ 1"	CENTERLINE	W8-11	W8-11
> 1"	CENTERLINE	W8-11 AND CENTERLINE LANE STRIPING	W8-11 AND CENTERLINE LANE STRIPING
≤ 3"	CENTERLINE	STANDARD LANE CLOSURE ⁽⁶⁾	STANDARD LANE CLOSURE ⁽⁶⁾
≤ 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9 AND TRAFFIC DRUMS ⁽¹⁾	W8-9 AND TRAFFIC DRUMS ⁽¹⁾
> 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 18"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	A STABILIZED WEDGE, W8-17, EDGE LINE STRIPING AND TRAFFIC DRUMS ⁽³⁾
> 24"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER ⁽⁴⁾ & EDGE LINES	PRECAST CONCRETE BARRIER ⁽⁴⁾ & EDGE LINES

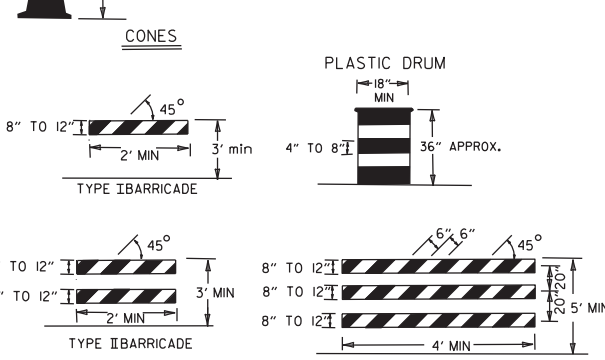
INTERSTATE		
VERTICAL DIFFERENTIAL	LOCATION	TRAFFIC CONTROL
≤ 3"	CENTERLINE	W8-11 AND LANE STRIPING
≤ 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER & EDGE LINES

INTERSTATE AND NON-INTERSTATE		
FORESLOPE	HEIGHT	TRAFFIC CONTROL
1:1	> 2 FT	PRECAST CONCRETE BARRIER
2:1	≤ 5 FT	TRAFFIC DRUMS
2:1	> 5 FT	PRECAST CONCRETE BARRIER
Flatter than 2:1	N/A	TRAFFIC DRUMS

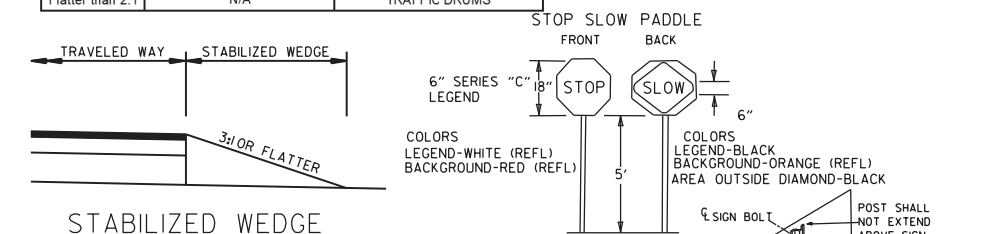
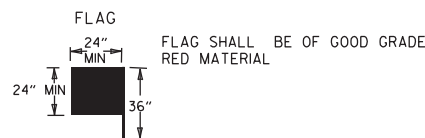
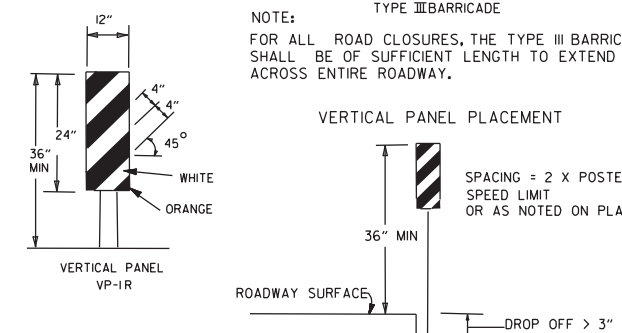
- GENERAL NOTES:
- WHEN THE SHOULDER AREA IS USED AS PART OF THE TRAVELED LANE AND THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, THEN VERTICAL PANELS SHOULD BE USED.
 - WHEN THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, A STABILIZED WEDGE SHALL BE USED. PRECAST CONCRETE BARRIER WALL CAN BE USED IN LIEU OF A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS.
 - A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS CAN BE USED IN LIEU OF PRECAST CONCRETE BARRIER WALL, IF AND WHERE DIRECTED BY THE ENGINEER.
 - W21-5, W21-5a, AND/OR W21-5b SIGNS SHALL BE USED WHERE THE ROADWAY IS UNOBSTRUCTED IF AND WHERE DIRECTED BY THE ENGINEER.
 - TIME LIMITATIONS MUST CONFORM TO SECTION 603 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION).

CHANNELIZING DEVICES

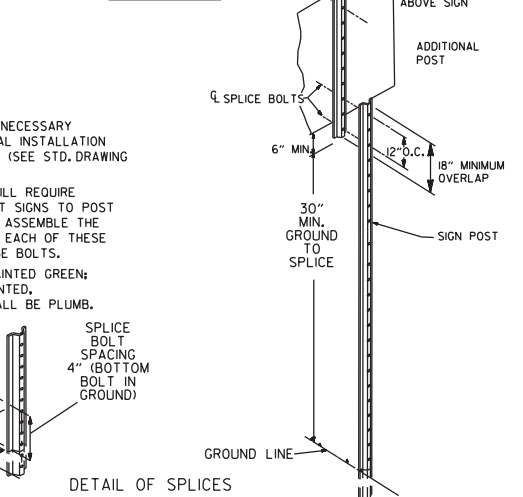
* WHEN CONES ARE USED ON FREEWAYS AND MULTI-LANE HIGHWAYS, THEY SHALL BE 28" MIN. DURING HOURS OF DARKNESS. 28" CONES SHALL BE USED ON ALL ROADWAYS, AND SHALL BE REFLECTORIZED IN ACCORDANCE WITH THE M.U.T.C.D.



NOTE: FOR ALL ROAD CLOSURES, THE TYPE III BARRICADES SHALL BE OF SUFFICIENT LENGTH TO EXTEND ACROSS ENTIRE ROADWAY.



NOTE: MATERIALS FOR THE STABILIZED WEDGE SHALL MEET THE REQUIREMENTS PROVIDED IN SECTION 603.02 OF THE STANDARD SPECIFICATIONS.



(A) TYPICAL APPLICATION - DAYTIME MAINTENANCE OPERATIONS OF SHORT DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

(B) TYPICAL APPLICATION - 3-LANE ONEWAY ROADWAY WHERE CENTER LANE IS CLOSED.

KEY:

- ○ ○ ○ ARROW PANEL (IF REQUIRED)
- CHANNELIZING DEVICE
- TRAFFIC DRUM

GENERAL NOTES:

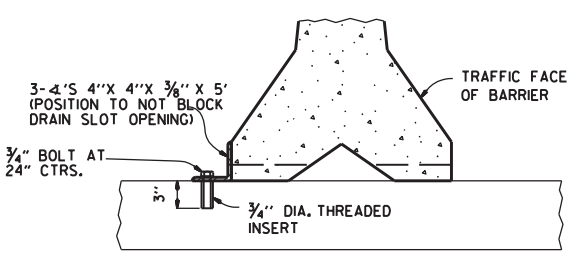
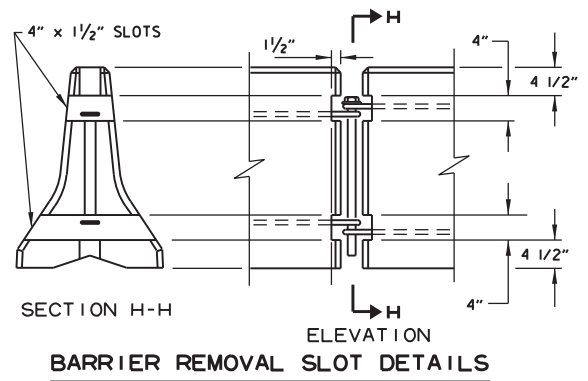
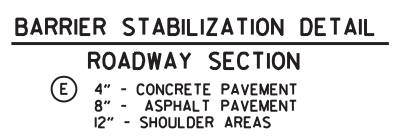
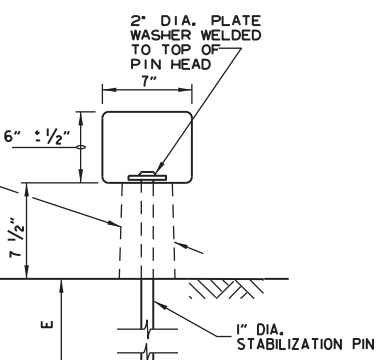
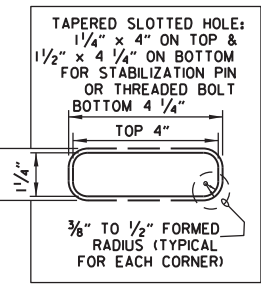
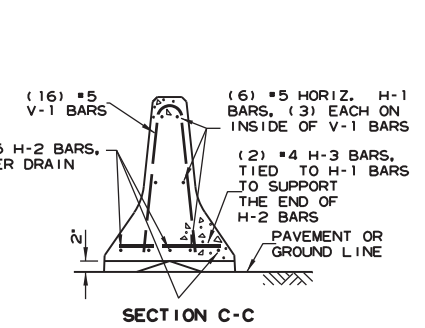
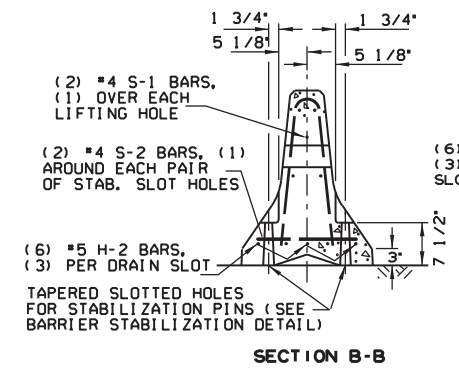
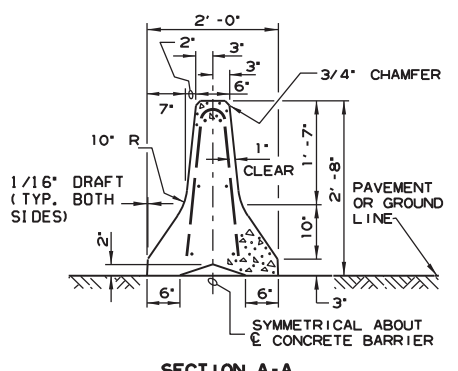
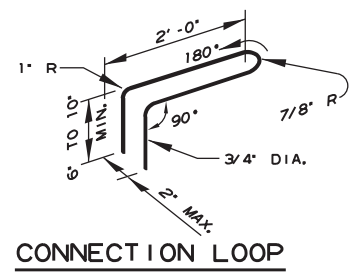
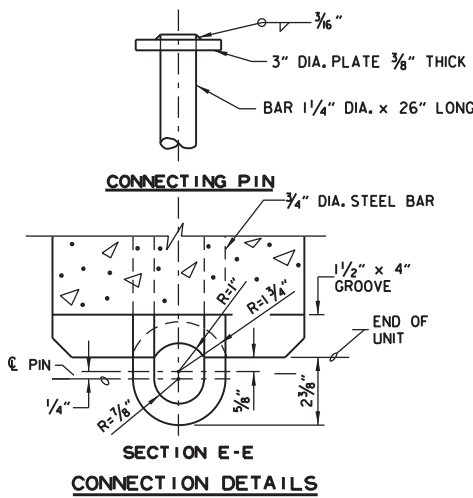
- A SPEED LIMIT REDUCTION MAY BE IMPLEMENTED ONLY WHEN DESIGNATED IN THE PLAN OR WHEN RECOMMENDED BY THE ROADWAY DESIGN DIVISION.
- WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
- WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-1(55)MPH SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1/4 MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
- THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT OR AS DIRECTED BY THE ENGINEER.
- WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
- PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
- THE G20-1 SIGN WILL BE REQUIRED ON JOBS OF OVER TWO MILES IN LENGTH. WHEN THE LANE CLOSURE IS NOT AT THE BEGINNING OF THE PROJECT, THE G20-1 SIGN SHALL BE ERRECTED 125' IN ADVANCE OF THE JOB LIMIT. ADDITIONAL W20-1(1/4 MILE) SIGNS ARE NOT REQUIRED IN ADVANCE OF LANE CLOSURES THAT BEGIN INSIDE THE PROJECT LIMITS.
- FLAGGERS SHALL USE STOP/SLOW PADDLES FOR CONTROLLING TRAFFIC THROUGH WORK ZONES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- ALL PLASTIC DRUMS AND CONES SHALL MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
- TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE. PAYMENT FOR TRAFFIC DRUMS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR VARIOUS TRAILER MOUNTED DEVICES.
- ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

(C) TYPICAL APPLICATION - CONSTRUCTION OPERATIONS OF INTERMEDIATE TO LONG TERM DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

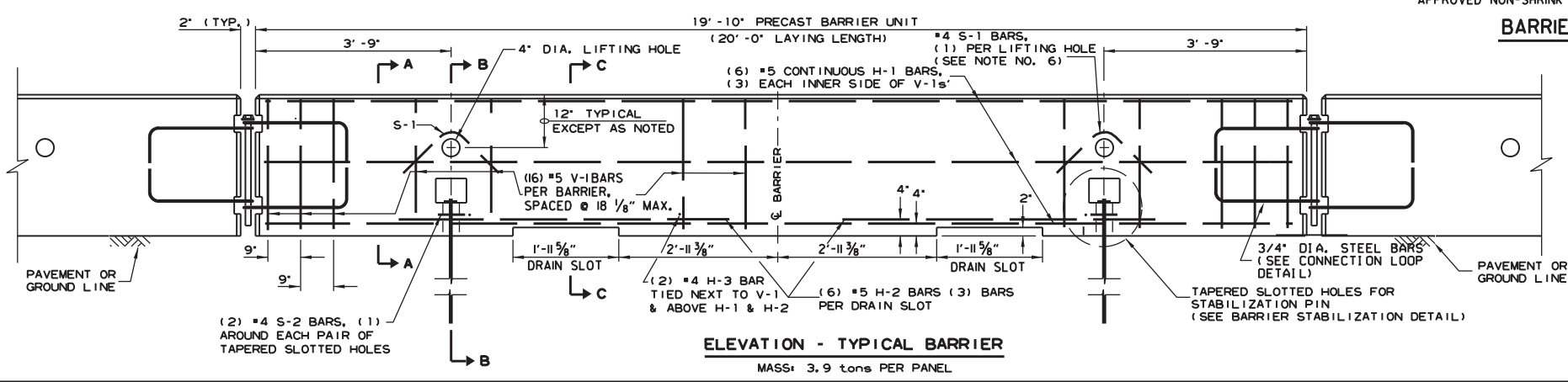
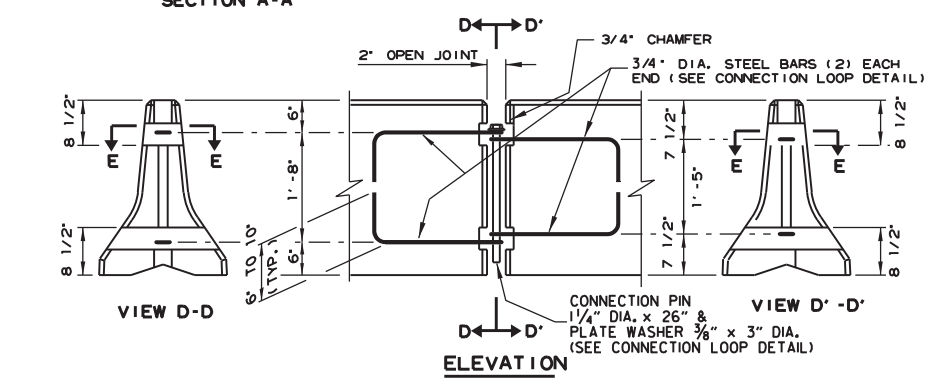
(D) TYPICAL APPLICATION - CLOSING MULTIPLE LANES OF A MULTILANE HIGHWAY.

DATE	REVISION	REVISION	FILMED
08-12-21	REVISED TRAFFIC CONTROL DEVICES AND NOTES		
05-20-21	REVISED NOTE 10		
2-27-20	REVISED TRAFFIC CONTROL DEVICES DETAILS		
11-07-19	REVISED NOTE 9, ADDED NOTE II		
7-25-19	REVISED TRAFFIC CONTROL DEVICES DETAILS		
9-2-15	REVISED NOTE 2 & REPLACED R2-5A WITH W3-5		
10-15-09	ADDED REFERENCE TO MASH		
11-20-08	REVISED SIGN DESIGNATIONS		
11-18-04	ADDED NOTE		
10-1-98	ADDED NOTE		
4-03-97	ADDED (SP) TO W6-1 & REVISED TRAFFIC CONTROL DEVICES NOTE		
10-18-96	ADDED R55-1		
10-12-95	MOVED UPPER SPLICE		
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95	
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993		
8-15-91	DRAWN AND PLACED IN USE		

REINFORCING BAR TABLE PER BARRIER UNIT				
MARK	LOCATION	BAR SIZE	(NO. BARS)	SKETCH
H-1	HORIZONTAL IN BARRIER TIED INSIDE V-1 BARS	#5	(6)	19'-3"
H-2	CENTERED ABOVE DRAIN SLOTS LONG. & TRANSVERSELY	#5	(6)	6'-6"
H-3	TIED ABOVE H-1 BARS TO SUPPORT H-2, TIED TO V-1	#4	(2)	1'-6"
S-1	OVER LIFT HOLES	#4	(2)	
S-2	HORIZ. AROUND SLOTS BETWEEN V-1'S & DRAIN SLOTS	#4	(2)	
V-1	VERTICAL IN BARRIER (3) EACH END & (2) AT EACH DRAIN SLOTS	#5	(16)	



NOTE: " THREADED INSERTS SHALL BE CAST IN PLACE FOR ALL NEW BRIDGE DECKS AND DRILLED AND GROUTED FOR EXISTING BRIDGE DECKS. INSERTS SHALL HAVE A MINIMUM ULTIMATE LOAD CAPACITY OF 8000 LBS. IN TENSION, AFTER REMOVAL OF BARRIER, BOLTS, AND ANGLES, THE INSERTS SHALL BE FILLED WITH APPROVED NON-SHRINK EPOXY.



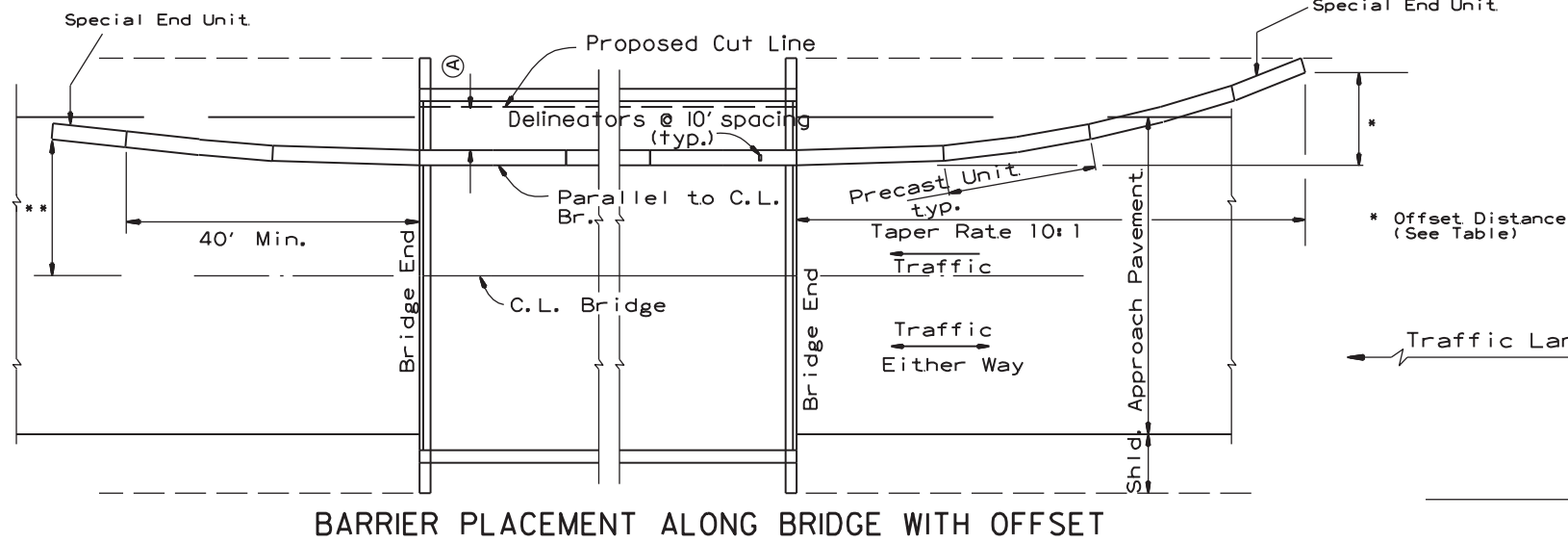
- GENERAL NOTES**
- THE CONTRACTOR SHALL FURNISH THE PRECAST CONCRETE BARRIER UNITS AND SHALL BE RESPONSIBLE FOR THE MANUFACTURE, SHIPMENT, STORAGE, PLACEMENT AND REMOVAL. AT THE COMPLETION OF THE PROJECT, THE PRECAST UNITS WILL REMAIN THE PROPERTY OF THE CONTRACTOR.
 - MATERIALS SHALL MEET THE FOLLOWING MINIMUM REQUIREMENTS:
CONCRETE: 2500 PSI COMPRESSIVE STRENGTH AT 28 DAYS.
REINFORCING STEEL: AASHTO M 31 OR M 53, GRADE 60
STRUCTURAL STEEL: AASHTO-M270 GRADE 36 SHALL BE USED FOR THE CONNECTION PIN, CONNECTION LOOPS, AND STABILIZATION PINS. A ONE PIECE PIN WITH A 3" ROUNDED TOP MAY BE USED IN PLACE OF THE DETAILED CONNECTION PIN. DELINEATORS: DELINEATORS SHALL BE MOUNTED AT 10' SPACING ON TOP OF PRECAST BARRIER.

IN APPLICATIONS WHERE BARRIER WALL IS WITHIN 6 FEET OF A TRAFFIC LANE, ADDITIONAL DELINEATORS SHALL BE PLACED ON THE BARRIER AT 10' SPACING APPROXIMATELY ONE (1) FOOT FROM THE TOP OF THE BARRIER. DELINEATORS SHALL BE ON THE ADOT QUALIFIED PRODUCTS LIST FOR CONSTRUCTION CONCRETE BARRIER MARKERS. DELINEATOR COLOR SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR DELINEATORS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID PER LIN. FT. FOR "FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER". THE CONTRACTOR SHALL CERTIFY TO THE ENGINEER THAT THE MATERIAL AND THE DESIGN USED IN THE PRECAST BARRIER UNITS MEETS THE REQUIREMENTS AS SHOWN ON THIS STANDARD DRAWING.
 - OTHER PRECAST CONCRETE BARRIERS THAT HAVE BEEN CRASH TESTED AND APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION TO MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) WILL BE ACCEPTED IN LIEU OF THE BARRIER SHOWN. DRAIN SLOTS SHALL BE PROVIDED AS NEEDED OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH A CERTIFICATION OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) COMPLIANCE FOR ANY OTHER TYPES OF PRECAST BARRIER TO BE USED. THE CERTIFICATION SHALL STATE THAT THE PRECAST CONCRETE BARRIER MEETS THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH). MIXING OF SHAPES WILL NOT BE ALLOWED IN A CONTINUOUS LINE OF UNITS.
 - DOWEL HOLES IN PAVEMENT OR BRIDGE SLABS THAT ARE TO REMAIN IN PLACE SHALL BE FILLED. HOLES IN CONCRETE PAVEMENT AND BRIDGE SLABS SHALL BE FILLED WITH AN APPROVED NON-SHRINK EPOXY GROUT. HOLES IN ASPHALT PAVEMENT SHALL BE FILLED WITH AN APPROVED ASPHALT JOINT FILLER. PAYMENT FOR DRILLING AND FILLING HOLES TO BE INCLUDED IN THE PRICE FOR VARIOUS BARRIER ITEMS.
 - ATTACH UNITS TO ROADWAY SURFACE WITH STABILIZATION PINS AND TO DECK SLABS USING BOLTS WHEN REQUIRED.
 - A 4" WHITE PVC SLEEVE MAY BE USED TO FORM THE LIFTING HOLE AND IF USED THE SLEEVE IS TO BE LEFT IN PLACE.

DATE	REVISION	FILMED
11-07-19	REVISED NOTE 3	
2-27-14	REVISED BARRIER STABILIZATION DETAIL	
10-15-09	ADDED REFERENCE TO MASH	
8-5-09	REV. NOTE 3 CONCERNING DRAIN SLOTS	
11-29-07	REVISED NOTE 3	
5-25-06	DELETED GENERAL NOTE 7	
11-18-04	REVISED BARRIER STABILIZATION DETAIL BRIDGE DECKS	
4-10-03	REVISED GENERAL NOTE 2	
8-22-02	ISSUED NEW DRAWING	

ARKANSAS STATE HIGHWAY COMMISSION
STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION - TEMPORARY PRECAST BARRIER
STANDARD DRAWING TC-4

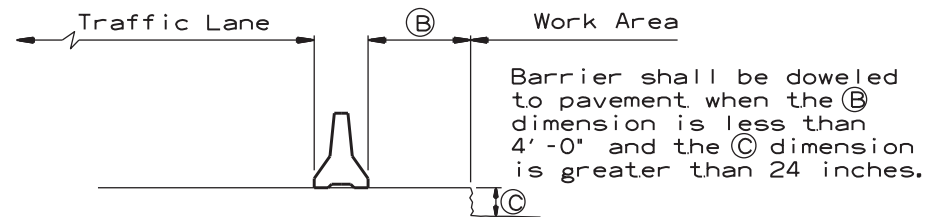
(A) 4 feet or greater preferred. If less than 4 feet, Precast Units shall be connected to slab (SEE BARRIER STABILIZATION DETAIL-BRIDGE DECKS STD. DRWG. TC-4)



BARRIER PLACEMENT ALONG BRIDGE WITH OFFSET

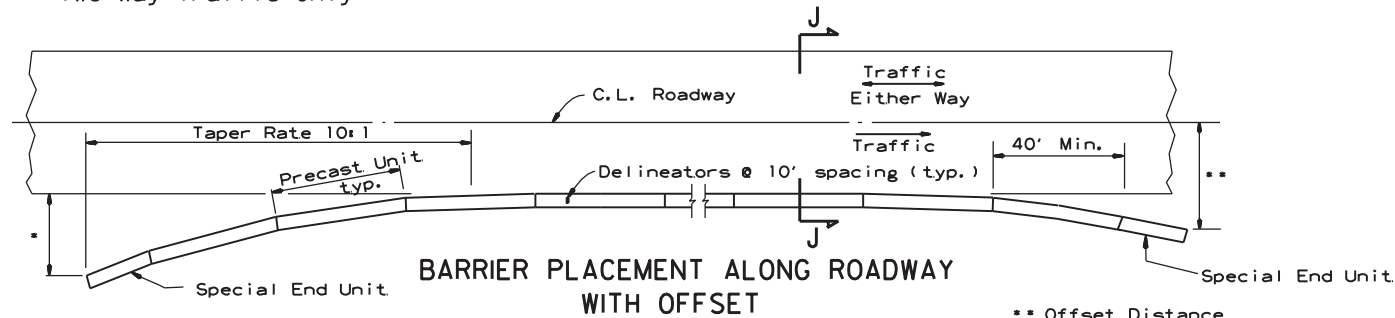
No Scale

** Offset Distance for Two Way Traffic Only



SECTION J-J

No Scale



BARRIER PLACEMENT ALONG ROADWAY WITH OFFSET

No Scale

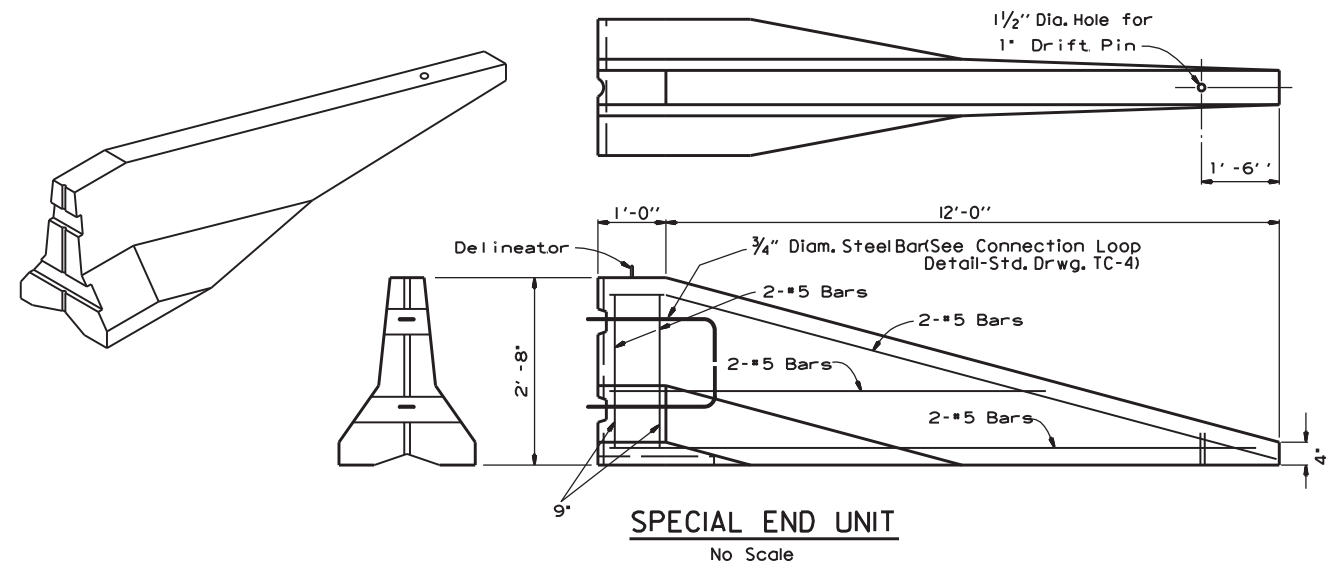
* Offset Distance (See Table)

** Offset Distance For Two Way Traffic Only

Offset Distance Table

Speed (MPH)	Offset Distance (FT.)
≤ 45	12
> 45	18

If offset distance is not attainable, then see 'Barrier Placement With Attenuator' Detail shown below.

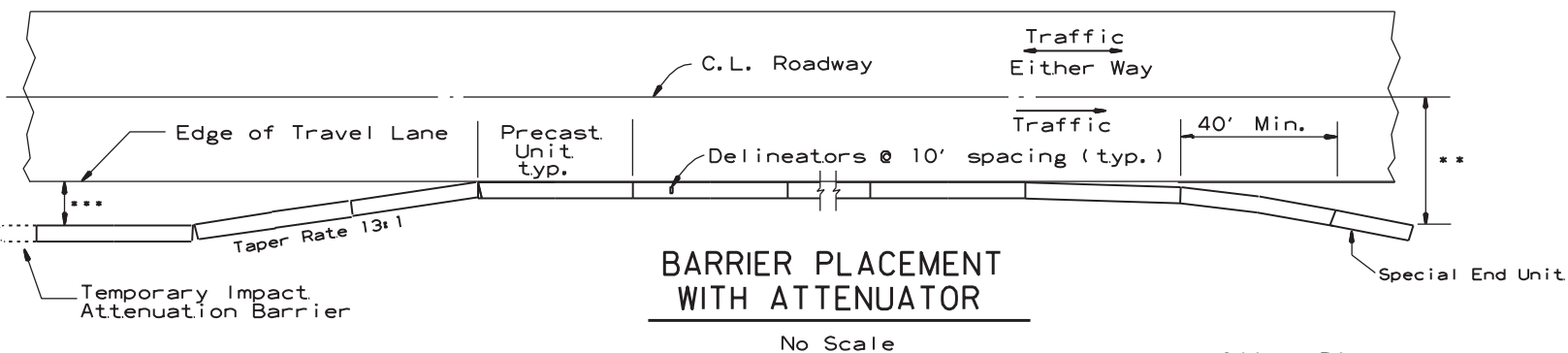


SPECIAL END UNIT

No Scale

General Notes

When shown on the Plans, the ends of the Temporary Precast Concrete Barrier shall be protected with a Manual For Assessing Safety Hardware (MASH) approved Crash Cushion. Payment for Crash Cushions shall be made under the item of "Temporary Impact Attenuation Barrier."



BARRIER PLACEMENT WITH ATTENUATOR

No Scale

** Offset Distance For Two Way Traffic Only

*** Min. 3'-0" From Edge of Travel Lane to Nearest Edge of Attenuator

DATE	REVISION	FILMED
11-07-19	REVISED NOTE	
10-15-09	ADDED REFERENCE TO MASH	
5-25-06	REVISED BARRIER PLACEMENT	
8-22-02	ISSUED NEW DRAWING	

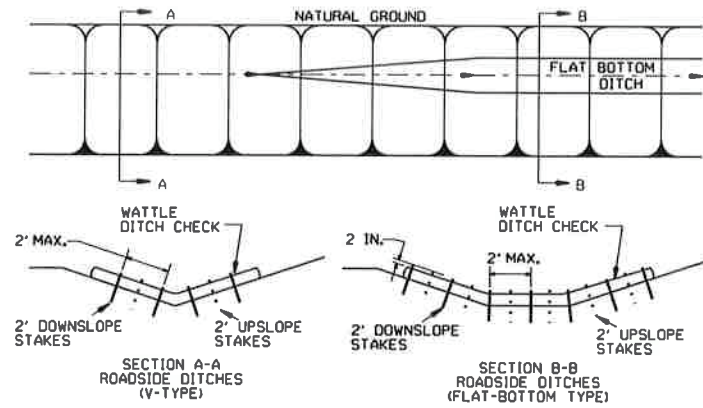
ARKANSAS STATE HIGHWAY COMMISSION

**STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION -
TEMPORARY PRECAST BARRIER**

STANDARD DRAWING TC-5

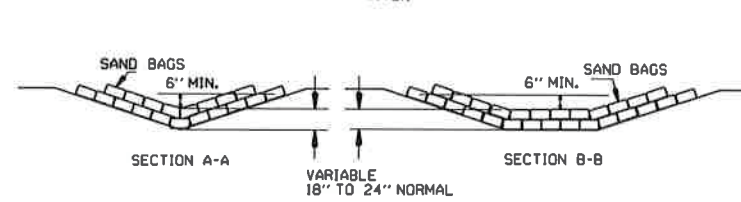
GENERAL NOTES

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

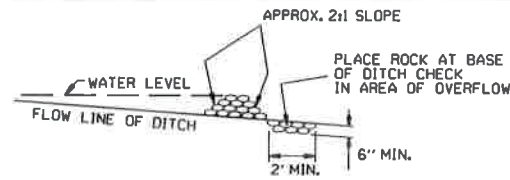


WATTLE DITCH CHECK (E-1)

NUMBER OF SAND BAGS AND ARRANGEMENT VARIABLE WITH ON-SITE CONDITIONS. PLACE SAND BAGS AT BASE OF DITCH CHECK IN AREA OF OVERFLOW.

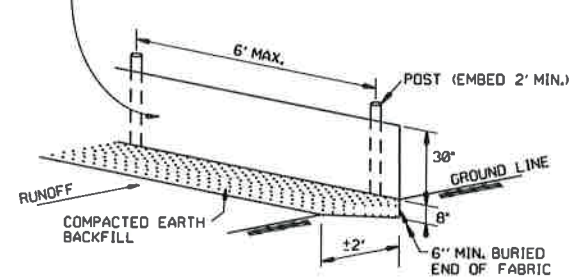


SAND BAG DITCH CHECK (E-5)

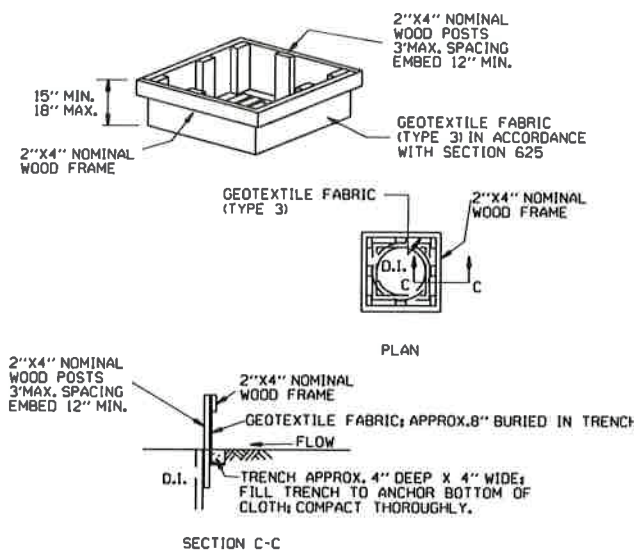


ROCK DITCH CHECK (E-6)

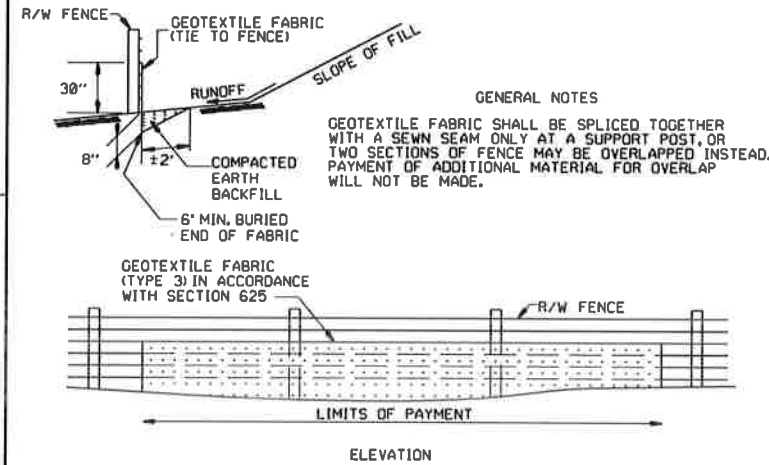
GENERAL NOTES
 GEOTEXTILE FABRIC (TYPE 4) IN ACCORDANCE WITH SECTION 625
 GEOTEXTILE FABRIC SHALL BE SPLICED TOGETHER WITH A SEWN SEAM ONLY AT A SUPPORT POST OR TWO SECTIONS OF FENCE MAY BE OVERLAPPED INSTEAD. PAYMENT OF ADDITIONAL MATERIAL FOR OVERLAP WILL NOT BE MADE.



SILT FENCE (E-11)

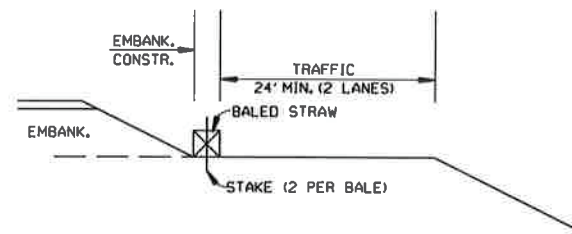


DROP INLET SILT FENCE (E-7)

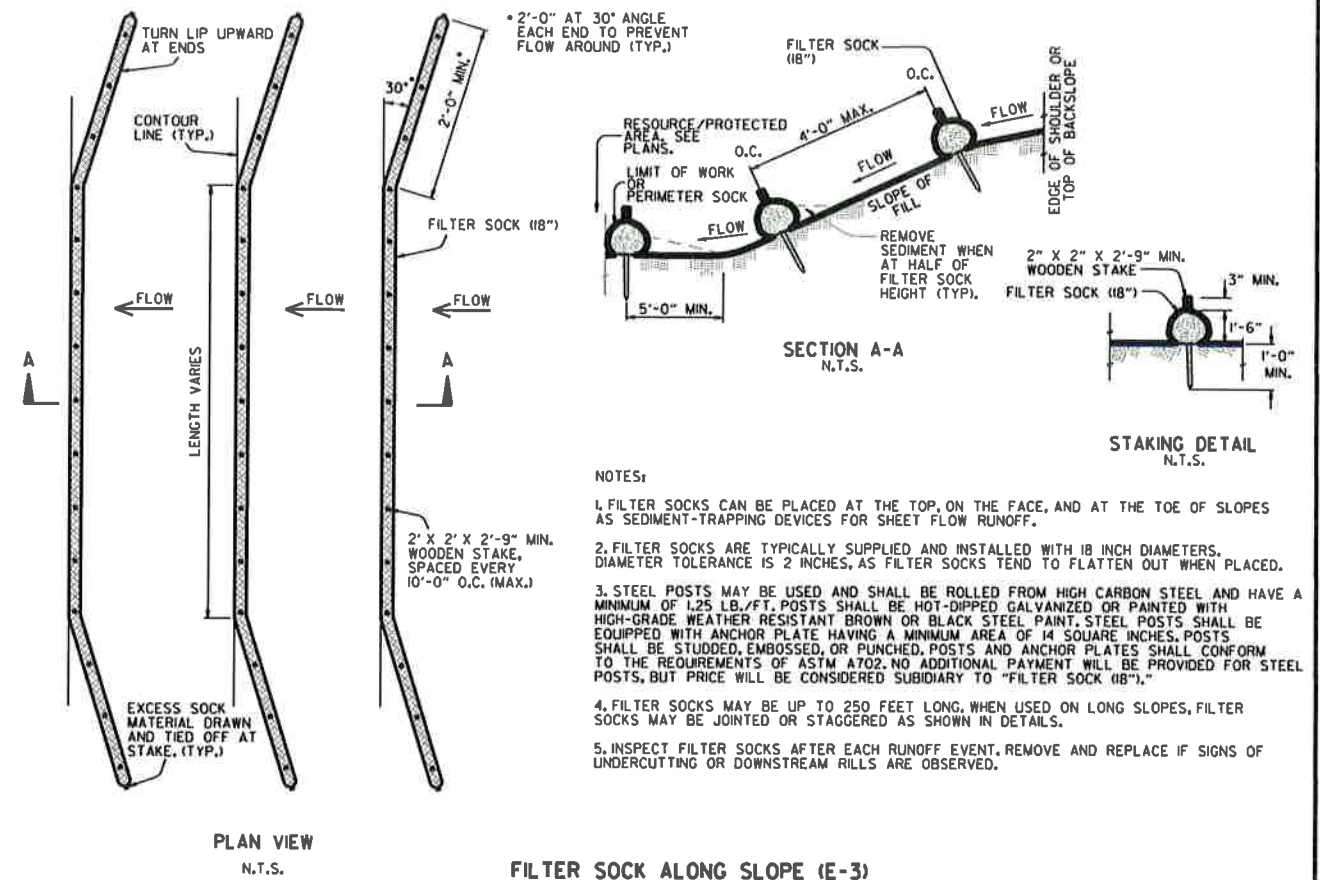


SILT FENCE ON R/W FENCE (E-4)

GENERAL NOTES
 1. STRAW BALES SHALL BE INSTALLED SO THAT THE BINDINGS ARE ORIENTED AROUND THE SIDES RATHER THAN ALONG THE TOPS AND BOTTOMS OF THE BALES. THE BALES SHALL BE A MINIMUM OF 30 INCHES IN LENGTH.
 2. NO GAPS SHALL BE LEFT BETWEEN BALES.
 3. BALED STRAW FILTER BARRIERS COMPLETED AND ACCEPTED WILL BE MEASURED BY THE BALE IN PLACE AS AUTHORIZED BY THE ENGINEER AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE BID PER BALE FOR BALED STRAW DITCH CHECKS.

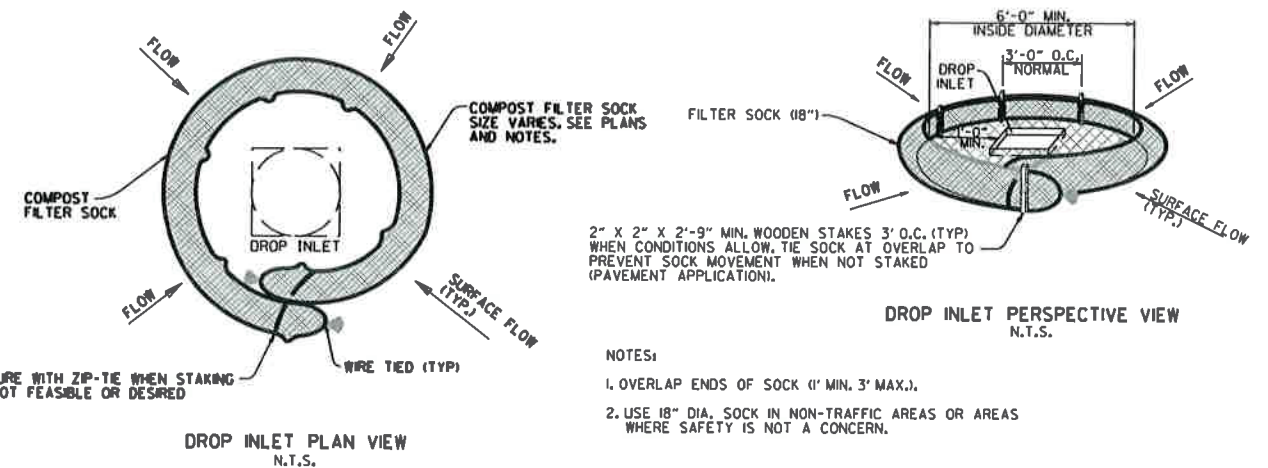


BALED STRAW FILTER BARRIER (E-2)



FILTER SOCK ALONG SLOPE (E-3)

NOTES:
 1. FILTER SOCKS CAN BE PLACED AT THE TOP, ON THE FACE, AND AT THE TOE OF SLOPES AS SEDIMENT-TRAPPING DEVICES FOR SHEET FLOW RUNOFF.
 2. FILTER SOCKS ARE TYPICALLY SUPPLIED AND INSTALLED WITH 18 INCH DIAMETERS. DIAMETER TOLERANCE IS 2 INCHES, AS FILTER SOCKS TEND TO FLATTEN OUT WHEN PLACED.
 3. STEEL POSTS MAY BE USED AND SHALL BE ROLLED FROM HIGH CARBON STEEL AND HAVE A MINIMUM OF 1.25 LB./FT. POSTS SHALL BE HOT-DIPPED GALVANIZED OR PAINTED WITH HIGH-GRADE WEATHER RESISTANT BROWN OR BLACK STEEL PAINT. STEEL POSTS SHALL BE EQUIPPED WITH ANCHOR PLATE HAVING A MINIMUM AREA OF 14 SQUARE INCHES. POSTS SHALL BE STUDDED, EMBOSSED, OR PUNCHED. POSTS AND ANCHOR PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A702. NO ADDITIONAL PAYMENT WILL BE PROVIDED FOR STEEL POSTS, BUT PRICE WILL BE CONSIDERED SUBSIDIARY TO "FILTER SOCK (18\"/>

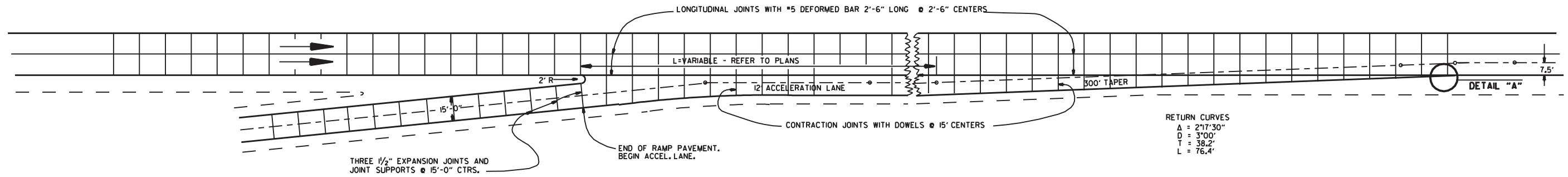


COMPOST FILTER SOCK DROP INLET PROTECTION (E-13)

NOTES:
 1. OVERLAP ENDS OF SOCK (1\"/>

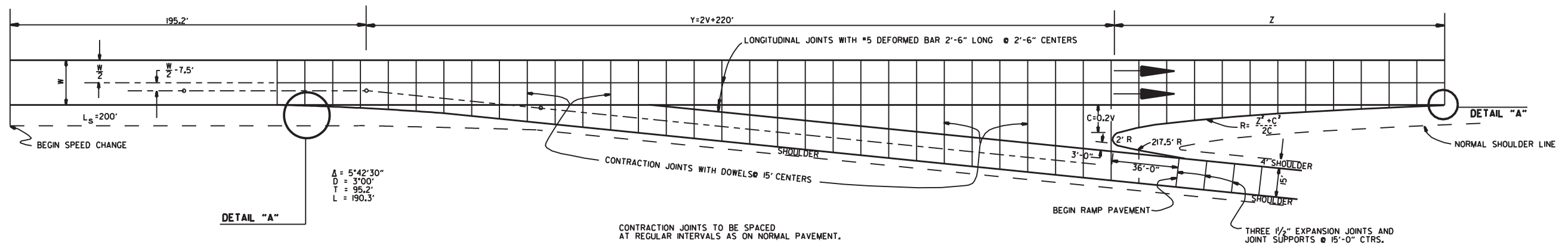
11-16-17	ADDED FILTER SOCK E-3 AND E-13	
12-15-11	DELETED BALED STRAW DITCH CHECK & ADDED WATTLE DITCH CHECK	
1-18-98	ADDED NOTES	
07-02-98	ADDED BALED STRAW FILTER BARRIER (E-2)	
07-20-95	REVISED SILT FENCE E-4 AND E-11	7-20-95
07-15-94	REV. E-4 & E-11 MIN. 13\"/>	
06-02-94	REVISED E-1, 4, 7 & 11 DELETED E-2 & 3	6-2-94
04-01-93	REDRAWN	
10-01-92	REDRAWN	
08-02-76	ISSUED R.O.M.	298-7-28-76
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION
 TEMPORARY EROSION CONTROL DEVICES
 STANDARD DRAWING TEC-1



ENTRANCE RAMP

NOTE: JOINT SPACING ON THE MAIN LANES SHALL BE ADJUSTED AS NECESSARY TO CONFORM TO THESE JOINT LAYOUTS. THE MAIN LANE JOINT SPACING MAY BE REDUCED TO A 12' MINIMUM.



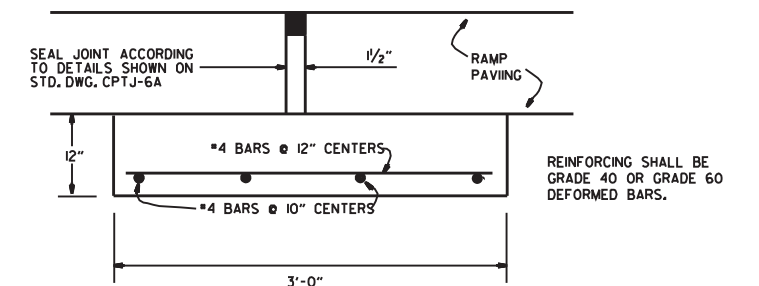
EXIT RAMP

EXIT RAMP

DESIGN SPEED V	Y	NOSE OFFSET C	LENGTH NOSE TAPER Z	RETURN RADIUS R	ADDITIONAL SURFACING SQ. YDS.
40	300.0	8.0	96.0	580.0	602.43
50	320.0	0.0	120.0	725.0	687.29
60	340.0	2.0	168.0	1182.0	790.55
70	360.0	4.0	210.0	1582.0	902.27



DETAIL "A"



DETAIL OF EXPANSION JOINT & JOINT SUPPORT

NOTE: THE EXPANSION JOINTS SHALL BE MEASURED AND PAID FOR AS P.C.C. PAVEMENT (RAMP THICKNESS). WHEN RAMP PAVING IS ASPHALT, EXPANSION JOINT IS NOT REQUIRED. THE JOINT SUPPORT MAY BE CONSTRUCTED WITH CLASS "A", "S", OR PAVING CONCRETE. PAYMENT FOR THE JOINT SUPPORT SHALL BE FOR THE CONTRACT UNIT PRICE BID FOR THE CLASS OF CONCRETE USED. ALL OTHER WORK AND MATERIALS REQUIRED FOR THE CONSTRUCTION OF THE JOINT SUPPORT SHALL BE INCLUDED IN THE PRICE BID FOR THE ABOVE ITEMS.

DATE	REVISION	DATE FILMD
8-22-02	DELETED NOTE	
11-16-01	CORRECTED SPELLING ON ENTRANCE RAMP NOTE	
5-13-99	ADDED, EDITED AND DELETED NOTES	
11-03-94	ADDED NOTE RE: REINF. BARS	
10-1-92	ADDED DETAIL A & OTHER MINOR CHANGES	10-1-92
1-25-90	REVISED EXPANSION JOINT	1-25-90
7-15-88	CONFORM D TO 1988 SPECIFICATIONS	85C-7-15-88
3-2-81	ISSUED	511-10-2-72

ARKANSAS STATE HIGHWAY COMMISSION

DETAILS OF STANDARD TURNOUT

FOR

ENTRANCE & EXIT RAMPS (NON-REINFORCED)

STANDARD DRAWING TR-1A